



Request for City Council Committee Action from the Department of Community Planning & Economic Development - Planning Division

Date: June 25, 2015

To: Council Member Lisa Bender, Chair, Zoning & Planning Committee and Members of the Committee

Referral to: Zoning & Planning Committee

Subject:

Name of Appellant: The Lander Group, on behalf of 3535 Grand II, LLC

Name of Appellant: Tran Muehller

Name of Original Applicant: The Lander Group, on behalf of 3535 Grand II, LLC

Property Address: 3535, 3539 and 3543 Grand Avenue South

Ward #: 8

Appeal of decision of the City Planning Commission: The Lander Group, on behalf of 3535 Grand II, LLC has filed an appeal of the decision of the City Planning Commission denying two setback variances for a proposed parking area accessory to a new multiple-family residential structure located at 3535, 3539 and 3543 Grand Avenue South in the R5 Multiple-Family District.

Appeal of decision of the City Planning Commission: Tran Muehller has filed an appeal of the decision of the City Planning Commission approving parking variances, setback variances and site plan review for a new multiple-family residential structure located at 3535, 3539 and 3543 Grand Avenue South in the R5 Multiple-Family District.

Recommendation:

The City Planning Commission adopted staff findings and made the following decisions on June 1, 2015, for the properties at 3535, 3539 and 3543 Grand Avenue South, Ward #8, as follows:

6. 3535-43 Grand Ave S, Ward 8

Staff report by Shanna Sether, BZZ-7112

The City Planning Commission adopted staff findings for the applications by The Lander Group, on behalf of 3535 Grand II, LLC.

A. Variance to reduce the off-street parking requirement.

Action: **Approved** the application for a variance to reduce the off-street parking requirement from 22 spaces to **17 spaces** to allow for a new 24-unit, multiple-family dwelling.

Aye: Gisselman, Kronzer, Luepke-Pier and Slack

Absent: Bender, Forney, Gagnon and Tucker

B. Variance to increase the percentage of required parking spaces.

Action: **Approved** the application for a variance the percentage of required parking spaces that may be satisfied by providing compact parking stalls from 25% to 47% to allow for a new 24-unit, multiple-family dwelling.

Aye: Gisselman, Kronzer, Luepke-Pier and Slack

Absent: Bender, Forney, Gagnon and Tucker

C. Variance to reduce the front yard setback along Grand Ave S.

Action: **Approved** the application for a variance to reduce the front yard setback along Grand Ave S from 22.5 feet to 15 feet for the proposed residential structure and to 10 feet for the proposed canopy to allow for a new 24-unit, multiple-family dwelling.

Aye: Gisselman, Kronzer, Luepke-Pier and Slack

Absent: Bender, Forney, Gagnon and Tucker

D. Variance to reduce the south interior side yard setback for the proposed structure.

Action: **Approved** the application for a variance to reduce the south interior side yard setback from 9 feet to 8.5 feet for the proposed residential structure to allow for a new 24-unit, multiple-family dwelling.

Aye: Gisselman, Kronzer, Luepke-Pier and Slack

Absent: Bender, Forney, Gagnon and Tucker

E. Variance to reduce the north interior side yard setback for the proposed parking area.

Action: **Denied** the application for a variance to reduce the north interior side yard setback from 5 feet to 2 feet for the proposed parking lot to allow for a new 24-unit, multiple-family dwelling.

Aye: Gisselman, Luepke-Pier and Slack

Nay: Kronzer

Absent: Bender, Forney, Gagnon and Tucker

F. Variance to reduce the south interior side yard setback for the proposed parking area.

Action: **Denied** the application for a variance to reduce the south interior side yard setback from 5 feet to 3 feet for the proposed parking lot to allow for a new 24-unit, multiple-family dwelling.

Aye: Gisselman, Luepke-Pier and Slack

Nay: Kronzer

Absent: Bender, Forney, Gagnon and Tucker

G. Site plan review.

Action: **Approved** the application for site plan review for a new 24-unit, multiple-family dwelling, subject to the following conditions:

1. All mechanical equipment shall be arranged so as to minimize visual impact by using screening consistent with section 535.70 of the zoning code.
2. Lighting shall comply with section 535.590 of the zoning code.

3. All of the proposed parking stalls shall be located within 50 feet of an on-site deciduous tree.
4. A maximum of 30% of the building elevations shall be fiber cement lap-siding to ensure durability and compatibility.
5. Department of Community Planning and Economic Development staff review and approval of the final building elevations, floor, site, lighting and landscape plans.
6. Site improvements required by Chapter 530 or by the City Planning Commission shall be completed by June 1, 2017, unless extended by the Zoning Administrator, or the permit may be revoked for non-compliance.
7. The applicant shall work with staff to participate in a fair-share manner to help solve the storm water issue in the alley.

Aye: Forney, Gisselman, Kronzer, Luepke-Pier and Slack

Absent: Bender, Gagnon and Tucker

Previous Directives: None

Prepared by: Shanna Sether, Senior City Planner, 612-673-2307 Approved by: Jason Wittenberg, Planning Manager, 612-673-2297 Steve Poor, Interim Development Services Director, 612-673-5837 Presenters in Committee: Shanna Sether, Senior City Planner, 612-673-2307
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Community Impact

- Neighborhood Notification: Lyndale Neighborhood Association was sent notification of the appeal on June 15, 2015.
- City Goals: See staff report
- Comprehensive Plan: See staff report
- Zoning Code: See staff report
- End of 60/120-day decision period: On May 11, 2015, staff sent a letter to the applicant extending the City's decision period for an additional 60 days, to August 8, 2015.

Background/Supporting Information

Staff has received two appeals of the decision of the City Planning Commission. The Lander Group, on behalf of 3535 Grand II, LLC, has filed an appeal of the decision of the City Planning Commission denying two setback variances for a proposed parking area accessory to a new multiple-family residential structure located at 3535, 3539 and 3543 Grand Avenue South. The City Planning Commission adopted staff findings and denied the two variances 3-1.

Tran Muehler has filed an appeal of the decision of the City Planning Commission approving parking variances, setback variances and site plan review for a new multiple-family residential structure located at 3535, 3539 and 3543 Grand Avenue South in the R5 Multiple-Family District. The City Planning Commission adopted staff findings and approved a variance to reduce the off-street parking requirement, a variance to increase the maximum number of compact parking spaces, a variance to reduce the front yard setback and south interior side yard setback for the building voting 4-0. The site plan review application was approved 5-0.

Both of the appellants have submitted statements and reasons for the appeal; copies are attached. The staff report and the attachments presented to the City Planning Commission are also attached.

APPLICATION WORKSHEET

Appellant	Name	The Lander Group
	Mailing Address Including City, State and Zip Code	3802 Nicollet Avenue South Minneapolis, MN 55409
	Phone Number	612-986-2511
	Fax	
	Email	tony@landergroup.com

NOTICE OF APPEAL

Choose one:

_____ I, _____ (print name) do hereby file an exception to the Decision of the **Zoning Administrator** as provided for in Chapter 525.170;

_____ I, _____ (print name) do hereby file an exception to the Decision of the **Board of Adjustment** as provided for in Chapter 525.180;

X _____ I, **Tony Kriha** (print name) do hereby file an exception to the Decision of the **City Planning Commission** as provided for in Chapter 525.180;

Project Name	3535 Grand Avenue South
Project Address	3535 Grand Avenue South
BZZ Number	7112

Further, I do hereby request that I be given an opportunity to express my case before the Board of Adjustment or the proper committee of the City Council.

The action being appealed and the reasons for appealing the decision are attached and made a part of this notice of appeal.

Appellant's Name: **Tony Kriha**

Appellant's Signature:  Date: **6/11/2015**



City of Minneapolis

06.10.2015

Community Planning & Economic Development
250 S Fourth Street Rm. #300
ATTN: Shanna M. Sether

RE: 3535 – 3539 – 3543 Grand Avenue South : Planning Commission Appeal

The applicant for the project would like to appeal the decision to deny the variance request for the side yard setback for parking.

1. Parking Setback: Variance to reduce the setback on an interior side yard setback from 5' to 1'. The parking setback is proposed so that additional walk and planting area can be added in the middle of the site. This is done for ease of the residents getting to and from the trash in a convenient and safe manner. The additional area also provides the opportunity to get trees in this location.

- The site is trapezoidal such that the proposed parking is setback on the side yard vary from a minimum of 2' on the north to a minimum of 3' on the south.
- Parking is allowed within one foot of the property line for the first 40' of the rear yard. The proposed setback is a minimum 2' setback for 45'-9". The proposed side-yard average area side back in the first 40' is 64 sf. Allowed would be 40 sf. 24 square feet over the allowed. (Note on the south side the area is 118 s.f. in lieu of 40 s.f.)
- The proposed additional area in the setback is 17.8 additional square feet of area on the north side. (11.9 s.f. on the south side).
- The use of the adjacent properties is such that there is minimal impact to adjacent property owners. The property to the south is currently a parking lot adjacent to the proposed parking. The property to the north has a garage structure next to the property line.
- The parking is screened for the full length of the parking, and from the adjacent properties the extra 3' in setback proposed would not be visible.
- The applicant proposes to add two trees at the corner of the parking area as indicated on the attached diagram.

Thank you for consideration

Sincerely,

COLLAGE ARCHITECTS

A handwritten signature in black ink, appearing to read "Pete Keely", written over a circular stamp or mark.

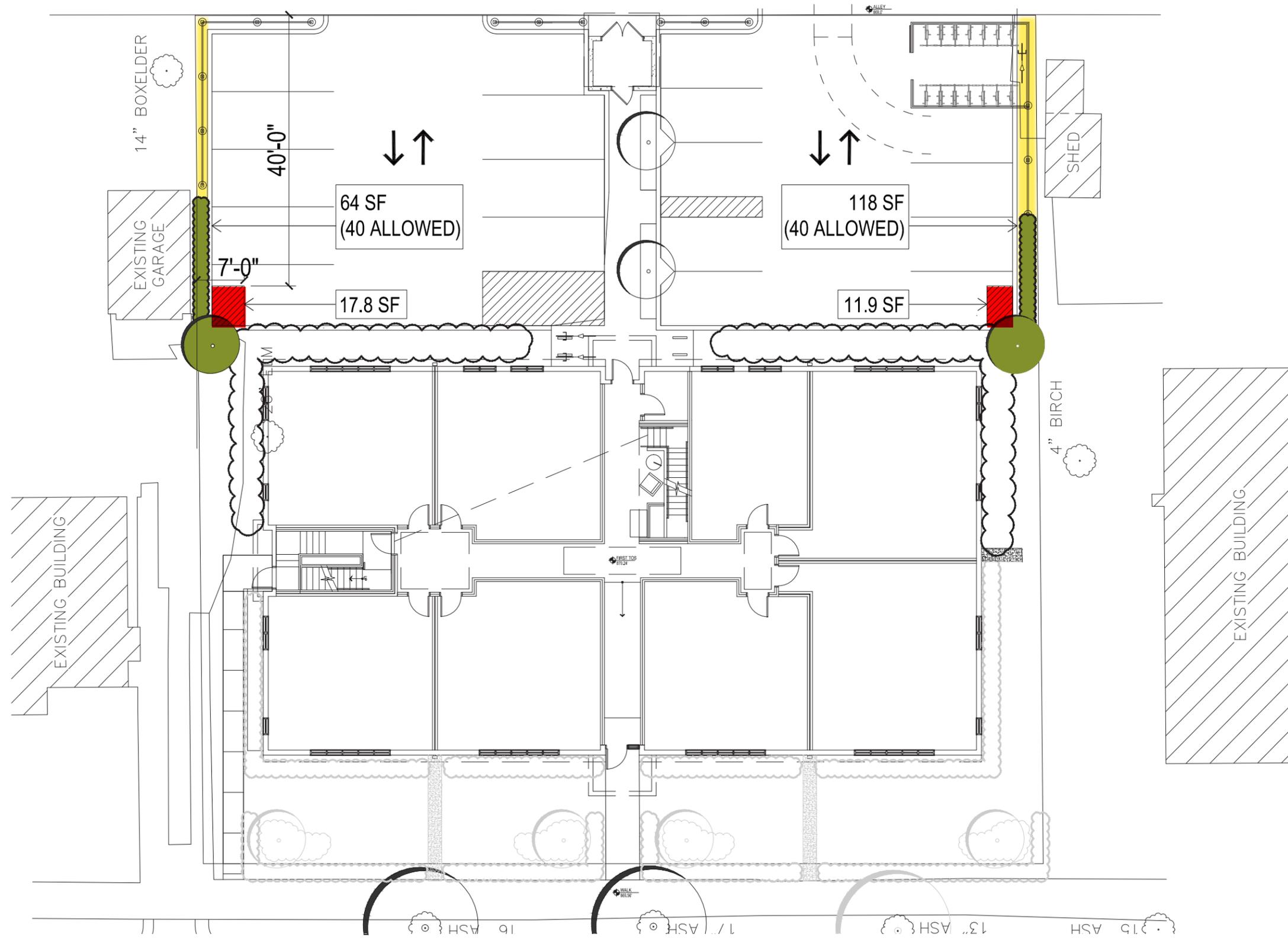
Pete Keely, A.I.A.



GRAND APARTMENTS

Minneapolis, MN
JUN. 11. 2015

- > 24 DWELLING UNITS (ALL ONE BEDROOM)
- > 18 PARKING STALLS



FIRST FLOOR PLAN
1/16" = 1'-0"

APPLICATION WORKSHEET

Appellant	Name	Tran Muehler
	Mailing Address Including City, State and Zip Code	4323 Fremont Ave. N. Minneapolis, MN 55412 ← mailing
	Phone Number	612-306-8828
	Fax	NA
	Email	tran_zen@yahoo.com
		3546 Grand Ave. S. Minneapolis, MN 55408 ← workplace & owner/mgr.

NOTICE OF APPEAL

Choose one:

_____, ~~Tran Muehler~~ (print name) do hereby file an exception to the Decision of the Zoning Administrator as provided for in Chapter 525.170;

_____, _____ (print name) do hereby file an exception to the Decision of the Board of Adjustment as provided for in Chapter 525.180;

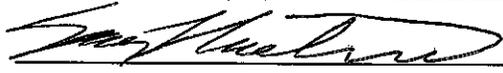
I, Tran Muehler (print name) do hereby file an exception to the Decision of the City Planning Commission as provided for in Chapter 525.180;

Project Name	3535 Grand
Project Address	3535, 3539, 3543 Grand Ave. S. Minneapolis, MN 55408
BZZ Number	BZZ-7112

Further, I do hereby request that I be given an opportunity to express my case before the Board of Adjustment or the proper committee of the City Council.

The action being appealed and the reasons for appealing the decision are attached and made a part of this notice of appeal.

Appellant's Name: Tran Muehler

Appellant's Signature: 

Date: 06/11/15

Request for Appeal to Minneapolis Zoning & Planning Committee Regarding Opposition to Minneapolis City Planning Commission's approval of the 3535 Grand Project being presented by Lander Group

Let this letter be part of my testament, 'against' the decision made by the Minneapolis City Planning Commission to approve the proposed development at 3535 Grand Ave. S. Minneapolis by Lander Group, against the granting of 4 of the requested variances, and all for the reason of helping to save this longstanding cherished community of the Lyndale Neighborhood.

There are many many reasons why not to approve neither the 4 approved variances nor the project as a whole. I have found through referencing of findings of fact from city zoning codes, variance requirements, and the Comprehensive City Plan (Minneapolis Plan for 'Sustainable' Growth) that the developers' variance requests and overall project requests definitely do not comply with the currently applicable legislation adopted by the city of Minneapolis.

There are also an abundance of equally important arguments or reasons to not allow this type of project in the specific area in question due to strong concerns and issues the project will create for the community that should be 'implicated' when considering the legal findings also.

Note : This is an abstract of my total and complete letter that will subsequently be submitted in the following few days to accompany the already submitted information.

Thank you dearly

with honesty, integrity, respect, and utmost sincerity

Tran Muehler
3546 Grand Ave. S.
Minneapolis, MN 55408
612-306-8828
tran_zen@yahoo.com

LAND USE APPLICATION SUMMARY

Property Location: 3535, 3539 and 3543 Grand Avenue South
Project Name: 3535 Grand
Prepared By: Shanna Sether, Senior City Planner, (612) 673-2307
Applicant: 3535 Grand II, LLC
Project Contact: The Lander Group
Request: To allow a new 24-unit, multiple-family residence.
Required Applications:

Variance	To reduce the off-street parking requirement from 22 spaces to 18 spaces.
Variance	To increase the percentage of required parking spaces that may be satisfied by providing compact parking stalls from 25% to 46%.
Variance	To reduce the front yard setback along Grand Avenue South from 22.5 feet to 15 feet for the proposed residential structure and to 10 feet for the proposed canopy.
Variance	To reduce the south interior side yard setback from 9 feet to 8.5 feet for the proposed residential structure.
Variance	To reduce the north interior side yard setback from 5 feet to 2 feet for the proposed parking area.
Variance	To reduce the south interior side yard setback from 5 feet to 3 feet for the proposed parking area.
Site Plan Review	For a new three-story, multiple-family structure.

SITE DATA

Existing Zoning	R5 Multiple-Family District
Lot Area	15,557 square feet / .36 acres
Ward(s)	8
Neighborhood(s)	Lyndale Neighborhood Association
Designated Future Land Use	Urban Neighborhood
Land Use Features	Community Corridor (Lyndale Ave – 2 blocks west) Neighborhood Commercial Node (36 th St W and Lyndale Ave – 2 blocks west)
Small Area Plan(s)	Not applicable

Date Application Deemed Complete	April 10, 2015	Date Extension Letter Sent	May 11, 2015
End of 60-Day Decision Period	June 9, 2015	End of 120-Day Decision Period	August 8, 2015

BACKGROUND

SITE DESCRIPTION AND PRESENT USE. The subject property has three parcels that are currently vacant. The property located at 3535 Grand Avenue previously had a single-family dwelling that was constructed in 1977 and was later demolished. The property located at 3543 Grand Avenue was constructed as a mixed-use, commercial and office building in 1956 and demolished in 2006.

SURROUNDING PROPERTIES AND NEIGHBORHOOD. The surrounding land uses are residential of various densities from low to high and there is one mixed use building at the corner of Grand Avenue South and 35th Street West. The adjacent land use to the north is a single-family dwelling with a parking lot at the rear serving the multiple-family dwelling at 3528 Pleasant Avenue South. The adjacent land use to the south is an 11-unit, multiple-family dwelling with a surface parking lot.

PROJECT DESCRIPTION.

The applicant is proposing to construct a new three-story building, approximately 37 feet tall, with a total of 24 dwelling units. The proposed development would include 18 surface parking stalls, 7 standard stalls, 10 compact stalls and one van accessible handicapped stall. The site does qualify for the multiple-family dwelling transit incentive, which reduces the required off-street parking by 10%. The site is zoned R5 Multiple Family District and *The Minneapolis Plan for Sustainable Growth* designates this site as Urban Neighborhood on the future land use map. Previous approvals were granted by the City Planning Commission on [June 11, 2012](#), to allow for a multiple-family dwelling with 30 units. These approvals have now expired.

RELATED APPROVALS.

Planning Case #	Application	Description	Action
V-1160	Variances to reduce the side yard setbacks from 9 feet to 5 feet.	Multiple-family dwelling.	Application filed in 1977 and was withdrawn by applicant
BZZ-2829	Grand Avenue Townhomes at 3535 and 3539 Grand Avenue South	Conditional Use Permit, Variances and Site Plan Review for a new 5-unit Townhouse	City Planning Commission approved 3/6/2006 Appealed actions to the City Council 4/14/2006
BZZ-2830	Grand Avenue Townhomes at 3539 and 3543 Grand Avenue South	Rezoning, Conditional Use Permit, Variances, Site Plan Review and Minor Subdivision for a new 5-unit Townhouse	City Planning Commission approved 3/6/2006 Appealed actions to the City Council 4/14/2006
BZZ-5562	3535 Grand at 3535, 3539 and 3543 Grand Avenue South	Variances and Site Plan Review for a 4-story, 30-unit multiple-family residence	City Planning Commission approved 6/11/2012 Appealed actions to the City Council 7/20/2012

PUBLIC COMMENTS. At the time of preparing this report, staff has received public comments regarding the requested land use application – copies are attached to the staff report. Any additional correspondence received prior to the public meeting will be forwarded on to the Planning Commission for consideration.

ANALYSIS

VARIANCE – Off-Street Parking

The Department of Community Planning and Economic Development has analyzed the application for a variance to reduce the off-street parking required for a 24-unit multiple-family dwelling and to allow 46% of the required parking to be compact stalls, based on the following findings:

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

Staff finds unique circumstances of the property that create a practical difficulty in complying with the ordinances requiring 22 off-street parking stalls where only 6 are allowed to be compact. The subject property is located within 300 feet of transit stops with midday service headways of thirty (30) minutes or less in each direction. This allows for a reduction in the off-street parking by 10%. In addition, the applicant has stated that the soil condition is poor and a geopier system will be used to support the building. This system will only require minimal excavation. Staff finds that these circumstances have not been created by the applicant.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

Staff finds that the applicant is proposing to utilize the property in a reasonable manner consistent with the spirit and intent of the ordinance and the comprehensive plan. As previously mentioned, the subject property qualifies for the transit incentive for multiple-family dwellings. Staff and a city council member are currently proposing a text amendment to increase the transit incentive for multiple-family dwellings to better align with comprehensive plan and transportation policies. As proposed, this development would qualify for a far greater transit incentive due to its size (less than 50 dwellings) and proximity to high-frequency transit. Additionally, the applicant is proposing to provide off-street bicycle parking for every dwelling unit.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

The proposed variance will not alter the essential character of the locality. The surrounding area is zoned R5 Multiple-Family District and there are nine multiple-family properties on this block, many of which are deficient to the required off-street parking requirement. The subject property is well-served by transit and the applicant is providing off-street bicycle parking for all of the dwellings. Staff finds that the proposed density is compatible with the surrounding area and uses and would support a greater reduction in the off-street parking requirement to allow for the parking lot to be reconfigured allowing additional landscaped yards in the parking area. Therefore, staff finds that the proposed variances will not be injurious to the use or enjoyment of other property in the vicinity. The parking variance will not have an effect on the health, safety or welfare of the general public

VARIANCE – Building Setbacks

The Department of Community Planning and Economic Development has analyzed the application for variances to reduce the front and south interior side yard setbacks based on the following findings:

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

Front Yard: Development of this property is constrained through the existence of an established front yard setback created by the property directly adjacent to the north of the site. By applying the established setback standard, the property has a required setback of approximately 22.5 feet at its northwest corner. This established setback line tapers to approximately 15 feet at the southwest portion of the site. The applicant proposes to place the building 15 feet from the front property line in an area of the site where the established front yard setback ranges from 22 feet 5 ½ inches on the north end to 15 feet 11 ½ inches on the south end. The requested 15 foot setback is consistent with the condition on the property directly south of the subject site. The uniquely inconsistent setback of buildings found on either side of the property presents a practical difficulty for the applicant in complying with the ordinance.

South Interior Side Yard: The south property line of the subject property is not perpendicular to the front property line, the public sidewalk or Grand Avenue South. When siting the building parallel to the front property line, the southeast corner of the structure projects into the required side yard of 9 feet. The structure, at its closest point is located 8.5 feet from the property line and requires a variance. The fact that the property is a parallelogram and not a rectangular is a unique circumstance that has created a practical difficulty in complying with the ordinance.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

Front Yard: The intent of the front yard setback standard is to create a uniform street frontage that is appropriate for the types of development allowed in a given district. Considering that the applicant has provided the district setback of 15 feet to the building, continuing that street frontage consistently to the north is in keeping with the spirit and intent of the ordinance. *The Minneapolis Plan for Sustainable Growth* also supports the reasonableness of the request in policy 10.8.1: “Infill development shall reflect the setbacks, orientation, pattern, materials, height and scale of surrounding dwellings.”

South Interior Side Yard: The purpose of required yards is to provide for orderly development and use of land and to minimize conflicts among land uses by providing adequate light, air, open space and separation of uses. The proposed structure encroaches into the south interior side yard for approximately 23 square feet at the footprint and 69 square feet of gross floor area. Therefore, staff finds that the applicant is proposing to utilize the property in a reasonable manner, consistent with the spirit and intent of the ordinance.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

Both variances: As mentioned in the background of the report, the character of the street is largely residential, predominated by multi-family housing. Properties closer to the intersection of 36th Street South and Grand Avenue South contain buildings that are generally closer to the front lot

line. Continuing this pattern with redevelopment at the district setback of 15 feet at the 3535 Grand site is in keeping with the essential character of the area and will reinforce a pedestrian focused street wall extending from the intersection just south of the site. Additionally, the minimal amount of the building that is proposed to be within the south interior side yard setback will not alter the essential character or be injurious to other property in the vicinity. In this manner, the requested variances will not detrimental to the health, safety, or welfare of the general public as long as the building complies with the building code requirements and life safety ordinances.

VARIANCE – Parking Lot Setbacks

The Department of Community Planning and Economic Development has analyzed the application for a variances to reduce the north and south interior side yard setbacks to allow for a new surface parking lot accessory to a multiple-family dwelling based on the following findings:

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

Staff is unable to find practical difficulties exist in complying with the five-foot minimum interior side yard setbacks for the proposed parking area due to circumstances unique to the property. The proposed parking area is located outside of the rear forty feet of the lot and therefore, there is a minimum side yard setback. The subject property is 15,557 square feet and the applicant is proposing to have approximately a 2- and 3-foot side yard on each side of the parking lot. The applicant is proposing to create two parking areas at the rear of the building divided by a walkway, approximately 8 feet in width. Staff has determined that the applicant is able to co-locate the trash enclosure with the bicycle storage, which will require the elimination of one additional parking stall. The 8-foot walkway could then be reduced in width in order to allow for a minimum of 5 feet on each of the interior sides. Staff finds that the applicant has created the need for the variance based on the design of the parking area.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

Staff finds that the applicant is not proposing to utilize the property in a reasonable manner consistent with the ordinance. Parking areas located entirely in the rear 40 feet are allowed to have a reduced setback to 1 foot on the interior and rear property lines. The proposed parking area will be located approximately 46 feet from the rear property line. The applicant has stated that they are providing screening along the property lines and the adjacent properties will be unaware of the proximity of vehicles. Staff has concerns that vehicles parked in this location may have off-site impacts on to the adjacent residential properties. Staff finds that the applicants are able to provide the minimum interior side yard setbacks for the proposed parking lots, allowing more substantial landscaped areas, and maintain a reasonable use of the property.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

Staff finds that the proposed variances may alter the essential character and be injurious to the use and enjoyment of the adjacent properties. There are nine other multiple-family residences on this block and none appear to have parking area located outside of the rear forty feet and in the interior

side yard setback. Additionally, the alley is known to flood near to the subject property and staff has instead suggested that additional landscaped area be provided to allow for additional on-site retention of stormwater.

SITE PLAN REVIEW

The Department of Community Planning and Economic Development has analyzed the application based on the required findings and applicable standards in the site plan review chapter:

I. Conformance to all applicable standards of Chapter 530, Site Plan Review.

Building Placement and Design – Meets requirements

- The placement of building reinforces street wall. The proposed building would be located the district setback of 15 feet to the front property line and have a canopy over the entrance.
- The front yard is landscaped along the public sidewalk and against the building.
- The structure is oriented with one principal entrance facing Grand Avenue South.
- On-site accessory parking located to the rear or interior of the site.
- The proposed construction provides architectural detail and windows to create visual interest and increase security of adjacent outdoor spaces.
- The proposed building does not show any blank, uninterrupted walls exceeding 25 feet in length.
- Exterior materials are durable; the first floor is a prefinished horizontal metal siding, the upper floors are 5/8” fiber cement board lap siding and there are two prefinished metal panel details on the front façade on the north and south ends of the building. The maximum percentage of fiber cement board lap siding on each elevation is 30%; the applicant is proposing 45% on the front and rear elevations, 48% on the north elevation and 58% on the south elevation. The applicant is requesting alternative compliance and staff is recommending a maximum of 30% fiber cement lap siding.
- The rear and side walls are similar to and compatible to the front.
- The applicant is proposing burnished block at the base; plain face concrete block is not a proposed exterior material.
- The entrance and windows are vertical in proportion and are evenly distributed.
- The ground floor includes a small lobby and residential dwellings; these are active uses.
- The proposed roof is flat and is similar to surrounding buildings.

Table 1. Percentage of Windows Required for Elevations Facing a Public Street, Sidewalk, Pathway, or On-Site Parking

	Code Requirement		Proposed	
Residential Uses				
1st Floor	20% minimum	170 sq. ft.	26%	336 sq. ft.
1st Floor – facing the on-site parking	20% minimum	170 sq. ft.	26%	336 sq. ft.
2nd Floor and Above	10% minimum	85 sq. ft.	35%	454 sq. ft.
2nd Floor and Above – facing the on-site parking	10% minimum	85 sq. ft.	35%	454 sq. ft.

Access and Circulation – Meets requirements with Conditions of Approval

- The entrances from the public sidewalk will be well-lit and at least 4 feet in width. With the proposed staff direction, the applicant would be able to provide a well-lit 3-foot walkway connecting the building and the parking area.
- The applicant is not proposing to add a transit shelter into the development.
- Vehicular access is from the existing public alley. There are essentially two parking lots and circulation will be conducted on the property to minimize conflicts with pedestrians and surrounding residential uses.
- The subject property is currently vacant. With the proposed additional landscaped area recommended by staff, staff finds that the applicant has minimized impervious surface.

Landscaping and Screening – Requires alternative compliance

- With the proposed site improvements, the composition and location of landscaped areas complement scale of development and surroundings.
- The applicant is providing the required screening of at least 6 feet, 95% opaque, to adequately screen the parking area from the adjacent residential uses and the public alley.
- A landscaped yard at least seven (7) feet wide shall be provided along the property line and alley. The applicant is proposing 2 feet of a landscaped yard at the north, 3 feet along the south and no landscaping along the alley. The applicant is seeking alternative compliance and has stated that it is impractical to provide additional landscaping and enough parking to support the proposed development. Staff recognizes the site constraints and recommends that the planning commission grant alternative compliance to requiring a 7-foot landscaped yard along the alley. However, staff finds that the applicant has the ability to reconfigure the walkway and co-locate the trash enclosure with the bicycle storage to allow for a minimum of a 5-foot landscaped yard. This will require the elimination of one off-street parking stall.
- A 6-foot tall, 95% opaque screen is required around the perimeter of the parking lot. The applicant has shown a fence that complies with this requirement.
- Many of the parking stalls are not located within 50 feet of an on-site deciduous tree. The applicant is seeking alternative compliance; however, staff finds the applicant has the ability to redesign the parking lot to allow for greater landscaped yards along the north and south property line to allow for more deciduous trees.
- Areas not occupied by buildings shall be covered by landscaping.
- Installation and maintenance of all landscape materials shall comply with section 530.210 of the zoning code.

Table 2. Landscaping and Screening Requirements

	Code Requirement	Proposed
Lot Area	--	15,557 sq. ft.
Building footprint	--	6,311 sq. ft.
Remaining Lot Area	--	9,246 sq. ft.
Landscaping Required	1,849 sq. ft.	3,277 sq. ft.
Canopy Trees (1: 500 sq. ft.)	4 trees	6 trees
Shrubs (1: 100 sq. ft.)	18 shrubs	65 shrubs

Additional Standards – Meets requirements

- The parking lots and driveways are designed with 6x6” continuous curb to direct stormwater towards building where it is piped to the front of the lot to allow for on-site retention. The remainder of the stormwater will be directed towards the alley. Public Works – Surface Waters and Sewers has reviewed the plan and has given their preliminary approval for stormwater management.
- The proposed development does not block views of important city elements.
- The proposed development does not shadow any public spaces and would only minimally shadow adjacent properties.
- The development has been designed to minimize wind currents at ground.
- The site plan shows Crime prevention through environmental design, using natural surveillance with large open windows, lighting, and space delineation, natural access control with landscaping and screening.
- The proposed development does not include the rehabilitation of a locally designated historic structures or the reuse of significant features of historic buildings.

2. Conformance with all applicable regulations of the zoning ordinance.

The proposed use is *permitted* in the R5 District.

Off-street Parking and Loading – Requires variance(s)

- The proposed project requires two variances; one to reduce the off-street parking requirement from 22 spaces to 18 and the other is to allow for 46% of the required parking to be compact stalls, where 25% is the maximum.

Table 3. Vehicle Parking Requirements Per Use (Chapter 54I)

	Minimum Vehicle Parking	Applicable Reductions	Total Minimum Requirement	Maximum Parking Allowed	Proposed
Residential dwellings	24	Transit Incentives (2)	22	--	--
Total	--	--	22		18

Table 4. Bicycle Parking and Loading Requirements (Chapter 54I)

	Minimum Bicycle Parking	Minimum Short-Term	Minimum Long-Term	Proposed	Loading Requirement	Proposed
Residential dwellings	12	--	Not less than 90%	XX	None	
Total	12	--	11	24 (15 long-term)	None	None

Building Bulk and Height – Meets requirements

Table 5. Building Bulk and Height Requirements

	Code Requirement	Proposed
Lot Area	--	15,557 sq. ft. / .36 acres
Gross Floor Area (GFA)	--	18,933 sq. ft.
Maximum Floor Area Ratio (GFA/Lot Area)	1.7	1.2
Maximum Building Height	4 stories or 56 feet, whichever is less	3 stories, 37 ft.

Lot Requirements – Meets requirements

Table 5. Lot Requirements Summary

	Code Requirement	Proposed
Dwelling Units (DU)	--	24 DUs
Density (DU/acre)	--	67 DU/acre
Minimum Lot Area	--	648 sq. ft. per DU
Maximum Impervious Surface Area	85%	79%
Maximum Lot Coverage	70%	41%
Minimum Lot Width	40 ft.	124 ft. 2 in.

Yard Requirements – Requires variance(s)

- A variance to reduce the front yard setback along Grand Avenue South from approximately 22.5 feet to 15 feet has been requested and staff is recommending approval.
- A variance to reduce the south interior side yard setback from 9 feet to approximately 8.5 feet has been requested and staff is recommending approval.
- Variances to reduce the north and south interior side yard setbacks from 5 feet to 2 and 3 feet, respectively; staff is recommending denial of these requested variances.

Table 6. Minimum Yard Requirements

	Zoning District	Overriding Regulations	Total Requirement	Proposed
Front	15 ft.	22.5 feet	22.5 ft.	15 ft.
Interior Side (North)	9 ft. Building 5 ft. Parking Lot	--	9 ft. Building 5 ft. Parking Lot	9 ft. Building 2 ft. Parking Lot
Interior Side (South)	9 ft. Building 5 ft. Parking Lot	--	9 ft. Building 5 ft. Parking Lot	9 ft. Building 3 ft. Parking Lot
Rear	9 ft. Building 1 ft. Parking Lot	--	9 ft. Building 1 ft. Parking Lot	9 ft. Building 1 ft. Parking Lot

Signs – Not applicable

Refuse Screening – Meets requirements

- Refuse and recycling storage containers are required to be enclosed on all four sides by screening compatible with the principal structure not less than two feet higher than the refuse container or must be otherwise effectively screened from the street, adjacent residential uses located in a residence or office residence district and adjacent permitted or conditional residential uses. The applicant is proposing to screen the refuse storage area with material compatible to the screening of the parking lot.

Screening of Mechanical Equipment – Meets requirements with Conditions of Approval

- All mechanical equipment is required to be arranged so as to minimize visual impact by using screening and must comply with Chapter 535 and district requirements. The existing mechanical equipment is not screened from the roof. The applicant is proposing to add additional mechanical equipment to the roof. Staff recommends that the Planning Commission require screening for the mechanical equipment in compliance with section 535.70 of the zoning code.

Lighting – Meets requirements with Conditions of Approval

- Lighting must comply with Chapter 535 and Chapter 541 of the zoning code. Lighting information has not been provided for the site. A full lighting plan will need to be provided.

Specific Development Standards – Not applicable

- Specific development standards for multiple-family dwellings are only required for outdoor children's play equipment where dwellings have a minimum of three bedrooms. The proposed development has only one-bedroom units and therefore the specific development standards to not apply.

3. Conformance with the applicable policies of The Minneapolis Plan for Sustainable Growth.

The Minneapolis Plan for Sustainable Growth identifies the site as Urban Neighborhood on the future land use map. The proposed development is consistent with the following principles and policies outlined in the comprehensive plan:

Urban Design Policy 10.4: Support the development of residential dwellings that are of high quality and compatible with surrounding development.

10.4.1 Maintain and strengthen the architectural character of the city's various residential neighborhoods.

10.4.2 Promote the development of new housing that is compatible with existing development in the area and the best of the city's existing housing stock.

10.4.3 Advance the understanding of urban housing and retail design among members of the design and development community.

Urban Design Policy 10.6: New multi-family development or renovation should be designed in terms of traditional urban building form with pedestrian scale design features at the street level.

10.6.1 Design buildings to fulfill light, privacy, and view requirements for the subject building as well as for adjacent properties by building within required setbacks.

10.6.3 Provide appropriate physical transition and separation using green space, setbacks or orientation, stepped down height, or ornamental fencing to improve the compatibility between higher density and lower density residential uses.

10.6.4 Orient buildings and building entrances to the street with pedestrian amenities like wider sidewalks and green spaces.

10.6.5 Street-level building walls should include an adequate distribution of windows and architectural features in order to create visual interest at the pedestrian level.

10.6.6 Integrate transit facilities and bicycle parking amenities into the site design.

4. Conformance with applicable development plans or objectives adopted by the City Council.

There are no additional development plans or objectives for the subject property.

5. Alternative compliance.

The Planning Commission or zoning administrator may approve alternatives to any site plan review requirement upon finding that the project meets one of three criteria required for alternative compliance. Alternative compliance is requested for the following requirements:

• **Building Design**

- Exterior materials are durable; the first floor is a prefinished horizontal metal siding, the upper floors are 5/8" fiber cement board lap siding and there are two prefinished metal panel details on the front façade on the north and south ends of the building. The maximum percentage of fiber cement board lap siding on each elevation is 30%; the applicant is proposing 45% on the front and rear elevations, 48% on the north elevation and 58% on the south elevation. The applicant is requesting alternative compliance and staff is recommending a maximum of 30% fiber cement lap siding.

• **Landscaping**

- A landscaped yard at least seven (7) feet wide shall be provided along the property line and alley. The applicant is proposing 2 feet of a landscaped yard at the north, 3 feet along the south and no landscaping along the alley. The applicant is seeking alternative compliance and has stated that it is impractical to provide additional landscaping and enough parking to support the proposed development. Staff recognizes the site constraints and recommends that the planning commission grant alternative compliance to requiring a 7-foot landscaped yard along the alley. However, staff finds that the applicant has the ability to reconfigure the walkway and co-locate the trash enclosure with the bicycle storage to allow for a minimum of a 5-foot landscaped yard. This will require the elimination of one off-street parking stall.

- Many of the parking stalls are not located within 50 feet of an on-site deciduous tree. The applicant is seeking alternative compliance; however, staff finds the applicant has the ability to redesign the parking lot to allow for greater landscaped yards along the north and south property line to allow for more deciduous trees.

RECOMMENDATIONS

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt staff findings for the application(s) by The Lander Group for the properties located at 3535, 3539 and 3543 Grand Avenue South in the R5 Multiple-Family District:

A. Variance to reduce the off-street parking requirement.

Recommended motion: **Approve** the application for a variance to reduce the off-street parking requirement from 22 spaces to **17 spaces** to allow for a new 24-unit, multiple-family dwelling.

B. Variance to increase the percentage of required parking spaces that may be satisfied by providing compact parking stalls from 25% to 46%.

Recommended motion: **Approve** the application for a variance the percentage of required parking spaces that may be satisfied by providing compact parking stalls from 25% to 47% to allow for a new 24-unit, multiple-family dwelling.

C. Variance to reduce the front yard setback along Grand Avenue South.

Recommended motion: **Approve** the application for a variance to reduce the front yard setback along Grand Avenue South from 22.5 feet to 15 feet for the proposed residential structure and to 10 feet for the proposed canopy to allow for a new 24-unit, multiple-family dwelling.

D. Variance to reduce the south interior side yard setback along for the proposed building.

Recommended motion: **Approve** the application for a variance to reduce the south interior side yard setback from 9 feet to 8.5 feet for the proposed residential structure to allow for a new 24-unit, multiple-family dwelling.

E. Variance to reduce the north interior side yard setback along for the proposed parking lot.

Recommended motion: **Deny** the application for a variance to reduce the north interior side yard setback from 5 feet to 2 feet for the proposed parking lot to allow for a new 24-unit, multiple-family dwelling.

F. Variance to reduce the south interior side yard setback along for the proposed parking lot.

Recommended motion: **Deny** the application for a variance to reduce the south interior side yard setback from 5 feet to 3 feet for the proposed parking lot to allow for a new 24-unit, multiple-family dwelling.

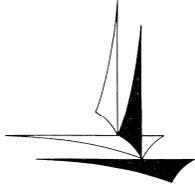
G. Site Plan Review for a new 24-unit, multiple-family dwelling.

Recommended motion: **Approve** the application for site plan review for a new 24-unit, multiple-family dwelling, subject to the following conditions:

1. All mechanical equipment shall be arranged so as to minimize visual impact by using screening consistent with section 535.70 of the zoning code.
2. Lighting shall comply with section 535.590 of the zoning code.
3. All of the proposed parking stalls shall be located within 50 feet of an on-site deciduous tree.
4. A maximum of 30% of the building elevations shall be fiber cement lap-siding to ensure durability and compatibility.
5. Department of Community Planning and Economic Development staff review and approval of the final building elevations, floor, site, lighting and landscape plans.
6. Site improvements required by Chapter 530 or by the City Planning Commission shall be completed by June 1, 2017, unless extended by the Zoning Administrator, or the permit may be revoked for non-compliance.

ATTACHMENTS

1. PDR report
2. Written description and findings submitted by applicant
3. Zoning map
4. Site plan
5. Site survey
6. Plans
7. Building elevations
8. Renderings
9. Alternative site plan as proposed by staff
10. Photos
11. Oblique aerials
12. Correspondence



Minneapolis Development Review
250 South 4th Street
Room 300
Minneapolis, MN 55415

Preliminary Development Review Report

Development Coordinator Assigned: **PATRICIA MURZYN**
(612) 673-5827
patricia.murzyn@minneapolismn.gov

Status *
RESUBMISSION REQUIRED

Tracking Number:	PDR 1001303
Applicant:	3535 GRAND II LLC C/O LANDER GROUP 3802 NICOLLET AVE S #200 MINNEAPOLIS, MN 55409
Site Address:	3535 GRAND AVE S 3539 GRAND AVE S 3543 GRAND AVE S
Date Submitted:	15-APR-2015
Date Reviewed:	22-APR-2015

Purpose

The purpose of the Preliminary Development Review (PDR) is to provide Customers with comments about their proposed development. City personnel, who specialize in various disciplines, review site plans to identify issues and provide feedback to the Customers to assist them in developing their final site plans.

The City of Minneapolis encourages the use of green building techniques. For additional information please check out our green building web page at: http://www.ci.minneapolis.mn.us/mdr/GreenBuildingOptions_home.asp.

DISCLAIMER: The information in this review is based solely on the preliminary site plan submitted. The comments contained in this report are preliminary ONLY and are subject to modification.

Project Scope

Three story, 24 unit, 1 bedroom apartment building.

Review Findings (by Discipline)

Business Licensing

- There is no Minneapolis Business Licensing review required for the proposed project.

□ Addressing

- Per City of Minneapolis Street Naming and Address Standard V1.22, the City of Minneapolis holds authority for assignment of all addresses, verification, change, and/or additions. Each assigned address number uses the street that provides the best/direct access for life safety equipment and best/direct access to the occupants.
- The address for the proposed apartment building will be **3539 Grand Avenue**. This address meets the City of Minneapolis Street Naming and Address Standard requirements.
 - When assigning suite sequences the following guidelines are as follows:
 - The first one to two digits of the suite sequence number will designate the floor number of the site.
 - The last two digits of the suite sequence number will designate the unique ID for the unit (condo, suite, unit, or apartment).
 - Suite sequence digit numbers will be assigned to dwelling, commercial and retail units, not common areas. For example, laundry rooms, saunas, workout rooms, etc., would not be assigned numbers.
- Please provide each condo, suite, unit or apartment number.

□ Zoning - Planning

- CPED staff has identified the following land use applications will be required for this development:
 - Variance to reduce the required off-street parking from 22 spaces to 18 spaces
 - Variance to increase the percentage of required parking spaces that may be satisfied by providing compact stalls
 - Variance to reduce the front yard setback along Grand Avenue South from the established setback created by connecting a line between the two adjacent structures from approximately 21.5 feet to 15 feet.
 - Variance to reduce the south interior side yard setback for the proposed structure
 - Variance to reduce the side yard setbacks from 5 feet to 2 and 3 feet 9 inches for an on-site parking area
 - Site plan review
- The project requires approval by the City Planning Commission. Staff will continue to work with the applicant on compliance with Chapter 530 Site Plan Review, Chapter 541 Off-Street Parking and Loading and meeting the requirements per the site plan review application.

□ Parks - Forestry

- Contact Craig Pinkalla (612-499-9233 cpinkalla@minneapolisparks.org.) regarding any questions related to planting, removal or the process for protection of trees during construction in the city right of way.
- Effective January 1, 2014, the City of Minneapolis and the Minneapolis Park and Recreation Board adopted an update to the existing Parkland Dedication Ordinance. The adopted City of Minneapolis Parkland Dedication ordinance is located in Section 598.340 of the City's Land Subdivision ordinance: <http://library.municode.com/index.aspx?clientId=11490>.
- As adopted, the fee in lieu of dedication for new residential units is \$1,500 per unit (affordable units excluded per ordinance) and for commercial and industrial development it is \$200 per development employee (as defined in ordinance). Any dedication fee (if required) must be paid at the time of building permit issuance.
- There is also an administration fee that is 5% of the calculated park dedication fee.
- As proposed, the calculated dedication fee is as follows:
 - Park Dedication Fee Calculation =

• Residential (24 units x \$1,521 per unit)	= \$36,504
• 5% of \$36,504 (Administration Fee)	= \$ 1,000
• Total Park Dedication Administrative Fee:	\$37,504
- This is a preliminary calculation based on your current proposal; a final calculation will be made at the time of building permit submittal.
- For further information, please contact Patty Murzyn 612-673-5827.

❑ Fire Safety

- Provide required fire suppression system throughout building.
- Fire department connection must be located on the address side of building and within 150 feet of a fire hydrant.
- Provide required fire alarm system throughout building.
- Provide and maintain fire apparatus access at all times.

❑ Right of Way

- The plan as submitted meets the requirements of the Public Works Right-Of-Way Division.
- Note to the Applicant: An encroachment permit shall be required for all streetscape elements in the Public right-of-way such as: plants & shrubs, planters, tree grates and other landscaping elements, sidewalk furniture (including bike racks and bollards), and sidewalk elements other than standard concrete walkways such as pavers, stairs, raised landings, retaining walls, access ramps, and railings (NOTE: railings may not extend into the sidewalk pedestrian area). Please contact Bob Boblett at (612) 673-2428 for further information.

❑ Street Design

- The plan as submitted meets the requirements of the Public Works Street Design Division.

❑ Sidewalk

- The plan as submitted meets the requirements of the Public Works Sidewalk Inspections Division.

❑ Traffic and Parking

- The plan as submitted meets the requirements of the Public Works Traffic & Parking Services Division.
- Note to the Applicant: the parking lot and access to the Public alley appears to be very constricted; it is recommended that turning maneuvers for all vehicles types that will be using the parking lot be shown on the plans.

❑ Water

- The plan as submitted meets the requirements of the Public Works Water Maintenance & Distribution Division.

❑ Sewer Design

- Surface Drainage: It is preferred that a majority of the drainage from the roof be directed to the front of lot, not towards the alley. Roof and plumbing plans were not provided, however, please consider this in development of those plans. There is a low point in the alley that may be susceptible to flooding.
- Utility Connections: It is preferred that the sanitary sewer connection be made at the existing manhole just downstream of the current proposed connection location. Please revise the connection location. The invert of the service at the structure should be above the spring line of the outgoing sewer, but no more than 2' above the crown of the outgoing sewer.
- For comments or questions on Public Works Surface Water & Sewers Division related requirements please contact Jeremy Strehlo, (Professional Engineer) at (612) 673-3973, or jeremy.strehlo@minneapolismn.gov.

❑ Construction Code Services

- A Service Availability Charge (SAC) determination will need to be submitted to the Metropolitan Council for the proposed project. Please refer to this link for more information.
http://www.ci.minneapolis.mn.us/mdr/docs/sac_availability_charge.pdf.
- There are no additional site comments at this time.

❑ Historical Preservation Committee

- There is no HPC flag on this property. HPC review is not required at this time.

❑ Environmental Health

- If impacted soil is encountered during site activities work will need to stop and notification provided to the MN State Duty officer at (615) 649-5451.
- If dewatering is required during site construction see below for city permit requirements. The highest groundwater level expected for this site should be determined and used in establishing the low point for finishing subgrade structures to prevent infiltration of groundwater without the need for a permanent dewatering system being installed. If a continuously operating permanent dewatering system is needed it must be approved as part of the sanitary sewer and storm drain site plan approval prior to construction beginning.
- No construction, demolition or commercial power maintenance equipment shall be operated within the city between the hours of 6:00 p.m. and 7:00 a.m. on weekdays or during any hours on Saturdays, Sundays and state and federal holidays, except under permit. Contact Environmental Services at 612-673-3867 for permit information.
- Permits and approval are required from Environmental Services for the following activities: Temporary storage of impacted soils on site prior to disposal or reuse; Reuse of impacted soils on site; Dewatering and discharge of accumulated storm water or ground water, underground or aboveground tank installation or removal, well construction or sealing. Contact Tom Frame at 612-673-5807 for permit applications and approvals.
- A review of the project, permits issued and an inspection from Environmental Service for identification of equipment and site operations that require annual registration with the City of Minneapolis will occur for this project.

END OF REPORT



04.09.2015

City of Minneapolis
Community Planning & Economic Development
250 S Fourth Street Rm. #300
ATTN: Shanna M. Sether

RE: 3535 – 3539 – 3543 Grand Avenue South

Project Description: The project sits on three properties, 3535 – 3539 – 3543 Grand Avenue S. The property is vacant of any structures at this time. The building is comprised of 24 one bedroom units. The units range in size from 460 square feet to 768 square feet. Many of the units will be geared to more affordable budgets with the smaller sizing, and the possibility of reducing automobile dependence. The project will be promoting alternative transportation choices.

The building design is broken into parts with primary features facing Grand Avenue. This portion is intended to approximate the historical pattern of three-story apartments in the neighborhood, and matches the scale of the apartments directly to the west. Grand Avenue has a two-three story building rhythm mixed with single family and rental apartments with individual front entrances, porches and stoops. This sizing approximates the pattern of the large multifamily apartments along Grand. The building is comprised of lap and metal siding.

Unit Mix: The building is comprised of 24 one bedroom units. The units range in size from 460 square feet to 768 square feet. The project is looking to attract a mix of residents, and a mixture of incomes and transit needs with the smaller units geared to a more affordable budget, with the second and third floor units designed with more glass to south exposure and attract a resident with more means.

24 one bedroom units total

Height: The building is three stories and thirty-five (35') in height.

Massing: The building massing is consistent with the adjacent apartment properties to the south and west along Grand Avenue.

Parking: Surface parking is located on the east side of the site. There are 18 total parking stalls.

	standard	compact	handicap	Total
Parking	7	10	1	18

Bicycle Parking: A dedicated bike enclosure/repair is being provided on the southeast corner of the site.

Ground mount bike loops : 8 stalls

Exterior Bike Enclosure: 16 stalls

Variances:

1. **Front Yard Setback:** Variance to reduce the front yard setback along Grand Avenue South from the established setback created by connecting a line between the two adjacent structures from approximately 20 feet to 15 feet.
 - The project aligns with the property to the south, which is of similar scale, along Grand Avenue reinforcing and improving the street edge and public realm.
 - The property owner or authorized applicant proposed to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan. The street edge is consistent with the fabric of the neighborhood and promotes a better connection to the pedestrian realm for the individual apartment dweller. This connection promotes greater street life as is consistent with single family dwellings.
 - The variance will not alter the essential character of the area and is not detrimental to health and safety.

2. **Parking Reduction / Setback / Compact Increase:** Variance to reduce the required off-street parking from 22 spaces to 18 spaces. Variance to increase the number of compact stalls. Variance to reduce the side yard setbacks from 5 feet to 1 and 2 feet 9 inches for an on-site parking area.
 - Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. Due to the compact nature of the site the surface parking was reduced. The project site has the benefits of two adjacent transit bus lines as well as an on-site bicycle storage and repair enclosure to help offset the reduced parking.
 - The bicycle enclosure and perimeter privacy fence will mirror the adjacent property's garages in order to mimic the adjacent scale along the alley and side yards. The intent is to control light pollution, and add continuity to the site design.
 - The property owner or authorized applicant proposed to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the

comprehensive plan. The proposal follows the intent of the ordinance by maintaining a more than 6'-0" vertical separation from a dwelling unit and exterior parking stalls.

- The variance will not alter the essential character of the area and is not detrimental to health and safety.

3. Side Yard Setback: Variance to reduce the south interior side yard setback for the proposed structure.

- Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The east – west running lot lines are not perpendicular to Grand Avenue. In an effort to continue the urban fabric we have placed the building parallel to Grand Avenue resulting in the southeast corner of the building to encroach into the sideyard by 8".
- The variance will not alter the essential character of the area and is not detrimental to health and safety.

*Egress stair to the north will not be the primary entrance to the building and will be emergency only. (Variance to allow for a principal entrance facing an interior side lot line from 15 feet to 9 feet (this variance will be required if the side entrance is not 'emergency-only'))

I believe all the necessary variance items have been described. Please let me know if there are additional items or discrepancies.

Please let me know if any of the items need additional information or clarifications.

Sincerely,

COLLAGE ARCHITECTS



Joshua Jansen, Assoc. A.I.A.
Vice President

Tony

From: Tony
Sent: Friday, March 27, 2015 1:33 PM
To: 'Norma Pietz'
Cc: 'Mark Hinds'; 'Pete Keely'; 'Andrew Moeding'; 'Josh Jansen'
Subject: RE: 36th & Bryant
Attachments: 150326 3535 Grand - Progress Set.pdf; 150325 3535Grand Site Plan.pdf

Norma,
Attached is the preliminary package regarding the development at 3535 Grand Avenue South in order to forward to the Housing Committee per your and Mark's direction from our March 12 meeting. We look forward and plan to attend the committee meeting next Wednesday April 1 at 6:30PM and present the project.

Please confirm receipt. Thank you and enjoy the weekend.

Tony Kriha

LANDERGROUP

urban development
3802 Nicollet Avenue South, Ste. 200
Minneapolis, MN 55409
612.986.2511 cell
tony@landergroup.com

From: Norma Pietz [mailto:norma@lyndale.org]
Sent: Tuesday, March 24, 2015 10:57 AM
To: Tony
Subject: RE: 36th & Bryant

Our meeting starts at 6:30 and it is here at the Lyndale Community Center. I will be sending out information to the committee on either Friday afternoon or Monday morning and will deliver flyers on Monday. You will be on the agenda about 7pm. Norma

From: Tony [mailto:Tony@landergroup.com]
Sent: Monday, March 23, 2015 4:31 PM
To: norma@lyndale.org
Cc: Mark Hinds
Subject: 36th & Bryant

Norma,

Thank you for the call and it was a pleasure to meet with you and Mark last week. We do not have anything to share at this point for your meeting tonight. Per our discussion last week, we are planning to have some basic information in a package for you on Friday in preparation for the housing committee meeting on April 1. Can you share with me the time and location for that meeting? Thank you.

Tony Kriha

LANDERGROUP

urban development
3802 Nicollet Avenue South, Ste. 200
Minneapolis, MN 55409
612.986.2511 cell

Josh Jansen

From: Tony <Tony@landergroup.com>
Sent: Thursday, April 9, 2015 4:18 PM
To: elizabeth.glidden@minneapolismn.gov
Cc: andrew@moedingpartners.com; Josh Jansen
Subject: 3535/35939/3543 Grand Avenue South
Attachments: 150408 3535 Grand COW_PDR.PDF

Councilmember Glidden,

In relation to the properties at 3535/3539/3543 Grand Avenue South in the Lyndale district of your ward, I am submitting the attached development package for your review and awareness. We look forward to working with the respective city departments leading another positive development for the city of Minneapolis.

For your reference, we have met informally with Mark Hinds of the Lyndale Neighborhood Association on March 12 and formally presented the project to the housing committee of the Lyndale Neighborhood Association on April 1. Let me know should you have any questions.

Thank you.

Tony Kriha

LANDERGROUP

urban development

3802 Nicollet Avenue South, Ste. 200

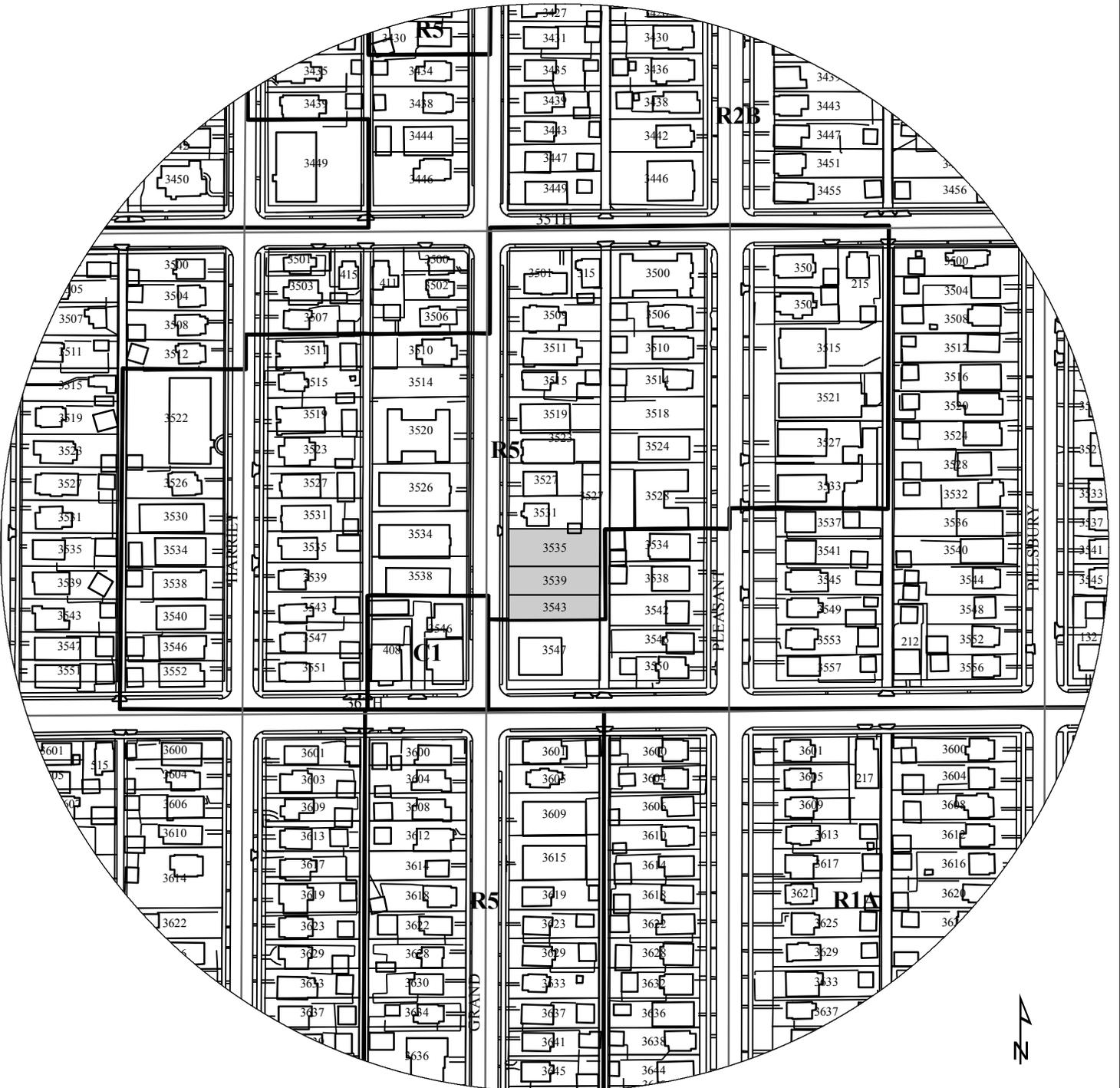
Minneapolis, MN 55409

612.986.2511 cell

tony@landergroup.com

NAME OF APPLICANT

WARD



PROPERTY ADDRESS

3535 Grand Avenue South

FILE NUMBER

BZZ 7112



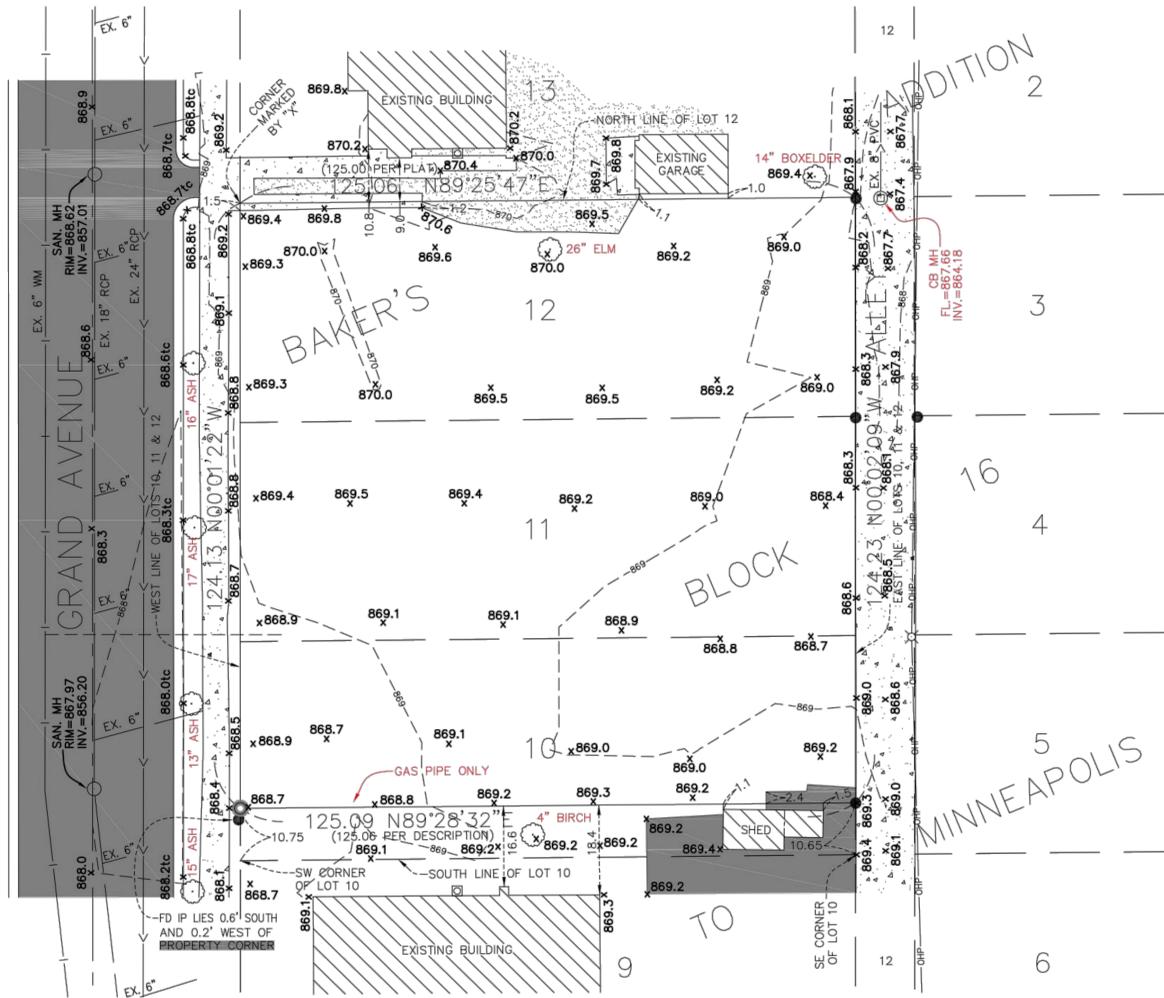
GRAND APARTMENTS

Minneapolis, MN
APR.09.2015

- > 24 DWELLING UNITS (ALL ONE BEDROOM)
- > 24 BICYCLE STALLS
- > 18 PARKING STALLS

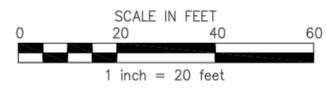
FIRST FLOOR PLAN
1/16" = 1'-0"





LEGEND

- | | | | |
|------|------------------------------|-------|----------------------|
| ○ | IRON SET | ■ | ASPHALT SURFACE |
| ● | IRON FOUND | ▨ | CONCRETE SURFACE |
| ⊗ | GAS METER | ▩ | GRAVEL SURFACE |
| ⊕ | POWER POLE | --- | EXISTING CONTOURS |
| ⊙ | LIGHT POLE | —OHP— | OVERHEAD POWER LINE |
| ⊗ | 6" x 6" x 3' WOOD GUARD POST | —S— | SANITARY SEWER |
| ⊗ | CATCH BASIN MANHOLE | —>— | STORM SEWER |
| ⊗ | SANITARY MANHOLE | — — | WATER MAIN |
| TC | TOP OF CURB | —G— | UNDERGROUND GAS LINE |
| RIM | TOP OF CASTING | ○ | DECIDUOUS TREE |
| X900 | EXISTING ELEVATION | | |
| PVC | POLYVINYL CHLORIDE PIPE | | |
| WM | WATER MAIN | | |
| RCP | REINFORCED CONCRETE PIPE | | |



PROPERTY DESCRIPTION

All that part of Lot 10, Block 16, Baker's Addition to Minneapolis, which lies North of the following described line: Beginning at a point in the West line of said Lot 10, distant 10.75 feet North of the Southwest corner of said Lot 10; thence running East 125.06 feet more or less to a point in the East line of said Lot 10, which point is distant 10.65 feet North of the Southeast corner of said Lot 10, according to the plat thereof on file and of record in the office of the Register of Deeds in and for Hennepin County, Minnesota.

(Abstract)

AND

Lots 11 and 12, Block 16, Baker's Addition to Minneapolis, according to the plat thereof on file or of record in the office of the Register of Deeds in and for said County.

(Torrens - Certificate of Title No. 715263)

We hereby certify that this is a true and correct survey of the above described property and that it was performed by me or under my direct supervision and that I am a duly licensed Professional Land Surveyor under the laws of the State of Minnesota. That this survey does not purport to show all improvements, easements or encroachments to the property except as shown thereon.

Signed this 15th day of February, 2012 For: James R. Hill, Inc.

By: Harold C. Peterson, Land Surveyor, MN License No. 12294

NOTES

ORIENTATION OF THIS BEARING SYSTEM IS BASED ON THE WEST LINE OF LOTS 10, 11 AND 12, BLOCK 16, BAKER'S ADDITION TO MINNEAPOLIS TO HAVE AN ASSUMED BEARING OF N 00°01'22" W.

NO SPECIFIC SOILS INVESTIGATION HAS BEEN COMPLETED ON THIS LOT BY JAMES R. HILL, INC. THE SUITABILITY OF SOILS TO SUPPORT THE SPECIFIC STRUCTURE PROPOSED IS NOT THE RESPONSIBILITY OF JAMES R. HILL INC. OR THE SURVEYOR.

THIS SURVEY HAS BEEN PREPARED WITHOUT BENEFIT OF A TITLE COMMITMENT OR TITLE OPINION. A TITLE SEARCH FOR RECORDED OR UNRECORDED EASEMENTS WHICH MAY BENEFIT OR ENCUMBER THIS PROPERTY HAS NOT BEEN COMPLETED BY THE SURVEYOR.

THE LOCATION AND INFORMATION SHOWN REGARDING UTILITIES, SERVING THIS PROPERTY OR EXISTING ON THIS PROPERTY AS SHOWN AS A PART OF THIS SURVEY, HAVE BEEN LOCATED BY ON-SITE OBSERVATION OR TAKEN FROM PLANS PROVIDED BY OTHERS. FOR FURTHER INFORMATION CONCERNING THESE UTILITIES PLEASE CONTACT THE CITY OF MINNEAPOLIS ENGINEERING DEPARTMENT AND/OR GOPHER STATE ONE CALL. (GOPHER STATE ONE LOCATE TICKET NUMBER 121030701)

CONTOUR INTERVAL IS 1 FEET. ALL ELEVATIONS ARE BASED ON THE CITY OF MINNEAPOLIS DATUM. (NGVD 29)

BENCHMARK: TOP NUT HYDRANT AT NORTHWEST CORNER OF GRAND AVENUE SOUTH AND WEST 36TH STREET. ELEVATION = 870.28 FEET

CURRENT ZONING FOR LOTS 10, 11 AND 12, BLOCK 16, BAKER'S ADDITION TO MINNEAPOLIS IS R5 MULTIPLE-FAMILY DISTRICT.

MINIMUM YARD REQUIREMENTS OF R5 MULTIPLE-FAMILY DISTRICT PER CITY OF MINNEAPOLIS ZONING ORDINANCE.

- FRONT - 15 FEET
- REAR AND INTERIOR SIDE - 5 + 2X FEET
- CORNER SIDE - 8 + 2X FEET

X = NUMBER OF STORIES ABOVE THE FIRST FLOOR

NOTE: MINIMUM SETBACK REQUIREMENTS ARE ESTABLISHED BY CITY ORDINANCE. THE SETBACK LINES ARE NOT DRAWN ON ANY OFFICIAL MAP FOR THIS SITE. THE LOCATION OF SETBACK LINES ARE SUBJECT TO LEGAL INTERPRETATION AND PLANNING CONSIDERATIONS. THEREFORE THE MINIMUM SETBACKS ARE LISTED BUT NOT SHOWN ON THIS SURVEY. SPECIFIC QUESTIONS REGARDING SETBACK REQUIREMENTS SHOULD BE DIRECTED TO THE REGULATING AUTHORITY.

OVERALL GROSS AREA = 15,557 SQUARE FEET OR 0.357 ACRES

ADDRESS OF THIS PROPERTY IS 3543, 3539 AND 3535 GRAND AVENUE SOUTH, MINNEAPOLIS, MINNESOTA 55408

PID NUMBER = 03-028-24-32-0207, 03-028-24-32-0235 AND 03-028-24-32-0236

SANITARY SEWER SERVICES SHOWN ARE FROM CITY OF MINNEAPOLIS AS-BUILT PLANS. SERVICES SHOWN ARE 6" CLAY OR CAST IRON PIPE PER AS-BUILT PLANS.

James R. Hill, Inc.
 PLANNERS / ENGINEERS / SURVEYORS
 2500 W. CT. RD. 42, SUITE 120, BURNVILLE, MN 55337
 PHONE: (952)880-6044 FAX: (952)880-6244

3543 Grand Avenue South
 MINNEAPOLIS, MINNESOTA
 BOUNDARY AND TOPOGRAPHIC SURVEY
 FOR
 CPM Development
 2919 Knox Avenue South, Suite 200, Minneapolis, MN 55408

DRAWN BY	SHP
DATE	2/15/12
REVISIONS	4/16/12 Additional utility info
	5/21/12 Sanitary sewer services loc. & note
CAD FILE	22071-20s.dwg
PROJECT NO.	22071-20
FILE NO.	1-12-012
SHEET 1 OF 1	



3535 GRAND
3535 GRAND AVENUE
MINNEAPOLIS, MN



CivilSite
GROUP
4801 W. 56TH ST. SUITE 200
ST. LOUIS PARK, MN 55416
CivilSiteGroup.com
Matt Pavik 763-213-3944 Pat Sarver 952-250-2003

DATE: 04.09.2015
I HEREBY CERTIFY THAT THIS PLAN SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
Matthew R. Pavik
MATTHEW R. PAVIK
REGISTRATION NO: 44263

Collage | architects
Architect
Pete Keely
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St. Paul, Minnesota 55114
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DATE: ARPIL 9, 2015

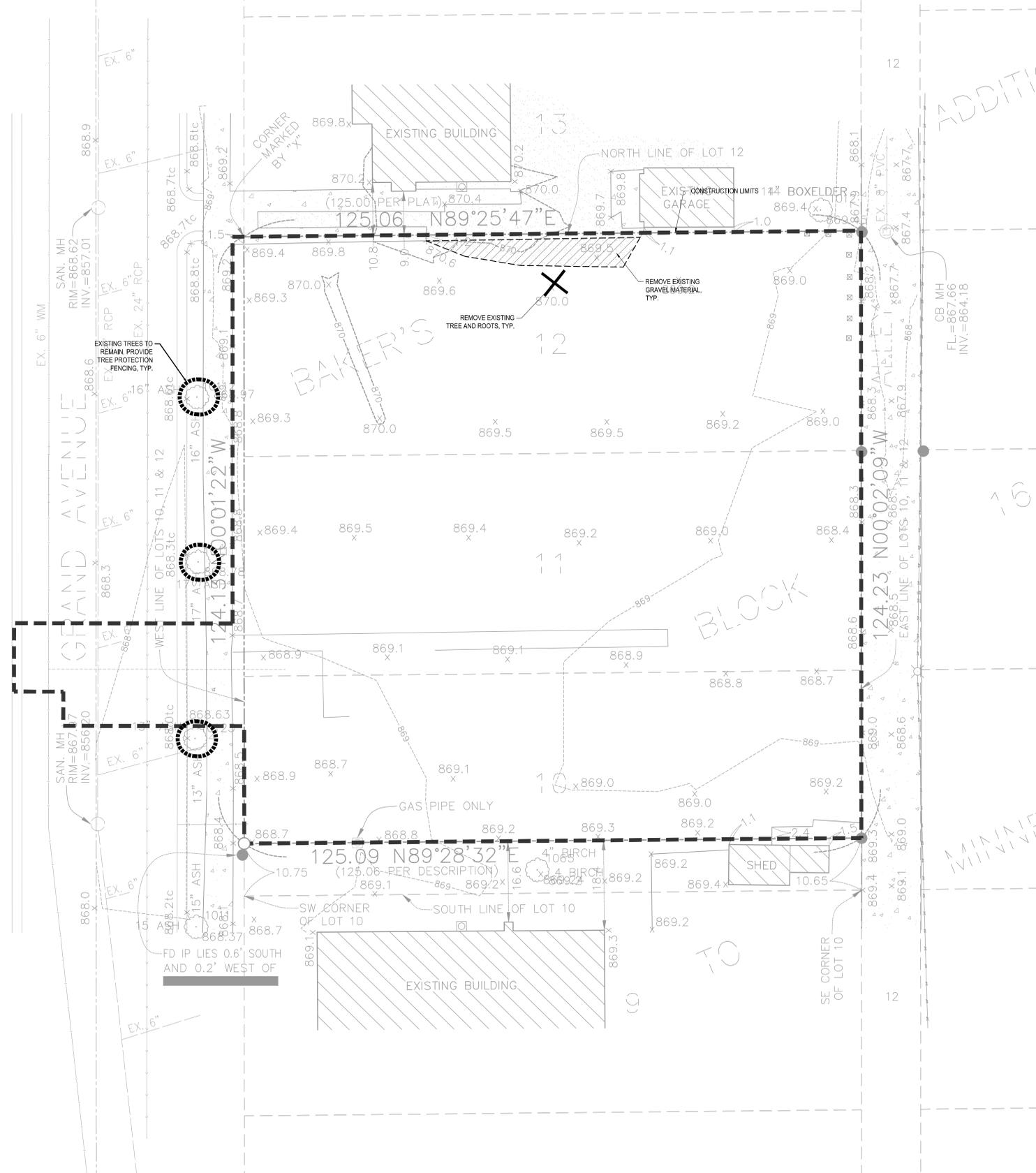
PDR PLAN REVIEW

MARK	DATE	DESCRIPTION

PROJECT NO: 15074
DRAWN BY: DJK
CHKD BY: MRP

SHEET TITLE
REMOVALS PLAN

C1.0



CITY OF MINNEAPOLIS REMOVAL NOTES:

- RESERVED FOR CITY SPECIFIC REMOVAL NOTES.

EROSION CONTROL NOTES:

- SEE SWPPP ON SHEETS SW1.0-SW1.3

REMOVALS LEGEND:

- EX. 1' CONTOUR ELEVATION INTERVAL
- REMOVAL OF PAVEMENT AND ALL BASE MATERIAL, INCLUDING BIT., CONC., AND GRAVEL PAVTS.
- REMOVAL OF STRUCTURE INCLUDING ALL FOOTINGS AND FOUNDATIONS.
- REMOVAL OF UTILITY LINES - COORDINATE WITH UTILITY COMPANY PRIOR TO CONSTRUCTION
- TREE REMOVAL - INCLUDING ROOTS AND STUMPS

REMOVAL NOTES:

- SEE STORM WATER POLLUTION PREVENTION PLAN (SWPPP) PLAN FOR CONSTRUCTION STORM WATER MANAGEMENT PLAN.
- REMOVAL OF MATERIALS NOTED ON THE DRAWINGS SHALL BE IN ACCORDANCE WITH MNDOT, STATE AND LOCAL REGULATIONS.
- REMOVAL OF PRIVATE UTILITIES SHALL BE COORDINATED WITH UTILITY OWNER PRIOR TO CONSTRUCTION ACTIVITIES.
- EXISTING PAVEMENTS SHALL BE SAWCUT IN LOCATIONS AS SHOWN ON THE DRAWINGS OR THE NEAREST JOINT FOR PROPOSED PAVEMENT CONNECTIONS.
- REMOVED MATERIALS SHALL BE DISPOSED OF TO A LEGAL OFF-SITE LOCATION AND IN ACCORDANCE WITH STATE AND LOCAL REGULATIONS.
- ABANDON, REMOVAL, CONNECTION, AND PROTECTION NOTES SHOWN ON THE DRAWINGS ARE APPROXIMATE. COORDINATE WITH PROPOSED PLANS.
- EXISTING ON-SITE FEATURES NOT NOTED FOR REMOVAL SHALL BE PROTECTED THROUGHOUT THE DURATION OF THE CONTRACT.
- PROPERTY LINES SHALL BE CONSIDERED GENERAL CONSTRUCTION LIMITS UNLESS OTHERWISE NOTED ON THE DRAWINGS. WORK WITHIN THE GENERAL CONSTRUCTION LIMITS SHALL INCLUDE STAGING, DEMOLITION AND CLEAN-UP OPERATIONS AS WELL AS CONSTRUCTION SHOWN ON THE DRAWINGS.
- MINOR WORK OUTSIDE OF THE GENERAL CONSTRUCTION LIMITS SHALL BE ALLOWED AS SHOWN ON THE PLAN AND PER CITY REQUIREMENTS.
- DAMAGE BEYOND THE PROPERTY LIMITS CAUSED BY CONSTRUCTION ACTIVITY SHALL BE REPAIRED IN A MANNER APPROVED BY THE ENGINEER/LANDSCAPE ARCHITECT OR IN ACCORDANCE WITH THE CITY.

OWNER, DEVELOPER, CONTRACTOR SIGNED STATEMENT

ALL CLEARING, GRADING, CONSTRUCTION OR DEVELOPMENT WILL BE DONE PURSUANT TO THE PLAN, SIGNED BY PARTIES BELOW:

OWNER: _____
DEVELOPER: _____
CONTRACTOR: _____

OWNER INFORMATION

OWNER:
3535 GRAND LLC
3802 NICOLLET AVENUE SOUTH, SUITE 200
MINNEAPOLIS, MN 55409

- PROPOSED WORK (BUILDING AND CIVIL) SHALL NOT DISTURB EXISTING UTILITIES UNLESS OTHERWISE SHOWN ON THE DRAWINGS AND APPROVED BY THE CITY PRIOR TO CONSTRUCTION.
- SITE SECURITY MAY BE NECESSARY AND PROVIDED IN A MANNER TO PROHIBIT VANDALISM AND THEFT, DURING AND AFTER NORMAL WORK HOURS, THROUGHOUT THE DURATION OF THE CONTRACT. SECURITY MATERIALS SHALL BE IN ACCORDANCE WITH THE CITY.
- VEHICULAR ACCESS TO THE SITE SHALL BE MAINTAINED FOR DELIVERY AND INSPECTION ACCESS DURING NORMAL OPERATING HOURS. AT NO POINT THROUGHOUT THE DURATION OF THE CONTRACT SHALL CIRCULATION OF ADJACENT STREETS BE BLOCKED WITHOUT APPROVAL BY THE CITY PRIOR TO CONSTRUCTION ACTIVITIES.
- ALL TRAFFIC CONTROLS SHALL BE PROVIDED AND ESTABLISHED PER THE REQUIREMENTS OF THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MMUTCD) AND THE CITY. THIS SHALL INCLUDE, BUT NOT BE LIMITED TO, SIGNAGE, BARRICADES, FLASHERS, AND FLAGGERS AS NEEDED. ALL PUBLIC STREETS SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES. NO ROAD CLOSURES SHALL BE PERMITTED WITHOUT APPROVAL BY THE CITY.
- SHORING FOR BUILDING EXCAVATION MAY BE USED AT THE DISCRETION OF THE CONTRACTOR AND AS APPROVED BY THE OWNERS REPRESENTATIVE AND THE CITY PRIOR TO CONSTRUCTION ACTIVITIES.
- STAGING, DEMOLITION, AND CLEAN-UP AREAS SHALL BE WITHIN THE PROPERTY LIMITS AS SHOWN ON THE DRAWINGS AND MAINTAINED IN A MANNER AS REQUIRED BY THE CITY.

11. PROPOSED WORK (BUILDING AND CIVIL) SHALL NOT DISTURB EXISTING UTILITIES UNLESS OTHERWISE SHOWN ON THE DRAWINGS AND APPROVED BY THE CITY PRIOR TO CONSTRUCTION.

12. SITE SECURITY MAY BE NECESSARY AND PROVIDED IN A MANNER TO PROHIBIT VANDALISM AND THEFT, DURING AND AFTER NORMAL WORK HOURS, THROUGHOUT THE DURATION OF THE CONTRACT. SECURITY MATERIALS SHALL BE IN ACCORDANCE WITH THE CITY.

13. VEHICULAR ACCESS TO THE SITE SHALL BE MAINTAINED FOR DELIVERY AND INSPECTION ACCESS DURING NORMAL OPERATING HOURS. AT NO POINT THROUGHOUT THE DURATION OF THE CONTRACT SHALL CIRCULATION OF ADJACENT STREETS BE BLOCKED WITHOUT APPROVAL BY THE CITY PRIOR TO CONSTRUCTION ACTIVITIES.

14. ALL TRAFFIC CONTROLS SHALL BE PROVIDED AND ESTABLISHED PER THE REQUIREMENTS OF THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MMUTCD) AND THE CITY. THIS SHALL INCLUDE, BUT NOT BE LIMITED TO, SIGNAGE, BARRICADES, FLASHERS, AND FLAGGERS AS NEEDED. ALL PUBLIC STREETS SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES. NO ROAD CLOSURES SHALL BE PERMITTED WITHOUT APPROVAL BY THE CITY.

15. SHORING FOR BUILDING EXCAVATION MAY BE USED AT THE DISCRETION OF THE CONTRACTOR AND AS APPROVED BY THE OWNERS REPRESENTATIVE AND THE CITY PRIOR TO CONSTRUCTION ACTIVITIES.

16. STAGING, DEMOLITION, AND CLEAN-UP AREAS SHALL BE WITHIN THE PROPERTY LIMITS AS SHOWN ON THE DRAWINGS AND MAINTAINED IN A MANNER AS REQUIRED BY THE CITY.

GOPHER STATE ONE CALL
WWW.GOPHERSTATEONECALL.ORG
(800) 252-1166 TOLL FREE
(651) 454-0002 LOCAL





3535 GRAND
3535 GRAND AVENUE
MINNEAPOLIS, MN

MOEDING
LANDERGROUP
urban development

CivilSite
GROUP
4931 W. 35TH ST. SUITE 200
ST. LOUIS PARK, MN 55416
CivilSiteGroup.com
Matt Pavik 763-213-3944 Pat Sarver 952-250-2003

DATE: 04.09.2015

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Voigt & Associates, Inc.
Structural Engineer
Paul Voigt
651.686.7727
4635 Nicols Road, Suite 204
Eagan, Minnesota 55122

DATE: **ARPL 9, 2015**

PDR PLAN REVIEW

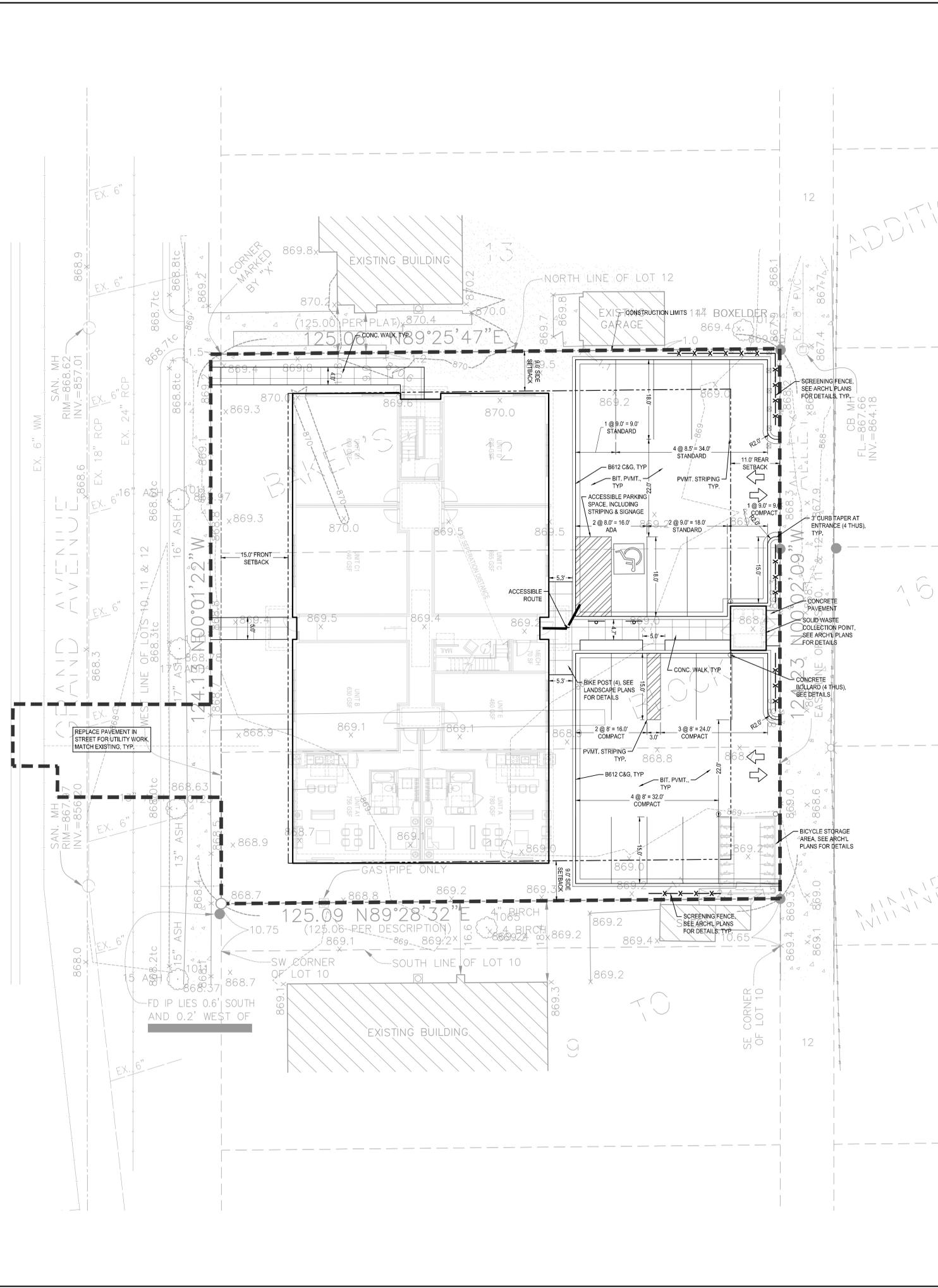
MARK	DATE	DESCRIPTION

PROJECT NO: 15074
DRAWN BY: DJK
CWD BY: MRP

SHEET TITLE

SITE PLAN

C2.0



CITY OF MINNEAPOLIS SITE SPECIFIC NOTES:

- RESERVED FOR CITY SPECIFIC NOTES.

OWNER INFORMATION

OWNER:
3535 GRAND LLC
3802 NICOLLET AVENUE SOUTH, SUITE 200
MINNEAPOLIS, MN 55409

SITE PLAN LEGEND:

- CONCRETE PAVEMENT AS SPECIFIED (PAD OR WALK)
- PROPERTY LINE
- CURB AND GUTTER-SEE NOTES
- TRAFFIC DIRECTIONAL ARROWS
- SIGN AND POST ASSEMBLY. SHOP DRAWINGS REQUIRED.
- HC = ACCESSIBLE SIGN
- NP = NO PARKING FIRE LANE
- ST = STOP
- CP = COMPACT CAR PARKING ONLY

OPERATIONAL NOTES:

- ALL SNOW WILL BE EITHER BE REMOVED FROM SITE AT EACH PLOWING OR STORED IN THE LANDSCAPED AREAS- NO SPECIFIC SNOW STORAGE LOCATIONS WILL BE REQUIRED.
- TRASH REMOVAL AND REGULAR DELIVERIES SHALL BE OCCUR THROUGH THE ALLEY EAST OF THE PROPOSED PROPERTY.

CITY COORDINATION NOTES:

- TRAFFIC & PARKING
 - COORDINATE WITH BILL PRINCE, CITY OF MINNEAPOLIS, (612) 673-3901 FOR ALL WORK REGARDING STREET LIGHTING AND ELECTRICAL SYSTEMS. STREET LIGHTING INSTALLED AS PART OF THE PROJECT SHALL BE INSPECTED BY THE CITY. CONTACT DAVE PREHALL (612) 673-5759. ANY LIGHTING INSTALLATIONS NOT MEETING CITY SPECIFICATIONS WILL BE REQUIRED TO BE REINSTALLED AT CONTRACTOR'S EXPENSE.
 - COORDINATE WITH BOB BOBLETT CITY OF MINNEAPOLIS, (612) 673-2428 FOR ALL ISSUES REGARDING WORK IN AND ADJACENT TO CITY RIGHTS-OF-WAY. AN ENCROACHMENT PERMIT SHALL BE REQUIRED FOR ALL STREETScape ELEMENTS IN THE PUBLIC ROW. ANY ELEMENTS OF AN EARTH RETENTION SYSTEM AND RELATED OPERATIONS WILL REQUIRE AN ENCROACHMENT PERMIT. ANY EXCAVATIONS THAT FALL WITHIN THE PUBLIC ROW WILL REQUIRE A ROW EXCAVATION PERMIT.
 - COORDINATE WITH SCOTT KRAMER, CITY OF MINNEAPOLIS, (612) 673-2385 REGARDING ANY WORK THAT IS PERFORMED IN THE RIGHT-OF-WAY. AN OBSTRUCTION PERMIT WILL BE REQUIRED FOR ALL WORK IN THE RIGHT-OF-WAY. CONTACT SCOTT KRAMER REGARDING DETAILS OF SIDEWALK AND LANE CLOSURES.
 - COORDINATE WITH CRAIG PINKALLA, CITY OF MINNEAPOLIS, (612) 499-9233 FOR ALL WORK REGARDING REMOVAL OR PROTECTION OF TREES DURING CONSTRUCTION IN THE CITY RIGHT-OF-WAY.
 - COORDINATE WITH PAUL CAO, CITY OF MINNEAPOLIS, (612) 673-2943 FOR ALL WORK REGARDING BIKE RACKS IN THE CITY RIGHT-OF-WAY.
 - COORDINATE WITH ALAN KLUGMAN, CITY OF MINNEAPOLIS, (612) 673-5750 PRIOR TO CONSTRUCTION FOR THE TEMPORARY REMOVAL/RELOCATION OF ANY CITY OF MINNEAPOLIS SIGNAL SYSTEM. ALL COSTS FOR RELOCATION AND/OR REPAIR OF CITY TRAFFIC SHALL BE BORNE BY THE CONTRACTOR.
 - COORDINATE WITH DOUG MADAY, CITY OF MINNEAPOLIS, (612) 673-5755 PRIOR TO CONSTRUCTION FOR THE REMOVAL OR RELOCATION OF ANY CITY OF MINNEAPOLIS RIGHT-OF-WAY SIGNS.
- ENVIRONMENTAL HEALTH
 - COORDINATE WITH ENVIRONMENTAL SERVICES, CITY OF MINNEAPOLIS, (612) 673-3867 FOR PERMITS RELATING TO AFTER HOURS WORK, TEMPORARY STORAGE OF IMPACTED SOILS ON SITE PRIOR TO DISPOSAL OR REUSE, REMEDIATION OF CONTAMINATED SOIL AND GROUNDWATER, REUSE OF IMPACTED SOILS ON SITE, DEWATERING AND DISCHARGE OF ACCUMULATED STORM WATER OR GROUND WATER TO CITY SEWERS, FLAMMABLE WASTE TRAPS, UNDERGROUND OR ABOVEGROUND TANK INSTALLATION OR REMOVAL, WELL CONSTRUCTION OR SEALING, OR ON-SITE ROCK CRUSHING.
 - NO CONSTRUCTION, DEMOLITION OR COMMERCIAL POWER MAINTENANCE EQUIPMENT SHALL BE OPERATED WITHIN THE CITY BETWEEN THE HOURS OF 6:00 PM AND 7:00 AM ON WEEKDAYS OR DURING ANY HOURS ON SATURDAYS, SUNDAYS AND STATE AND FEDERAL HOLIDAYS, EXCEPT UNDER PERMIT.
 - IF CONTAMINATED SOIL IS ENCOUNTERED, IT MUST BE REPORTED TO THE MINNESOTA DUTY OFFICER AT (651) 649-5451. PRE-APPROVAL FOR REMOVAL OR REUSE MUST OCCUR FROM THE MPCA AND THE CITY OF MINNEAPOLIS.
- FORESTRY DEPARTMENT
 - TO PROTECT ROOT ZONES, NO CONSTRUCTION EQUIPMENT OR MATERIALS SHALL BE PLACED, PARKED, OR STORED ON ANY UNPAVED AREA WITHIN THE DRIP LINE OF ANY CITY OWNED TREE. NO CHEMICALS OR PETROLEUM PRODUCTS SHALL BE DEPOSITED ON ANY UNPAVED AREA IN THE CITY RIGHT-OF-WAY.
 - ANY TREE ROOTS ENCOUNTERED ARE TO BE CLEANLY CUT USING HAND TOOLS.
 - NO OPEN EXCAVATION OR BORE PITS ALLOWED WITHIN 8 FEET OF CITY STREET CURBS.
 - CARE SHALL BE TAKEN NOT TO DAMAGE TREE TRUNKS OR BRANCHES. CONTRACTOR MUST CONTACT FORESTRY DEPARTMENT INSPECTION AT (612) 499-9233 AT LEAST 3 DAYS PRIOR TO STARTING WORK. TO DISCUSS PROBLEMS OF OVERHANGING BRANCHES THAT MAY BE DAMAGED.

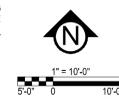
SITE LAYOUT NOTES:

- CONTRACTOR SHALL VERIFY LOCATIONS AND LAYOUT OF ALL SITE ELEMENTS PRIOR TO BEGINNING CONSTRUCTION, INCLUDING BUT NOT LIMITED TO, LOCATIONS OF EXISTING AND PROPOSED PROPERTY LINES, EASEMENTS, SETBACKS, UTILITIES, BUILDINGS AND PAVEMENTS. CONTRACTOR IS RESPONSIBLE FOR FINAL LOCATIONS OF ALL ELEMENTS FOR THE SITE. ANY REVISIONS REQUIRED AFTER COMMENCEMENT OF CONSTRUCTION, DUE TO LOCAL ADJUSTMENTS SHALL BE CORRECTED AT NO ADDITIONAL COST TO OWNER. ADJUSTMENTS TO THE LAYOUT SHALL BE APPROVED BY THE ENGINEER/LANDSCAPE ARCHITECT PRIOR TO INSTALLATION OF MATERIALS. STAKE LAYOUT FOR APPROVAL.
- THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS PRIOR TO CONSTRUCTION, INCLUDING A RIGHT-OF-WAY AND STREET OPENING PERMIT.
- THE CONTRACTOR SHALL VERIFY RECOMMENDATIONS NOTED IN THE GEO TECHNICAL REPORT PRIOR TO INSTALLATION OF SITE IMPROVEMENT MATERIALS.
- CONTRACTOR SHALL FIELD VERIFY COORDINATES AND LOCATION DIMENSIONS OF THE BUILDING AND STAKE FOR REVIEW AND APPROVAL BY THE OWNERS REPRESENTATIVE PRIOR TO INSTALLATION OF FOOTING MATERIALS.
- LOCATIONS OF STRUCTURES, ROADWAY PAVEMENTS, CURBS AND GUTTERS, BOLLARDS, AND WALKS ARE APPROXIMATE AND SHALL BE STAKED IN THE FIELD, PRIOR TO INSTALLATION. FOR REVIEW AND APPROVAL BY THE ENGINEER/LANDSCAPE ARCHITECT.
- CURB DIMENSIONS SHOWN ARE TO FACE OF CURB. BUILDING DIMENSIONS ARE TO FACE OF CONCRETE FOUNDATION. LOCATION OF BUILDING IS TO BUILDING FOUNDATION AND SHALL BE AS SHOWN ON THE DRAWINGS.
- THE CONTRACTOR SHALL SUBMIT SHOP DRAWINGS OR SAMPLES AS SPECIFIED FOR REVIEW AND APPROVAL BY THE ENGINEER/LANDSCAPE ARCHITECT PRIOR TO FABRICATION FOR ALL PREFABRICATED SITE IMPROVEMENT MATERIALS SUCH AS, BUT NOT LIMITED TO THE FOLLOWING: FURNISHINGS, PAVEMENTS, WALLS, BOLLARDS, BENCHES, FLAGPOLES, LANDING PADS FOR CURB RAMPS, AND LIGHT AND POLES. THE OWNER RESERVES THE RIGHT TO REJECT INSTALLED MATERIALS NOT PREVIOUSLY APPROVED.
- PEDESTRIAN CURB RAMPS SHALL BE CONSTRUCTED WITH TRUNCATED DOME LANDING AREAS IN ACCORDANCE WITH A.D.A. REQUIREMENTS-SEE DETAIL.
- CROSSWALK STRIPING SHALL BE 24" WIDE WHITE PAINTED LINE, SPACED 48" ON CENTER PERPENDICULAR TO THE FLOW OF TRAFFIC. WIDTH OF CROSSWALK SHALL BE 5' WIDE. ALL OTHER PAVEMENT MARKINGS SHALL BE WHITE IN COLOR UNLESS OTHERWISE NOTED OR REQUIRED BY ADA OR LOCAL GOVERNING BODIES.
- CURB AND GUTTER TYPE SHALL BE B612 UNLESS OTHERWISE NOTED ON THE DRAWINGS-TAPER BETWEEN CURB TYPES-SEE DETAIL.
- ALL CURB RADI ARE MINIMUM 3' UNLESS OTHERWISE NOTED.
- CONTRACTOR SHALL REFER TO FINAL PLAN FOR LOT BOUNDARIES, NUMBERS, AREAS AND DIMENSIONS PRIOR TO SITE IMPROVEMENTS.
- FIELD VERIFY ALL EXISTING SITE CONDITIONS, DIMENSIONS.
- PARKING IS TO BE SET PARALLEL OR PERPENDICULAR TO EXISTING BUILDING UNLESS NOTED OTHERWISE.
- ALL PARKING LOT PAINT STRIPING TO BE WHITE, 4" WIDE TYP.
- BITUMINOUS PAVING TO BE "LIGHT DUTY" UNLESS OTHERWISE NOTED. SEE DETAIL SHEETS FOR PAVEMENT SECTIONS.
- ALL TREES THAT ARE TO REMAIN ARE TO BE PROTECTED FROM DAMAGE WITH A CONSTRUCTION FENCE AT THE DRIP LINE. SEE LANDSCAPE DOCUMENTS.

SITE AREA CALCULATIONS:

	EXISTING	PROPOSED
BUILDING COVERAGE	0 SF 0.0%	6,311 SF 40.6%
ALL PAVEMENTS	0 SF 0.0%	5,631 SF 36.3%
ALL NON-PAVEMENTS	15,532 SF 100.0%	3,590 SF 23.1%
TOTAL SITE AREA	15,532 SF 100.0%	15,532 SF 100.0%
IMPERVIOUS SURFACE		
EXISTING CONDITION	0 SF 0.0%	
PROPOSED CONDITION	11,942 SF 76.9%	
DIFFERENCE	11,942 SF 76.9%	

GOPHER STATE ONE CALL
WWW.GOPHERSTATEONECALL.ORG
(800) 252-1166 TOLL FREE
(651) 454-0002 LOCAL





3535 GRAND
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MINNEAPOLIS, MN



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4691 W. 35TH ST. SUITE 200
ST. LOUIS PARK, MN 55416
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Matt Pavik 763-213-3944 Pat Sarver 952-250-2003

DATE: 04.09.2015

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DATE: ARPIL 9, 2015

PDR PLAN REVIEW

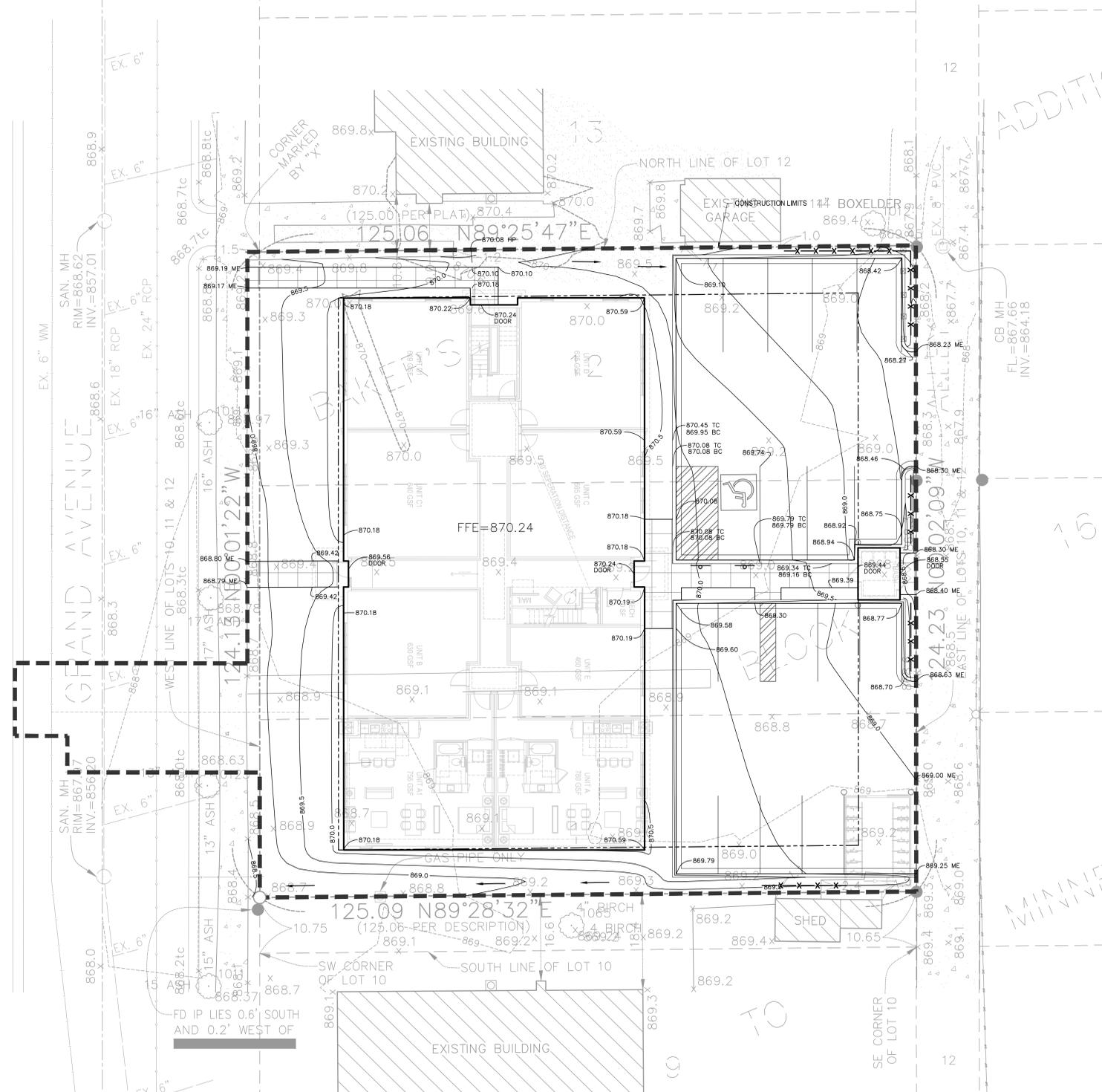
MARK	DATE	DESCRIPTION

PROJECT NO: 15074
DRAWN BY: DJK
CHKD BY: MRP

SHEET TITLE

GRADING PLAN

C3.0



NON STORM WATER DISCHARGES:

THERE ARE NO KNOWN NON-STORM WATER DISCHARGES ON THE EXISTING SITE AND NONE ARE PROPOSED AS PART OF THIS DEVELOPMENT.

GROUNDWATER STATEMENT:

THIS PROJECT DOES NOT PROPOSE ANY PERMANENT GROUNDWATER DISCHARGE TO THE STORM WATER SYSTEM.

OWNER, DEVELOPER, CONTRACTOR SIGNED STATEMENT

ALL CLEARING, GRADING, CONSTRUCTION OR DEVELOPMENT WILL BE DONE PURSUANT TO THE PLAN, SIGNED BY PARTIES BELOW:

OWNER: _____
DEVELOPER: _____
CONTRACTOR: _____

GRADING PLAN LEGEND:

- 891 - - - - - EX. 1' CONTOUR ELEVATION INTERVAL
- 819 - - - - - 1.0' CONTOUR ELEVATION INTERVAL
- 869.0 - - - - - SPOT GRADE ELEVATION (FLOW LINE UNLESS OTHERWISE NOTED)
- 891.0 G - - - - - SPOT GRADE ELEVATION GUTTER
- 891.0 BC - - - - - SPOT GRADE ELEVATION BACK OF CURB (TOP OF CURB)
- 891.0 BS/TS - - - - - SPOT GRADE ELEVATION BOTTOM OF STAIRS/TOP OF STAIRS
- T.O. - - - - - TIP OUT (T.O.) CURB AND GUTTER WHERE APPLICABLE - TAPER GUTTERS TO DRAIN AS SHOWN
- - - - - - EXISTING AND PROPOSED DRAINAGE ARROWS

GENERAL GRADING NOTES:

1. SEE SITE PLAN FOR HORIZONTAL LAYOUT & GENERAL GRADING NOTES.
2. THE CONTRACTOR SHALL COMPLETE THE SITE GRADING CONSTRUCTION (INCLUDING BUT NOT LIMITED TO SITE PREPARATION, SOIL CORRECTION, EXCAVATION, EMBANKMENT, ETC.) IN ACCORDANCE WITH THE REQUIREMENTS OF THE OWNER'S SOILS ENGINEER. ALL SOIL TESTING SHALL BE COMPLETED BY THE OWNER'S SOILS ENGINEER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING ALL REQUIRED SOIL TESTS AND INSPECTIONS WITH THE SOILS ENGINEER.
3. GRADING AND EXCAVATION ACTIVITIES SHALL BE PERFORMED IN ACCORDANCE WITH THE NATIONAL POLLUTION DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT REQUIREMENTS & PERMIT REQUIREMENTS OF THE CITY.
4. PROPOSED SPOT GRADES ARE FLOW-LINE FINISHED GRADE ELEVATIONS, UNLESS OTHERWISE NOTED.
5. GRADES OF WALKS SHALL BE INSTALLED WITH 5% MAX. LONGITUDINAL SLOPE AND 1% MIN. AND 2% MAX. CROSS SLOPE, UNLESS OTHERWISE NOTED.
6. PROPOSED SLOPES SHALL NOT EXCEED 3:1 UNLESS INDICATED OTHERWISE ON THE DRAWINGS. MAXIMUM SLOPES IN MAINTAINED AREAS IS 4:1
7. PROPOSED RETAINING WALLS, FREESTANDING WALLS, OR COMBINATION OF WALL TYPES GREATER THAN 4' IN HEIGHT SHALL BE DESIGNED AND ENGINEERED BY A REGISTERED RETAINING WALL ENGINEER. DESIGN DRAWINGS SHALL BE SUBMITTED FOR REVIEW AND APPROVAL PRIOR TO CONSTRUCTION.
8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTENANCE OF GRADE STAKES THROUGHOUT THE DURATION OF CONSTRUCTION TO ESTABLISH PROPER GRADES. THE CONTRACTOR SHALL ALSO BE RESPONSIBLE FOR A FINAL FIELD CHECK OF FINISHED GRADES ACCEPTABLE TO THE ENGINEER/LANDSCAPE ARCHITECT PRIOR TO TOPSOIL AND SODDING ACTIVITIES.
9. IF EXCESS OR SHORTAGE OF SOIL MATERIAL EXISTS, THE CONTRACTOR SHALL TRANSPORT ALL EXCESS SOIL MATERIAL OFF THE SITE TO AN AREA SELECTED BY THE CONTRACTOR, OR IMPORT SUITABLE MATERIAL TO THE SITE.
10. EXCAVATE TOPSOIL FROM AREAS TO BE FURTHER EXCAVATED OR REGRADED AND STOCKPILE IN AREAS DESIGNATED ON THE SITE. THE CONTRACTOR SHALL SALVAGE ENOUGH TOPSOIL FOR RESPREADING ON THE SITE AS SPECIFIED. EXCESS TOPSOIL SHALL BE PLACED IN EMBANKMENT AREAS, OUTSIDE OF BUILDING PADS, ROADWAYS AND PARKING AREAS. THE CONTRACTOR SHALL SUBCUT CUT AREAS, WHERE TURF IS TO BE ESTABLISHED, TO A DEPTH OF 6 INCHES. RESPREAD TOPSOIL IN AREAS WHERE TURF IS TO BE ESTABLISHED TO A MINIMUM DEPTH OF 6 INCHES.
11. FINISHED GRADING SHALL BE COMPLETED. THE CONTRACTOR SHALL UNIFORMLY GRADE AREAS WITHIN LIMITS OF GRADING, INCLUDING ADJACENT TRANSITION AREAS. PROVIDE A

CITY OF MINNEAPOLIS GRADING NOTES:

1. RESERVED FOR CITY SPECIFIC GRADING NOTES.

EROSION CONTROL NOTES:

SEE SWPPP ON SHEETS SW1.0-SW1.3

GROUNDWATER INFORMATION:

PER GEOTECHNICAL REPORT BY AMERICAN ENGINEERING TESTING, INC., DATED 05-18-2012 GROUNDWATER WAS NOT OBSERVED IN ANY OF THE BORINGS. BORING DEPTHS RANGED FROM 26 FEET - 31 FEET.

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MINNEAPOLIS, MN



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CivilSiteGroup.com
Matt Pavik 763-213-3944 Pat Sarver 952-250-2003

DATE: 04.09.2015

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DATE: APRIL 9, 2015

PDR PLAN REVIEW

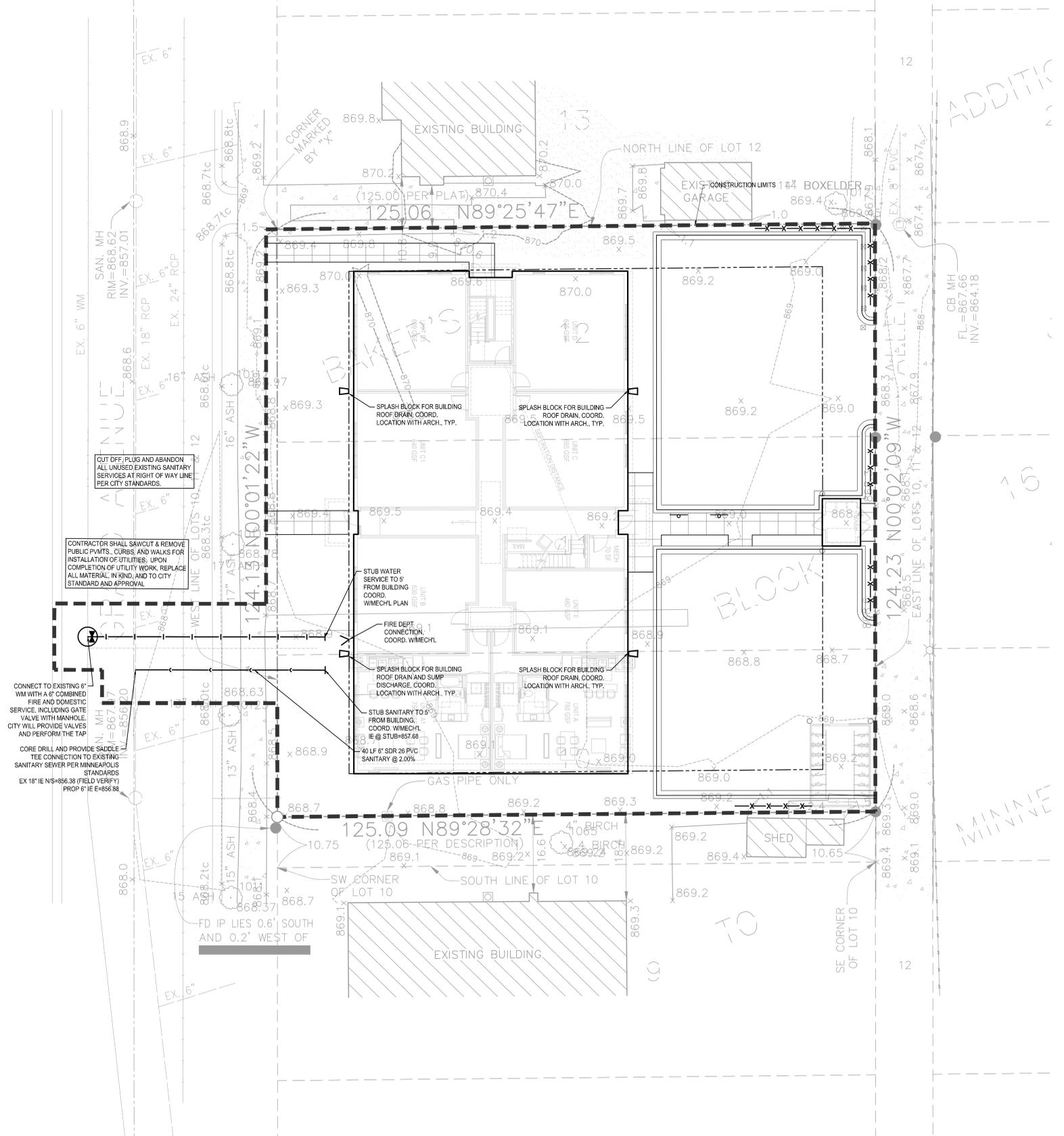
MARK	DATE	DESCRIPTION

PROJECT NO: 15074
DRAWN BY: DJK
CHKD BY: MRP

SHEET TITLE

UTILITY PLAN

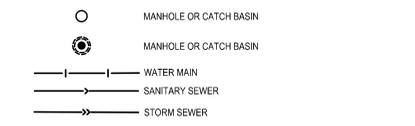
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CITY OF MINNEAPOLIS UTILITY NOTES:

- RESERVED FOR CITY SPECIFIC UTILITY NOTES.

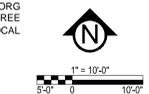
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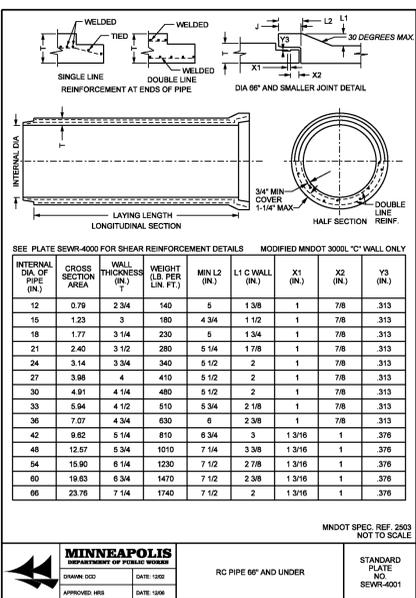
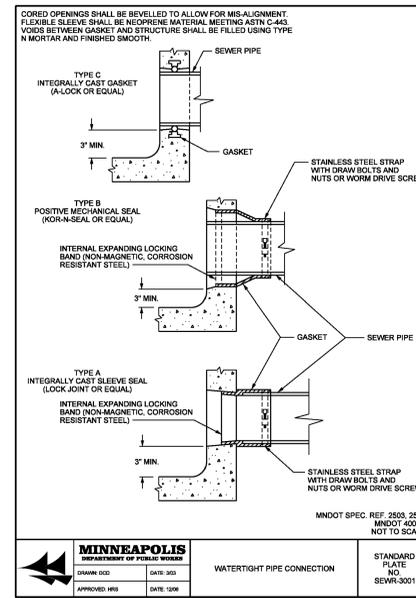
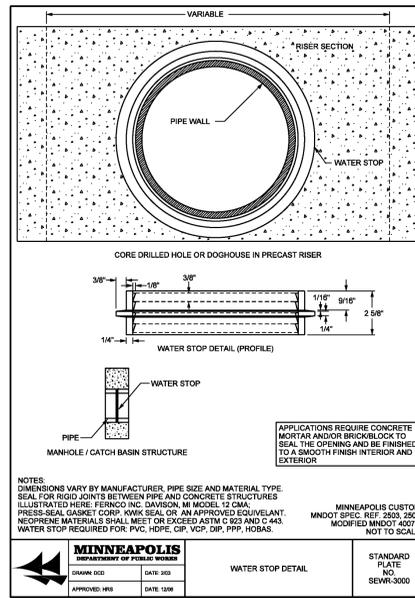
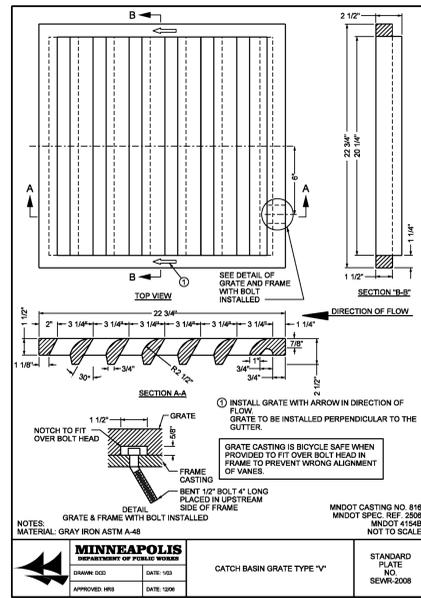
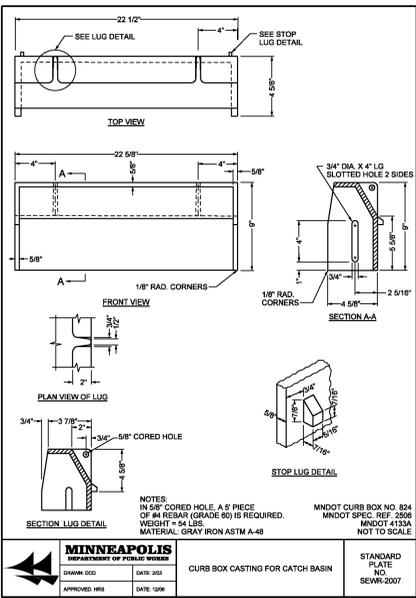
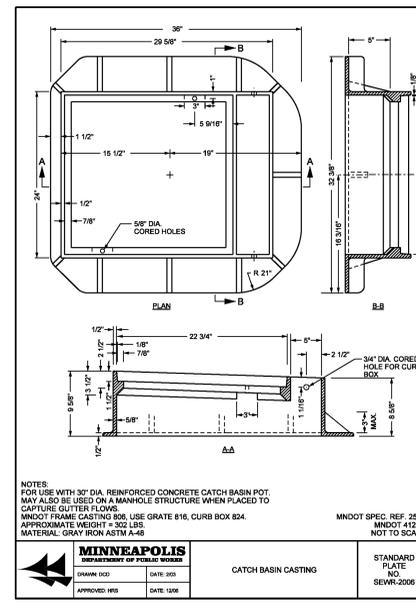
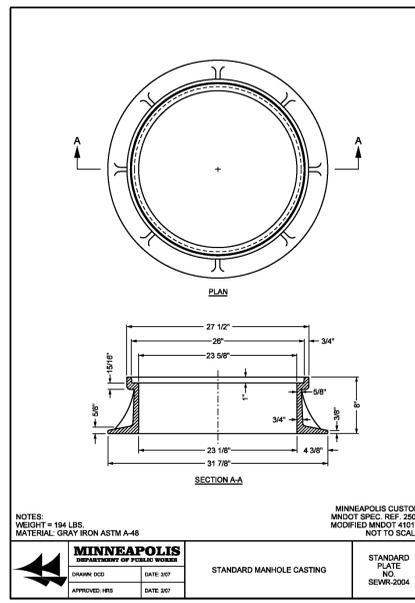
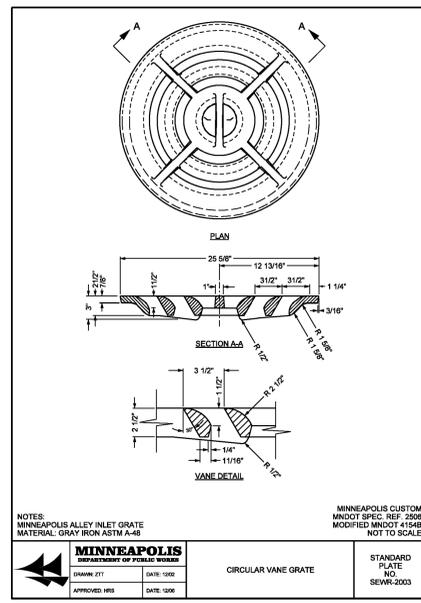
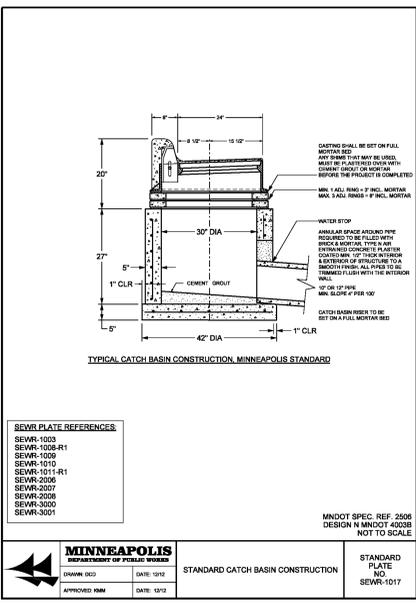


GENERAL UTILITY NOTES:

- SEE SITE PLAN FOR HORIZONTAL DIMENSIONS AND LAYOUT.
- CONTRACTOR SHALL FIELD VERIFY LOCATION AND ELEVATION OF EXISTING UTILITIES AND TOPOGRAPHIC FEATURES PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE ENGINEER OF DISCREPANCIES OR VARIATIONS FROM THE PLANS.
- ALL EXISTING UTILITY LOCATIONS SHOWN ARE APPROXIMATE. CONTACT "GOPHER STATE ONE CALL" (651-454-0002 OR 800-252-1188) FOR UTILITY LOCATIONS. 48 HOURS PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL REPAIR OR REPLACE ANY UTILITIES THAT ARE DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.
- UTILITY INSTALLATION SHALL CONFORM TO THE CURRENT EDITION OF "STANDARD SPECIFICATIONS FOR WATER MAIN AND SERVICE LINE INSTALLATION" AND "SANITARY SEWER AND STORM SEWER INSTALLATION" AS PREPARED BY THE CITY ENGINEERS ASSOCIATION OF MINNESOTA (CEAM), AND SHALL CONFORM WITH THE REQUIREMENTS OF THE CITY AND THE PROJECT SPECIFICATIONS.
- CASTINGS SHALL BE SALVAGED FROM STRUCTURE REMOVALS AND RE-USED OR PLACED AT THE DIRECTION OF THE OWNER.
- ALL WATER PIPE SHALL BE CLASS 52 DUCTILE IRON PIPE (DIP) UNLESS OTHERWISE NOTED.
- ALL SANITARY SEWER SHALL BE SDR 26 POLYVINYL CHLORIDE (PVC) UNLESS OTHERWISE NOTED.
- ALL STORM SEWER PIPE SHALL BE HDPE UNLESS OTHERWISE NOTED.
- PIPE LENGTHS SHOWN ARE FROM CENTER TO CENTER OF STRUCTURE OR TO END OF FLARED END SECTION.
- UTILITIES ON THE PLAN ARE SHOWN TO WITHIN 5' OF THE BUILDING FOOTPRINT. THE CONTRACTOR IS ULTIMATELY RESPONSIBLE FOR THE FINAL CONNECTION TO BUILDING LINES. COORDINATE WITH ARCHITECTURAL AND MECHANICAL PLANS.
- CATCH BASINS AND MANHOLES IN PAVED AREAS SHALL BE SUMPED 0.04 FEET. ALL CATCH BASINS IN GUTTERS SHALL BE SUMPED 0.15 FEET PER DETAILS. RIM ELEVATIONS SHOWN ON THIS PLAN DO NOT REFLECT SUMPED ELEVATIONS.
- ALL FIRE HYDRANTS SHALL BE LOCATED 5 FEET BEHIND BACK OF CURB UNLESS OTHERWISE NOTED.
- HYDRANT TYPE, VALVE, AND CONNECTION SHALL BE IN ACCORDANCE WITH CITY REQUIREMENTS. HYDRANT EXTENSIONS ARE INCIDENTAL.
- A MINIMUM OF 6 FEET OF COVER IS REQUIRED OVER ALL WATERMAIN UNLESS OTHERWISE NOTED. EXTRA DEPTH MAY BE REQUIRED TO MAINTAIN A MINIMUM OF 18" VERTICAL SEPARATION TO SANITARY OR STORM SEWER LINES. EXTRA DEPTH WATERMAIN IS INCIDENTAL.
- A MINIMUM OF 18 INCHES OF VERTICAL SEPARATION AND 10 FEET OF HORIZONTAL SEPARATION IS REQUIRED FOR ALL UTILITIES UNLESS OTHERWISE NOTED.
- ALL CONNECTIONS TO EXISTING UTILITIES SHALL BE IN ACCORDANCE WITH CITY STANDARDS AND COORDINATED WITH THE CITY PRIOR TO CONSTRUCTION.
- CONNECTIONS TO EXISTING STRUCTURES SHALL BE CORE-DRILLED.
- COORDINATE LOCATIONS AND SIZES OF SERVICE CONNECTIONS WITH THE MECHANICAL DRAWINGS.
- COORDINATE INSTALLATION AND SCHEDULING OF THE INSTALLATION OF UTILITIES WITH ADJACENT CONTRACTORS AND CITY STAFF.
- ALL STREET REPAIRS AND PATCHING SHALL BE PERFORMED PER THE REQUIREMENTS OF THE CITY. ALL PAVEMENT CONNECTIONS SHALL BE SAWCUT. ALL TRAFFIC CONTROLS SHALL BE PROVIDED BY THE CONTRACTOR AND SHALL BE ESTABLISHED PER THE REQUIREMENTS OF THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AND THE CITY. THIS SHALL INCLUDE BUT NOT BE LIMITED TO SIGNAGE, BARRICADES, FLASHERS, AND FLAGGERS AS NEEDED. ALL PUBLIC STREETS SHALL BE OPEN TO TRAFFIC AT ALL TIMES. NO ROAD CLOSURES SHALL BE PERMITTED WITHOUT APPROVAL BY THE CITY.
- ALL STRUCTURES, PUBLIC AND PRIVATE, SHALL BE ADJUSTED TO PROPOSED GRADES WHERE REQUIRED. THE REQUIREMENTS OF ALL OWNERS MUST BE COMPLIED WITH. STRUCTURES BEING RESET TO PAVED AREAS MUST MEET OWNERS REQUIREMENTS FOR TRAFFIC LOADING.
- CONTRACTOR SHALL COORDINATE ALL WORK WITH PRIVATE UTILITY COMPANIES.
- CONTRACTOR SHALL COORDINATE CONNECTION OF IRRIGATION SERVICE TO UTILITIES. COORDINATE THE INSTALLATION OF IRRIGATION SLEEVES NECESSARY AS TO NOT IMPACT INSTALLATION OF UTILITIES.
- CONTRACTOR SHALL MAINTAIN AS-BUILT PLANS THROUGHOUT CONSTRUCTION AND SUBMIT THESE PLANS TO ENGINEER UPON COMPLETION OF WORK.
- ALL JOINTS AND CONNECTIONS IN STORM SEWER SYSTEM SHALL BE GASTIGHT OR WATERTIGHT. APPROVED RESILIENT RUBBER JOINTS MUST BE USED TO MAKE WATERTIGHT CONNECTIONS TO MANHOLES, CATCHBASINS, OR OTHER STRUCTURES.

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CIRCULAR CONCRETE PIPE
 HEIGHT OF BACKFILL FEET FOR CLASS B BEDDING MEASURED AT TOP OF PIPE IN FEET, 120 PSF SOIL DENSITY

PIPE CLASS	CLASS II		CLASS III		CLASS IV		CLASS V	
	N	W	N	W	N	W	N	W
12	6	6	9	9	13	13	21	19
15	6	6	9	9	13	13	22	19
18	7	7	9	9	14	14	24	20
21	7	7	9	9	14	14	24	20
24	7	7	9	9	14	14	24	20
27	7	7	9	9	16	16	24	20
30	7	7	10	10	16	16	24	20
33	7	7	10	10	16	16	24	20
36	7	7	10	10	17	17	24	21
42	7	7	10	10	17	17	24	21
48	8	8	11	11	18	18	24	21
54	8	8	11	11	18	18	24	21
60	8	8	11	11	18	18	24	21
66	8	8	11	11	18	18	24	21
72	8	8	12	12	19	19	24	21
78	9	9	12	12	20	20	24	21
84	9	9	12	12	20	20	24	21
90	9	9	12	12	20	20	24	22
96	9	9	13	13	20	20	24	22
102	9	9	13	13	20	20	24	22
108	9	9	13	13	20	20	24	22

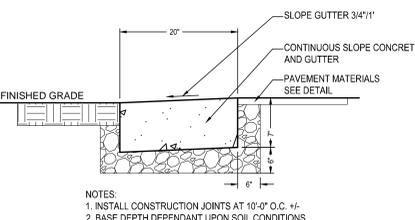
N = NARROW TRENCH MINIMUM WIDTH
 W = WIDE TRENCH TRANSITION WIDTH
 PIPE DIA. (IN.) = INSIDE DIAMETER IN CHART

CLASS C (ORDINARY BEDDING)
 CLASS C BEDDING CONSISTS OF CAREFULLY SHAPING THE FOUNDATION SOIL TO FIT THE LOWER EXTERIOR OF THE PIPE TO A DEPTH OF AT LEAST 15% OF THE OUTSIDE DIAMETER FOR CIRCULAR PIPE, AND AT LEAST EQUAL TO 1/2 OF THE HEIGHT OF ARCH PIPE. ADDITIONAL CONSIDERATION FOR BELL END PIPE IS USED. THE BEDDING MUST BE EXCAVATED TO ACCEPT THE BELL END SO THAT THE PIPE IS SUPPORTED ALONG ITS FULL LENGTH AND NOT JUST AT THE BELL.

MINNEAPOLIS DESIGN REFERENCE
 MNDOT DRAINAGE MANUAL
 SECTION 2.6, AUGUST 30, 2000
 FIG. 2-3.1 CLASS B OF BEDDING FOR TRENCH CONDITIONS NOT TO SCALE

MINNEAPOLIS DEPARTMENT OF PUBLIC WORKS
 DRAWN: [] DATE: 12/02
 APPROVED: [] DATE: 12/02

STANDARD PLATE NO. SEWR-6002



3535 GRAND
 3535 GRAND AVENUE
 MINNEAPOLIS, MN



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 CivilSiteGroup.com
 Matt Pavik 763-213-3944 Pat Sarver 952-250-2003

DATE: 04.09.2015

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 Structural Engineer
 Paul Voigt
 651.686.7727
 4835 Nicols Road, Suite 204
 Eagan, Minnesota 55122

DATE: APRIL 9, 2015

PDR PLAN REVIEW

MARK	DATE	DESCRIPTION

PROJECT NO: 15074
 DRAWN BY: DJK
 CWD BY: MRP

SHEET TITLE

CIVIL DETAILS

C5.1



3535 GRAND
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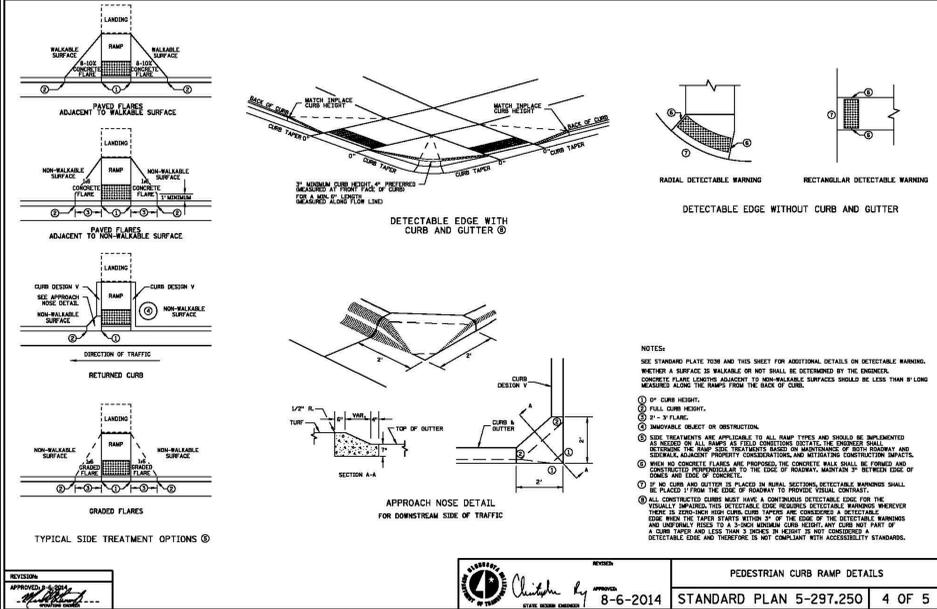
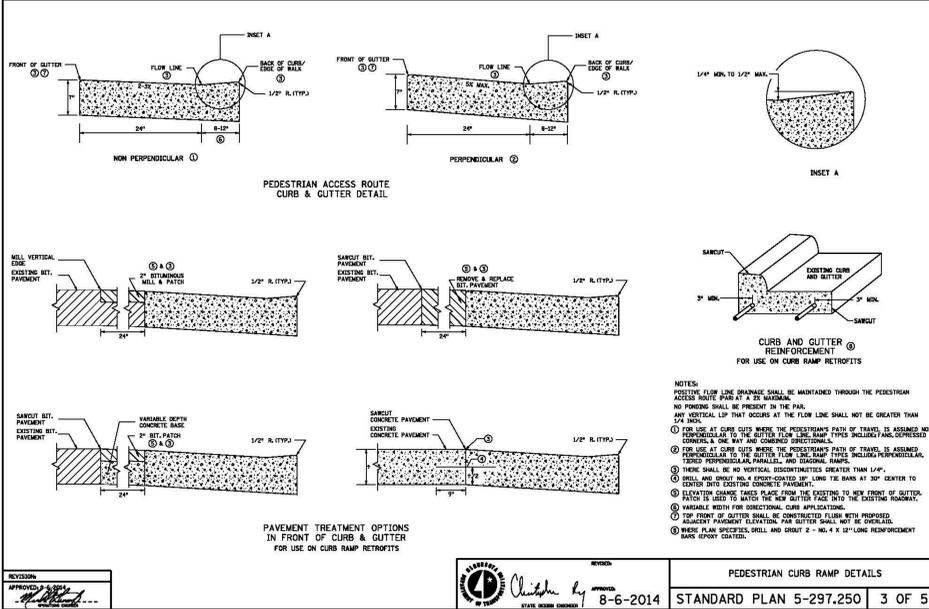
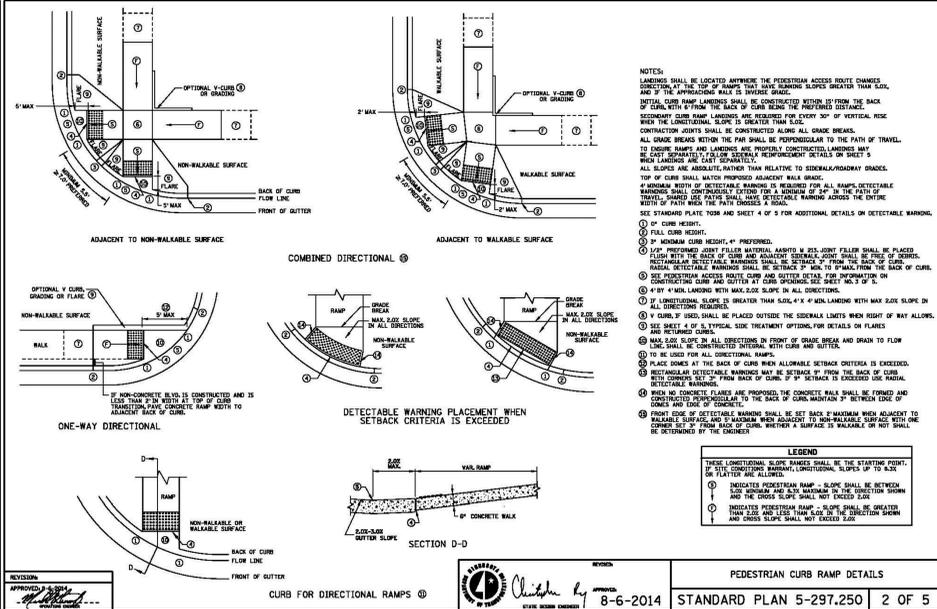
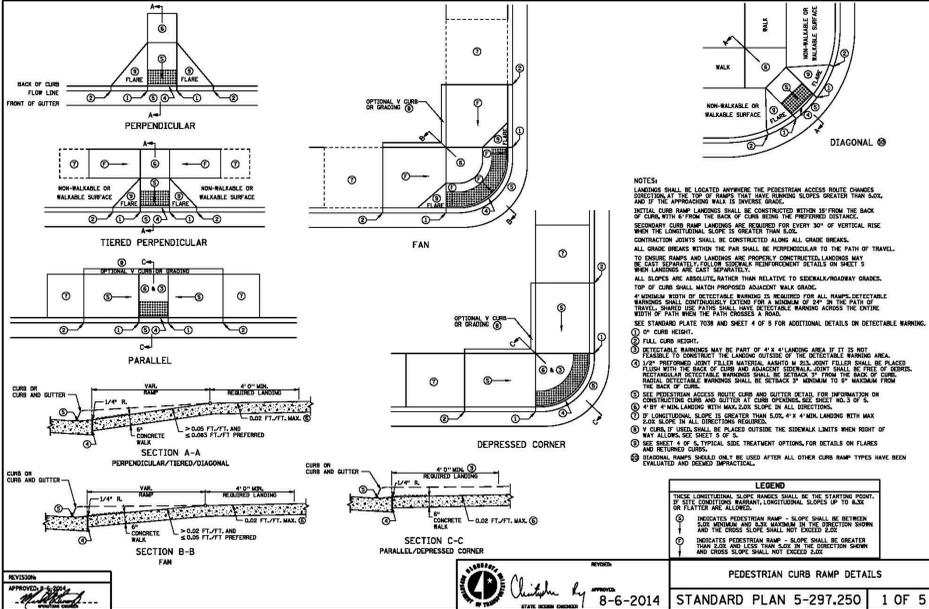
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PROJECT NO: 15074
DRAWN BY: DJK
CHKD BY: MRP

SHEET TITLE

CIVIL DETAILS

C5.2



SHEET TITLE

CIVIL DETAILS

C5.2



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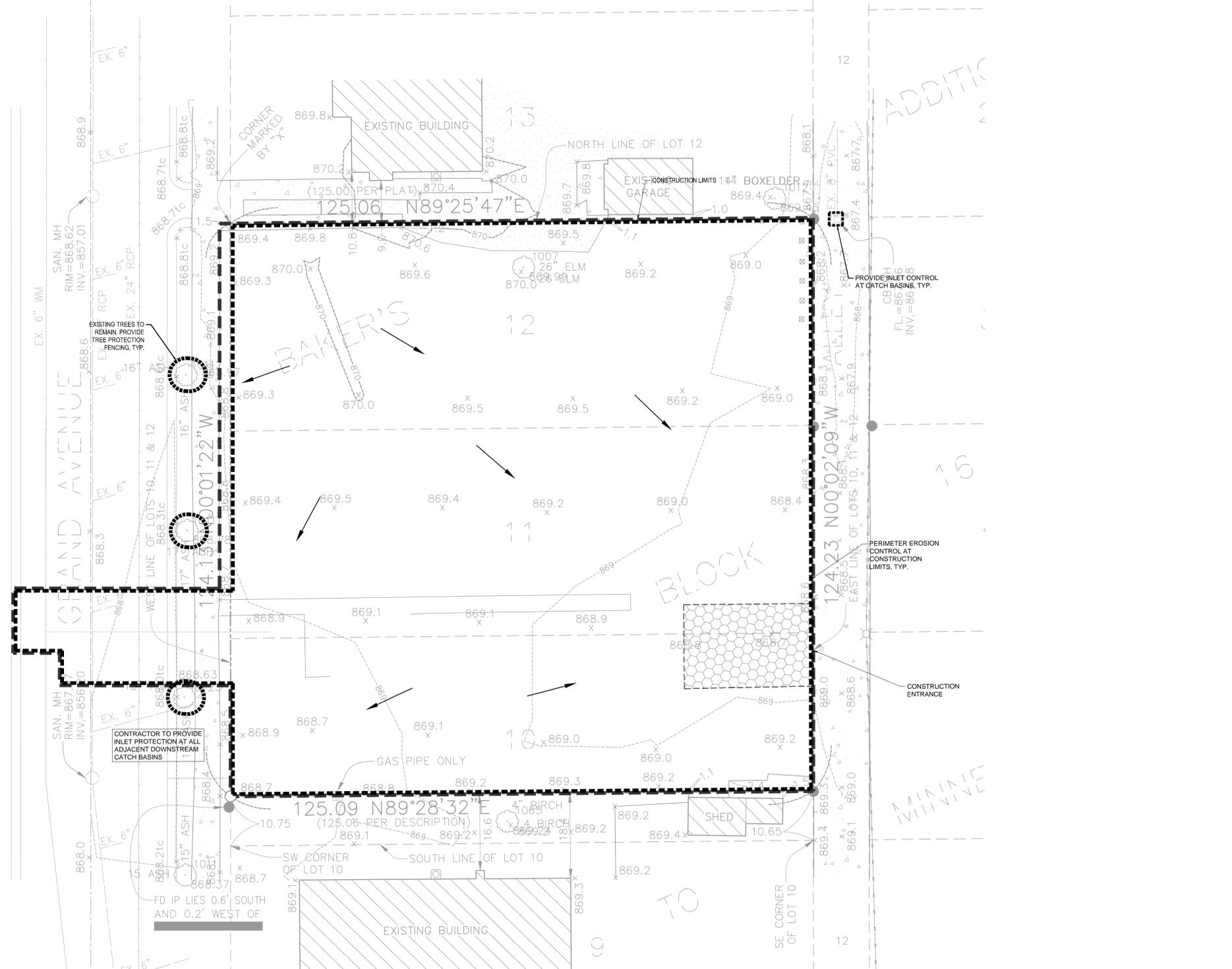
PDR PLAN REVIEW

MARK	DATE	DESCRIPTION

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CHKD BY:	MRP
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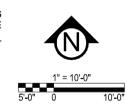
SWPPP - EXISTING CONDITIONS

SW1.0



- LEGEND:**
- 932 --- EX. 1' CONTOUR ELEVATION INTERVAL
 - 932 --- 1' CONTOUR ELEVATION INTERVAL
 - 869.0 EXISTING SPOT GRADE ELEVATION
 - 932.0 PROPOSED SPOT GRADE ELEVATION
 - DRAINAGE ARROW
 - SILT FENCE / BIOROLL - GRADING LIMIT
 - INLET PROTECTION
 - ▨ STABILIZED CONSTRUCTION ENTRANCE
 - PROPOSED MANHOLE OR CATCH BASIN
 - ⊕ PROPOSED GATE VALVE
 - ⊕ PROPOSED FIRE HYDRANT
 - PROPOSED SANITARY SEWER
 - PROPOSED STORM SEWER
 - EXISTING STORM SEWER
 - EXISTING WATER MAIN
 - EXISTING GAS MAIN
 - EXISTING UNDERGROUND ELECTRIC
 - EXISTING UNDERGROUND CABLE
 - EXISTING MANHOLE
 - EXISTING CATCH BASIN
 - ⊕ EXISTING HYDRANT
 - ⊕ EXISTING STOPBOX
 - ⊕ EXISTING GATE VALVE
 - ⊕ EXISTING ELECTRIC BOX
 - ⊕ EXISTING LIGHT
 - ⊕ EXISTING GAS METER
 - ⊕ EXISTING GAS VALVE

- SWPPP NOTES:**
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 - SEE SHEETS SW1.0 - SW1.3 FOR ALL EROSION CONTROL NOTES, DESCRIPTIONS, AND PRACTICES.
 - SEE GRADING PLAN FOR ADDITIONAL GRADING AND EROSION CONTROL NOTES.
 - CONTRACTOR IS RESPONSIBLE FOR SWPPP IMPLEMENTATION, INSPECTIONS, AND COMPLIANCE WITH NPDES PERMIT.





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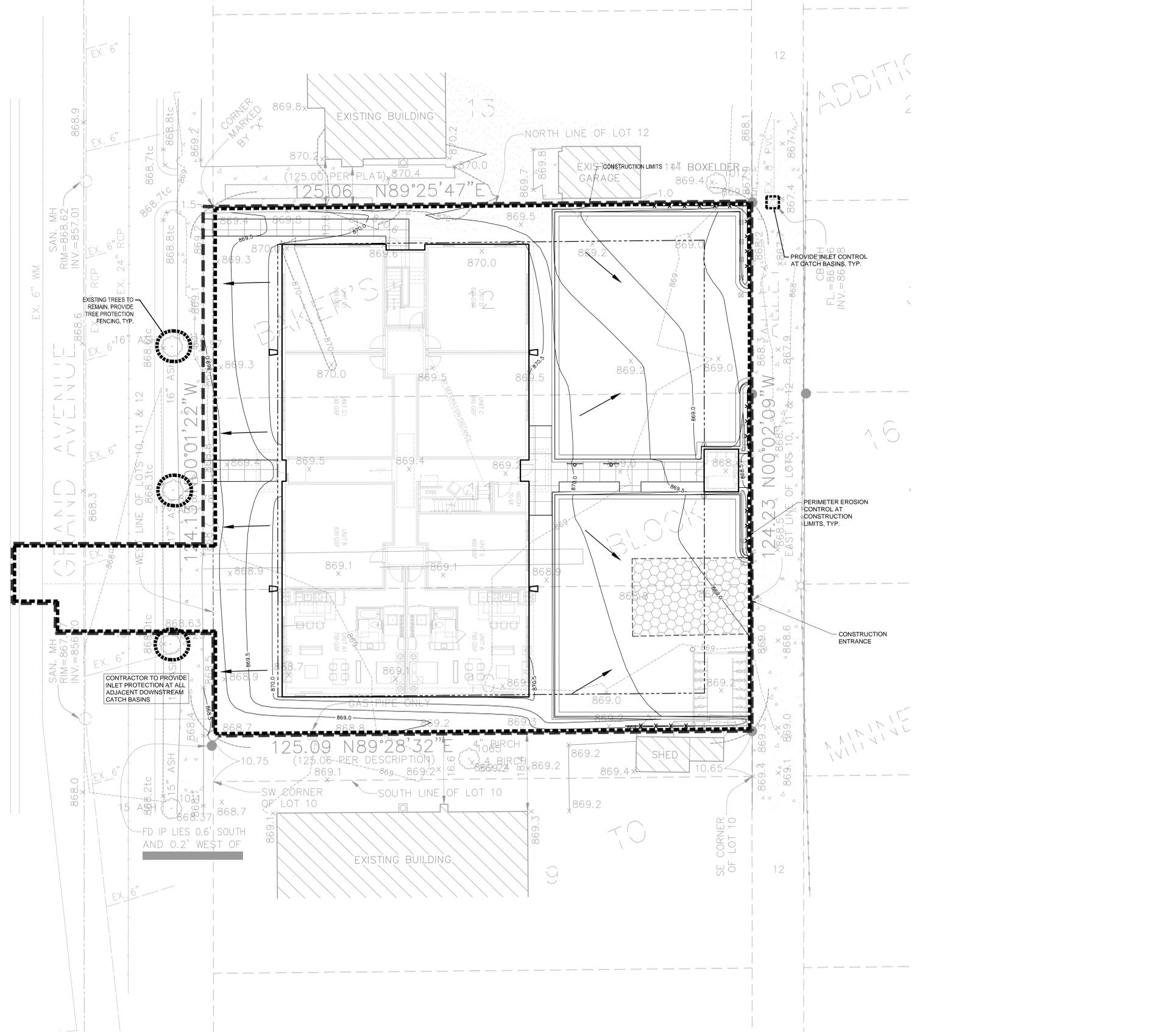
MARK	DATE	DESCRIPTION

PROJECT NO:	15074
DRAWN BY:	DJK
CHECKED BY:	MRP

SHEET TITLE

SWPPP - PROPOSED CONDITIONS

SW1.1

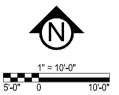


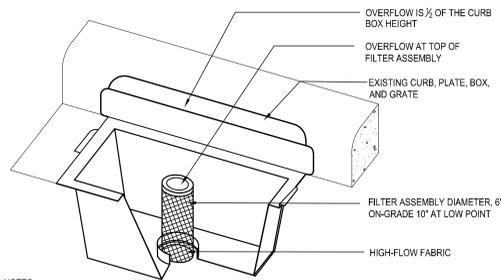
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- EXISTING GAS METER
- EXISTING GAS VALVE

- SWPPP NOTES:**
- THIS PROJECT IS LESS THAN ONE ACRE AND AN MPCA NPDES PERMIT IS NOT REQUIRED. AN EROSION CONTROL PERMIT IS REQUIRED FROM THE CITY OF MINNEAPOLIS.
 - SEE SHEETS SW1.0 - SW1.3 FOR ALL EROSION CONTROL NOTES, DESCRIPTIONS, AND PRACTICES.
 - SEE GRADING PLAN FOR ADDITIONAL GRADING AND EROSION CONTROL NOTES.
 - CONTRACTOR IS RESPONSIBLE FOR SWPPP IMPLEMENTATION, INSPECTIONS, AND COMPLIANCE WITH NPDES PERMIT.

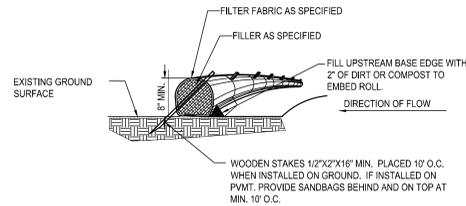
GOPHER STATE ONE CALL
WWW.GOPHERSTATEONECALL.ORG
(800) 252-1166 TOLL FREE
(651) 454-0002 LOCAL





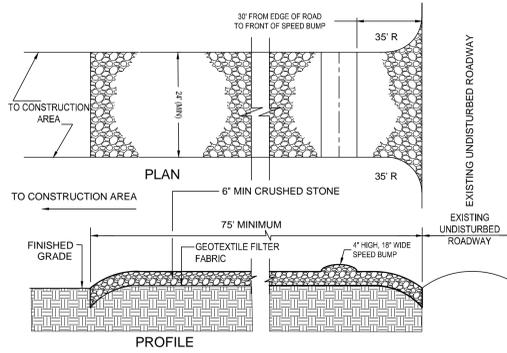
- NOTES:
1. REPLACE INLET GRATE UPON COMPLETE INSTALLATION OF INLET PROTECTION FABRIC.
 2. CONTRACTOR SHALL REMOVE ALL ACCUMULATED SEDIMENT AND DEBRIS FROM THE SURFACE OF THE SYSTEM AFTER EACH STORM EVENT AND AT THE COMPLETION OF THE CONTRACT.
 3. REFERENCE APPLE VALLEY STANDARD PLATE ERO-4C.

1 CURB INLET FILTER
N T S



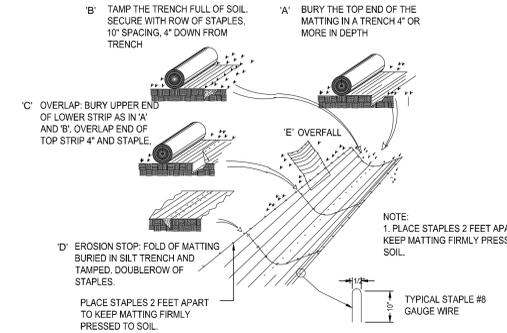
- NOTE:
1. COMPOST FILTER LOGS (BIO ROLLS) SHALL BE FILTREXX EROSION CONTROL SOXX OR APPROVED EQUAL.
 2. COMPOST FILLER TO BE MADE FROM A COMPOST BLEND 30%-40% GRADE 2 (SPEC 3890) AND 60%-70% PARTIALLY DECOMPOSED WOOD CHIPS, PER MNDOT SPEC 3897.
 3. FILTER FABRIC SHALL BE GEOTEXTILE KNITTED MATERIAL WITH MAX. OPENINGS OF 3/8".
 4. IF MULTIPLE ROLLS NEEDED, OVERLAP BY MIN. 12" AT ENDS AND STAKE.
 5. SILT SHALL BE REMOVED ONCE IT REACHES 80% OF THE HEIGHT OF THE ROLL OR AS DEEMED NECESSARY BY SITE CONTRACTOR TO MAINTAIN PROPER FUNCTION.

4 SEDIMENT BIO-ROLL / COMPOST FILTER LOG
N T S

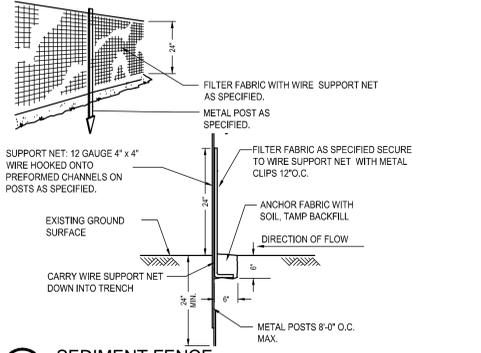


- NOTES:
1. PROVIDE APPROPRIATE TRANSITION BETWEEN STABILIZED CONSTRUCTION ENTRANCE AND UNDISTURBED ROADWAY.
 2. THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO UNDISTURBED ROADWAY. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH ADDITIONAL STONE OR ADDING STONE TO THE LENGTH OF THE ENTRANCE.
 3. REPAIR AND CLEANOUT MEASURES USED TO TRAP SEDIMENT.
 4. ALL SEDIMENT SPILLED, DROPPED, WASHED, OR TRACKED ONTO UNDISTURBED ROADWAY SHALL BE REMOVED AS DIRECTED BY THE ENGINEER.
 5. FINAL LOCATION AND INSTALLATION SHALL BE COORDINATED WITH THE CITY PRIOR TO CONSTRUCTION ACTIVITIES.
 6. CRUSHED STONE SHALL BE 1-1/2" DIA. CLOSE GRADED, AND IN ACCORDANCE TO MNDOT SECTION 2118.

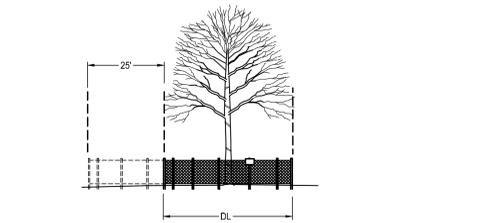
2 STABILIZED CONSTRUCTION ACCESS
N T S



3 EROSION BLANKET
N T S



5 SEDIMENT FENCE
N T S



FURNISH AND INSTALL TEMPORARY FENCE AT THE TREE'S DRIP LINE OR CONSTRUCTION LIMITS AS SHOWN ON PLAN, PRIOR TO ANY CONSTRUCTION. WHERE POSSIBLE PLACE FENCE 25' BEYOND DRIP LINE. PLACE TREE PROTECTION SIGN ON POSTS, ONE PER INDIVIDUAL TREE (FACING CONSTRUCTION ACTIVITY), OR ONE EVERY 100' LF ALONG A GROVE OR MULTI-TREE PROTECTION AREA.

6 TREE PROTECTION
N T S



3535 GRAND
3535 GRAND AVENUE
MINNEAPOLIS, MN

MOEDING
LANDERGROUP
urban development

CivilSite
GROUP

4931 W. 35TH ST. SUITE 200
ST. LOUIS PARK, MN 55416
CivilSiteGroup.com

Matt Pavik 763-213-3944 Pat Sarver 952-250-2003

DATE: 04.09.2015

I HEREBY CERTIFY THAT THIS PLAN SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

Matthew R. Pavik
MATTHEW R. PAVIK
REGISTRATION NO: 44263

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Voigt & Associates, Inc.
Structural Engineer
Paul Voigt
651.686.7727
4835 Nicols Road, Suite 204
Eagan, Minnesota 55122

DATE: ARPIL 9, 2015

PDR PLAN REVIEW

MARK	DATE	DESCRIPTION

PROJECT NO: 15074
DRAWN BY: DJK
CHECK BY: MRP

SHEET TITLE

SWPPP - DETAILS

SW1.2

GENERAL SWPPP REQUIREMENTS AND NOTES:

THE CONTRACTOR AND ALL SUBCONTRACTORS INVOLVED WITH A CONSTRUCTION ACTIVITY THAT DISTURBS SITE SOIL OR WHO IMPLEMENT A POLLUTANT CONTROL MEASURE IDENTIFIED IN THE STORM WATER POLLUTION PREVENTION PLAN (SWPPP) MUST COMPLY WITH THE REQUIREMENTS OF THE NATIONAL POLLUTION DISCHARGE ELIMINATION SYSTEM (NPDES) GENERAL PERMIT (DATED AUGUST 1, 2013) (MNDOT0001) (PAGES 1-35) AND ANY LOCAL GOVERNING AGENCY HAVING JURISDICTION CONCERNING EROSION AND SEDIMENTATION CONTROL.

PART III STORMWATER DISCHARGE DESIGN REQUIREMENTS

SWPPP (PART III.A)

THE NATURE OF THIS PROJECT WILL BE CONSISTENT WITH WHAT IS REPRESENTED IN THIS SET OF CONSTRUCTION PLANS AND SPECIFICATIONS. SEE THE SWPPP PLAN SHEETS AND SWPPP NARRATIVE (ATTACHMENT A: CONSTRUCTION SWPPP TEMPLATE) FOR ADDITIONAL SITE SPECIFIC SWPPP INFORMATION. THE PLANS SHOW LOCATIONS AND TYPES OF ALL TEMPORARY AND PERMANENT EROSION PREVENTION AND SEDIMENT CONTROL BMP'S. STANDARD DETAILS ARE ATTACHED TO THIS SWPPP DOCUMENT.

THE INTENDED SEQUENCING OF MAJOR CONSTRUCTION ACTIVITIES IS AS FOLLOWS:

- 1. INSTALL STABILIZED ROCK CONSTRUCTION ENTRANCE
- 2. INSTALLATION OF SILT FENCE AROUND SITE
- 3. INSTALL ORANGE CONSTRUCTION FENCING AROUND INFILTRATION AREAS
- 4. CLEAR AND GRUB FOR TEMPORARY SEDIMENT BASIN / POND INSTALL
- 5. CONSTRUCT TEMPORARY SEDIMENT BASIN / POND (PART III.B)
- 6. CLEAR AND GRUB REMAINDER OF SITE
- 7. STRIP AND STOCKPILE TOPSOIL
- 8. ROUGH GRADING OF SITE
- 9. STABILIZE DENuded AREAS AND STOCKPILES
- 10. INSTALL SANITARY SEWER, WATER MAIN STORM SEWER AND SERVICES
- 11. INSTALL SILT FENCE / INLET PROTECTION AROUND CFS
- 12. INSTALL STREET SECTION
- 13. INSTALL CURB AND GUTTER
- 14. BITUMINOUS ON STREETS
- 15. FINAL GRADE SOILWARD, INSTALL SEED AND MULCH
- 16. REMOVE ACCUMULATED SEDIMENT FROM BASIN / POND
- 17. FINAL GRADE POND / INFILTRATION BASINS (DO NOT COMPACT SOILS IN INFILTRATION AREAS.) (PART III.C)
- 18. WHEN ALL CONSTRUCTION ACTIVITY IS COMPLETE AND THE SITE IS STABILIZED BY EITHER SEED OR SOIL/LANDSCAPING, REMOVE SILT FENCE AND RESEED ANY AREAS DISTURBED BY THE REMOVAL.

RECORDS RETENTION (PART III.E):

THE SWPPP (ORIGINAL OR COPIES) INCLUDING, ALL CHANGES TO IT, AND INSPECTIONS AND MAINTENANCE RECORDS MUST BE KEPT AT THE SITE DURING CONSTRUCTION BY THE PERMITTEE WHO HAS OPERATIONAL CONTROL OF THAT PORTION OF THE SITE. THE SWPPP CAN BE KEPT IN EITHER THE FIELD OFFICE OR IN AN ON SITE VEHICLE DURING NORMAL WORKING HOURS.

ALL OWNER(S) MUST KEEP THE SWPPP ALONG WITH THE FOLLOWING ADDITIONAL RECORDS, ON FILE FOR THREE (3) YEARS AFTER SUBMITTAL OF THE NOT AS OUTLINED IN PART III.C. THIS DOES NOT INCLUDE ANY RECORDS AFTER SUBMITTAL OF THE NOT

- 1. THE FINAL SWPPP.
- 2. ANY OTHER STORMWATER RELATED PERMITS REQUIRED FOR THE PROJECT.
- 3. RECORDS OF ALL INSPECTION AND MAINTENANCE CONDUCTED DURING CONSTRUCTION (SEE PART IV.E, INSPECTIONS AND MAINTENANCE).
- 4. ALL PERMANENT OPERATION AND MAINTENANCE AGREEMENTS THAT HAVE BEEN IMPLEMENTED, INCLUDING ALL RIGHT OF WAY, CONTRACTS, COVENANTS AND OTHER BINDING REQUIREMENTS REGARDING PERPETUAL MAINTENANCE, AND
- 5. ALL REQUIRED CALCULATIONS FOR DESIGN OF THE TEMPORARY AND PERMANENT STORMWATER MANAGEMENT SYSTEMS.

PART IV. CONSTRUCTION ACTIVITY REQUIREMENTS

EROSION PREVENTION (PART IV.B):

THE CONTRACTOR IS RESPONSIBLE FOR PLANNING FOR AND IMPLEMENTING APPROPRIATE CONSTRUCTION PHASING, VEGETATIVE BUFFER STRIPS, HORIZONTAL SLOPE GRADING, AND OTHER CONSTRUCTION PRACTICES THAT MINIMIZE EROSION, SO THAT THE INSPECTION AND MAINTENANCE REQUIREMENTS OF PART IV.E ARE COMPLIED WITH. THE LOCATION OF AREAS NOT TO BE DISTURBED MUST BE DELINEATED (E.G. WITH FLAGS, STAKES, SIGNS, SILT FENCE ETC.) ON THE DEVELOPMENT SITE BEFORE WORK BEGINS.

ALL EXPOSED SOIL AREAS MUST BE STABILIZED AS SOON AS POSSIBLE TO LIMIT SOIL EROSION BUT NO LATER THAN THE END OF THE NEXT WORK DAY WHEN EARTH-DISTURBING ACTIVITIES WILL CEASE FOR AT LEAST 14 DAYS. TEMPORARY STOCKPILES WITHOUT SIGNIFICANT SILT, CLAY OR ORGANIC COMPONENTS (E.G. CLEAN AGGREGATE STOCKPILES, DEMOLITION CONCRETE STOCKPILES, SAND STOCKPILES) AND THE CONSTRUCTED BASE COMPONENTS OF ROADS, PARKING LOTS AND SIMILAR SURFACES ARE EXEMPT FROM THIS REQUIREMENT BUT MUST COMPLY WITH PART IV.C.5.

SOILS WITHIN 200 FEET OF A PUBLIC WATER (AS DESIGNATED BY THE MINNESOTA DNR) MUST BE STABILIZED WITHIN 24 HOURS DURING FINE SPRAWING TIMES.

THE NORMAL WETTED PERIMETER OF ANY TEMPORARY OR PERMANENT DRAINAGE DITCH OR SWALE THAT DRAINS WATER FROM ANY PORTION OF THE CONSTRUCTION SITE, OR DIVERTS WATER AROUND THE SITE, MUST BE STABILIZED WITHIN 200 LINEAL FEET FROM THE PROPERTY EDGE, OR FROM THE POINT OF DISCHARGE INTO ANY SURFACE WATER. STABILIZATION OF THE LAST 200 LINEAL FEET MUST BE COMPLETED WITHIN 24 HOURS AFTER CONNECTING TO A SURFACE WATER.

STABILIZATION OF THE REMAINING PORTIONS OF ANY TEMPORARY OR PERMANENT DITCHES OR SWALES MUST BE COMPLETED WITHIN 14 DAYS AFTER CONNECTING TO A SURFACE WATER AND CONSTRUCTION IN THAT PORTION OF THE DITCH HAS TEMPORARILY OR PERMANENTLY CEASED.

TEMPORARY OR PERMANENT DITCHES OR SWALES THAT ARE BEING USED AS A SEDIMENT CONTAINMENT SYSTEM (WITH PROPERLY DESIGNED ROCK DITCH CHECKS, BIO ROCKS, SILT Dikes ETC.) DO NOT NEED TO BE STABILIZED. THESE AREAS MUST BE STABILIZED WITHIN 24 HOURS AFTER NO LONGER BEING USED AS A SEDIMENT CONTAINMENT SYSTEM.

PIPE OUTLETS MUST BE PROVIDED WITH TEMPORARY OR PERMANENT ENERGY DISSIPATION WITHIN 24 HOURS AFTER CONNECTION TO A SURFACE WATER.

SEED NOTES (PART III.A.4.A):

ALL SEED MIXES AND APPLICATION SHALL BE IN ACCORDANCE WITH THE MNDOT SEEDING MANUAL.

GENERAL RECOMMENDATIONS:
IMMEDIATELY BEFORE SEEDING THE SOIL SHALL BE TILLED TO A MINIMUM DEPTH OF 3 INCHES.

TEMPORARY EROSION CONTROL SEEDING, MULCHING & BLANKET.

- SEED
 - TEMPORARY SEED SHALL BE MNDOT SEED MIX 21-112 (WINTER WHEAT COVER CROP) FOR WINTER AND 21-111 (OATS COVER CROP) FOR SPRING/SUMMER APPLICATIONS. BOTH SEED MIXES SHALL BE APPLIED AT A SEEDING RATE OF 100 LBS/ACRE.

- MULCH
 - IMMEDIATELY AFTER SEEDING, WITHIN 24 HOURS, MNDOT TYPE 1 MULCH SHOULD BE APPLIED TO PROTECT AND ENHANCE SEED GERMINATION. MULCH SHALL BE APPLIED AT 90% COVERAGE (3 TONS PER ACRE OF STRAW MULCH)

- SLOPES
 - 3:1 (HORIZ:VERT.) OR FLATTER MULCH SHALL BE COVERED WITH MULCH
 - SLOPES STEEPER THAN 3:1 OR DITCH BOTTOMS SHALL BE COVERED WITH EROSION CONTROL BLANKET.
 - SEE PLAN FOR MORE DETAILED DITCH AND STEEP SLOPE EROSION CONTROL TREATMENTS.

SEDIMENT CONTROL (PART IV.C):

SEDIMENT CONTROL PRACTICES MUST MINIMIZE SEDIMENT FROM ENTERING SURFACE WATERS, INCLUDING CURB AND GUTTER SYSTEMS AND STORM SEWER INLETS.

- a. TEMPORARY OR PERMANENT DRAINAGE DITCHES AND SEDIMENT BASINS THAT ARE DESIGNED AS PART OF A SEDIMENT CONTAINMENT SYSTEM (E.G. DITCHES WITH ROCK CHECK DAMS) REQUIRE SEDIMENT CONTROL PRACTICES ONLY AS APPROPRIATE FOR SITE CONDITIONS.
- b. IF THE DOWN GRADIENT TREATMENT SYSTEM IS OVERLOADED, ADDITIONAL UPGRADIENT SEDIMENT CONTROL PRACTICES OR REDUNDANT BMP'S MUST BE INSTALLED TO ELIMINATE THE OVERLOADING, AND THE SWPPP MUST BE AMENDED TO IDENTIFY THESE ADDITIONAL PRACTICES AS REQUIRED IN PART III.A.4.A THROUGH C.
- c. IN ORDER TO MAINTAIN SHEET FLOW AND MINIMIZE RILLS AND/OR GULLIES, THERE SHALL BE NO UNBROKEN SLOPE LENGTH OF GREATER THAN 75 FEET FOR SLOPES WITH A GRADE OF 3:1 OR STEEPER.

SEDIMENT CONTROL PRACTICES MUST BE ESTABLISHED ON ALL DOWN GRADIENT PERIMETERS BEFORE ANY UPGRADIENT LAND DISTURBING ACTIVITIES BEGIN. THESE PRACTICES SHALL REMAIN IN PLACE UNTIL FINAL STABILIZATION HAS BEEN ESTABLISHED IN ACCORDANCE WITH PART IV.G.

THE TIMING OF THE INSTALLATION OF SEDIMENT CONTROL PRACTICES MAY BE ADJUSTED TO ACCOMMODATE SHORT-TERM ACTIVITIES SUCH AS CLEARING OR GRUBBING, OR PASSAGE OF VEHICLES. ANY SHORT-TERM ACTIVITY MUST BE COMPLETED AS QUICKLY AS POSSIBLE AND THE SEDIMENT CONTROL PRACTICES MUST BE INSTALLED IMMEDIATELY AFTER THE ACTIVITY IS COMPLETED. HOWEVER, SEDIMENT CONTROL PRACTICES MUST BE INSTALLED BEFORE THE NEXT PRECIPITATION EVENT EVEN IF THE ACTIVITY IS NOT COMPLETE.

ALL STORM DRAIN INLETS MUST BE PROTECTED BY APPROPRIATE BMP'S DURING CONSTRUCTION UNTIL ALL SOURCES WITH POTENTIAL FOR DISCHARGING TO THE INLET HAVE BEEN STABILIZED. INLET PROTECTION MAY BE REMOVED FOR A PARTICULAR INLET IF A SPECIFIC SAFETY CONCERN (STREET FLOODING/FREEZING) HAS BEEN IDENTIFIED AND THE PERMITTEE(S) HAVE RECEIVED WRITTEN CORRESPONDENCE FROM THE JURISDICTIONAL AUTHORITY (E.G. CITY/COUNTY/TOWNSHIP/MNDOT ENGINEER) VERIFYING THE

NEED FOR REMOVAL. THE WRITTEN CORRESPONDENCE MUST BE DOCUMENTED IN THE SWPPP OR AVAILABLE WITHIN 72 HOURS UPON REQUEST. WHEN WRITTEN CORRESPONDENCE CAN NOT BE OBTAINED IN A TIMELY MANNER, THE SPECIFIC INLET PROTECTION CAN BE REMOVED TO ALLEViate THE IMMEDIATE SAFETY CONCERN. HOWEVER, EFFORTS TO OBTAIN WRITTEN CORRESPONDENCE MUST BE DOCUMENTED IN THE SWPPP AND AVAILABLE WITHIN 72 HOURS UPON REQUEST. PERMISSION TO REMOVE INLET PROTECTION BASED ON A SPECIFIC SAFETY CONCERN MUST STILL BE OBTAINED FROM THE JURISDICTIONAL AUTHORITY WITHIN 30 DAYS OF REMOVAL.

TEMPORARY SOIL STOCKPILES MUST HAVE SILT FENCE OR OTHER EFFECTIVE SEDIMENT CONTROLS, AND CANNOT BE PLACED IN SURFACE WATERS, INCLUDING STORMWATER CONVEYANCES SUCH AS CURB AND GUTTER SYSTEMS, OR CONDUITS AND DITCHES UNLESS THERE IS A BYPASS IN PLACE FOR THE STORMWATER.

VEHICLE TRACKING OF SEDIMENT FROM THE CONSTRUCTION SITE (OR ONTO STREETS WITHIN THE SITE) MUST BE MINIMIZED BY BMP'S SUCH AS STONE PADS, CONCRETE OR STEEL WASH RACKS, OR EQUIVALENT SYSTEMS. STREET SWEEPING MUST BE USED IF SUCH BMP'S ARE NOT ADEQUATE TO PREVENT SEDIMENT FROM BEING TRACKED ONTO THE STREET (SEE PART IV.E.4.D).

THE PERMITEE MUST MINIMIZE SOIL COMPACTION AND, UNLESS INFEASIBLE, PRESERVE TOPSOIL. MINIMIZING SOIL COMPACTION IS NOT REQUIRED WHERE THE FUNCTION OF THE SPECIFIC AREA OF THE SITE DICTATES THAT IT BE COMPACTED. METHODS FOR MINIMIZING COMPACTION INCLUDE THE USE OF TRACKED EQUIPMENT, AND STAYING OFF OF AREAS TO BE LEFT UN-COMPACTED. METHODS TO PRESERVE TOPSOIL INCLUDE STRAPPING AND STOCKPILING TOPSOIL PRIOR TO GRADING OR EXCAVATION OPERATIONS.

THE PERMITTEE MUST INSTALL TEMPORARY SEDIMENTATION BASINS AS REQUIRED IN PART III.B. OF THIS PERMIT.

DEWATERING AND BASIN DRAINING (PART IV.D):

DEWATERING OR BASIN DRAINING (E.G. PUMPED DISCHARGES, TRENCH/DITCH CUTS FOR DRAINAGE) RELATED TO THE CONSTRUCTION ACTIVITY THAT MAY HAVE TURBID OR SEDIMENT LADEN DISCHARGE WATER MUST BE DISCHARGED TO A TEMPORARY OR PERMANENT SEDIMENTATION BASIN ON THE PROJECT SITE WHENEVER POSSIBLE. IF THE WATER CANNOT BE DISCHARGED TO A SEDIMENTATION BASIN PRIOR TO ENTERING THE SURFACE WATER, IT MUST BE TREATED WITH THE APPROPRIATE BMP'S SUCH THAT THE DISCHARGE DOES NOT ADVERSELY AFFECT THE RECEIVING WATER. DOWNSTREAM LANDOWNERS OR WETLANDS. THE CONTRACTOR MUST ENSURE THAT DISCHARGE POINTS ARE ADEQUATELY PROTECTED FROM EROSION AND SCOUR. THE DISCHARGE MUST BE DISPERSED OVER NATURAL ROCK RIPRAP, SAND BAGS, PLASTIC SHEATHING OR OTHER ACCEPTED ENERGY DISSIPATION MEASURES. ADEQUATE SEDIMENTATION CONTROL MEASURES ARE REQUIRED FOR DISCHARGE WATER THAT CONTAINS SUSPENDED SOLIDS.

FILTER BACKWASH WATERS MUST BE HAULED AWAY FOR DISPOSAL, RETURNED TO THE BEGINNING OF THE TREATMENT PROCESS, OR INCORPORATE INTO THE SITE IN A MANNER THAT DOES NOT CAUSE EROSION. DISCHARGE OF THE BACKWASH WATER TO SANITARY SEWER IS ALLOWED WITH PERMISSION OF THE SANITARY SEWER AUTHORITY.

INSPECTIONS AND MAINTENANCE (PART IV.E):

THE CONTRACTOR IS RESPONSIBLE AT ALL TIMES FOR THE MAINTENANCE AND PROPER OPERATION OF EROSION AND SEDIMENT CONTROL FACILITIES. THE CONTRACTOR SHALL AT A MINIMUM, INSPECT, MAINTAIN AND REPAIR ALL DISTURBED SURFACES AND ALL EROSION AND SEDIMENT CONTROL FACILITIES AND SOIL STABILIZATION MEASURES ONCE EVERY SEVEN (7) DAYS DURING ACTIVE CONSTRUCTION AND WITHIN 24 HOURS FOLLOWING A RAINFALL OF 0.5 INCHES OR GREATER AND WITHIN 7 DAYS AFTER THAT. CONTINUE INSPECTION ACTIVITIES UNTIL LAND-DISTURBING ACTIVITY HAS CEASED. THEREAFTER THE CONTRACTOR SHALL PERFORM THESE RESPONSIBILITIES AT LEAST WEEKLY UNTIL VEGETATIVE COVER IS ESTABLISHED. INSPECTIONS MUST INCLUDE STABILIZED AREAS, EROSION PREVENTION AND SEDIMENT CONTROL BMP'S AND INFILTRATION AREAS.

BASED ON INSPECTION RESULTS THE CONTRACTOR MAY MODIFY THE SWPPP IN ORDER TO PREVENT POLLUTANTS FROM LEAVING THE SITE VIA STORM WATER RUNOFF. THIS MODIFICATION MUST BE MADE WITHIN 7 CALENDAR DAYS OF THE INSPECTION UNLESS OTHERWISE REQUIRED BY THE TERMS OF THE PERMIT, LEGAL, REGULATORY, OR PHYSICAL ACCESS CONSTRAINTS.

INSPECTION REPORTS MUST BE RECORDED WITHIN 24 HOURS IN WRITING AND KEPT ON FILE BY THE CONTRACTOR AS AN INTEGRAL PART OF THE SWPPP ON SITE AND THEN FOR AT LEAST 3 YEARS FROM THE DATE OF COMPLETION OF THIS PROJECT.

ALL PERMETER CONTROL DEVICES MUST BE REPAIRED, REPLACED OR SUPPLEMENTED WHEN THEY BECOME NONFUNCTIONAL OR THE SEDIMENT REACHES 1/2 OF THE HEIGHT OF THE DEVICE. THESE REPAIRS MUST BE MADE BY THE END OF THE NEXT BUSINESS DAY AFTER DISCOVERY, OR THEREAFTER AS SOON AS FIELD CONDITIONS ALLOW ACCESS.

REMOVE ALL DELTAS AND SEDIMENT DEPOSITED IN SURFACE WATERS, INCLUDING DRAINAGE WAYS, CATCH BASINS, AND OTHER DRAINAGE SYSTEMS, AND RESTABLISH THE AREAS WHERE SEDIMENT REMOVAL RESULTS IN EXPOSED TOPSOIL. THIS REMOVAL AND STABILIZATION MUST TAKE PLACE WITHIN 7 DAYS OF DISCOVERY UNLESS PRECLUDED BY LEGAL, REGULATORY, OR PHYSICAL ACCESS CONSTRAINTS.

SEE (ATTACHMENT A: CONSTRUCTION SWPPP TEMPLATE) FOR SITE SPECIFIC INSPECTIONS AND MAINTENANCE REQUIREMENTS.

POLLUTION PREVENTION MANAGEMENT (PART IV.F):

THE CONTRACTOR SHALL IMPLEMENT THE FOLLOWING POLLUTION PREVENTION MANAGEMENT MEASURES ON THE SITE:

SOLID WASTE: COLLECTED SEDIMENT, ASPHALT AND CONCRETE MILLINGS, FLOATING DEBRIS, PAPER, PLASTIC, FABRIC, CONSTRUCTION AND DEMOLITION DEBRIS AND OTHER WASTES MUST BE DISPOSED OF PROPERLY AND MUST COMPLY WITH MINNESOTA POLLUTION CONTROL AGENCY (MPCA) DISPOSAL REQUIREMENTS.

HAZARDOUS MATERIALS: OIL, GASOLINE, PAINT AND ANY HAZARDOUS SUBSTANCES MUST BE PROPERLY STORED, INCLUDING SECONDARY CONTAINMENT, TO PREVENT SPILLS, LEAKS OR OTHER DISCHARGE. RESTRICTED ACCESS TO STORAGE AREAS MUST BE PROVIDED TO PREVENT VANDALISM, STORAGE AND DISPOSAL OF HAZARDOUS WASTE MUST BE IN COMPLIANCE WITH MPCA REGULATIONS.

EXTERNAL WASHING OF TRUCKS AND OTHER CONSTRUCTION VEHICLES MUST BE LIMITED TO A DEFINED AREA OF THE SITE. RUNOFF MUST BE CONTAINED AND WASTEWATER PROPERLY DISPOSED OF. NO ENGINE DEGREASING IS ALLOWED ON SITE.

CONCRETE WASHOUT: ALL LIQUID AND SOLID WASTES GENERATED BY CONCRETE WASHOUT OPERATIONS MUST BE CONTAINED IN A LEAK PROOF CONTAINMENT FACILITY OR IMPERMEABLE LINER. A COMPACTED CLAY LINER THAT DOES NOT ALLOW WASHOUT LIQUIDS TO ENTER THE GROUND IS CONSIDERED AN IMPERMEABLE LINER. THE LIQUID AND SOLID WASTES MUST NOT CONTACT THE GROUND, AND THERE MUST NOT BE RUNOFF FROM THE CONCRETE WASHOUT OPERATIONS OR AREAS. LIQUID AND SOLID WASTES MUST BE DISPOSED OF PROPERLY AND IN COMPLIANCE WITH MPCA REGULATIONS. A SIGN MUST BE INSTALLED ADJACENT TO EACH WASHOUT FACILITY TO INFORM CONCRETE EQUIPMENT OPERATORS TO UTILIZE THE PROPER FACILITIES. CONCRETE WASHOUT MAY ALSO OCCUR OFF SITE ACCORDING TO THE APPROPRIATE REGULATIONS.

FUELING OPERATION PLAN: ALL FUELING SHALL TAKE PLACE AT THE DESIGNATED FUELING LOCATION AND ACCORDING TO BEST PRACTICES FOR SITE FUELING OPERATIONS AS TO MINIMIZE THE POTENTIAL FOR SPILLS.

SPILL PREVENTION PLAN: ALL SPILLS SHALL BE IMMEDIATELY CLEANED UP AFTER DISCOVERY. THE SITE SUPERINTENDENT, WHO IS RESPONSIBLE FOR DAY-TO-DAY ONSITE CONSTRUCTION OPERATIONS WILL BE THE SPILL PREVENTION COORDINATOR AND WILL BE RESPONSIBLE FOR IMPLEMENTING CLEAN UP PROCEDURES, POSTING CLEAN UP RECOMMENDATIONS, AND ENSURING PROPER CLEAN UP TRAINING OF APPROPRIATE PERSONNEL.

SANITARY AND SEPTIC WASTE: SANITARY/SEPTIC FACILITIES SHALL BE PROVIDED AND MAINTAINED IN A NEAT AND SANITARY CONDITION. FOR THE USE OF THE CONTRACTORS EMPLOYEES. A LICENSED SANITARY WASTE MANAGEMENT CONTRACTOR AS REQUIRED BY STATE REGULATIONS WILL COLLECT SANITARY WASTE FROM PORTABLE UNITS.

FINAL STABILIZATION (PART IV.G):

THE CONTRACTOR MUST ENSURE FINAL STABILIZATION OF THE SITE ACCORDING TO THE DEFINITIONS IN THE NPDES GENERAL PERMIT PART V SECTION G. THE CONTRACTOR MUST SUBMIT A NOTICE OF TERMINATION (N.O.T.) WITHIN 30 DAYS AFTER FINAL STABILIZATION IS COMPLETE OR WITHIN 7 DAYS AFTER SELLING THE SITE OR PORTION OF THE SITE (THAT HAS NOT UNDERGONE FINAL STABILIZATION) TO ANOTHER PARTY. A COPY OF THIS NOTICE OF TERMINATION / PERMIT MODIFICATION FORM MUST GO TO THE NEW OWNER. THE ORIGINAL CURRENT OWNER MUST PROVIDE A SWPPP TO THE NEW OWNER THAT SPECIFICALLY ADDRESSES THE REMAINING CONSTRUCTION ACTIVITY. SEE THE SWPPP PLAN SHEETS AND SWPPP NARRATIVE (ATTACHMENT A: CONSTRUCTION SWPPP TEMPLATE) AND SWPPP PLAN SHEETS FOR FINAL STABILIZATION MEASURES

TRAINING (PART III.A.2)

DESIGN ENGINEER: MATTHEW R. PAVEK P.E.
TRAINING COURSE: DESIGN OF SWPPP
TRAINING ENTITY: UNIVERSITY OF MINNESOTA
INSTRUCTOR: JOHN CHAPMAN
DATES OF TRAINING COURSE: 5/15/2011 - 5/16/2011
TOTAL TRAINING HOURS: 12
RE-CERTIFICATION: 1/13/14-1/13/14 (8 HOURS), EXP. 5/31/2017

OWNER INFORMATION

OWNER:
3535 GRAND LLC
3802 NICOLLET AVENUE SOUTH, SUITE 200
MINNEAPOLIS, MN 55409
CONTACT:
MICHAEL LANDER
3535 GRAND LLC
3802 NICOLLET AVENUE SOUTH, SUITE 200
MINNEAPOLIS, MN 55409

AREAS AND QUANTITIES (PART III.A.4.B&C):

	EXISTING		PROPOSED
BUILDING COVERAGE	0 SF	0.0%	6,311 SF 40.6%
ALL PAVEMENTS	0 SF	0.0%	5,631 SF 36.3%
ALL NON-PAVEMENTS	15,532 SF	100.0%	3,590 SF 23.1%
TOTAL SITE AREA	15,532 SF	100.0%	15,532 SF 100.0%
IMPERVIOUS SURFACE			
EXISTING CONDITION	0 SF	0.0%	
PROPOSED CONDITION	11,942 SF	76.9%	
DIFFERENCE	11,942 SF	76.9%	
DISTURBED AREA	16,441 SF		
SILT FENCE/BIO-ROLL	600 LF		
EROSION CONTROL BLANKET	0 SF		
INLET PROTECTION DEVICES	4 EA		

NOTE: QUANTITIES ARE FOR INFORMATIONAL PURPOSES ONLY. CONTRACTOR SHALL DETERMINE FOR THEMSELVES THE EXACT QUANTITIES FOR BIDDING AND CONSTRUCTION.

SWPPP CONTACT PERSON

CONTRACTOR:
GEOFF MOEDING
MOEDING PARTNERS LLC
316 BARSTOW STREET
EAU CLAIRE, WI 54703

SWPPP INSPECTOR TRAINING:
ALL SWPPP INSPECTIONS MUST BE PERFORMED BY A PERSON THAT MEETS THE TRAINING REQUIREMENTS OF THE NPDES CONSTRUCTION SITE PERMIT. TRAINING CREDENTIALS SHALL BE PROVIDED BY THE CONTRACTOR AND KEPT ON SITE WITH THE SWPPP

PARTY RESPONSIBLE FOR LONG TERM OPERATION AND MAINTENANCE OF PERMANENT STORM WATER MANAGEMENT SYSTEM

PERMANENT STORMWATER MANAGEMENT IS NOT REQUIRED AS PART OF THIS PROJECT TO MEET NPDES PERMIT REQUIREMENTS.

SWPPP ATTACHMENTS:

NOT APPLICABLE

SUPPLEMENTARY SITE SPECIFIC EROSION CONTROL NOTES:

THESE NOTES SUPERCEDE ANY GENERAL SWPPP NOTES.

THIS PROJECT IS LESS THAN 1.0 ACRES, SO AN NPDES PERMIT IS NOT REQUIRED AND DOES NOT NEED TO BE SUBMITTED TO THE MPCA. HOWEVER, THE CONTRACTOR IS REQUIRED TO FOLLOW THE GUIDELINES IN THE NPDES PERMIT THROUGHOUT CONSTRUCTION.

PROJECT NARRATIVE:

PROJECT IS A DEVELOPMENT OF A VACANT SITE INTO A MULTI LEVEL COMMERCIAL BUILDING. SITE AND LANDSCAPE IMPROVEMENTS WILL OCCUR.

SPECIAL TMDL BMP REQUIREMENTS SITE SPECIFIC (IF REQUIRED):

NOT REQUIRED

PERMANENT STABILIZATION NOTES SITE SPECIFIC:

- PERMANENT SEED MIX
 - FOR THIS PROJECT ALL AREAS THAT ARE NOT TO BE SOODED OR LANDSCAPED SHALL RECEIVE A NATIVE PERMANENT SEED MIX
 - AREAS IN BUFFERS AND ADJACENT TO OR IN WET AREAS MNDOT SEED MIX 33-201 (STORMWATER SOUTH AND WEST) AT 35 LBS PER ACRE.
 - DRY AREAS MNDOT SEED MIX 35-221 (DRY PRAIRIE PRAIRIE) AT 40 LBS PER ACRE.
 - MAINTENANCE SHALL BE IN ACCORDANCE TO THE MNDOT SEEDING MANUAL.

CITY OF MINNEAPOLIS EROSION CONTROL NOTES:

City of Minneapolis Standard Erosion Control Notes



City of Minneapolis Standard Erosion and Sediment Control Notes For Plans
These notes may be used as performance standards or requirements for projects not meeting the threshold for a formal plan.

- 1. Contractor must call a construction start 48 hours prior to any land disturbances 612-673-3867. Failure to do so may result in fines, the revocation of permit and a stop work order being issued.
- 2. Install perimeter erosion control at the locations shown on the plans prior to the commencement of any land disturbance or construction activities.
- 3. Before beginning construction, install a temporary rock construction entrance at each point where vehicles exit the construction site. Use 2 inch or greater diameter rock in a layer at least 6 inches thick across the entire width of the entrance. Extend the rock entrance at least 50 feet into the construction zone using a geo-textile fabric beneath the aggregate to prevent migration of soil into the rock from below.
- 4. Remove all soils and sediments tracked or otherwise deposited onto public and private pavement areas. Removal shall be on a daily basis when tracking occurs and may be ordered by Minneapolis inspectors at any time if conditions warrant. Sweeping shall be maintained throughout the duration of the construction and done in a manner to prevent dust being blown to adjacent properties.
- 5. Install inlet protection at all public and private catch basin inlets, which receive runoff from the disturbed areas. Contractor shall clean, remove sediment or replace storm drain inlet protection devices on a routine basis such that the devices are fully functional for the next rain event. Sediment deposited in and/or plugging drainage systems is the responsibility of the contractor. Hay bales or filter fabric wrapped green net are not allowed for inlet protection.
- 6. Locate soil or dirt stockpiles no less than 25 feet from any public or private roadway or drainage channel. If remaining for more than seven days, stabilize the stockpiles by mulching, vegetative cover, tamps, or other means. Control erosion from all stockpiles by placing silt barriers around the piles. Temporary stockpiles located on paved surfaces must be no less than two feet from the drainage/gutter line and shall be covered if left more than 24 hours.
- 7. Maintain all temporary erosion and sediment control devices in place until the contributing drainage area has been stabilized. Inspect temporary erosion and sediment control devices on a daily basis and replace deteriorated, damaged, or rotted erosion control devices immediately.
- 8. Temporarily or permanently stabilize all construction areas which have undergone final grading, and all areas in which grading or site building construction operations are not actively underway against erosion due to rain, wind and running water within 7-14 days. Use seed and mulch, erosion control matting, and/or sodding and staking in green space areas. Remove all temporary synthetic, structural, non-biodegradable erosion and sediment control devices after the site has undergone final stabilization with permanent vegetation establishment. Final stabilization for purposes of this removal is 70% established cover over denuded area.
- 9. Ready mixed concrete and concrete batch mix plants are prohibited within the public right of way. All concrete related production, cleaning and mixing activities shall be done in the designated concrete mixing/transfer locations as shown in the erosion control plan. Under no circumstance may washout water drain onto the public right of way or into any public or private storm drain conveyance.
- 10. Changes to approved erosion control plan must be approved by the erosion control inspector prior to implementation. Contractor to provide installation and details for all proposed alternate type devices.
- 11. If dewatering or pumping of water is necessary, the contractor is responsible for obtaining any necessary permits and/or approvals prior to discharge of any water from the site. If the discharge from the dewatering or pumping process is turbid or contains sediment laden water, it must be treated through the use of sediment traps, vegetative filter strips, or other sediment reducing measures such that the discharge is not visibly different from the receiving water. Additional erosion control measures may be required at the discharge point to prevent scour erosion. The contractor shall provide a dewatering/pumping plan to the Erosion Control Inspector prior to initiating dewatering activities.



Prepared by: The City of Minneapolis Public Works Surface Water and Sewers: www.ci.minneapolis.mn.us/transport
January 7, 2011



www.collagearch.com



3535 GRAND
3535 GRAND AVENUE
MINNEAPOLIS, MN



LANDERGROUP
urban development

CivilSite
GROUP

4601 W. 35TH ST., SUITE 200
ST. LOUIS PARK, MN 55416
CivilSiteGroup.com

Matt Pavlek 763-213-3944 Pat Sarver 952-250-2003

DATE: 04.09.2015

I HEREBY CERTIFY THAT THIS PLAN SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

MP

MATTHEW R. PAVEK
REGISTRATION NO: 44263

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St. Paul, Minnesota 55114

Voigt & Associates, Inc.
Structural Engineer
Paul Voigt
651.686.7727
4635 Nicols Road, Suite 204
Eagan, Minnesota 55122

DATE: ARPIL 9, 2015

PDR PLAN REVIEW

MARK	DATE	DESCRIPTION

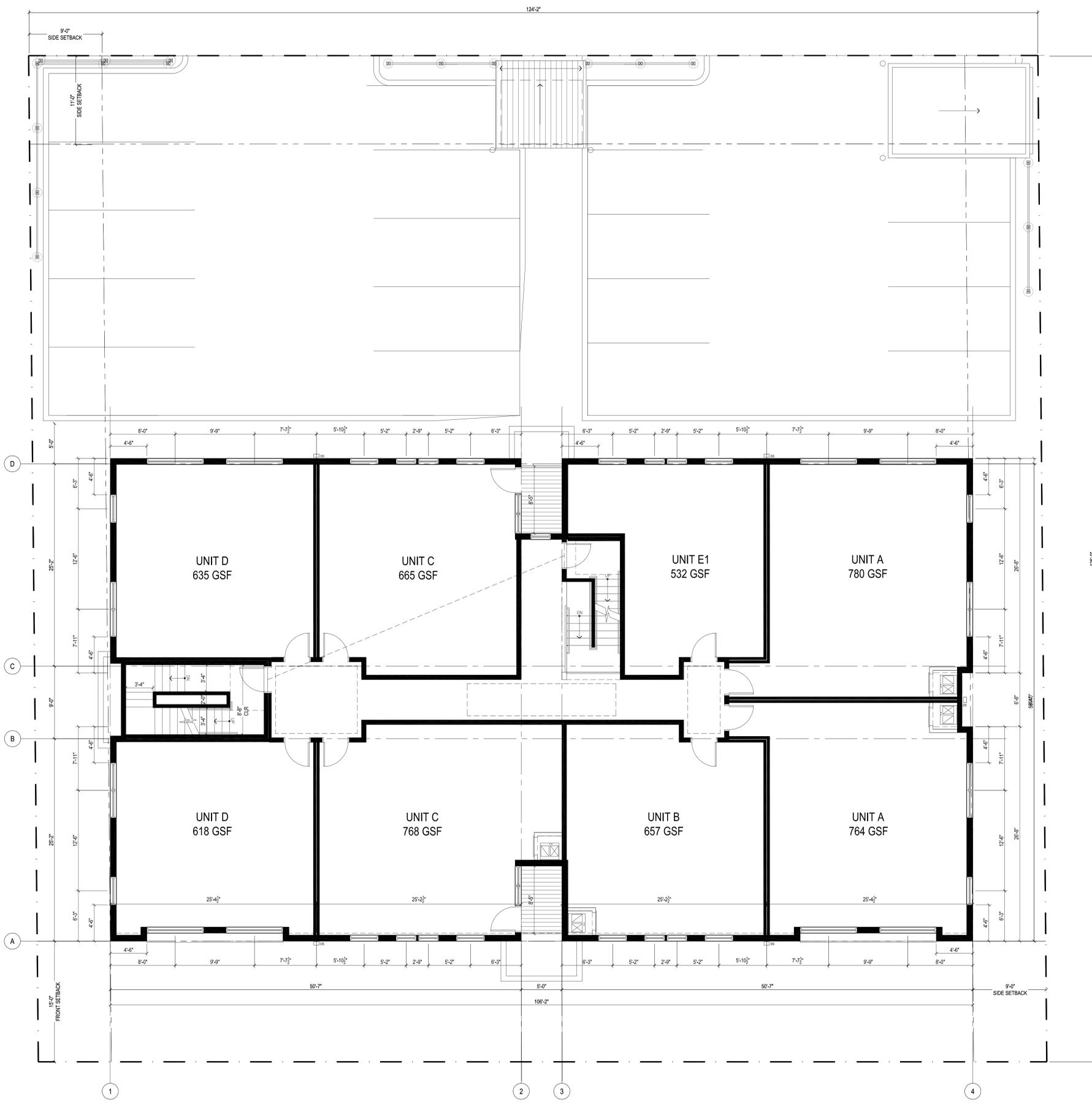
PROJECT NO: 15074
DRAWN BY: DJK
CHKD BY: MRP

SHEET TITLE

SWPPP - NARRATIVE

SW1.3

- GENERAL NOTES
- ALL EXTERIOR DIMENSIONS ARE TO OUTSIDE FACE OF SHEATHING OR OUTSIDE FACE OF MASONRY UNLESS OTHERWISE NOTED.
 - VERIFY SLAB EDGE LOCATIONS, BRICK AND MASONRY FACE WITH ARCHITECTURAL AND STRUCTURAL DETAILS.
 - INTERIOR WALL DIMENSIONS ARE TO CENTERLINE OF WALLS UNLESS NOTED OTHERWISE. WHERE CLEAR DIMENSIONS ARE INDICATED, THESE DIMENSIONS SHALL BE MAINTAINED.
 - ROUGH OPENING DIMENSIONS OF WINDOWS, DOORS AND OTHER COMPONENTS SHALL BE VERIFIED WITH ACTUAL DIMENSIONS OF SUPPLIED COMPONENTS.
 - DO NOT SCALE DRAWINGS. IF DIMENSION IS NOT CLEAR, NOT GIVEN, OR PRESENTS A CONFLICT, NOTIFY THE ARCHITECT IMMEDIATELY.
 - NOTIFY ARCHITECT IMMEDIATELY, IF DIMENSIONAL DISCREPANCIES OCCUR.
 - PROVIDE CONTROL JOINTS AT FLOORS AND WALLS PER MANUFACTURER'S RECOMMENDATIONS AND SPECIFICATIONS.
 - INSTALL ALL ELEMENTS AND MATERIALS TO MEET MANUFACTURER'S RECOMMENDATIONS. IF ARCHITECTURAL DRAWINGS CONFLICT WITH MANUFACTURER'S RECOMMENDATIONS, NOTIFY ARCHITECT IMMEDIATELY.
 - PROVIDE SEALED JOINTS AT MOISTURE CONTROL MEMBRANE.
 - PROTECT MATERIALS ON-SITE DURING CONSTRUCTION FROM MOISTURE AND OTHER DAMAGE.
 - INSULATE GAPS AT WOOD FRAMING BETWEEN WINDOWS, DOORS AND OTHER MATERIALS WITH RIGID INSULATION.
 - WOOD IN CONTACT WITH CONCRETE TO BE TREATED WOOD.
 - PROVIDE WOOD BLOCKING BEHIND ALL WALL HUNG CASEWORK, MILLWORK, SHELVING, FIXTURES AND ELSEWHERE AS NOTED OR AS NECESSARY BASED ON MANUFACTURER'S RECOMMENDATIONS.
 - GENERAL CONTRACTOR TO VERIFY WITH MECHANICAL, ELECTRICAL AND PLUMBING SUBCONTRACTORS AS TO LOCATIONS FOR EQUIPMENT, FIXTURES, METERS, DUCTWORK, ETC. NOTIFY ARCHITECT IMMEDIATELY OF ANY CONFLICTS BETWEEN ARCHITECTURAL DRAWINGS AND M.E.P. SUBCONTRACTORS.
 - CONSTRUCT RATED ASSEMBLIES AT ALL LOCATIONS INDICATED IN CODE REVIEW AND PER DRAWINGS. INCLUDING ALL CEILING AND FLOOR ASSEMBLIES, PROVIDE RATED ENCLOSURE WHERE FIXTURES OR EQUIPMENT PENETRATES THE ASSEMBLY.
 - PROVIDE FIRE STOPPING AT ALL PENETRATIONS OF RATED ASSEMBLIES. ANY DISTURBANCE OF ASSEMBLY SHALL BE REPAIRED.
 - FIRE-BLOCK ALL CONCEALED WALL SPACES, INCLUDING 10'-0" VERTICALLY AND HORIZONTALLY.
 - DO NOT PENETRATE STAIR OR ELEVATOR ENCLOSURE UNLESS SERVING STAIR OR ELEVATOR.
 - FINISH FLOOR ELEVATION CHANGE AT DOORS OR MATERIAL TRANSITIONS NOT TO EXCEED 1/2".
 - SEE SHEET A001 FOR ALL WALL TYPES.



1 SECOND FLOOR PLAN - 6222 GSF
SCALE: 3/16" = 1'-0"



3535 GRAND
3535 GRAND AVENUE
MINNEAPOLIS, MN



DATE: XX.XX.XXXX
I HEREBY CERTIFY THAT THIS PLAN SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED ARCHITECT UNDER THE LAWS OF THE STATE OF MINNESOTA.

PETER KEELY
REGISTRATION NO: 23570

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St. Paul, Minnesota 55114

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Civil Engineer
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St. Louis Park, MN 55416

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705 Raymond Avenue #200
St. Paul, Minnesota 55114

Voigt & Associates, Inc.
Structural Engineer
Paul Voigt
651.686.7727
4635 Nicols Road, Suite 204
Eagan, Minnesota 55122

DATE: APRIL 09, 2015

PDR PLAN REVIEW

MARK	DATE	DESCRIPTION

PROJECT NO: 15-002
DRAWN BY: JPJ
CHKD BY: PK

SHEET TITLE
SECOND AND THIRD FLOOR PLANS



3535 GRAND
3535 GRAND AVENUE
MINNEAPOLIS, MN



DATE: XX.XX.XXXX

I HEREBY CERTIFY THAT THIS PLAN SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED ARCHITECT UNDER THE LAWS OF THE STATE OF MINNESOTA.

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REGISTRATION NO: 23570

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Eagan, Minnesota 55122

DATE: APRIL 09, 2015

PDR PLAN REVIEW

MARK	DATE	DESCRIPTION

PROJECT NO: 15-002
DRAWN BY: JPK
CHKD BY: PK

SHEET TITLE

ELEVATIONS

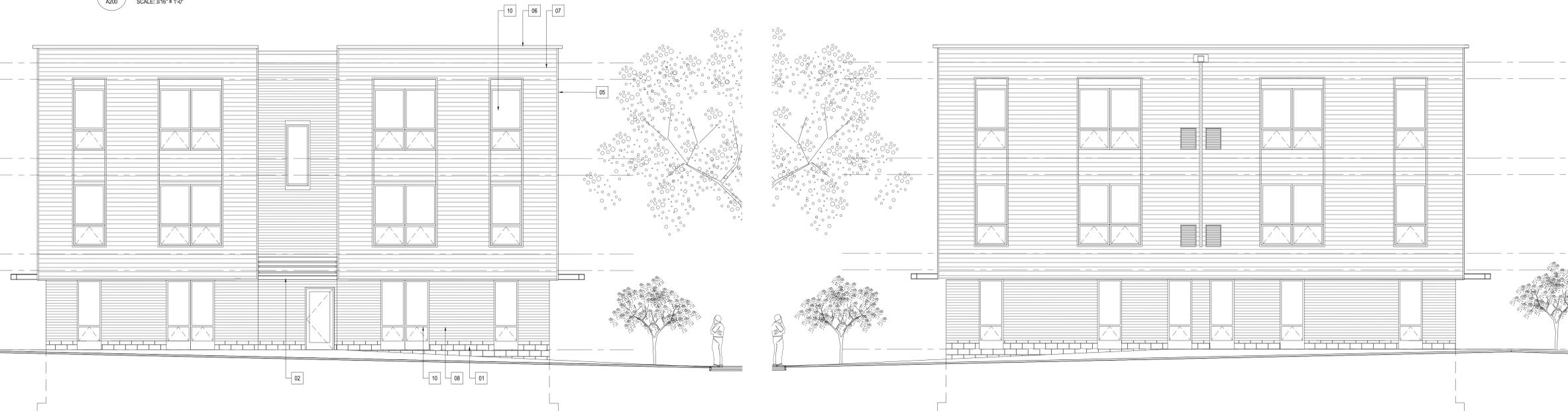
A200

MATERIAL INDEX

01	BURNISHED BLOCK
02	POWDERCOAT STEEL / WOOD CANOPY
03	POWDERCOAT STEEL / WOOD GUARDRAIL
04	PREFINISHED METAL PANEL
05	PREFINISHED METAL TRIM
06	PREFINISHED METAL PARAPET CAP
07	PREFINISHED 5/8" CEMENT BOARD LAP SIDING
08	PREFINISHED HORIZONTAL METAL SIDING
09	PREFINISHED METAL FASCIA
10	ALUMINUM CLAD WINDOWS
11	PREFINISHED COMMERCIAL DOWNSPOUT
12	SCUPPER
13	PREFINISHED STEEL COLUMNS
14	WOOD PRIVACY FENCE
15	MECHANICAL LOUVER



4 EAST ELEVATION
A200 SCALE: 3/16" = 1'-0"



3 NORTH ELEVATION
A200 SCALE: 3/16" = 1'-0"

2 SOUTH ELEVATION
A200 SCALE: 3/16" = 1'-0"



1 WEST ELEVATION
A200 SCALE: 3/16" = 1'-0"

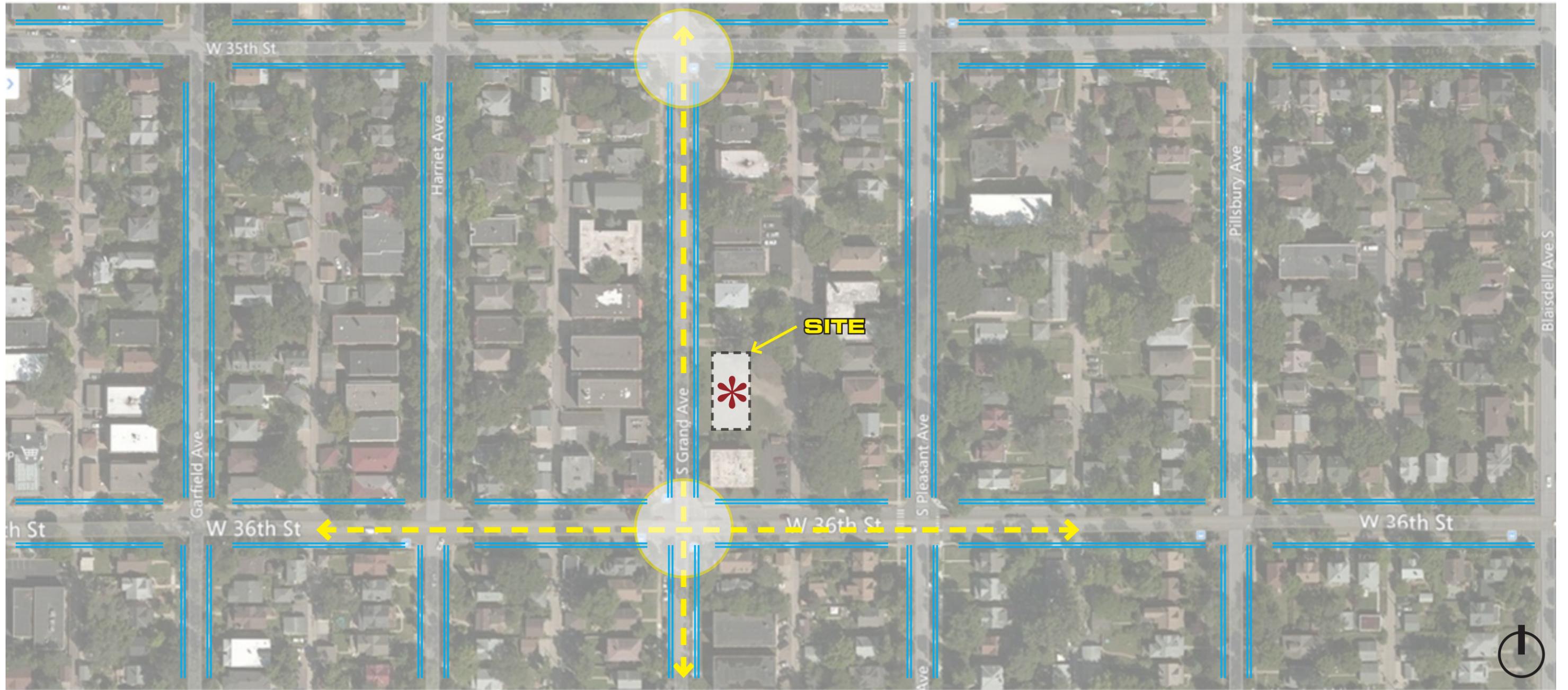


Collage | architects

LANDERGROUP
urban development





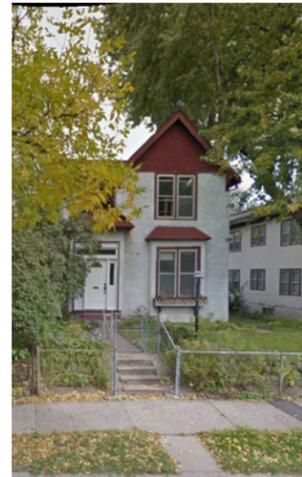
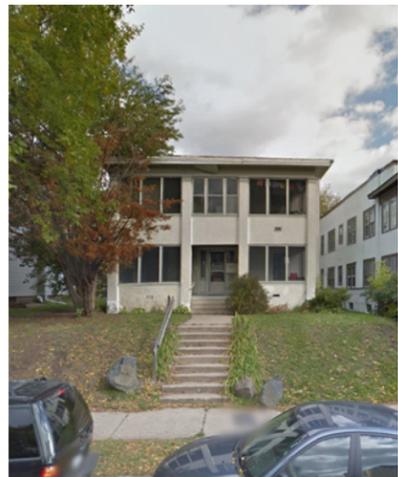


← - - - BUS ROUTES

==== OFF STREET PARKING

TRANSIT DIAGRAM

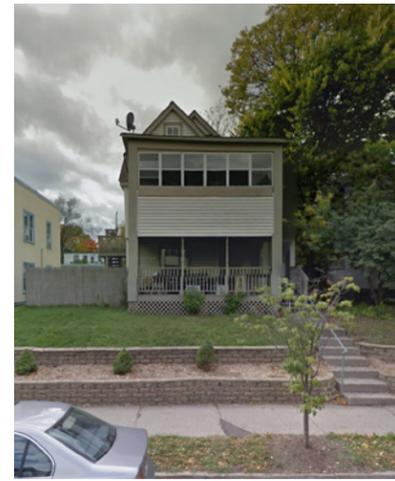
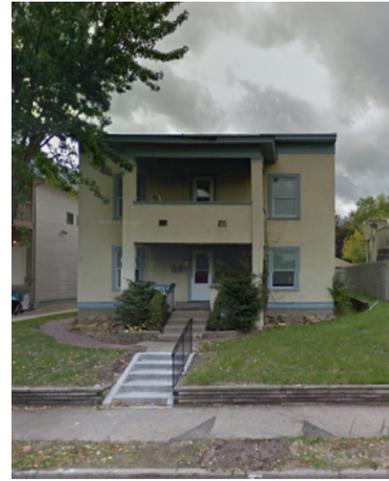
35TH STREET SOUTH



36TH STREET SOUTH

SITE - 3535 3539 3543 GRAND AVE S

EAST STREETSCAPE



35TH STREET SOUTH

36TH STREET SOUTH



SITE FRONTAGE

WEST STREETSCAPE



h St

W 36th St

S Grand

S Pleasant

50 feet 20 m

From: [Tom Scott](#)
To: [Sether, Shanna M](#)
Subject: Lander Group 3535 Grand Public Hearing
Date: Monday, May 18, 2015 3:34:45 PM

Hello,

I am a homeowner living at 3515 Harriet Ave and I received notice about the public hearing in regards to the proposed Lander Group development at 3535 Grand. I will be out of town during the hearing, so I wanted to voice my concerns by email.

My primary concern is the request for a variance to reduce the parking requirements.

We are a 1 car family, and street parking on the 3500 block of Harriet is already often at full or nearly full capacity. During snow emergencies, our street parking becomes totally overwhelmed by overflow from Grand Avenue, and 35th & 36th streets - all of which are snow emergency routes. Harriet avenue is one block from Grand, and half the residential structures on our block are apartments and condos. This means lots of cars looking for parking.

Please do not allow The Lander Group to compromise on their responsibilities to the community by giving them a 'pass' when it comes to providing adequate off street parking. As I said, street parking is already at capacity levels.

Thank you,

Thomas Scott
3515 Harriet Ave
Minneapolis

To: The Lander Group, Tony Kriha

From: Carole Megarry- 3536 Pleasant Ave. So. Minneapolis, MN 55408
carole.megarry@gmail.com
612-868-0621 (mobile)

Re: Water collection in alley behind this address

Date: May 18, 2015

I have been asked to summarize my experience with this problem over the 29 years that I have owned this property. (This is a duplex, and my co-owner, Dan Geheren, is now retired and a "snowbird". He will not be back in town in time for the May LNA Meeting or June 1 Planning Commission hearing on this matter. I have talked with him and he joins me in urging that the planned project not go forward without a solution to the water problem being made a part of the project.)

Pictures requested. Unfortunately, I do not have any pictures of the "lake" that forms in the alley when we have excess rain or rapid snow melt. There are probably several reasons I do not. Sometimes the problem has occurred at night, and when it happened during daylight hours I was most focused on standing in the alley with my rake and trying to get the drain to accept more water.

I have however recently walked the alley and taken some measurements and photos which I hope will illustrate the problem.

At its worst, the "lake" extends east about 20 feet onto our two-car driveway, and into our garage, north from the storm sewer opening on the alley (near our driveway) to the closest telephone pole (about 40 feet), south to the door end of the adjacent neighbor's garage (about 68 feet from the storm sewer opening). Because the ground on the west side of the alley is relatively higher, it does not lap onto those lots. I would estimate the depth of the water at about 18 inches in the middle of the alley, based on me wading in water up to my knees trying to get the drain open. See attached aerial photo and markings.

Nobody wants to drive a car through this, Depending where you are in the alley, you would probably head to the high ground on the north end of the block.

Frequency. As for frequency, I am sorry to say I have not kept a record. I believe we were first aware of it the fall or the summer after we purchased the property in September, 1986. I think it is fair to say it has happened at least once every two years since that time. There have also been years when there were several incidents.

Sometimes it happens as a result of a rain deluge, which I believe was the case in summer of 2014. The water was up and stayed up for many hours, I believe for more than 24. I recall contacting Public Works, which was very busy with such problems, and they ultimately came out on a weekend day and did something on the 36th St. storm sewers which apparently relieved the problem. There have been other calls to Public Works and sometimes they have been able to get the drain open with a long steel pole worked in the alley drain. Sometimes it has happened with a rapid snow melt in spring, especially if the storm sewer drain is clogged or partially clogged with ice and debris. Likewise, in fall, if many leaves have collected in the alley, and we get a hard rain, the leaves jam up and block the drain.

I try to keep an eye on the conditions in the alley, and remove ice and leaves which I see them threatening the drain. One of these days we will sell this property, and I don't know who will tend the drain.

Other factors come into play here as well. This is an old 1925 (I assume) concrete alley which slopes significantly downward from the high spot behind 3500 Pleasant to the alley low spot which is the storm drain closest to this property. The block is heavily rental buildings, and to accommodate tenant parking, the alley side of most properties is paved from the alley to the building. There is very little permeable surface available to accept the water. While there is another storm sewer in the alley about 100 feet north of "ours", the two are unable to drain all the water that is thrown off all the paved surfaces.

Assuming your projections are accurate, I am glad to hear you estimate that only about one quarter of the project's water will be drained into the alley. How much is that, however, in a deluge, and how much bigger will the lake be? In view of projections of increasingly erratic weather, it is reasonable to expect heavier rains in the coming years. You may well ask yourself how much of your project's surface area will be covered by this overflow, and how your tenants will like wading out to their vehicles or not being able to use the alley for 24 hours at a time.

I realize we live in an old part of town and that maintaining and improving infrastructure such as storm sewers is a big challenge and headache for city managers, residents and developers. However, it is deeply disappointing to think that after almost three decades of paying property tax in this city (this year alone, over \$5,300), no one seems to want to address this problem.

In general, I share the belief of many neighbors that we already have too many residents and cars in this neighborhood and that, ideally, this space would be garden or park. However I also live in the real world and understand that the lots are zoned for

multi-unit residential and that the city needs the tax revenue. I am also tired of the uncertainty, the weeds, dust, and garbage associated with these lots.

I would be very interested to hear an estimate of the property tax revenue your project will generate. I would hope that City Planning would consider this in deciding whether a catchment basin and pipe would be worth the investment.

Please contact me if you think there is further information which I have perhaps omitted. I attach some photos I recently shot that help illustrate the slopes in the alley, as well as an aerial shot with my notations as to the location and size of the water problem.

cc: Elizabeth Glidden, City Council member

Norma Pietz and LNA Committee heads



Parcel ID: 03-028-24-32-0201

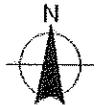
A-T-B: Abstract

Map Scale: 1" ≈ 50 ft.

Owner Name: C M Megarry & D J Geheren

Market Total: \$324,500

Print Date: 5/18/2015



Parcel Address: 3534 Pleasant Ave S
Minneapolis, MN 55408

Tax Total: \$5,303.38
(Payable: 2015)

Property Type: Residential-Two Unit

Sale Price: \$107,000

Homestead: Homestead

Sale Date: 06/1986

Parcel Area: 0.13 acres
5,628 sq ft

Sale Code: Other - See Certificate
Of Real Estate Value
(Crv)

This map is a compilation of data from various sources and is furnished "AS IS" with no representation or warranty expressed or implied, including fitness of any particular purpose, merchantability, or the accuracy and completeness of the information shown.

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Lyndale Neighborhood Association

3537 Nicollet Avenue South

Minneapolis, MN 55408

Phone: (612) 824-9402

www.lyndale.org

May 29th, 2015

Shanna Sether, Senior City Planner

RE: Proposed Variances for 3535 Grand Avenue S

To Ms. Sether,

The Lyndale Neighborhood Association (LNA) is opposed to the requested variances. The Lander Group has presented at several neighborhood meetings about the project. At our May Lyndale Neighborhood Meeting a motion was made to oppose the variances. After a close vote, the motion to oppose the following variances was approved.

- To reduce the off-street parking from the required 22 to 18 spaces
- To increase the percentage of required parking spaces that may be satisfied by providing compact parking stalls
- To reduce the front yard setback along Grand Avenue S
- To reduce the south interior side yard setback for the proposed structure
- To reduce the north interior side yard setback for the proposed parking area
- To reduce the south interior side yard setback for the proposed parking area

The primary concerns expressed at the meeting were regarding the water issue in the alley and a concern that this development will increase the severity of the problem; the lack of parking in the area; and the cost of the units.

Please let me know if you have any questions about the neighborhood's position.

Sincerely,

Mark Hinds
Executive Director

HELP stop 3535 Grand Project!

3535 Grand
Item 6
Commissioners Please Read & Share
Please attend LYNDAL NEIGHBORHOOD MEETING TUESDAY, MAY 26th, 2015
-----at PAINTER PARK (corner of 34th St. & Lyndale Ave. S. Minneapolis)

VOTE NO!! To 3535 Grand Project cast your VOTE on TUESDAY against this harmful project.
CONTEST! LANDERGROUPS proposed project also on JUNE 1ST, Monday 4:30pm at City Hall
[350 S. 5th St. - Downtown Minneapolis across street from Government Center] at MINNEAPOLIS
CITY PLANNING COMMISSION MEETING.

Please Help us contest/stop the construction of a large/oversized development project that stands to threaten our wonderful communities quality of life.

An Invasive project is in the works that is poised to harm/damage a long standing cherished area of the Lyndale Neighborhood in South Minneapolis. The Landergroup (a construction development company) is currently in the midst of seeking approval from both the Lyndale Neighborhood Organization and the City Planning Commission of Minneapolis in order to build a large oversized 24-unit high-end apartment complex on site of a former community garden.

Previous Developers have been met with clear majority disapproval of similar unsuitable projects for this space. Over 90% of petitioners interviewed disapproved of a previously similar project from a developer in 2012 and sided with the preferable option of having a community garden or something else besides the overscaled complex-dwelling projects. For some of the following obvious reasons:

-- GREENSPACE - with more & more development projects (usually non-affordable & overpriced) at the hands of greedy corporate developers, open spaces are looked at strictly as financial opportunities and not with a balanced view that should also include inner-city land that does not necessarily always have to be built on but can remain "as is" with positive results for neighborhoods in the form of community gardens, pocket parks, or simply left as 'greenspace' & natural the way nature intended it to be.

-- AFFORDABILITY OF HOUSING - while a developer may see things simply as: more housing equals more availability, reality is unfortunately not that simple. 1st if people cannot afford it they cannot live there (units scheduled to be \$1,000/mo & up for 1 BR apartments) these are not affordable units. Also with increased rental revenue potential, overall property market value & therefore taxable property value may increase overall for the neighborhood as a whole requiring landlords to raise rents on tenants to cover there increased property taxes. The city however will be all the happy for these changes to occur. Higher property taxes mean more revenue for the politicians and at the expense of our community!

-- PARKING - a very important reason a large apartment complex does not belong in this specific location. This is a concern that is made very obvious given the following considerations.

-----Parking now is already at its saturation point on the proposed sites nearby surroundings.

-----During snow emergency the added parking load will be doubled because of potential 1-sided street parking requirements.

-----with 24 extra units being added to the vicinity many vehicles will be added to the street for a variety of reasons. 18 parking spots will be provided in the proposed building plan, however 1/2 of these are compact stalls (potential street vehicle addition-tenants with non-compact vehicles).

-----City Zoning Code requires new construction provide a 1:1 ratio of parking stalls to apartment units within R5 zoned locales which the site is classified as. Six apartments will be without stalls (potential increase street vehicles).

-----There is a potential for single tenants with more than one vehicle (not excluding motorcycles). (potential increase street vehicles)

-----There is a potential for apartments with more than one tenant. Each tenant may contribute 1 or more

vehicles EACH. (potential increase street vehicles)

-----Gatherings, parties, & social events at any given time will contribute further to street parked vehicles.
(potential increase street vehicles)

-----Deliveries and Repairs will add further (potential increase street vehicles)

-- HARMFUL TO LOCAL SMALL BUSINESSES - With the consumption of what little parking there currently is available in the immediate area; locally owned & operated struggling small businesses positioned directly across street will be at a detriment. On average currently it is very common for people to park in one of the Bus Stops or over a block away due to parking congestion, and this is in the warmer months. With 1-sided street parking in winter the situation is exacerbated further. A Book Store "Present Moment Herbs & Books" has been in operation for over 35 years and the "City Coin Laundrymat" right next door has been doing business there for over 30 years also. Both helping serve the community for 30+ years. With the internet as strong as it is, brick and mortar stores aren't exactly at the top of the food chain. Especially small locally owned varieties such as these given our tough economy. But yet that being said, middle class small business has been considered by many to be "The Life Blood of our Economy." This is what keeps things financially balanced. Remove the middle class and you have no room for economic advancement no matter where you are positioned currently financially. The final result is a system with a few extremely wealthy people in one class on top and the majority of the rest of the population forever struggling at the bottom. HELP FIGHT LANDERGROUPS 3535 GRAND DEVELOPMENT PROJECT NOW! PLEASE PLEASE ATTEND THE MEETING TUESDAY MAY 26TH OF LYNDALE NEIGHBORHOOD GROUP AT PAINTER PARK (34TH STREET & LYNDALE AVE. S. MINNEAPOLIS). YOU WILL BE ABLE TO VOTE AT THIS MEETING!

VOTE TO STOP LANDERGROUP PROPOSED DEVELOPMENT; VOTE TO SUPPORT THE COMMUNITY, AFFORDABLE HOUSING, ACCESSIBLE PARKING, SMALL BUSINESSES, QUALITY OF LIFE, AND OUR ENVIRONMENT.

CAST YOUR VOTE (TUES. MAY 26) TO DEFEAT THE CURRENT DEVELOPERS PROPOSAL AND SAVE OUR COMMUNITY!

AND ALSO -- PLEASE PLEASE ATTEND MEETING ON JUNE 1ST MONDAY 4:30PM CITY HALL (350 S. 5TH ST. - ACROSS FROM GOVERNMENT CENTER) TO CONTEST MINNEAPOLIS CITY PLANNING COMMISSIONS DECISION ON WHETHER OR NOT TO APPROVE THE 6 VARIANCES LANDERGROUP NEEDS TO SECURE CITY APPROVAL TO BUILD WITH THERE CURRENT PLAN.

THANK YOU TO ALL BLESSED PEOPLE TO HELP AND ATTEND, VOTE, & SUPPORT THIS CAUSE TO PRESERVE OUR LONGSTANDING NEIGHBORHOOD & QUALITY OF LIFE FOR EVERYONE. AND THANK YOU TO EVERYONE WHO LIVES IN THE COMMUNITY FOR JUST THAT: LIVING AND THRIVING AND ENJOYING EACH & EVERY PERSONS RIGHT TO A HIGH STANDARD AND HIGH QUALITY OF LIFE.
~PEACE AND LOVE TO YOU~

-----Below is the schedule for the Lyndale Neighborhood meeting Tuesday



Lyndale Neighborhood Meeting
Tuesday, May 26th, 2015
6:30 pm @ Painter Park

- 6:30 pm **Welcome** (Taylor Rub, President)
1. Approval of minutes from April Meeting (pg 13 of April LNN)
 2. Approval of agenda
 3. Introductions
 4. Announcements: (Rub)

— Newspaper deadline for June is: Tuesday, May 26th e-mail to news@lyndale.org

→ **6:35 PM 3535 GRAND AVENUE PROPOSED DEVELOPMENT (LANDER GROUP)**

→ **COMMUNITY VOTE ON PROPOSED VARIANCES**

- 7:00 pm **Lyndale Strategic Plan Discussion** (Taylor Rub)
7:50 pm **Council Member Update** (CM Glidden)
8:00 pm **Adjourn**
People should feel free to stay after the meeting and enjoy the treats
- Announcements**
Fish Fest
Saturday, June 13th
5:00 to 8:30 pm
Garfield Aquarium
- Lyndale Annual Meeting**
Monday, June 22nd
Painter Park (34th & Lyndale)
5:30 to 7:00 pm Community Potluck and BBQ
7:00 to 8:00 pm Annual Meeting
8:00 pm — Announce the winner of the Dessert Contest

~~7777~~ 3535 Grand
Hem C
Commissioner's Rose Road

(Page 1) Good Afternoon members, of City Council and Planning Commission members I, and my friends are here to express Our strong opposition & Concerns regarding the building being proposed for construction by Landers group and College Architects at lot address 3535 Grand Ave South. Minneapolis MN. The plan as described at meetings presented by Landers group, may well be viewed by many Of the Grand Ave Residents as an Impractical and invasive Building in this Neighborhood. I hope the residents , and Committee members and all attending that meeting concur that this Proposal Requires much further Discussion as is necessary for any Commercial/ Multi Family new construction that is proposed for development in the City of Minneapolis. Among the Important issues I think demand careful Consideration include the following—

1) What would be the real impact on residents & Businesses existing & living on Grand Ave. S for a 6 Block radius, of a 3 story building that has limited above ground parking spaces, Inadequate parking for 24 Units? Also as is common knowledge, parking of vehicles on that block is difficult to find on street as often Full most hours, and any more cars additionally would Not be realistic without parking time limits of 20 minutes on both sides. This would Not be Acceptable. The developers of this proposed building are requesting Variances from zoning code requirements. Also they seek setback variances; the setback I believe would be in a different sense, to the persons living along the area blocks due to futile attempts to find Parking should this be built!

2) The overall Concept described of the Proposal at the March 25 meeting as I understood it to be is; for solely 1 Bedroom units leased/ for rent at up to \$1,500 per month {estimate stated}. This clearly is Not intended as an Affordable nor even reasonable monthly rent in this location. Indeed, this isn't Practical in many respects within the Neighborhood. I personally find this aspect Objectionable as it does NOT respect the existing Communities needs for truly affordable rent in housing.

3) An issue related to #2 of the list, is that this city does need more available low income housing. As has been reported for 8 to 10 years there is an ongoing crisis in low vacancies though out MPLS. This is especially true for lower income citizens here. What I would approximate to be affordable rates would be between 740 and 800 per month and in recent past that would be considered at high end of rates by Housing indexes/averages. At present there are hundreds of people in the city searching for affordable 1 or 2 Bedroom apartment and too often Not finding one affordable in 6 months' time and the must look elsewhere in Search for something perhaps out of the city. This has been the case with friends I've known, and is part of the Inequity referred to in the news reports. If the purpose of creating new housing is to be for mixed income residents of Mpls. in fair market values, constructing yet Another exclusively High rent complex is flat out Not in keeping with that Goal or purpose. There are also many places already built with Vacancies for those of means that desire them in the

Uptown vicinity. Concluding this point, there is not a particular demand for another High rent apartment complex in many of the peoples' view of those we have conversed with in Lyndale neighborhood area.

4) This plan does not seem well thought out in another respect in my view. I have frequented this area for many years about 30 as I often shop at Present Moment Books. I've worked there, attended classes there over the years, such as Yoga and meditation, Events. Thousands of people have also been going to this store and Educational center, since it in 1980. I mention this to say that many persons are thankful to have learned about Natural medicine, herbs, and dozens of other important topics of Study. To state this directly, a building as the one described will definitely alter in Negative way the traffic flow, Parking, and Whole experience of those visiting, shopping and living along blocks of Grand Ave. The residents that live there now do NOT approve of this proposal as many as perhaps 200 + if asked will not look upon this as Favorable construction in the community. Those that reside here already, moved here to live away from a Mall to Mall like urban, dense, hectic setting as other areas which have been recently "developed" are now often Difficult to live and survive in unless one leaves that environment for hours per Day to escape to peaceful Page 2 of 3 Page 3 of 3 surroundings, as is necessary to feel at ease. There is a more relaxed, and feeling of ease for the most part along Grand Ave @ blocks in this community. This is the way people prefer to live; perhaps there is a location this proposed development would be tolerated, certainly not our Community.

4) Another concern has to do with Zoning codes, requirements. These are established for a reason it is understood to be standards intended to keep construction within certain fixed dimensions and practical agreed upon limits. It is not sound planning of development practice, to permit any developer to play fast and loose with requirements that have been deemed to be required & important to adhere to by law.

Landers group contends that parking is not a problem, while in actuality it is presently difficult to park on street. Their proposed building does not have Sufficient parking space, as many spaces in rear are designed for Compact, and not even one per Unit, also on most days any number of other Vehicles will be looking to park: E.g. : Visitors to building, persons with two cars, repair/maintenance trucks, and persons coming for get together/parties in building, ETC. There seems to be No way of making this a viable 'plan" when you do a logical assessment of this Crucial factor.... Street or in the rear lot for up to 30 more Vehicles potentially circling to find that Space On Street must drive out of blocks near 35/36th and Grand Ave To Park farther away!!

To Conclude my Remarks—Myself and Many residents of Lyndale Neighborhood and along Grand Ave insist That you will Make the Responsible Decision in Denying Landers group the Six 6, Variances they are asking for to build a Building that Does not suit the Needs of the Neighborhood!! Many here gathered Stand Opposed to this Proposal, and thank you for Careful reflection regarding this Matter!