

# V. Specific Street Analysis, Concept Plans and Recommendations

Chapter five provides a street-by-street detailed analysis for each specific Heritage Street identified in this plan. This chapter also provides design recommendations based on the existing site conditions, planning policies, guidelines, primary project goals and objectives, historic context and precedent studies defined in the previous chapters.

For each specific Heritage Street, a detailed analysis graphic depicts the existing conditions of the streets, sidewalks, loading docks and public ROW areas. Each street-by-street analysis graphic identifies the following:

- Existing Right of Way (ROW) dimensions
- Current Street Width
- Current Street Average Daily Traffic Counts (ADT's)
- Current on-street parking
- Historic pavement conditions (pavement quantities, visual appearance of pavements, identification of drainage problems, and street crown conditions)
- Sidewalk conditions
- Accessibility issues and concerns

A series of general recommendations for each specific Heritage Street has been created with more street specific recommendations also defined. The following is a list of the primary recommendations for improvement of the streets:

## Primary Heritage Street Recommendations

- Narrow all streets where possible throughout the district in compliance with MSA standards and Access Minneapolis.
- Consider providing street trees on all non - Freight Streets. Integrate stormwater management into tree planting areas. Future tree plantings associated with Heritage Street projects will be considered on a case-by-case basis by the HPC.
- Provide accessible walks on both sides of the specific Heritage Streets. Sidewalks will be a minimum of 5'-6" but most will be at least 6'-0" wide.
  - Walks will utilize a standard City curb design (B6-24). This recommendation will serve to protect integrity of historic loading docks throughout the district.
  - Sidewalks around loading docks should be designed to meet City standards which is 8'-0" sidewalks. This provides sufficient space to place signs and light poles behind the back of curb. Each construction project should verify the opportunity to widen sidewalks to meet this standard. Away from loading docks, a minimum 12' sidewalk and planting/ furnishing zone between the face of curb and property line is recommended.
- Maintain a majority of on-street parking throughout the district except along 7th Avenue North.
- Recommendations call for the removal, cleaning and re-installation of historic pavements on all heritage streets except for along 5th Avenue North.
  - It is recommended that new bricks not be used for the reconstruction of any Heritage Street. Do not mix new pavers with any historic bricks when restoring or reconstructing streets.
  - A verification of historic brick quantities needs to occur prior



to the construction or restoration of a specific Heritage Street. If an insufficient quantity of bricks is available for a reconstruction or restoration project then City needs to verify if any other sources of bricks exist to augment the paver quantities.

- Recommendation for 3rd Street North is the removal and replacement of historic pavements within the drive lanes with concrete to replace the parking lanes.
- Historic street pavers will be removed from all intersections except at 6th Avenue North and 3rd Street North.
- Provide concrete crosswalks and ADA compliant pedestrian ramps.
- Add pedestrian-scaled lighting and other streetscape elements to improve neighborhood livability.

### Preferred Heritage Street Concept Plans

The following section identifies the site specific recommendations for each of the defined Heritage Streets.

The recommendations follow the primary street recommendations identified in the previous section but provide specific recommendations that respond to the unique characteristics of each of the Heritage streets.

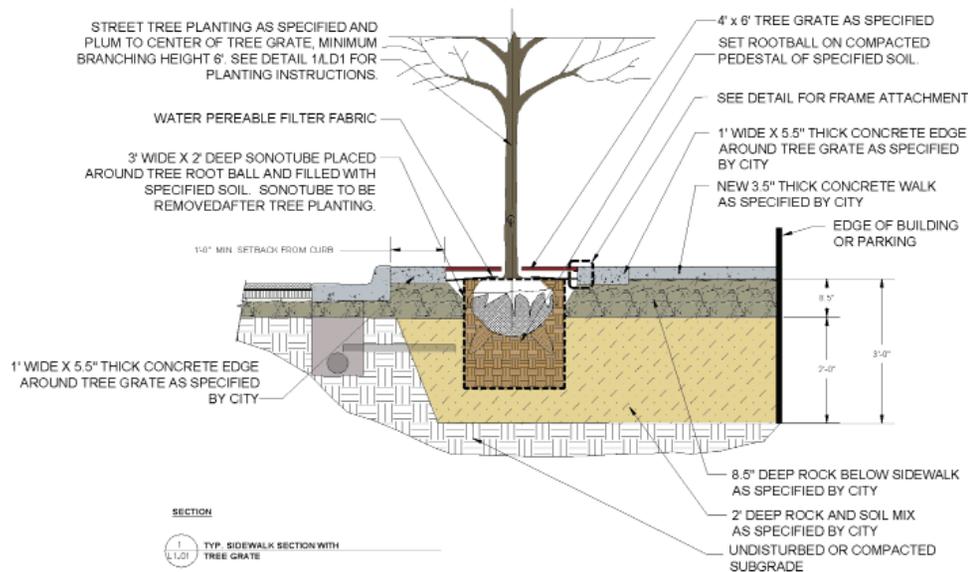
### Additional Project Recommendations

In addition to the primary recommendations identified for each of the Heritage Streets, a secondary list of recommendations has been created and should be considered on a case-by-case basis when there is an opportunity to rehabilitate or reconstruct a specific Heritage Street.

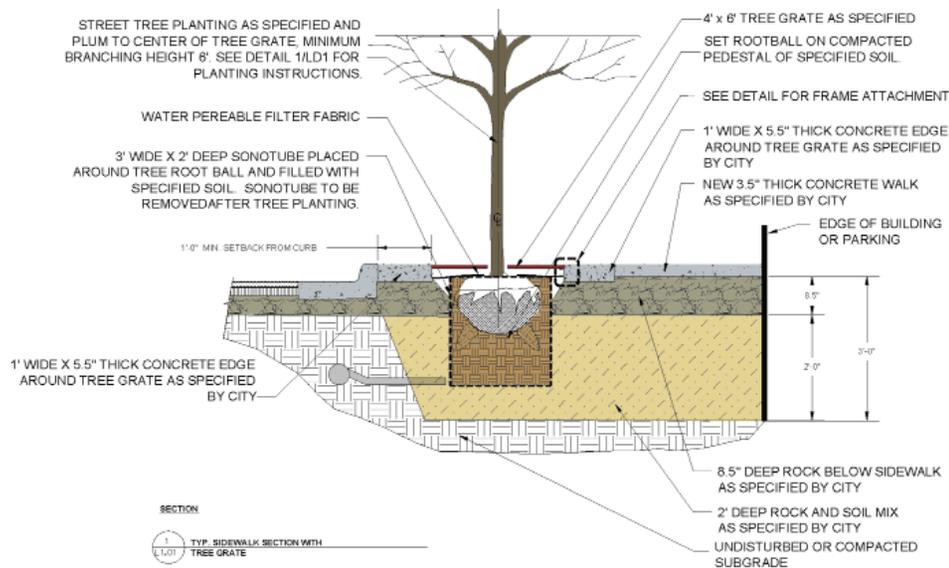
The following is a list of the secondary recommendations:

#### Street Trees

- The Heritage Street Plan recommends adding street trees on all streets within the project area that are not defined as Freight Streets. The primary streets we would define for street trees are along 3rd Street North, and portions of 6th and 7th Avenues.
- Future Heritage Street reconstruction projects should consider the location of street trees on a project-by-project basis with final review and approval by the HPC.
- Street trees should be planted in groups of three to five trees that are clustered together along the street edge. Approximate interval of the tree plantings within the clusters shall be 18'-0" on-center.
- Street trees should be planted to maintain visual access to the front of historically significant buildings along the street. It is recommended that street trees should be located between building bays or centered between buildings. Final street tree locations should be approved by the HPC.
- Street trees should be more columnar in form to prevent large arching branching structure that would grow over the street and block the significant view corridor to Downtown. Appropriate street tree species should be coordinated with the City of Minneapolis Park and Recreation Board Forester.



Street Tree Infiltration Detail A



Street Tree Infiltration Detail B

- The proposed street tree plantings should integrate stormwater management. The street trees should be planted in accordance with the recommended street tree planting detail found on this page.
- Coordinate street tree plantings with Public Works staff, Minneapolis Park Board and the Heritage Preservation Commission.

### Interpretation of Historic Spur Rail Lines

- Recommendations are to discuss and analyze in greater detail the opportunity and process to interpret the historic rail spur corridors that exist within the alley rights-of-way within the project area.
- Interpret the entire rail corridors rather than just portions of the rail corridors (say at a street crossing).
- Reinstall actual metal rail sections rather than trying to interpret the rails with a different material (concrete, bricks, etc.).

### Street Lights

- Conform to existing City Policies for lighting of Downtown urban districts. See Access Minneapolis Plan Chapter 7 - Objective 4.3.
- Follow City recommendations for low-to-mid level ornamental lighting types, styles, spacing and lighting quality.
- Consider Crime Prevention Through Environmental Design (CPTED) for the design and location of pedestrian lighting systems.
- Consider sustainable lighting alternatives.



### Stormwater Management

- Meet the requirements identified by the City of Minneapolis and the Mississippi Watershed Management Organization (MWMO).
- As projects are identified look for all available opportunities to improve and incorporate stormwater management into the project area while still adhering to historic guidelines.
- Utilize trees as stormwater infrastructure.

### District Energy/ Snow melt

- Opportunity exists to incorporate district energy into the project area
- Future projects should consider incorporating piping below sidewalk areas for the purpose of snow melting of sidewalks in the project area
- Determine phasing and possible coordination issues related to the incorporation of a snow melt system with Hennepin County Environmental Services.

### Snowmelt Service

- In addition to heating service, customers of a North Loop district energy system may also take advantage of its waste heat-generated hot water service for snowmelt. Snowmelt service provides a number of customer benefits:
  - Eliminates the need to maintain equipment and contracts for conventional snow removal;
  - Reduces wear and tear on sidewalks, parking areas, and other traffic platforms;
  - Provides clean, ice-free traffic areas at economical rates.

- Effective snow and ice removal will make a lasting financial impact on the development of the North Loop community. Walkways that are snow, ice, and salt-free will increase the comfort and confidence of the community. The potential of heating streets, sidewalks, malls, plazas, rail lines and benches will also substantially increase the comfort and safety for the entire community.



### **Heritage Street Analysis and Concept Plans**

The following section identifies the existing conditions and provides site specific recommendations for each Heritage street defined in this plan.

The recommendations follow the primary street recommendations identified in the previous section but provide specific recommendations that respond to the unique characteristics of each street.

## Existing Conditions

### 3rd Street North (10th Avenue N to 8th Avenue North)

- Existing ROW along 3rd Street N is 82.45' wide
- Current Street width is 52' wide
- ADT's for 3rd Street are 1700
- There is currently on-street parking on portions of both sides of the street
- It is estimated about 65% of the historic street pavement is exposed and roughly about 35% of the pavement is covered with bituminous patching.
  - Many visible areas of pavement settling that collect stormwater
  - Estimated 75% of visible pavement appears to be in good condition
- Sidewalks on both sides of the street between 10th Avenue and 9th Avenue are in fair condition. Sidewalk on the north side of the street is interrupted by numerous driveways leading to parking areas.
- 900 N 3rd Street building has about 61' lineal feet of sidewalk missing along 9th Avenue. There is roughly 32' of sidewalk missing along 3rd Street N in front of the building.
- Sidewalks on both sides of the street between 9th Avenue and 8th Avenue are in poor condition.

- ADA accessibility

- Accessibility on both sides of the street between 10th Avenue and 9th Avenue is considered fair. The missing section of sidewalk in front of the 900 N 3rd Street building needs to be completed.
- Accessibility on both sides of the street between 9th Avenue and 8th Avenue is considered poor.

## Existing Conditions

### 3rd Street North (8th Avenue N to 7th Avenue North)

- Existing ROW along 3rd Street N is 82.45' wide
- Current Street width is 52' wide
- ADT's for 3rd Street are 1700
- There is currently on-street parking on portions of both sides of the street
- It is estimated about 70% of the historic street pavement is exposed and roughly about 30% of the pavement is covered with bituminous patching.
  - Many visible areas of pavement settling that collect stormwater
  - Estimated 70% of visible pavement appears to be in good condition
- Sidewalks on both sides of the street between 8th Avenue and 7th Avenue are in poor condition. Sidewalk on both sides of the street is interrupted by numerous driveways leading to parking areas.
- ADA accessibility
  - Accessibility on both sides of the street between 8th Avenue and 7th Avenue is considered poor.

Existing Photos



1 Excessive areas of road patching between 9th and 10th Avenue



2 Wider ROW offers opportunities to reduce road width or provide on street bike lanes



3 Extensive areas of exposed pavers  
 • pavers in fair condition  
 • extensive areas of settling  
 • large areas of utility cuts with bituminous overlay



4 Complete sidewalk construction along 3rd Street N at 9th Avenue



5 Existing sidewalk in fair to poor condition



6 Existing curb and sidewalk in fair to poor condition  
 • areas of standing water

- A Concrete loading dock not ADA Accessible
- B No pedestrian ramp
- C No sidewalk connection
- D Sidewalk in poor condition
- E Areas of visible standing water

- F Multiple driveways create pedestrian/vehicular conflict
- G Damaged driveway
- H Bituminous patch overlay on road
- I Sidewalk in good/fair condition
- J Active delivery areas with trucks

- Pedestrian Door to Building
- Overhead delivery Door
- Active Loading Dock
- Active Loading Area

Existing Issues/ Concerns

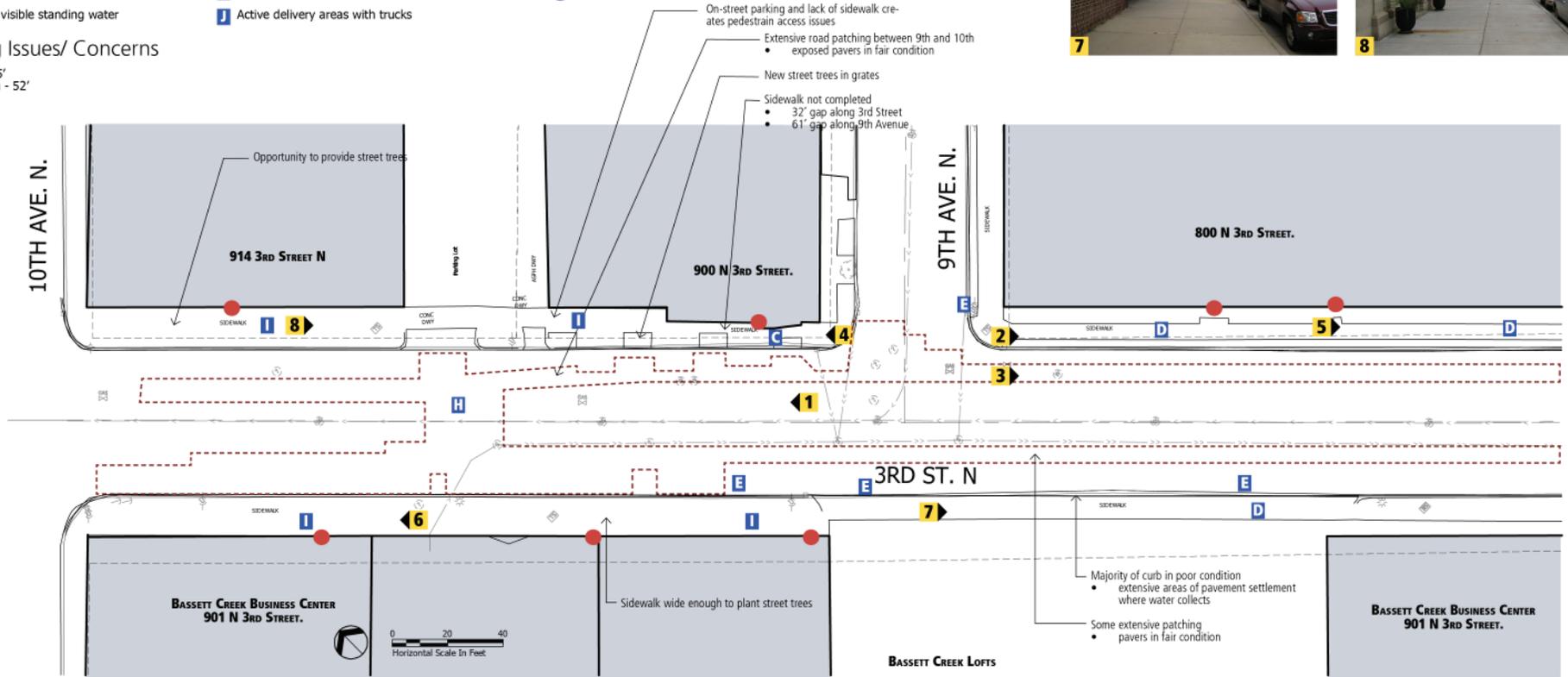
ROW - 82.45'  
 Street Width - 52'  
 ADT - 1700



7



8



Areas of street patching - Approximately 9,900 sf  
 38 parallel parking stalls

3rd Street Existing Conditions  
 WAREHOUSE DISTRICT HERITAGE STREET PLAN

Warehouse District Heritage Street Plan



**1** Excessive areas of road patching. Paver settling causes water to collect in certain areas. Variety of different materials has been used in patching



**2** Wide ROW offers the opportunity to adjust lane widths and sidewalk areas.



**3** Most sidewalks in poor condition throughout this section of 3rd Street N



**4** Sidewalks in poor condition and unfriendly to pedestrians. Most walks not ADA accessible



**5** Damaged pavers adjacent to utilities



**6** Sidewalks and infrastructure in poor condition

- A** Concrete loading dock not ADA Accessible
- B** No pedestrian ramp
- C** No sidewalk connection
- D** Sidewalk in poor condition
- E** Areas of visible standing water

- F** Multiple driveways create pedestrian/vehicular conflict
- G** Damaged driveway
- H** Bituminous patch overlay on road
- I** Sidewalk in good/fair condition
- J** Active delivery areas with trucks

- Pedestrian Door to Building
- Overhead delivery Door
- Active Loading Dock
- Active Loading Area

### Existing Issues/ Concerns

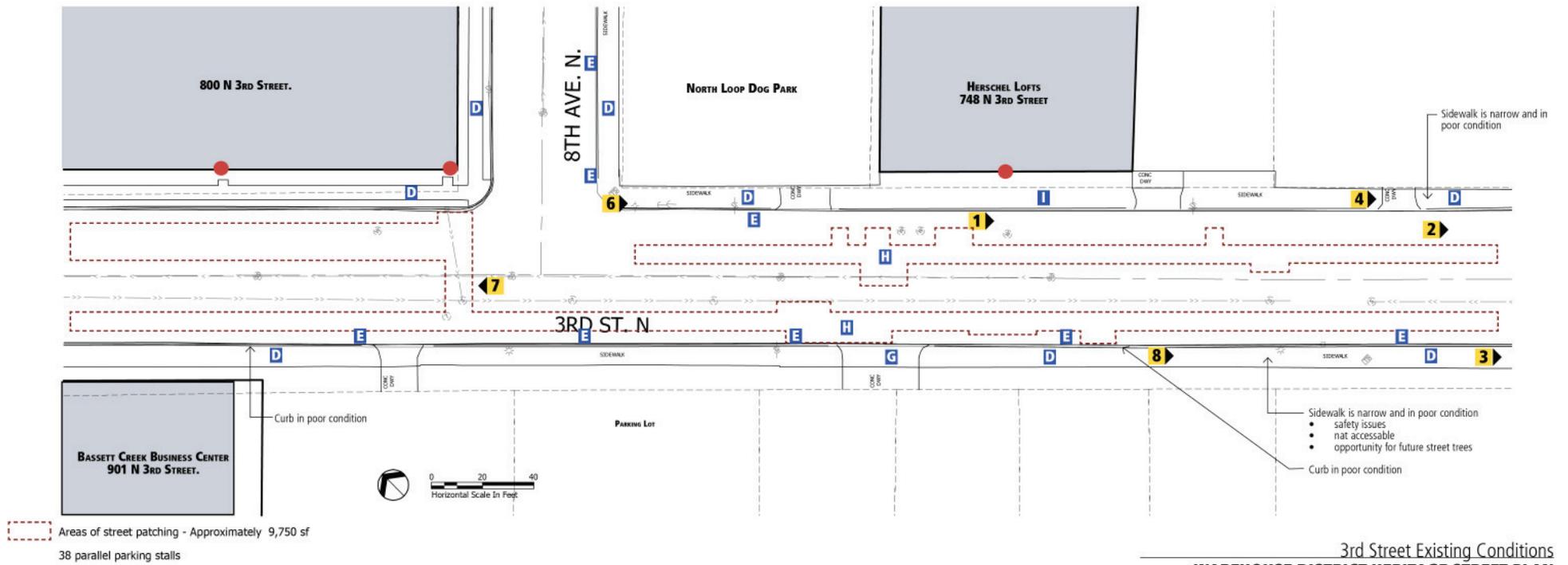
ROW - 82.45'  
Street Width - 52'  
ADT - 1700



**7** Some areas of pavers along this section of road are in good condition



**8** Sidewalks in poor condition and not accessible



Existing Photos



1 Excessive areas of pavement patching



2 Narrow sidewalks in poor condition are typical through this section or road



3 Extensive areas of pavement patching occur near intersection at 7th Avenue



4 Sidewalks and curb in poor condition



5 Areas of pavement settling



6 Wide ROW and street width offers opportunity to narrow street and widen sidewalk areas



7 Sidewalk in fair to poor condition  
• opportunity to add street trees

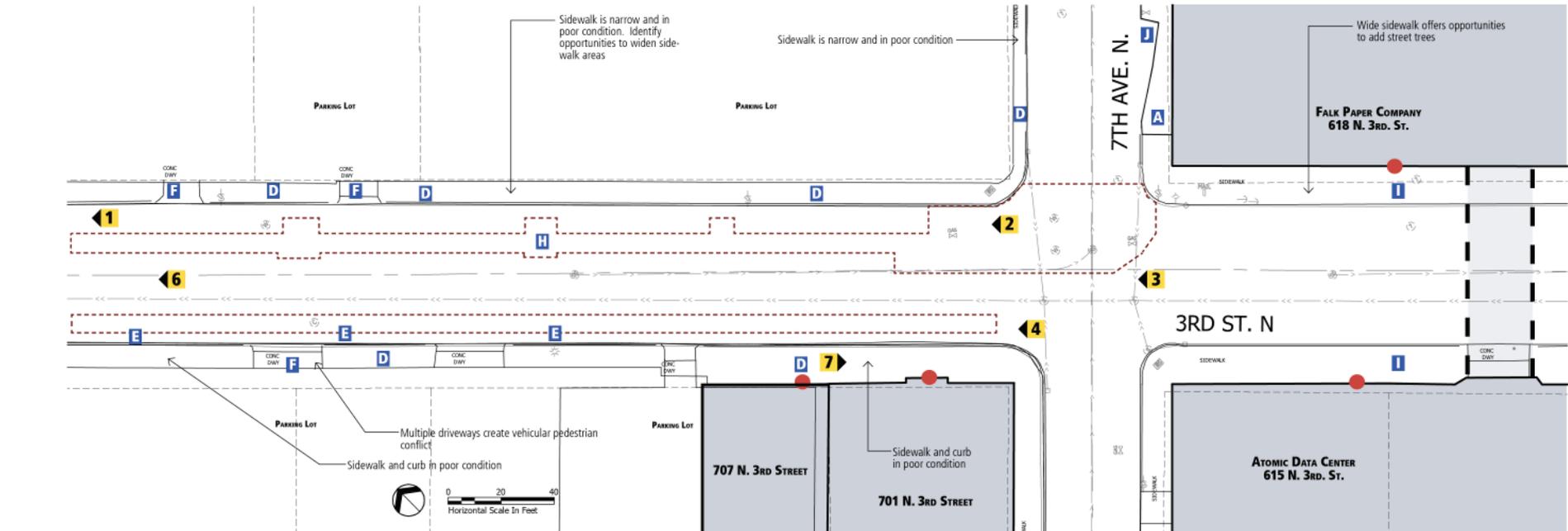
- A Concrete loading dock not ADA Accessible
- B No pedestrian ramp
- C No sidewalk connection
- D Sidewalk in poor condition
- E Areas of visible standing water

- F Multiple driveways create pedestrian/vehicular conflict
- G Damaged driveway
- H Bituminous patch overlay on road
- I Sidewalk in good/fair condition
- J Active delivery areas with trucks

- Pedestrian Door to Building
- Overhead delivery Door
- Active Loading Dock
- Active Loading Area

Existing Issues/ Concerns

ROW - 82.45'  
Street Width - 52'  
ADT - 1700



Areas of street patching - Approximately 4,800 sf  
33 parallel parking stalls

3rd Street Existing Conditions



## Primary Recommendations 3rd Street North (10th Avenue N to 7th Avenue North)

- ROW width: Existing ROW along 3rd Street N is 82.45' wide. Final recommendation to maintain the width of the existing ROW.
- Street width: Current Street width is 52'-0" wide. Final recommendation to narrow the existing street to 42'-0" wide. The proposed street section would be (2) 11'-0" drive lanes and (2) 10' wide parallel parking lanes on both sides of the street.
- Brick pavers: Historic brick pavers along 3rd Street N should be removed, cleaned and reinstalled to the proposed new 11'-0" wide drive lanes in the center of the new street cross-section. This recommendation would require approximately 32,768 sf. of pavers to be reinstalled.
- On-street parking: Concrete should replace the historic brick pavements in the 10'-0" wide parking lanes on both sides of the street. Proposed parking lanes are 10'-0" wide x 20'-0" long. There are approximately 99 parking stalls that will remain as part of the proposed street improvements.
- Sidewalks: Recommendation to widen sidewalk areas on both sides of the street by approximately 5'-0". The proposed sidewalks will be approximately 20'-0" wide and should be designed to meet City design standards and be ADA compliant.
- Crosswalks: Recommendation to provide new concrete crosswalks and ADA accessible pedestrian ramps at all street intersections.
- Pedestrian ramps: Pedestrian ramps shall conform to City of Minneapolis, ADA Standards for Accessible Design, and PROWAAC standards.
- Street trees: Recommendation to plant new street trees along the entire length of this section of 3rd Street.
  - Street trees should be planted to maintain visual access to the front of historically significant buildings along the street.
  - Street trees should be more columnar in form to prevent large arching branching structure that would grow over the street and block sign cant view corridor to Downtown. Street trees should be from the following list of appropriate species
  - Coordinate street tree plantings with Public Works staff and Minneapolis Park Board.
- Stormwater management: The proposed street tree plantings should integrate stormwater management. The street trees should be planted in accordance with the recommended street tree planting detail found on page V-39.



View west along 2nd Avenue N towards the River



View of historic paved street, sidewalk area and sandstone curb along 2nd Avenue N



View along loading dock on northwest edge of 2nd Avenue N



Close up view of historic Purington pavers placed in 2nd Avenue roadway



Wide view of 2nd Avenue N. Paving is generally in good condition and remains very level throughout the entire roadway

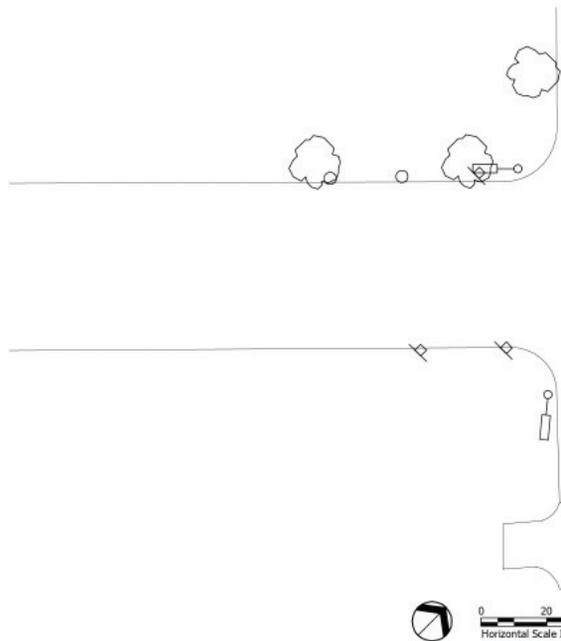
Concrete loading dock not ADA Accessible  
 pedestrian ramp  
 sidewalk connection  
 walk in poor condition  
 as of visible standing water

**Existing Issues/ Concerns**

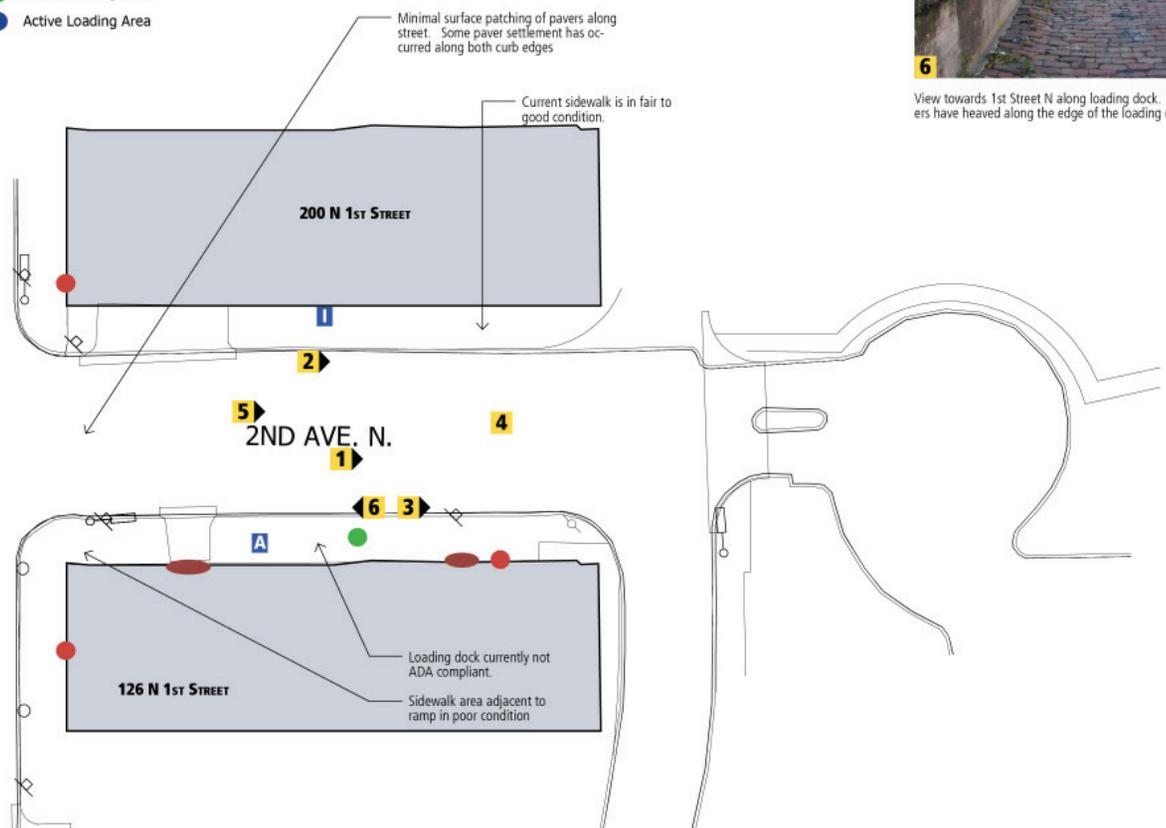
80'-0"  
 Width - 48'-0" typ.  
 Undefined (Not a Municipal State Aid)

- F** Multiple driveways create pedestrian/vehicular conflict
- G** Damaged driveway
- H** Bituminous patch overlay on road
- I** Sidewalk in good/fair condition
- J** Active delivery areas with trucks

- Pedestrian Door to Building
- Overhead delivery Door
- Active Loading Dock
- Active Loading Area

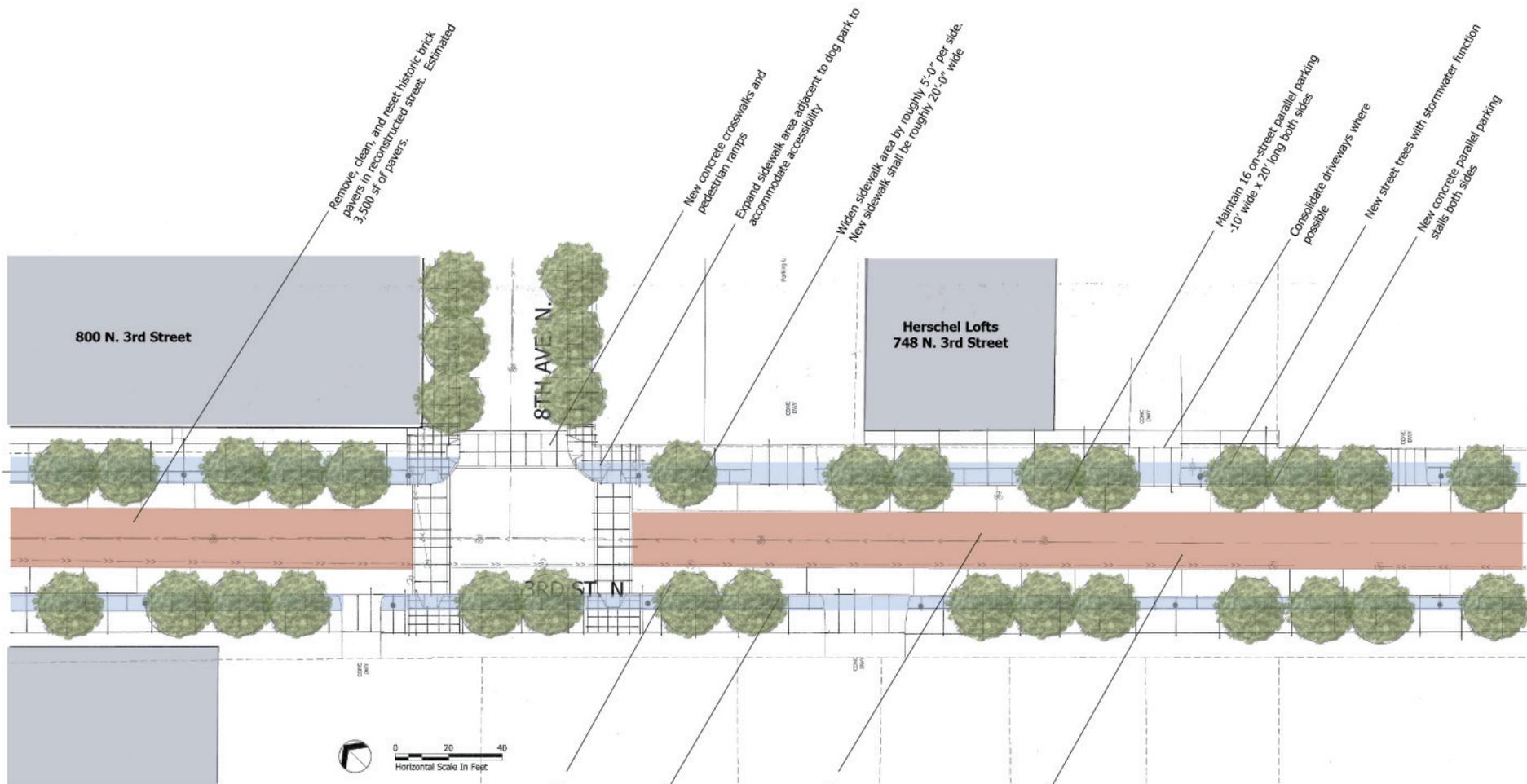


1ST ST. N.



View towards 1st Street N along loading dock. Some pavers have heaved along the edge of the loading dock area

2nd Avenue Existing Conditions  
**WAREHOUSE DISTRICT HERITAGE STREET PLAN**



Existing ROW: 82'-4"  
 Current Street Width: 52'-0"  
 Proposed Street Width: 42'-0"

- Key
- Historic Pavers
  - New Accessible Sidewalk  
-with City standard B6-24 curb
  - Existing Loading Docks
  - Active Loading Dock

Maintain 22 on-street parallel parking stalls

Widen sidewalk area by roughly 5'-0" per side. New sidewalk shall be roughly 20'-0" wide

42' wide Street (2) 11' drive lanes and (2) 10' wide parallel parking lanes on both sides of the street

Remove, clean, and reset historic brick pavers in reconstructed street. Estimated 10,198 sf of pavers.

Remove, clean, and reset historic brick pavers in reconstructed street. Estimated 3,500 sf of pavers.

New concrete crosswalks and pedestrian ramps

Expand sidewalk area adjacent to dog park to accommodate accessibility

Widen sidewalk area by roughly 5'-0" per side. New sidewalk shall be roughly 20'-0" wide

Maintain 16 on-street parallel parking -10' wide x 20' long both sides

Consolidate driveways where possible

New street trees

New street trees with stormwater function stalls both sides

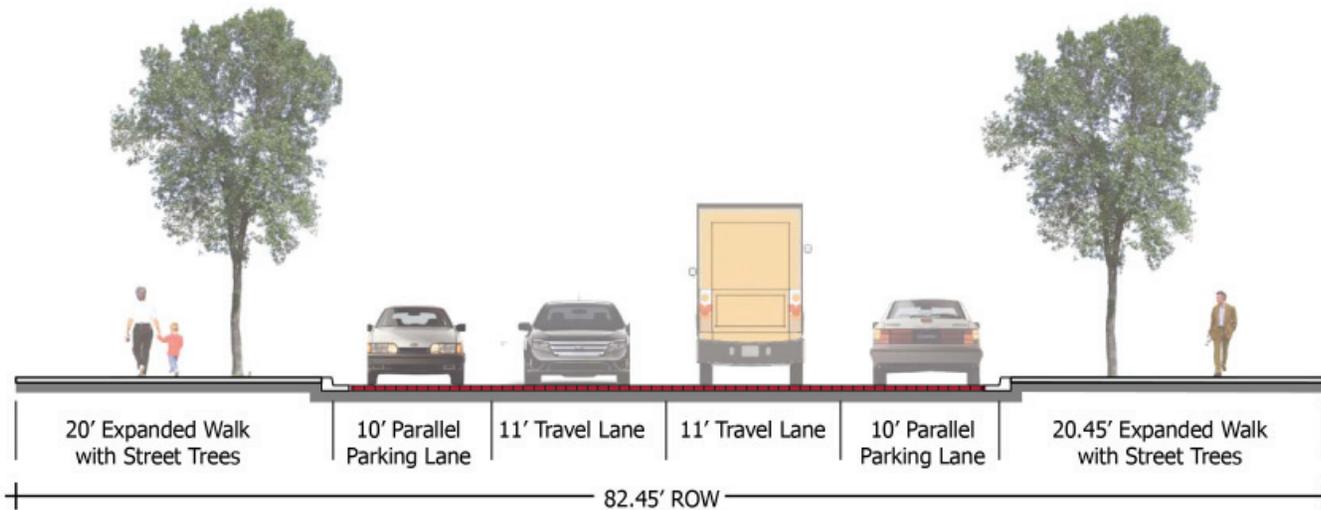


Existing view of 3rd Street looking east

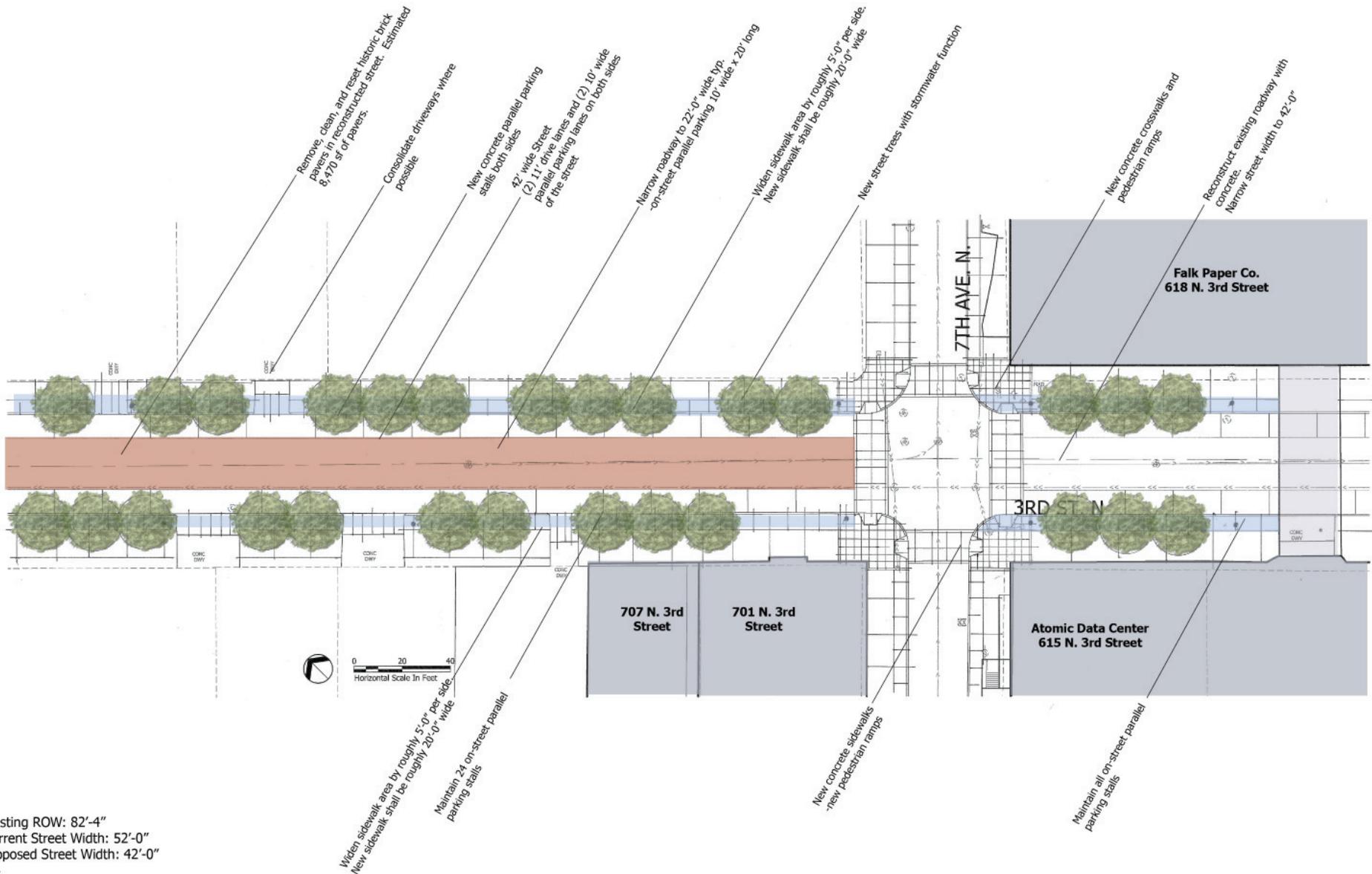
- Expand existing sidewalk and add street trees
- New concrete parallel parking stalls
- Remove, clean, and reset historic brick pavers on majority of 3rd Street.



Proposed view of 3rd Street looking east



Proposed Cross-Section  
3rd Street North (10th Avenue North to 7th Avenue North) looking east



Existing ROW: 82'-4"  
 Current Street Width: 52'-0"  
 Proposed Street Width: 42'-0"

- Key
- Historic Pavers
  - New Accessible Sidewalk -with City standard B6-24 curb
  - Existing Loading Docks
  - Active Loading Dock

3rd Street Concept Plan  
**WAREHOUSE DISTRICT HERITAGE STREET PLAN**



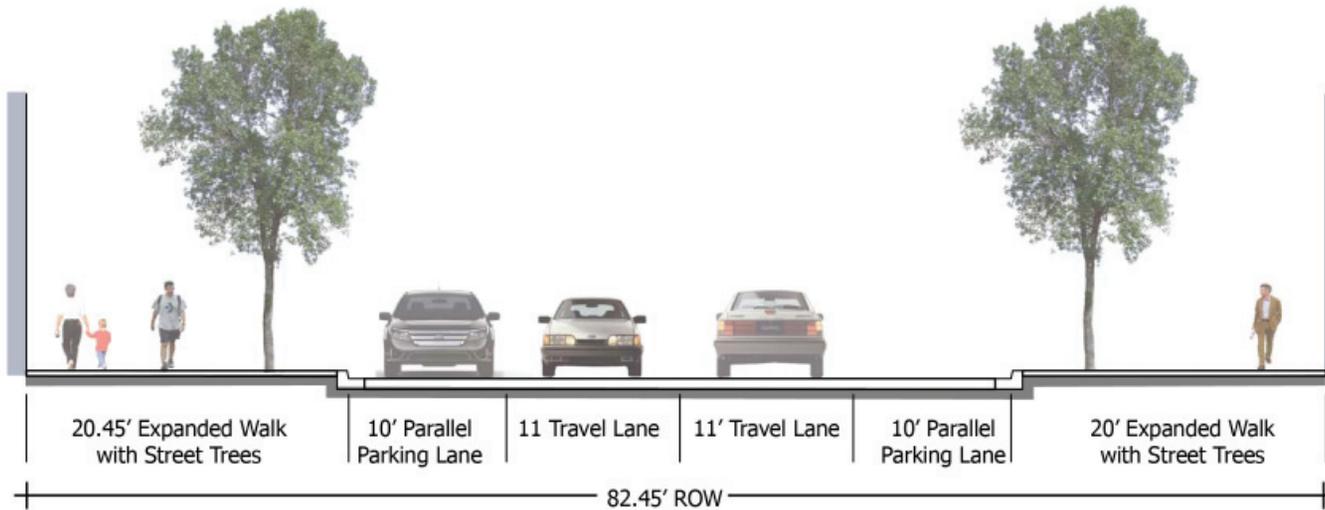


Existing view of 3rd Street looking west

New concrete parallel parking stalls  
 Remove, clean, and reset historic brick pavers on majority of 3rd Street  
 Expand existing sidewalk and add street trees



Proposed view of 3rd Street looking west



Proposed Cross-Section  
 3rd Street North (7th Avenue North to 5th Avenue North) looking west



## Existing Conditions

### 3rd Street North (7th Avenue N to 6th Avenue North)

- Existing ROW along 3rd Street N is 82.45' wide
- Current Street width varies between 54' and 52' wide
- ADT's for 3rd Street are 1700
- There is currently on-street parking on portions of both sides of the street.
- There are no historic pavements exposed along 3rd Street N.
- Sidewalks on both sides of the street between 7th Avenue and 6th Avenue are in good condition.
- ADA accessibility
  - Accessibility on both sides of the street between 7th Avenue and 6th Avenue is considered good.

Existing Photos



1 View North East towards 6th Avenue N. The current street is very wide and there are no visible pavers.



2 Wide sidewalks exist along 3rd Street adjacent to the Duffy Paper Building. The opportunity exists to enhance the existing ROW with street trees



3 Wide sidewalks exist along both sides of 3rd Street N.

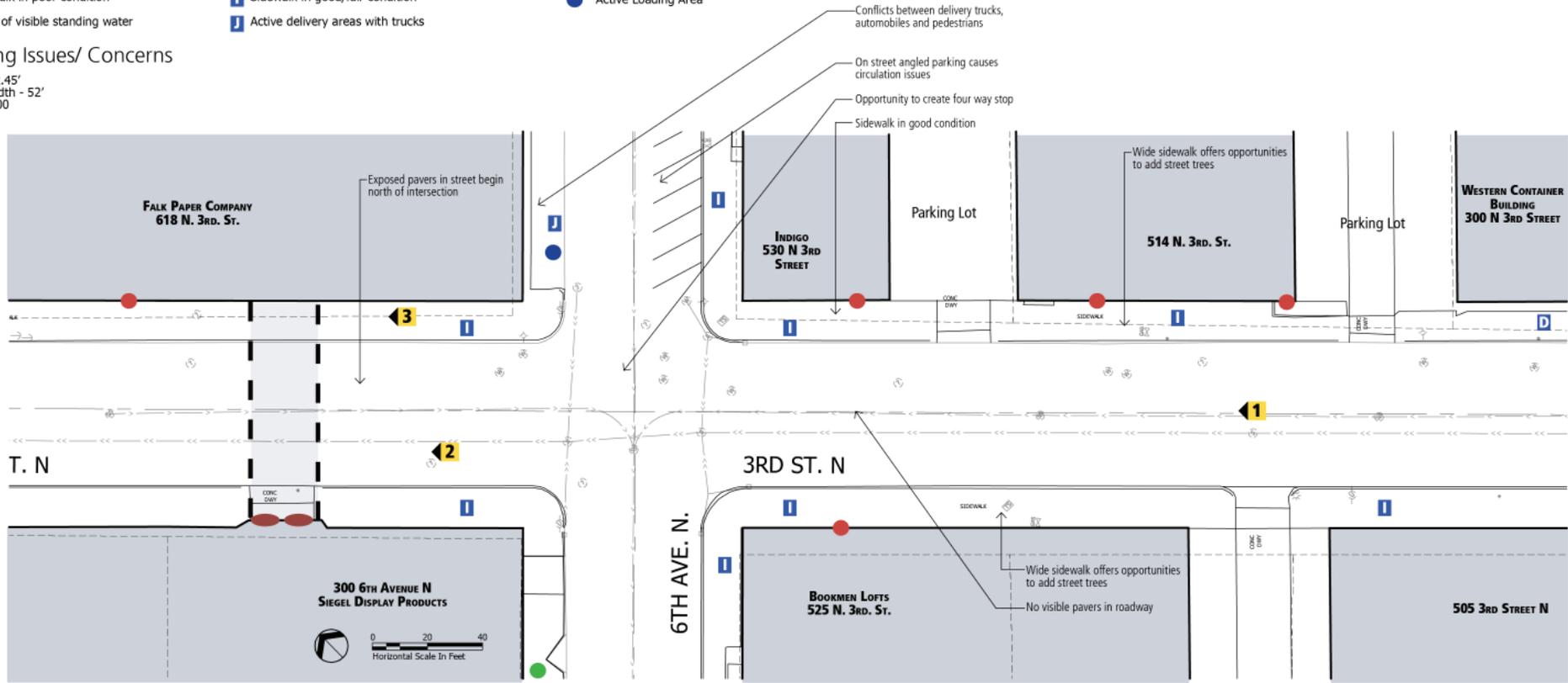
- A** Concrete loading dock not ADA Accessible
- B** No pedestrian ramp
- C** No sidewalk connection
- D** Sidewalk in poor condition
- E** Areas of visible standing water

- F** Multiple driveways create pedestrian/vehicular conflict
- G** Damaged driveway
- H** Bituminous patch overlay on road
- I** Sidewalk in good/fair condition
- J** Active delivery areas with trucks

- Pedestrian Door to Building
- Overhead delivery Door
- Active Loading Dock
- Active Loading Area

Existing Issues/ Concerns

ROW - 82.45'  
Street Width - 52'  
ADT - 1700



30 parallel parking stalls

3rd Street Existing Conditions

September 13, 2011

Warehouse District Heritage Street Plan

HERITAGE STREET ANALYSIS AND CONCEPT PLANS



## Existing Conditions

### 3rd Street North (6th Avenue N to 5th Avenue North)

- Existing ROW is along 3rd Street N is 82.45' wide
- Current Street width varies from 54' to 52' wide
- ADT's for 3rd Street are 1700
- There is currently on-street parking on portions of both sides of the street.
- There are no historic pavements exposed along 3rd Street N.
- Sidewalks on both sides of the street between 6th Avenue and 5th Avenue are in fair condition. Sidewalks and curb on the north side near 5th Avenue are in poor shape.
  - Sidewalk along the entire south side of the street is in good shape. Curb on south side near 5th Avenue in poor shape. Historic granite curb remains in short sections at the east end of the street.
- ADA accessibility
  - Accessibility on both sides of the street between 6th Avenue and 5th Avenue is considered fair to good.

Existing Photos



1 Some existing granite curbs remain



2 Sidewalks in fair condition on west side of street



3 Entire street has been paved over  
• no visible pavers



4 Sidewalks in fair to poor condition

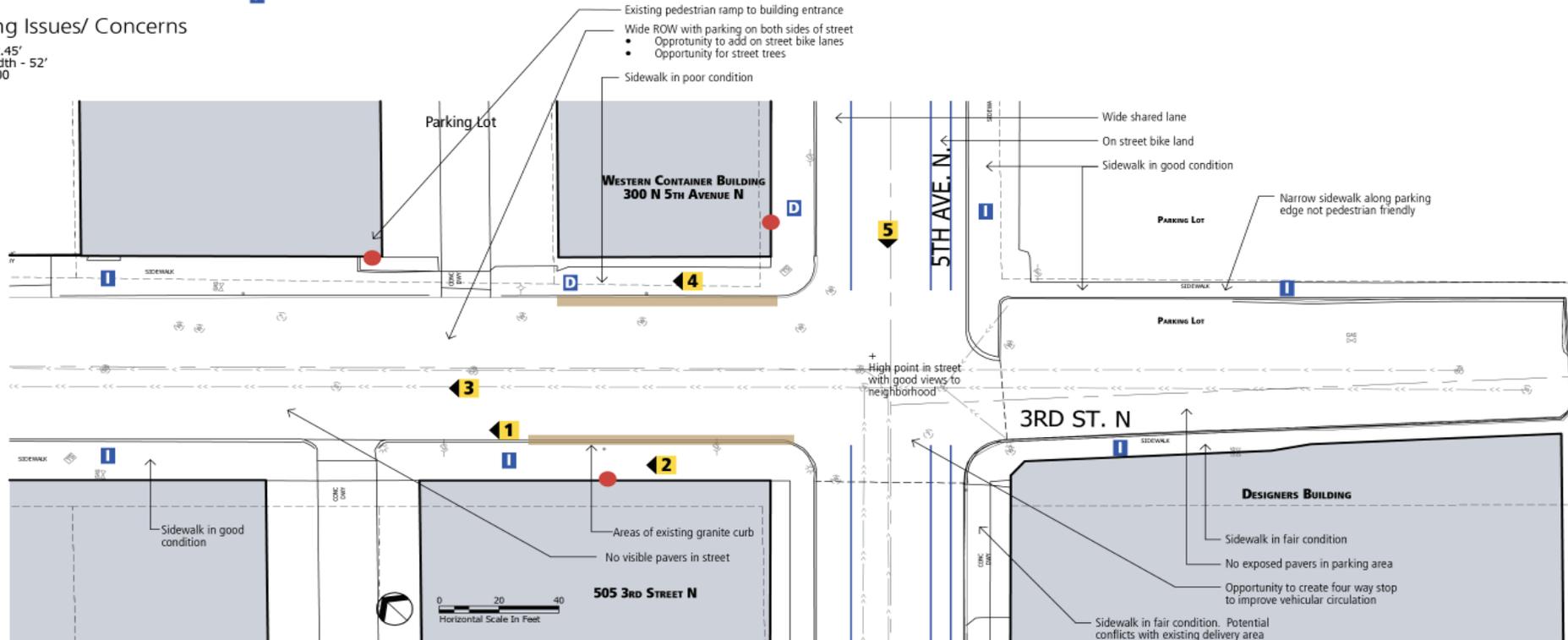


5 View at 3rd Street/ 5th Avenue Intersection. Good views towards Ford Center

- A** Concrete loading dock not ADA Accessible
- B** No pedestrian ramp
- C** No sidewalk connection
- D** Sidewalk in poor condition
- E** Areas of visible standing water
- F** Multiple driveways create pedestrian/vehicular conflict
- G** Damaged driveway
- H** Bituminous patch overlay on road
- I** Sidewalk in good/fair condition
- J** Active delivery areas with trucks
- Pedestrian Door to Building
- Overhead delivery Door
- Active Loading Dock
- Active Loading Area

Existing Issues/ Concerns

ROW - 82.45'  
Street Width - 52'  
ADT - 1700



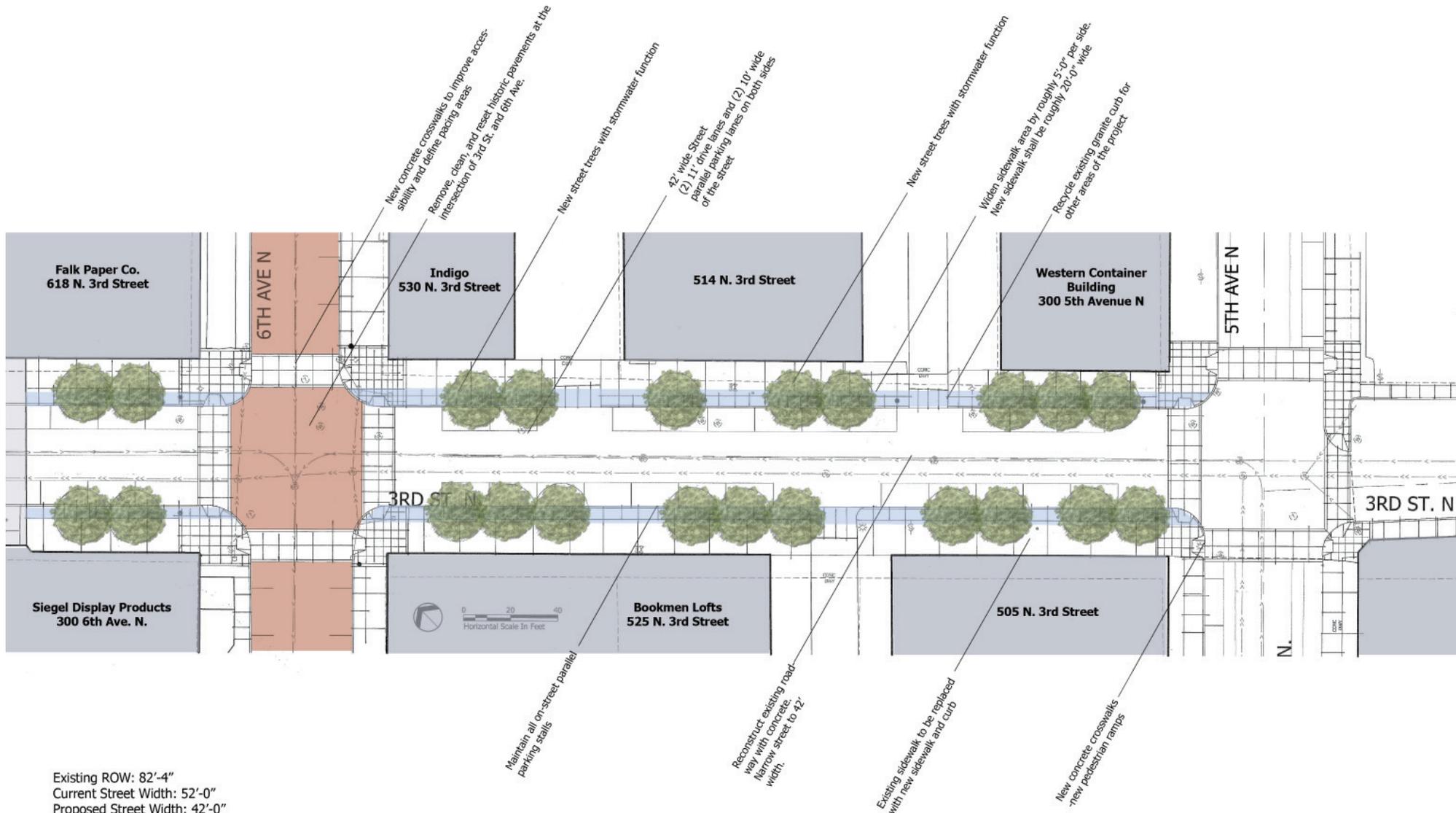
3rd Street



## Primary Recommendations

### 3rd Street North (7th Avenue North to 5th Avenue North)

- ROW width: Existing ROW is along 3rd Street N is 82.45' wide. Final recommendation to maintain existing ROW.
- Street width: Narrow the existing street to 42'-0" wide. The proposed street section would be (2) 11'-0" drive lanes and (2) 10' wide parallel parking lanes on both sides of the street.
- Brick pavers: No historic brick pavers currently exist along this portion of 3rd Street N. A proposed 42'-0" wide concrete street should replace the existing bituminous pavement surface.
- On-street parking: Proposed parking lanes are 10'-0" wide x 20'-0" long. There are approximately 37 (out of 41) parking stalls that will remain as part of the proposed street improvements.
- Sidewalks: Widen sidewalk areas on both sides of the street by approximately 5'-0". The proposed sidewalks will be approximately 20'-0" wide and should be designed to meet City design standards and be ADA compliant.
- Crosswalks: Provide new concrete crosswalks and ADA accessible pedestrian ramps at all street intersections. Because of the historic significance of the intersection related to the truckers strike of..... the center part of the intersection of 3rd Street and 6th Avenue should be repaved with historic brick pavements. This is approximately 1,750 sf. of pavers.
- Pedestrian ramps: Pedestrian ramps shall conform to City of Minneapolis, ADA Standards for Accessible Design, and PROWAAAC standards.
- Street trees: Plant new street trees along the entire length of this section of 3rd Street N.



Existing ROW: 82'-4"  
 Current Street Width: 52'-0"  
 Proposed Street Width: 42'-0"

- Key**
- Historic Pavers
  - New Accessible Sidewalk -with City standard B6-24 curb
  - Existing Loading Docks
  - Active Loading Dock

3rd Street Concept Plan  
**WAREHOUSE DISTRICT HERITAGE STREET PLAN**



## Existing Conditions

### 9th Avenue N. (Washington Avenue to 3rd Street N)

- Existing ROW along 9th Avenue N is 64' wide
- Current Street width is 38' wide
- 9th Avenue N is not a designated MSA (Municipal State Aid) route
- There is currently on-street parking on portions of both sides of the street.
- It is estimated about 70% of the historic street pavement is exposed and roughly about 30% of the pavement is covered with bituminous patching.
  - Some visible areas of pavement settling that collect stormwater
  - Estimated 85% of pavement appears to be in good condition
- New continuous sidewalk on the east side of the street. Sidewalk on the west side of the street is interrupted by numerous driveways leading to parking areas.
- 900 N 3rd Street building has about 61' lineal feet of sidewalk missing along 9th Avenue. There is roughly 32' of sidewalk missing along 3rd Street N in front of the building.
- Existing loading dock has been modified along the 900 N. Third Street Building.
  - Loading dock construction is not completed and not ADA compliant.
- ADA accessibility
  - Accessibility on the east side of the street is considered good and the west side of the street is considered poor.

## Existing Photos



1 Significant paver patching exists adjacent to new sidewalk areas.



2 Sidewalk area and loading dock not fully constructed. Parallel parking blocks pedestrian circulation.



3 Standing water in areas of paver settlement. Concrete overlay at manhole.



4 Sidewalk areas adjacent to loading docks in poor condition. Non ADA compliant loading dock ramp. Pavers at edge of ramp in street have buckled.



5 Address unfinished construction of loading dock.



6

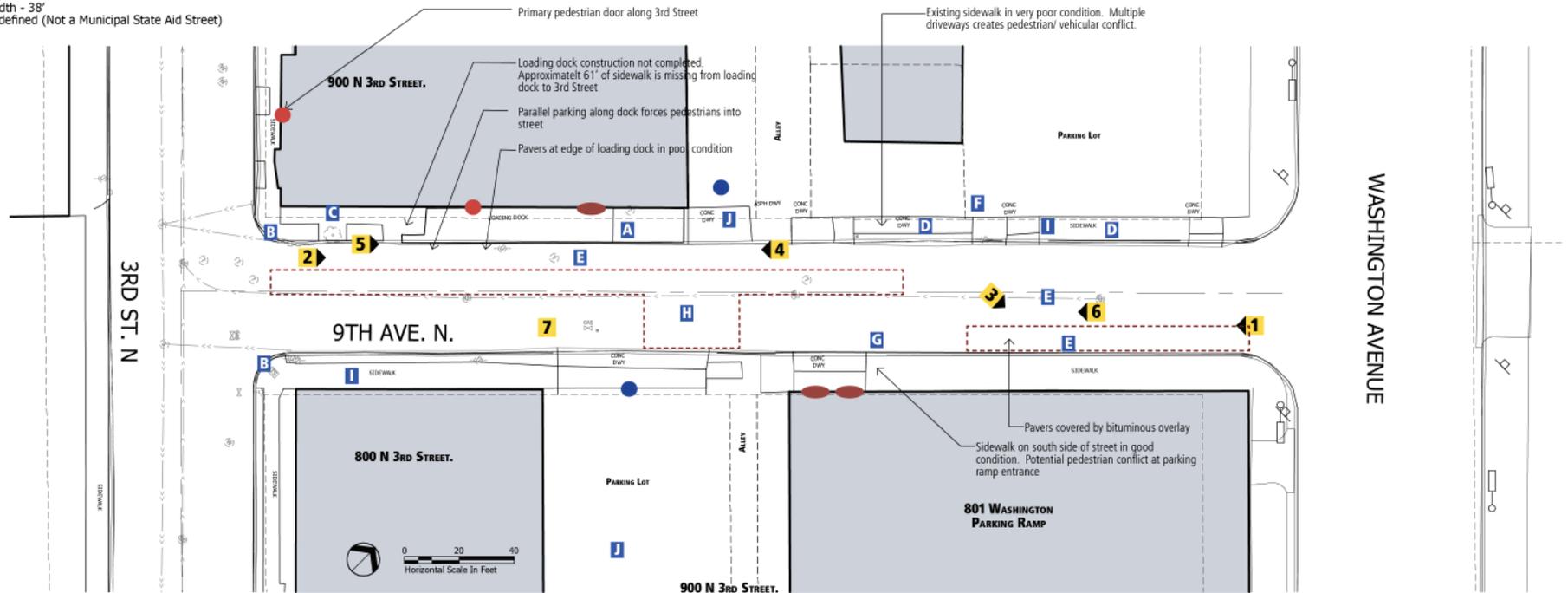


7 Photo of typical paver condition in roadway.

- |   |  |                               |
|---|--|-------------------------------|
| <b>A</b> Concrete loading dock not ADA Accessible | <b>F</b> Multiple driveways create pedestrian/vehicular conflict | ● Pedestrian Door to Building |
| <b>B</b> No pedestrian ramp                       | <b>G</b> Damaged driveway  | ● Overhead delivery Door      |
| <b>C</b> No sidewalk connection                   | <b>H</b> Bituminous patch overlay on road                        | ● Active Loading Dock         |
| <b>D</b> Sidewalk in poor condition               | <b>I</b> Sidewalk in good/fair condition                         | ● Active Loading Area         |
| <b>E</b> Areas of visible standing water          | <b>J</b> Active delivery areas with trucks                       |                               |

## Existing Issues/ Concerns

ROW - 64'  
Street Width - 38'  
ADT - Undefined (Not a Municipal State Aid Street)



Areas of street patching - Approximately 3,375 sf  
18 parallel parking stalls

9th Avenue Existing Conditions  
**WAREHOUSE DISTRICT HERITAGE STREET PLAN**



## Primary Recommendations 9th Avenue N. (Washington Avenue to 3rd Street N)

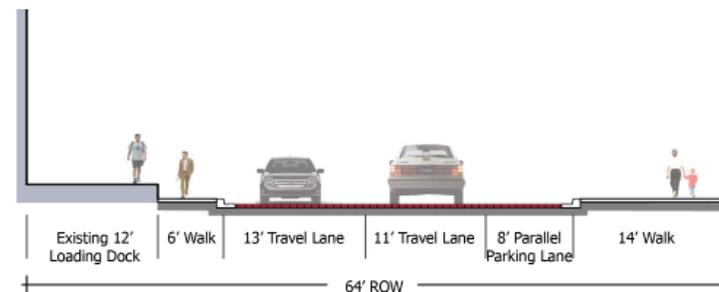
- ROW width: Existing ROW along along 9th Avenue N to remain at 66'-0". Final recommendation's to maintain existing ROW.
- Street width: Current Street width is 38' wide. Final recommendation is to narrow the existing street to 32'-0" wide. The proposed street section would be (1) 13'-0" drive lane, (1) 11'-0' drive lane and (1) 8' wide parallel parking lane on the east side of the street.
- Brick pavers: Historic brick pavers along 9th Avenue N should be removed, cleaned and reinstalled in the entire new street cross-section. There are approximately 11,460 sf. of pavers to be reinstalled.
- Curb cuts: Consolidate and narrow existing driveways, alleys and other curb cuts along the street in conjunction with future redevelopment opportunities.
- Crosswalks: Provide new concrete crosswalks and ADA accessible pedestrian ramps at all street intersections.
- Sidewalks: Widen sidewalk area on the west side of the street by approximately 6'-0" on the outside of the loading dock at the 900 3rd. Street N. building. The widened sidewalk that extends to Washington Avenue will be approximately 15'-0". The proposed sidewalk widening should be designed to meet City design standards and be ADA compliant.
- On-street parking: Proposed parking lanes are 8'-0" wide x 20'-0" long. There are approximately 10 parking stalls (13 existing stalls) that will remain as part of the proposed street improvements.



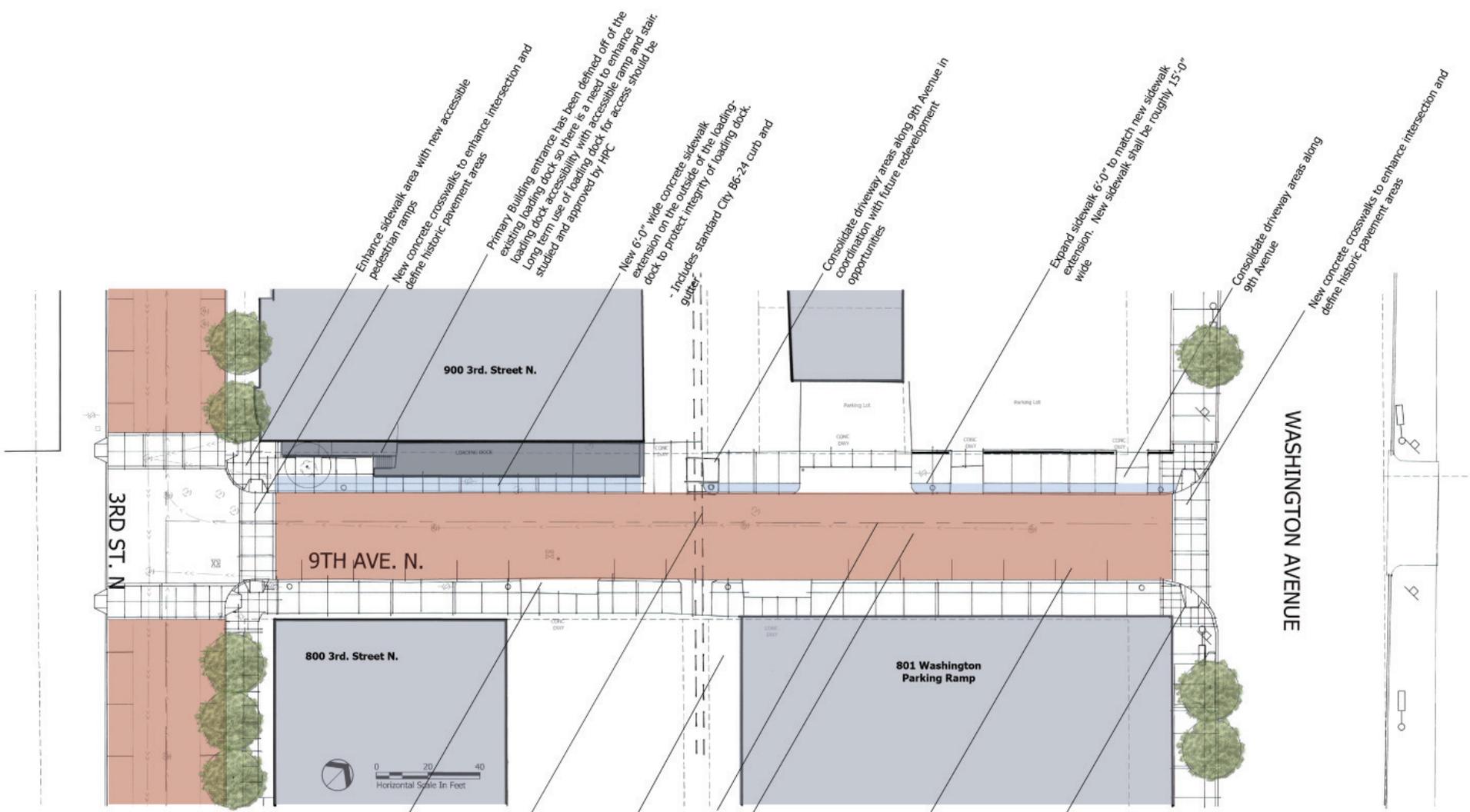
Existing View of 9th Ave.



Proposed view looking north along 9th Avenue.



Proposed Cross-Section  
9th Avenue North (3rd Street North to Washington Avenue North)



Existing ROW: 64'-0"  
 Current Street Width: 38'-0"  
 Proposed Street Width: 32'-0"

- Key**
- Historic Pavers
  - New Accessible Sidewalk  
-with City standard B6-24 curb
  - Existing Loading Docks
  - Active Loading Dock

- Consolidate and narrow driveway areas in conjunction with future redevelopment opportunities
- Future opportunity to interpret historic railroad track alignment and R.O.W.
- Recommended interpretation include replacement of metal track and preservation of signage
- Opportunity to include interpretive signage
- Define driveway area and existing alley with curb and gutter
- New 21' wide Street parking (13' drive lane, 11' drive lane and 8' Pavers in reconstructed street. Estimated 11,460 sf of Pavers.
- Remove, clean, and reset historic brick
- Maintain 10 on-street parallel parking stalls
- New concrete crosswalks and pedestrian ramps

- Enhance sidewalk area with new accessible pedestrian ramps
- New concrete crosswalks to enhance intersection and define historic pavement areas
- Primary Building entrance has been defined off of the existing loading dock so there is a need to enhance Long term use of loading dock with accessible ramp and stair studied and approved by HPC
- New 6'-0" wide concrete sidewalk extension on the outside of the loading dock to protect integrity of loading dock. Includes standard City B6-24 curb and gutter
- Consolidate driveway areas along 9th Avenue in coordination with future redevelopment opportunities
- Expand sidewalk 6'-0" to match new sidewalk wide
- New concrete crosswalks to enhance intersection and define historic pavement areas

September 13, 2011



## Existing Conditions

### 8th Avenue N. (Washington Avenue to 3rd Street N)

- Existing ROW along 8th Avenue N is 64' wide
- Current Street width is 40' wide
- 8th Avenue N is not a designated MSA (Municipal State Aid) route.
- There is currently on-street parking on portions of both sides of the street.
- It is estimated about 30% of the historic street pavement is exposed and roughly about 70% of the pavement is covered with bituminous patching.
  - Bituminous patching appears to be in multiple layers in certain spots along the street
  - Some visible areas of pavement settling that collect stormwater
  - Estimated 70% of pavement appears to be in fair to good condition
  - Existing creosote wood block pavers appear to be in good condition
- Sidewalks on both the east and west side of the street is interrupted by numerous driveways leading to parking areas.
  - Sidewalks are in poor condition based on visual analysis.
  - Curbs also appear to be in poor condition. There are existing granite curbs along both sides of the street at the intersection of 3rd Street North.
- Some active loading areas exist at the 800 N 3rd Street building.
- ADA accessibility
  - Accessibility on both sides of the street is considered poor.

## Existing Photos



1 Extensive areas of bituminous paving over historic pavers



2 Areas of settling along existing granite curb



3 Transition of brick pavers to wood block pavers



4 Areas of settling along existing granite curb



5 Existing areas of wood block pavers



6 Extensive bituminous patching has occurred over existing wood block pavers

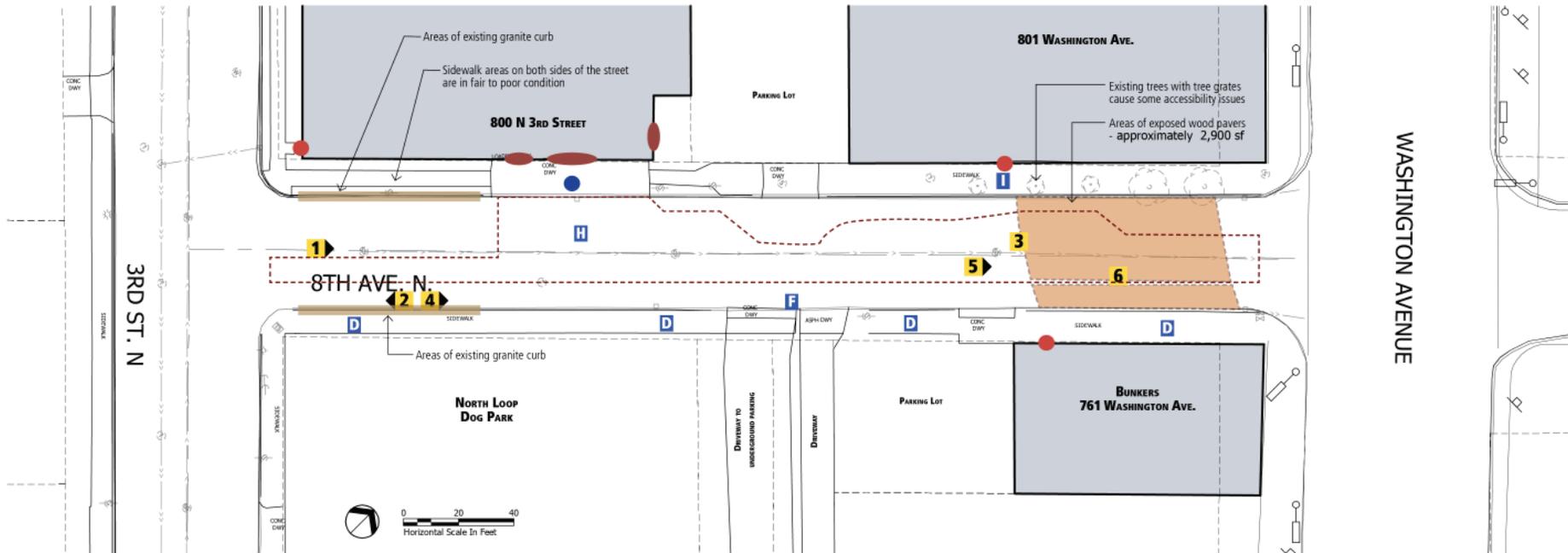
- A Concrete loading dock not ADA Accessible
- B No pedestrian ramp
- C No sidewalk connection
- D Sidewalk in poor condition
- E Areas of visible standing water

- F Multiple driveways create pedestrian/vehicular conflict
- G Damaged driveway
- H Bituminous patch overlay on road
- I Sidewalk in good/fair condition
- J Active delivery areas with trucks

- Pedestrian Door to Building
- Overhead delivery Door
- Active Loading Dock
- Active Loading Area

## Existing Issues/ Concerns

ROW - 64'  
Street Width - 40'  
ADT - Undefined (Not a Municipal State Aid Street)



Areas of street patching - Approximately 7,240 sf  
25 parallel parking stalls

8th Avenue Existing Conditions  
**WAREHOUSE DISTRICT HERITAGE STREET PLAN**

September 13, 2011



## Primary Recommendations 8th Avenue N. (Washington Avenue to 3rd Street N)

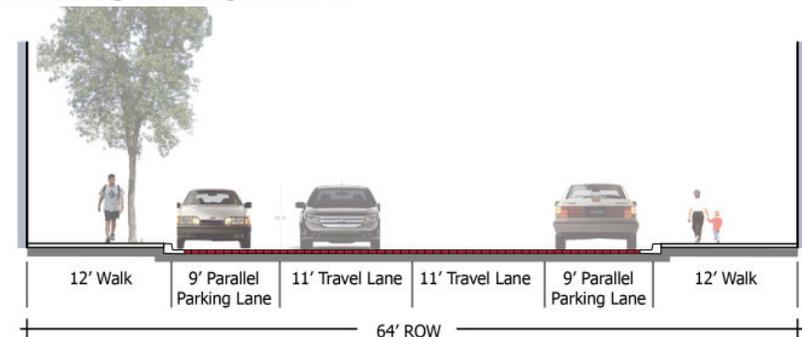
- ROW width: Existing ROW along 8th Avenue N to remain at 66'-0" wide.
- Street width: Maintain existing street width of 40'-0". The street section would remain at (2) 11'-0" drive lanes, and (2) 9'-0" wide parallel parking lane on both sides of the street.
- Brick Pavers: Historic brick pavers along 8th Avenue N should be removed, cleaned and reinstalled in to the existing street cross-section. There are approximately 10,470 sf. of pavers to be reinstalled. The street also contains historic creosote wood pavers in a small area adjacent to Washington Avenue that should be retained in-place. Any future project to reinstall historic brick pavements along 8th Avenue should maintain a minimum 10'-0" clear zone of historic brick pavers that should not be removed on either side of the historic wood pavers.
- On-street parking: Existing parking lanes are 9'-0" wide x 20'-0" long. There are approximately 23 parking stalls that currently exist along 8th Avenue and are proposed to remain as part of the proposed street improvements.
- Driveways: Consolidate and narrow existing driveways, alleys and other curb cuts along the street in conjunction with future redevelopment opportunities.
- Crosswalks: Provide new concrete crosswalks and ADA accessible pedestrian ramps at all street intersections. Pedestrian ramps shall conform to City of Minneapolis, ADA Standards for Accessible Design, and PROWAAC standards.
- Granite curbs: Existing granite curbs shall be maintained and salvaged granite curbs should be added along the street as part of any street construction project.
- Street trees: Plant new street trees along 8th Avenue between the alley and 3rd Street N.



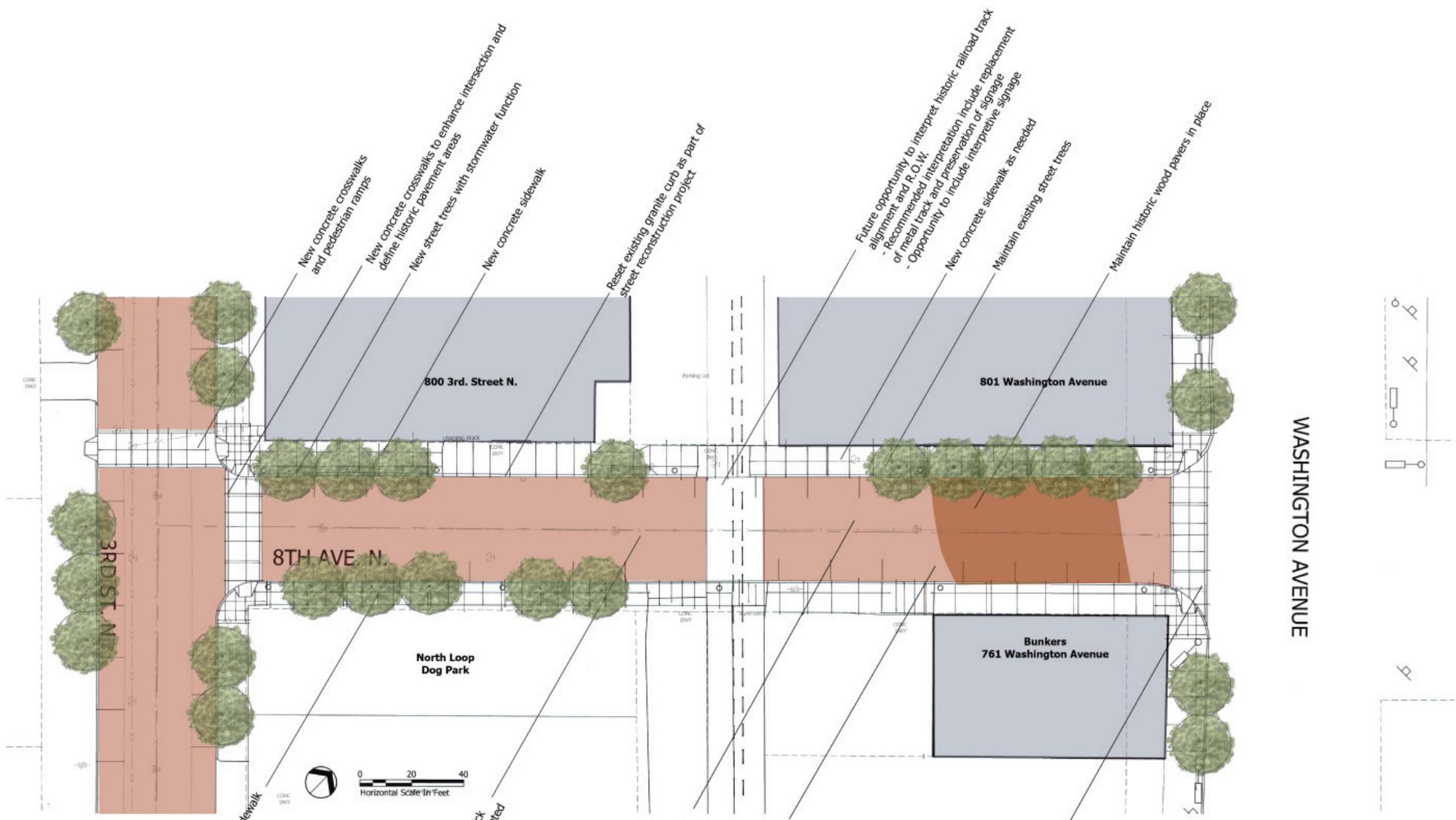
Existing View of 8th Ave.



Proposed view looking north along 8th Avenue



Proposed Cross-Section  
8th Avenue North (3rd Street North to Washington Avenue North)



Existing ROW: 64'-0"  
 Current Street Width: 40'-0"  
 Proposed Street Width: 40'-0"

- Key**
- Historic Pavers
  - New Accessible Sidewalk  
-with City standard B6-24 curb
  - Existing Loading Docks
  - Active Loading Dock



Remove, clean and reset historic brick pavers in reconstructed street. Estimated 10,470 sf of pavers.

40' wide street  
 (2) 11' drive lanes and (2) 9' wide parking stalls

Maintain 22 on-street parallel parking stalls

New concrete crosswalks to enhance intersection and define historic pavement areas

New concrete sidewalk with street trees

New concrete crosswalks and pedestrian ramps

New concrete crosswalks define historic pavement areas

New street trees with stormwater function

New concrete sidewalk

Reset existing granite curb as part of street reconstruction project

Future opportunity to interpret historic railroad track alignment and R.O.W.  
 - Recommended interpretation include replacement of metal track and preservation of signage  
 - Opportunity to include interpretive signage

New concrete sidewalk as needed

Maintain existing street trees

Maintain historic wood pavers in place



## Existing Conditions

### 7th Avenue N. (3rd Street N to 4th Street N)

- Existing ROW along 7th Avenue N is 60' wide
- Current Street width is 35' wide
- 7th Avenue N is a designated MSA (Municipal State Aid) route.
- ADT's for 7th Avenue are 1000
- There is currently on-street parking on portions of both sides of the street.
- There are no historic pavements exposed along 7th Avenue N.
- Sidewalks on both the east and west sides of the street is interrupted by loading docks and numerous driveways leading to loading areas.
  - Sidewalks are in poor condition based on visual analysis.
  - Curbs also appear to be in poor condition.
- Loading docks occur along both sides of the street.
  - 701 N 3rd Street building has a loading dock that appears to be in fair to poor condition. Loading dock contains a ramp that is not ADA compliant and provides no stair access. Loading dock does not lead to building primary entrance.
  - 311 7th Avenue building has a loading dock that appears to be in fair to poor condition. Loading dock is not ADA compliant and provides no stair access. Loading dock has recently been remodeled and has not been designed to meet ADA accessibility code. Loading dock does lead to a primary building entrance.
- ADA accessibility
  - Accessibility on both sides of the street are poor. Pedestrians currently can be seen walking in street adjacent to on-street parking to bypass the loading dock areas.

Existing Photos



1 Existing ramp in poor condition and not ADA accessible



2 Existing loading dock in poor condition



3 Existing loading dock and ramp in poor condition and not ADA accessible



4 Existing ramp in poor condition and not ADA accessible



5 Sidewalk in poor condition



6 Sidewalk in poor condition

- A Concrete loading dock not ADA Accessible
- B No pedestrian ramp
- C No sidewalk connection
- D Sidewalk in poor condition
- E Areas of visible standing water

- F Multiple driveways create pedestrian/vehicular conflict
- G Damaged driveway
- H Bituminous patch overlay on road
- I Sidewalk in good/fair condition
- J Active delivery areas with trucks

- Pedestrian Door to Building
- Overhead delivery Door
- Active Loading Dock
- Active Loading Area

Existing Issues/ Concerns

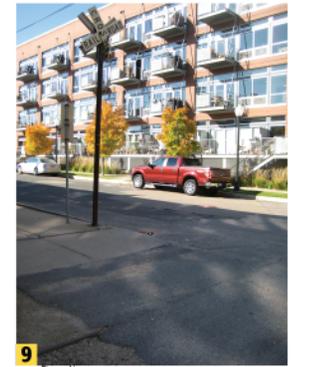
ROW - 60'  
Street Width - 35'  
ADT - 1000



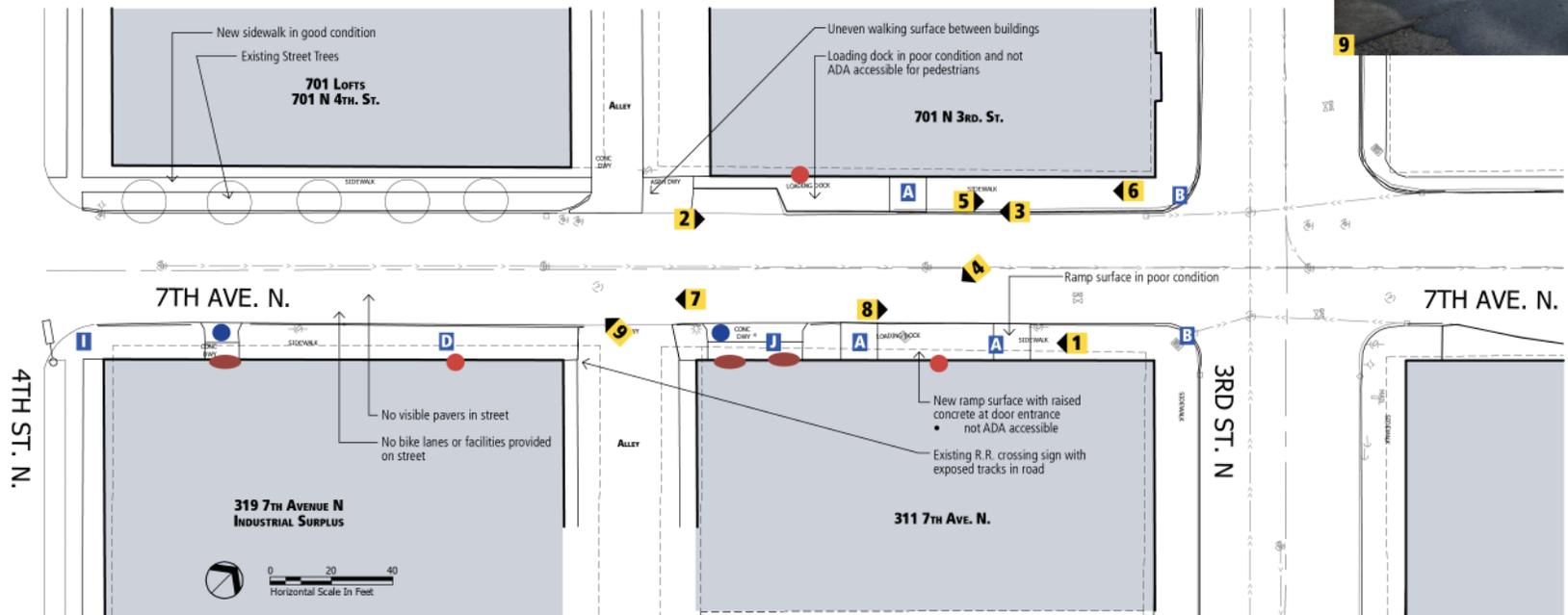
7 Existing R.R. crossing sign and exposed tracks



8 Parallel parking adjacent to loading docks require some pedestrians to walk in streets



9



Areas of street patching  
24 parallel parking stalls

7th Avenue Existing Conditions  
**WAREHOUSE DISTRICT HERITAGE STREET PLAN**



## Existing Conditions

### 7th Avenue N. (Washington Avenue to 3rd Street N)

- Existing ROW along 7th Avenue N is 60' wide
- Current Street width varies from 35' to 44' wide
- 7th Avenue N is a designated MSA (Municipal State Aid) route
- ADT's for 7th Avenue are 1000
- There is currently on-street parking on a short section along the east side of the street.
- There are no historic pavements exposed along 7th Avenue N. Based on review of the *Annual Report of the City Engineer of the City of Minneapolis (1889-1934)* it has been determined that there were no historic pavements ever placed along 7th Avenue N.
- Sidewalk exists on a short section along the west side and is interrupted by numerous driveways leading to parking areas.
  - The sidewalk is in poor condition based on visual analysis. Sidewalk also very narrow and does not meet ADA standards.
  - Curbs also appear to be in poor condition.
- Loading docks occur along both sides of the street.
  - 701 Washington building has a loading dock that appears to be in fair to good condition. Loading dock has been remodeled and includes pedestrian accessible ramp and stairs along the north side of the dock. The south side of the loading dock includes stairs. Loading dock leads to building secondary accessible entrance.
  - 604 N Washington Avenue building has a loading dock that appears to be in fair to poor condition. Loading dock contains a ramp that is not ADA compliant and provides no stair access. Loading dock does not lead to building primary entrance but is actively used.
- ADA accessibility
  - Accessibility on both sides of the street is poor. Pedestrians currently can be seen walking in street adjacent to on-street parking to bypass the loading dock areas.
  - Loading docks on both sides of the street at the intersection of Washington Avenue creates pedestrian/ vehicular conflicts.

Existing Photos



1 Existing ramp in poor condition and not ADA accessible



2 Parallel parking forces pedestrians to walk in road



3 Narrow R.O.W. with loading docks on both sides of streets creates circulation issues



4 Steep ramp access to loading dock. Current dock is active with truck deliveries.



5 Stairs at HGA loading dock



6 Loading dock not designed for pedestrian accessibility

- A Concrete loading dock not ADA Accessible
- B No pedestrian ramp
- C No sidewalk connection
- D Sidewalk in poor condition
- E Areas of visible standing water

- F Multiple driveways create pedestrian/vehicular conflict
- G Damaged driveway
- H Bituminous patch overlay on road
- I Sidewalk in good/fair condition
- J Active delivery areas with trucks

- Pedestrian Door to Building
- Overhead delivery Door
- Active Loading Dock
- Active Loading Area



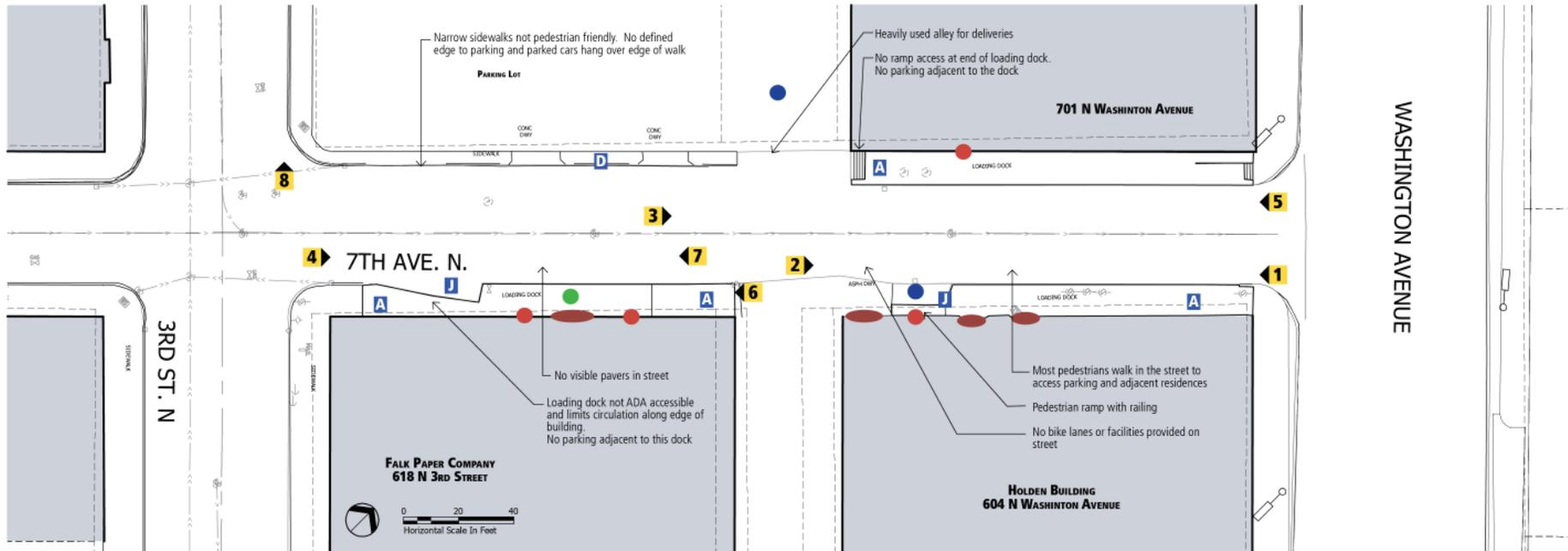
7 Limited pedestrian circulation



8 Narrow sidewalks and poor street conditions along 3rd St.

Existing Issues/ Concerns

ROW - 60'  
Street width - 35' to 44'  
ADt - 1000



- Areas of street patching
- 5 parallel parking stalls

7th Avenue Existing Conditions

WAREHOUSE DISTRICT HERITAGE STREET PLAN

Warehouse District Heritage Street Plan

HERITAGE STREET ANALYSIS AND CONCEPT PLANS

September 13, 2011

V-71

## Primary Recommendations 7th Avenue N. (Washington Avenue to 4th Street N)

- ROW width: Existing ROW along 7th Avenue N. is to remain at 60'-0" wide.
- Street width: Narrow existing street width to 26'-0" wide. The proposed street section would consist of (2) 13'-0" drive lanes and would require the removal of the existing parallel parking stalls along both sides of the street. There is a need to maintain a road width of at least 26'-0" to accommodate the turning movements of semi-trucks that need to access loading dock areas.
- Brick pavers: No historic brick pavers currently exist along 7th Avenue N. The proposed 26'-0" wide street would be reconstructed as a concrete street to replace the existing bituminous pavement surface.

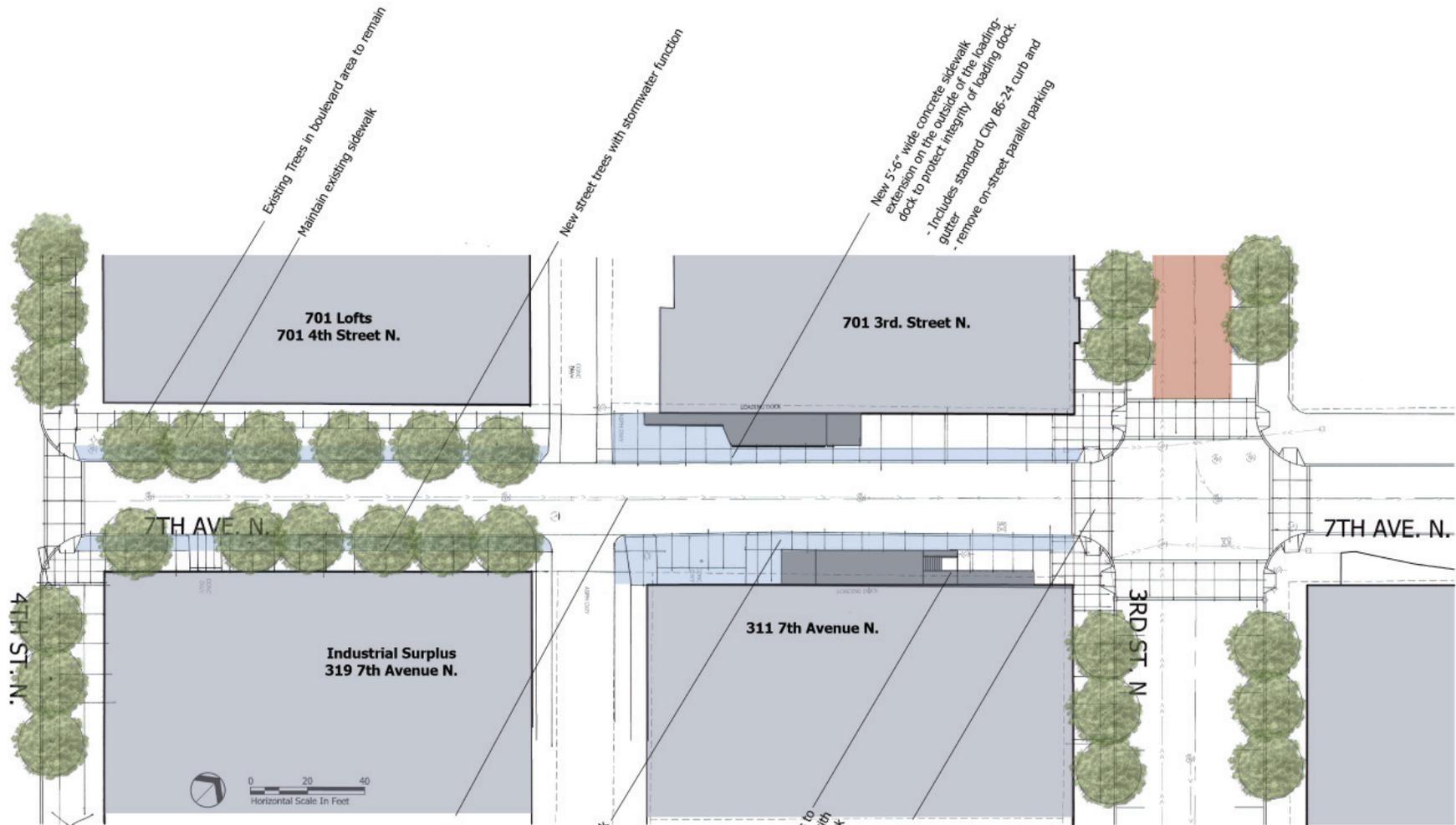
- Sidewalks: Widen sidewalk areas on both sides of the street to approximately 5'-6". The proposed widened sidewalks would provide access around all of the existing loading docks that exist along the entire length of 7th Avenue. The proposed sidewalks should be designed to meet City design standards and be ADA compliant.
- Driveways: Consolidate and narrow existing driveways, alleys and other curb cuts along the street in conjunction with future redevelopment opportunities.
- Crosswalks: Provide new concrete crosswalks and ADA accessible pedestrian ramps at all street intersections. Pedestrian ramps shall conform to City of Minneapolis, ADA Standards for Accessible Design, and PROWAAC standards.
- Street trees: Plant new street trees along 7th Avenue between the alley and 4th Street N.



Existing view of 7th Ave.



Proposed view looking south along 7th Avenue



Existing ROW: 60'-0"  
 Current Street Width: 35'-0"  
 Proposed Street Width: 26'-0"

- Key**
- Historic Pavers
  - New Accessible Sidewalk  
-with City standard B6-24 curb
  - Existing Loading Docks
  - Active Loading Dock

26' wide street  
 (2) 13' drive lanes and no parallel  
 parking stalls

New 5'-6" wide concrete sidewalk  
 extension on the outside of the loading  
 dock to protect integrity of loading dock.  
 - Includes standard City B6-24 curb and  
 gutter  
 - remove on-street parallel parking

If access is required because this is a primary access to  
 the building then enhance loading dock accessibility with  
 accessible ramp and stair. Long term use of loading dock  
 for access should be studied and approved by HPC

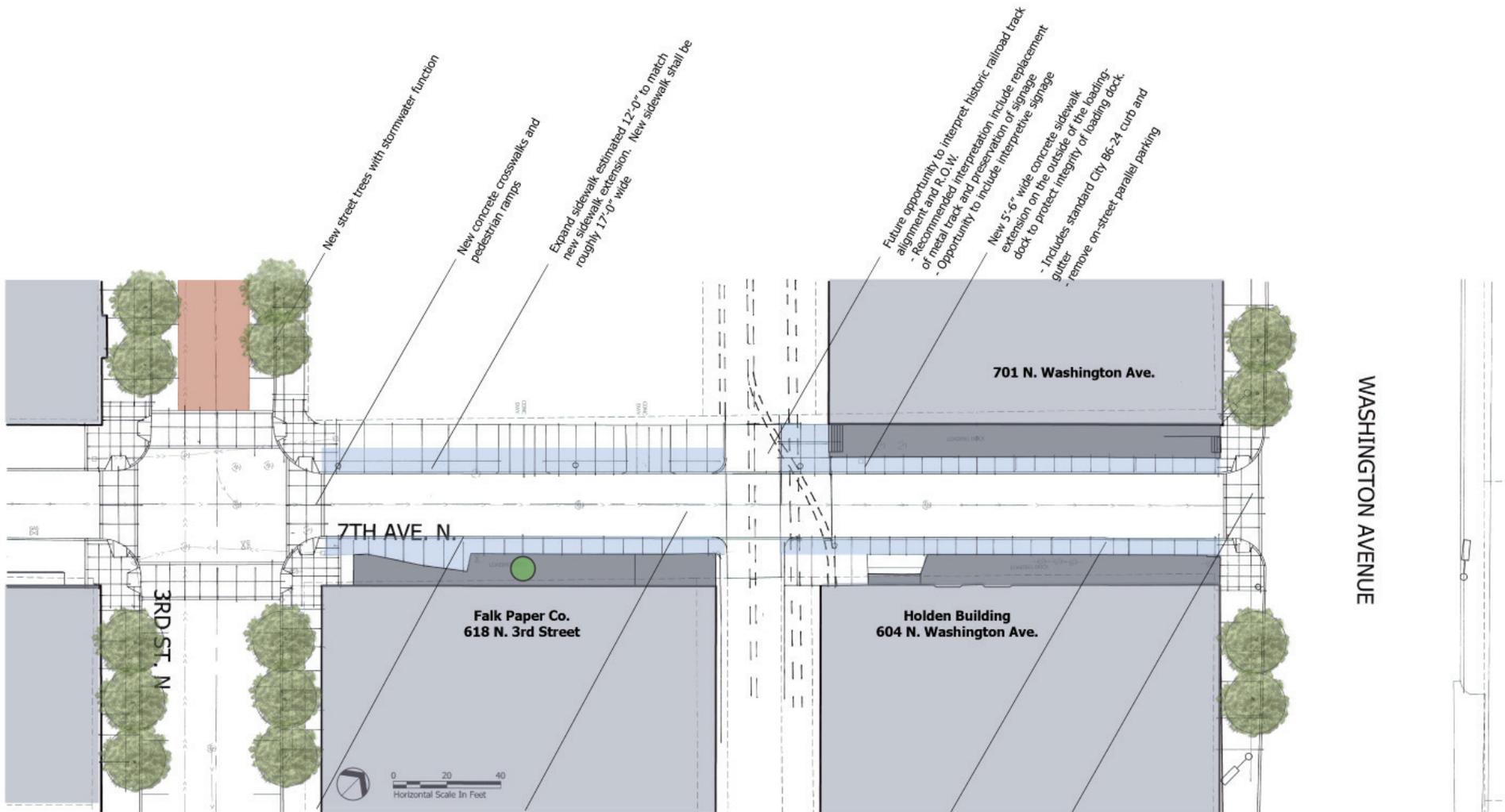
New concrete crosswalks and  
 pedestrian ramps

Existing Trees in boulevard area to remain  
 Maintain existing sidewalk

New street trees with stormwater function

New 5'-6" wide concrete sidewalk  
 extension on the outside of the loading  
 dock to protect integrity of loading dock.  
 - Includes standard City B6-24 curb and  
 gutter  
 - remove on-street parallel parking





Existing ROW: 60'-0"  
 Current Street Width: 35'-0" to 44'-0"  
 Proposed Street Width: 26'-0"

- Key**
- Historic Pavers
  - New Accessible Sidewalk  
-with City standard B6-24 curb
  - Existing Loading Docks
  - Active Loading Dock

Provide modified curb cut along active loading docks areas

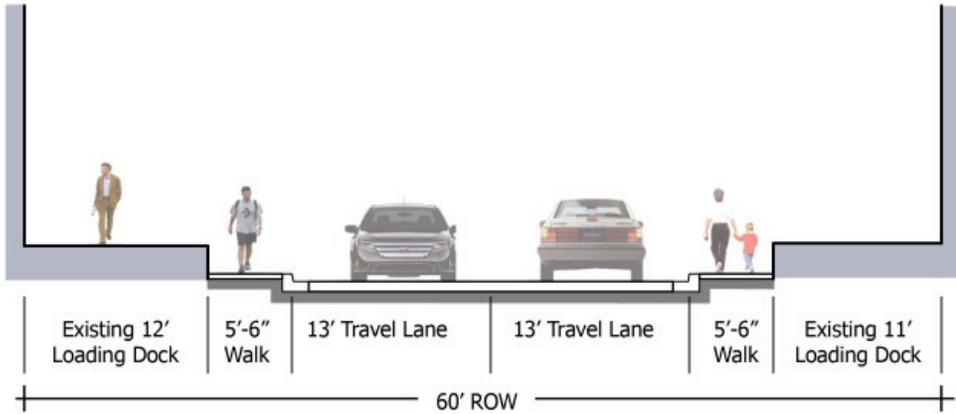
26' wide Street (2) 13' drive lanes and no parallel parking stalls

New 5'-6" wide concrete sidewalk extension on the outside of the loading dock to protect integrity of loading dock.  
 - Includes standard City B6-24 curb and gutter  
 - remove on-street parallel parking  
 New concrete crosswalks and pedestrian ramps

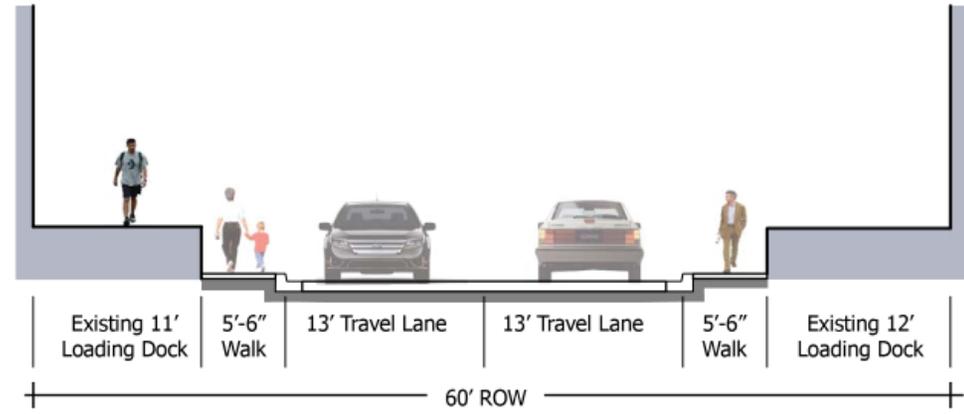
Future opportunity to interpret historic railroad track alignment and P.O. W.  
 - Recommended interpretation include replacement of metal track and preservation of signage  
 - Opportunity to include interpretive signage  
 New 5'-6" wide concrete sidewalk extension on the outside of the loading dock to protect integrity of loading dock.  
 - Includes standard City B6-24 curb and gutter  
 - remove on-street parallel parking

WASHINGTON AVENUE





Proposed Cross-Section  
7th Avenue North (4th Street North to 3rd Street North) looking north



Proposed Cross-Section  
7th Avenue North (3rd Street North to Washington Avenue North) looking north



Existing view of 7th Ave.

New accessible 5'-6" wide walk adjacent to loading dock area

Expand sidewalk to match new sidewalk

New accessible 5'-6" wide walk adjacent to loading dock area



Proposed view looking north along 7th Avenue

## Existing Conditions

### 6th Avenue N. (3rd Street N to 4th Street N)

- Existing ROW along 6th Avenue N is 80' wide
- Current Street width is 50' wide
- 6th Avenue N is a designated MSA (Municipal State Aid) route
- ADT's for 6th Avenue are 5600
- There are approximately (8) on-street parallel parking stalls on portions of both sides of the street.
- It is estimated about 55% of the historic street pavement is exposed and approximately 45% of the pavement is covered with bituminous patching.
  - Some visible areas of pavement settling that collect stormwater
  - Estimated 80% of pavement appears to be in fair to good condition
- New continuous sidewalk on the east side of the street. Sidewalk on the west side of the street is interrupted by loading areas and active loading dock.
  - Sidewalks on the SW side of street are in poor condition based on visual analysis.
  - Curbs also appear to be in good condition.
- Loading docks occur along the west side of the street.
  - 300 6th Avenue N building has a loading dock that appears to be in fair to good condition. Loading dock contains a ramp that is not ADA compliant and provides no stair access. Loading dock does not lead to building primary entrance.
  - Loading dock is active with semi-trucks
- ADA accessibility
  - Accessibility on the west side of the street is poor.
  - Good sidewalks with pedestrian curb ramps exist on the east side of the street.

Existing Photos



1 View north east along 6th Ave.



2 Edge of loading dock in poor condition  
Loading dock not ADA compliant



3 Pavers appear to be in good condition along street. some extensive areas of patching exists along the street



4 Some utility work and bituminous patching has occurred along the street



5 Surface of loading dock in good condition



6 Edge of loading dock in fair condition

- A Concrete loading dock not ADA Accessible
- B No pedestrian ramp
- C No sidewalk connection
- D Sidewalk in poor condition
- E Areas of visible standing water

- F Multiple driveways create pedestrian/vehicular conflict
- G Damaged driveway
- H Bituminous patch overlay on road
- I Sidewalk in good/fair condition
- J Active delivery areas with trucks

- Pedestrian Door to Building
- Overhead delivery Door
- Active Loading Dock
- Active Loading Area



7 Parallel parking along loading dock forces pedestrian to walk in street



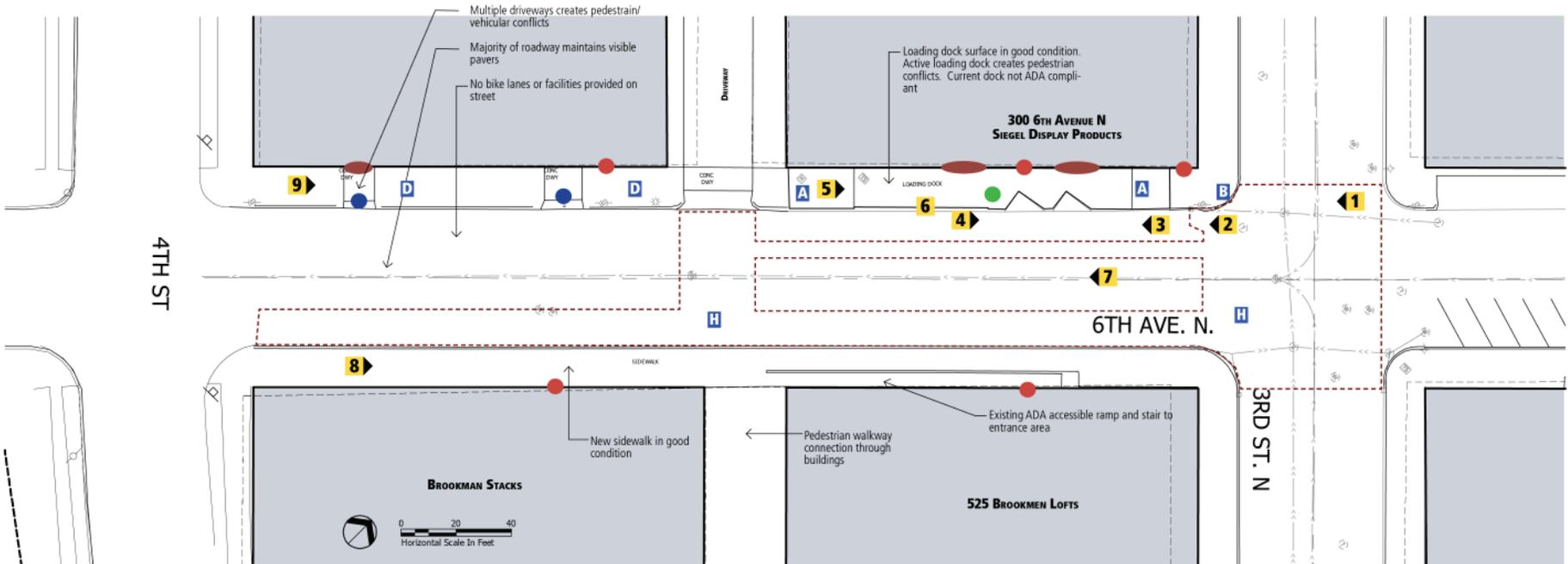
8 Sidewalk in front of Bookman Stacks is wide and offers opportunity for street trees



9 Sidewalk in northwest side of street in fair condition and is wide enough to accommodate trees

Existing Issues/ Concerns

ROW - 80'  
Street width - 50'  
ADT- 5600



Areas of street patching - Approximately 10,900 sf  
22 parallel parking stalls

6th Avenue Existing Conditions  
**WAREHOUSE DISTRICT HERITAGE STREET PLAN**



## Existing Conditions

### 6th Avenue N. (Washington Avenue to 3rd Street N)

- Existing ROW along 6th Avenue N is 80' wide
- Current Street width is 49' wide
- 6th Avenue N is a designated MSA (Municipal State Aid) route
- ADT's for 6th Avenue are 5600
- There is currently on-street parking on the east side of the street
  - The current (18) parking stalls along the east side of the street is angled parking.
- It is estimated about 70% of the historic street pavement is exposed and roughly about 30% of the pavement is covered with bituminous patching.
  - Extensive areas of pavement settlement. Settlement has created numerous areas for stormwater to collect. It can be almost certain that pavement base is in very poor condition.
  - Estimated 70% of pavement appears to be in fair to good condition.
- New continuous sidewalk on the east side of the street. No sidewalk on west side of street.
  - Sidewalks on east side of street terminate at loading area within the ROW. Sidewalk continues on other side of loading dock area.
- Loading docks occur along both sides of the street.
  - 507 N 3rd Street Building has a small loading dock area that projects into the public ROW and blocks sidewalk access along 6th Avenue N. Does not appear to be an active loading dock area.
  - 604 N Washington Avenue building has a loading dock that appears to be in poor condition. Loading dock contains a ramp that is not ADA compliant and provides no stair access. Loading dock does not lead to building primary entrance and is not actively used.
  - 618 N 3rd Street Building has a very active loading area with (8) delivery bays. Loading area is very active with semi-trucks. Pedestrians cannot walk along this side of the street when a truck is in a loading bay.
- ADA accessibility
  - Accessibility on both sides of the street is poor. Pedestrians can not move north or south along this section of street without walking into the street. Active truck delivery areas create pedestrian/ vehicular conflicts.

## Existing Photos



1 Current loading dock not ADA compliant



2 No clearly defined pedestrian access on north side of street



3 Parallel parking blocks pedestrian access past building. The existing ROW is narrow and limits potential improvements



4 Truck deliveries block street and limits pedestrian and vehicular circulation



5 Angled parking limits vehicular circulation



6 Existing stairs at loading dock in poor condition



7 Majority of street remains exposed pavers  
 • some patching and paver settling exists based on visual inspection, pavers appear to be in good condition

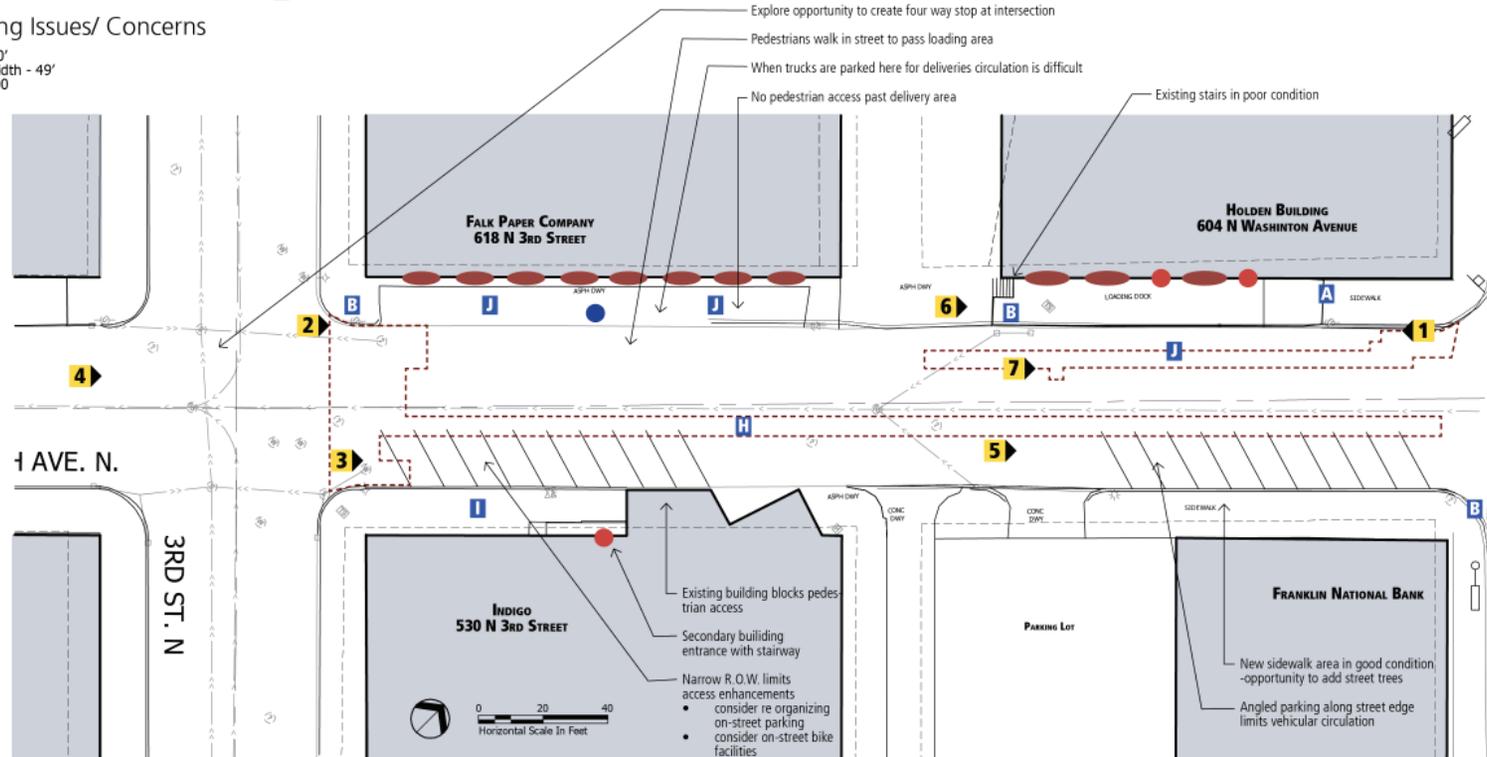
- A** Concrete loading dock not ADA Accessible
- B** No pedestrian ramp
- C** No sidewalk connection
- D** Sidewalk in poor condition
- E** Areas of visible standing water

- F** Multiple driveways create pedestrian/vehicular conflict
- G** Damaged driveway
- H** Bituminous patch overlay on road
- I** Sidewalk in good/fair condition
- J** Active delivery areas with trucks

- Pedestrian Door to Building
- Overhead delivery Door
- Active Loading Dock
- Active Loading Area

## Existing Issues/ Concerns

ROW - 80'  
 Street Width - 49'  
 ADT- 5600



Areas of street patching - Approximately 4,100 sf  
 18 angled parking stalls

6th Avenue Existing Conditions

**WAREHOUSE DISTRICT HERITAGE STREET PLAN**

Warehouse District Heritage Street Plan

**HERITAGE STREET ANALYSIS AND CONCEPT PLANS**

September 13, 2011

**V-79**

## Primary Recommendations 6th Avenue N. (Washington Avenue N to 4th Street N)

- ROW width: Current Existing ROW along 6th Avenue N is to remain at 80'-0" wide.
- Street width: Current Street width is 49'-0" to 50'-0" wide. Final recommendation to narrow existing street width to 44'-0" wide. The proposed street section would consist of (2) 12'-0" drive lanes and (2) 10' wide parallel parking lanes on both sides of the street. There is a need along 6th Avenue N ( between Washington Avenue and 3rd Street) to maintain a road width of at least 26'-0" to accommodate the turning movements of semi- trucks that need to access loading dock areas.
- Brick pavers: Historic brick pavers along 6th Avenue N should be removed, cleaned and reinstalled in to the entire new street cross-section. There are approximately 13,770 sf. of pavers to be reinstalled.
- On-street parking: Existing angled parking between Washington Avenue and 3rd Street N will be removed and replaced with parallel

parking. Proposed parallel parking lanes will be 10'-0" wide x 20'-0" long. There are approximately 33 parking stalls that will be created as part of the proposed street improvements.

- Sidewalks: Widen sidewalk areas on both sides of the street to approximately 6'-0". The proposed widened sidewalks would provide access around all of the existing loading docks that exist along the entire length of 6th Avenue. The proposed sidewalks should be designed to meet City design standards and be ADA compliant.
- Crosswalks: Provide new concrete crosswalks and ADA accessible pedestrian ramps at all street intersections. Pedestrian ramps shall conform to City of Minneapolis, ADA Standards for Accessible Design, and PROWAAC standards.
- Street trees: Plant new street trees along 6th Avenue on both sides of the street between the alley and 4th Street N.



Existing View of 6th Ave.

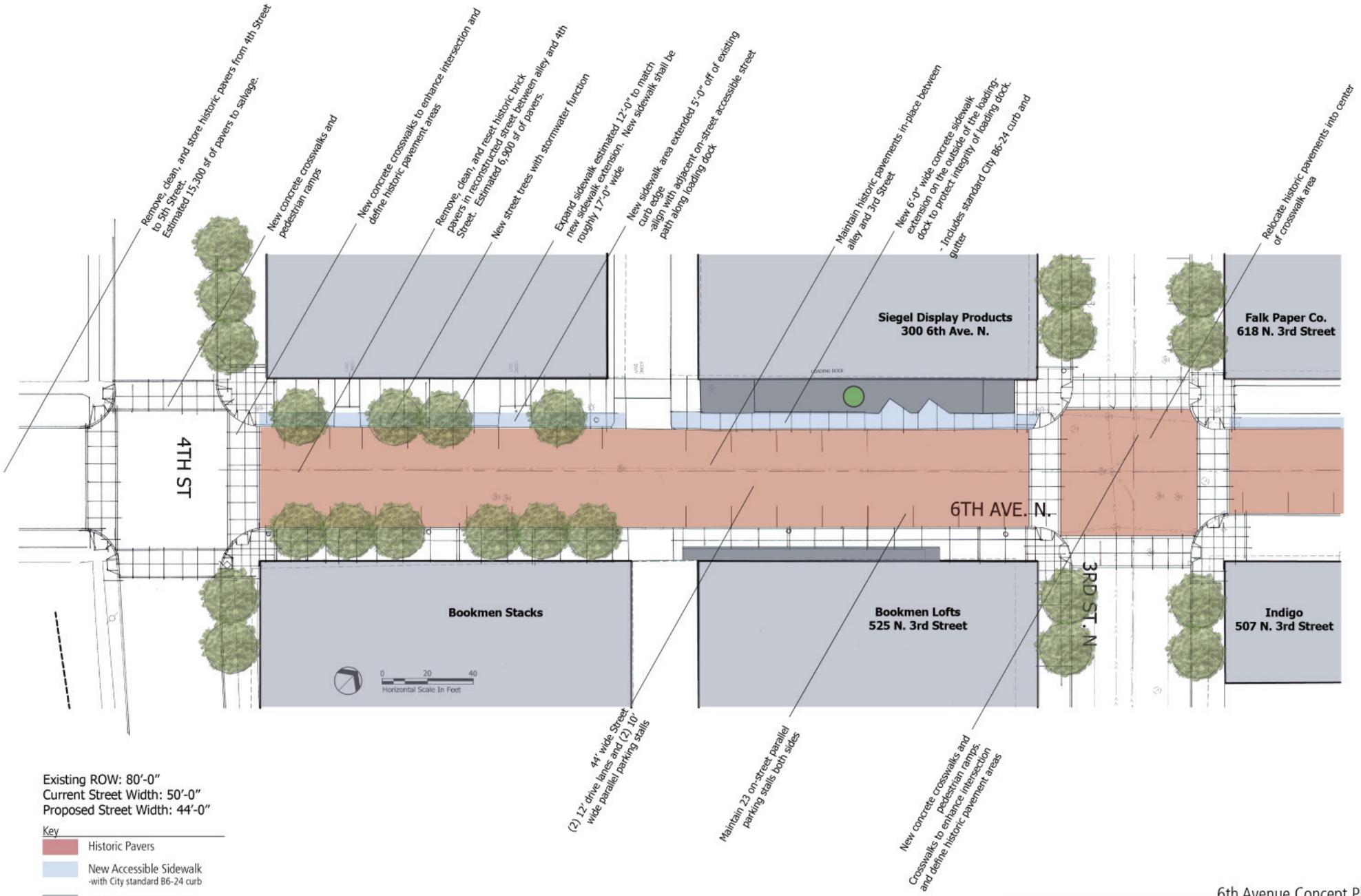
Remove, clean, and reset historic brick pavers on majority of 6th Ave. Maintain historic pavers in place as noted on plan.

New accessible 6' wide walk adjacent to loading dock areas

New concrete crosswalks with accessible curb ramps



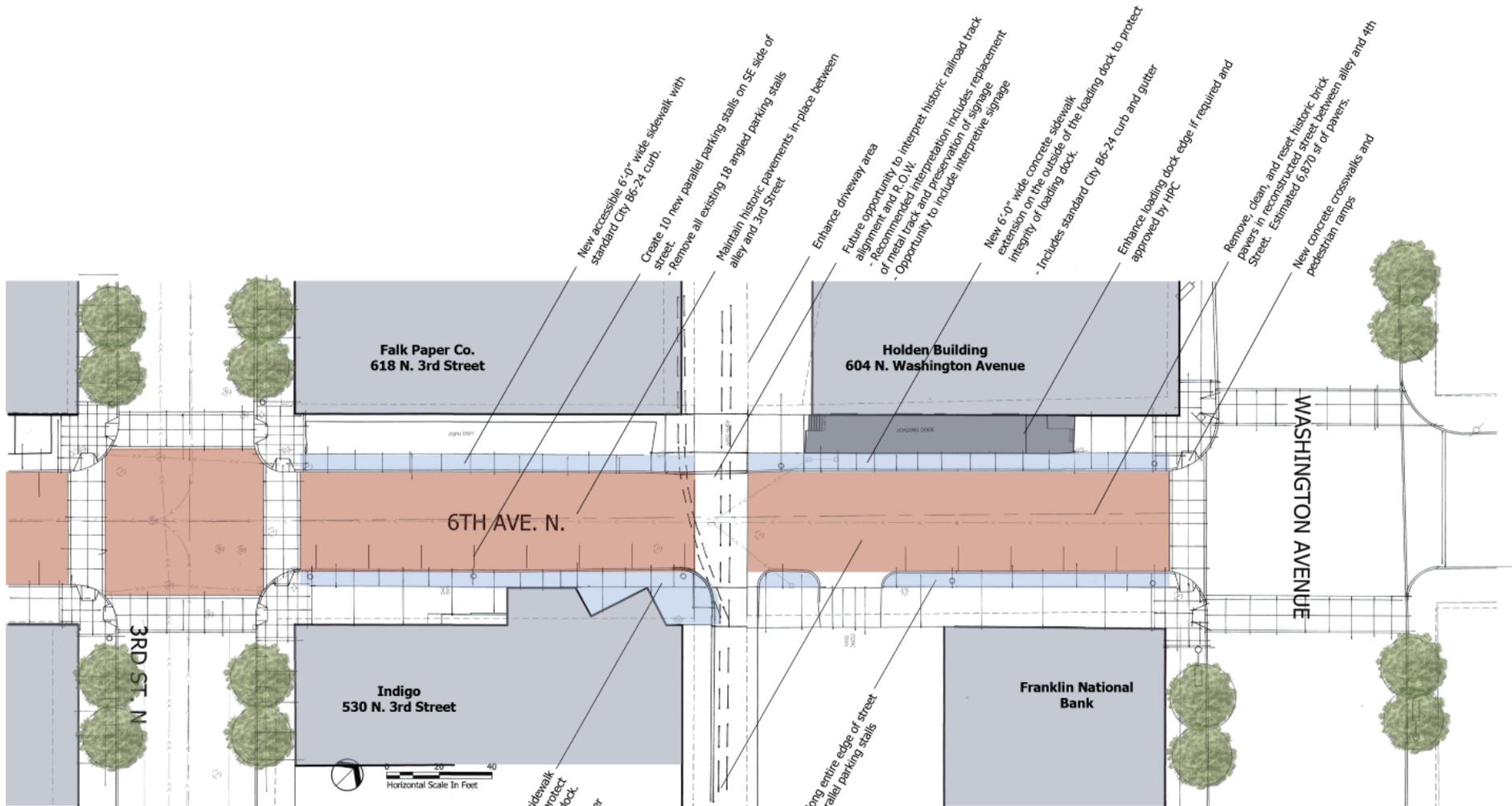
Proposed view looking north along 6th Avenue



Existing ROW: 80'-0"  
 Current Street Width: 50'-0"  
 Proposed Street Width: 44'-0"

- Key
- Historic Pavers
  - New Accessible Sidewalk -with City standard B6-24 curb
  - Existing Loading Docks
  - Active Loading Dock





Existing ROW: 80'-0"  
 Current Street Width: 49'-0"  
 Proposed Street Width: 38'-0"

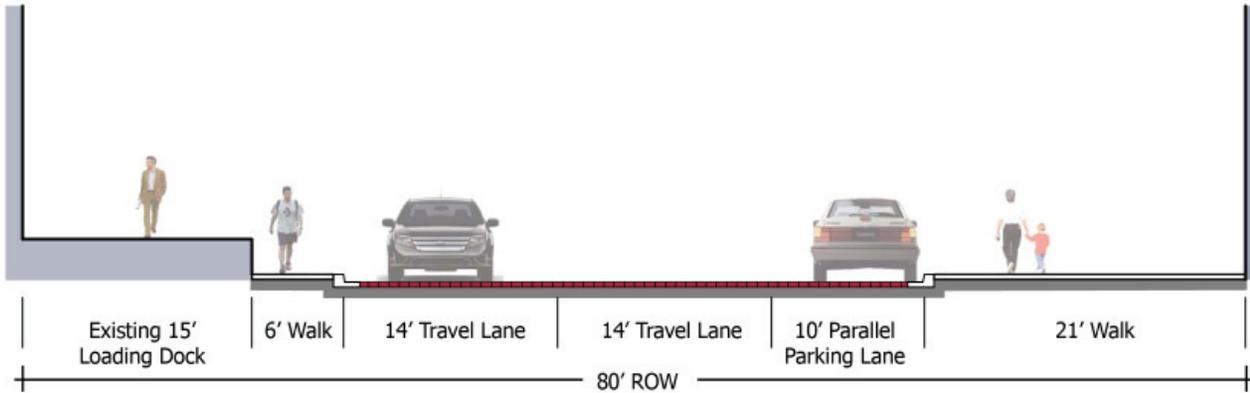
- Key**
- Historic Pavers
  - New Accessible Sidewalk  
-with City standard B6-24 curb
  - Existing Loading Docks
  - Active Loading Dock

extension on the outside of the loading dock to protect integrity of loading dock.  
 - Includes standard City B6-24 curb and gutter

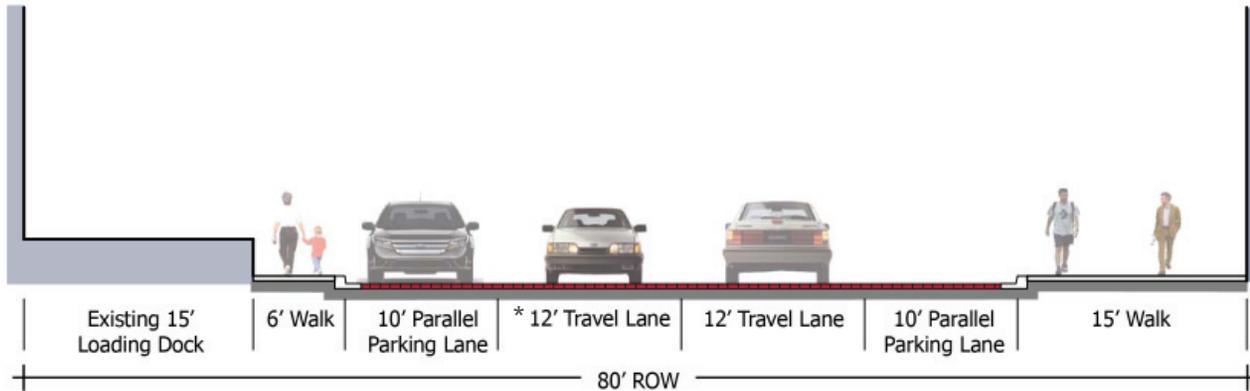
38' wide Street  
 (2) 1/4 drive lanes and (1) 10' wide parallel parking stall on SE side of street

Expand sidewalk area to align along entire edge of street.  
 -replace angled parking with 4 parallel parking stalls





Proposed Cross-Section  
6th Avenue North (4th Street North to 3rd Street North) looking south



Proposed Cross-Section  
6th Avenue North (3rd Street North to Washington Avenue North) looking south

*\*If future loading docks and loading areas decrease in use there will be an opportunity to re-evaluate the design of the roadway and narrow the two drive lanes to 11'-0" wide and provide an 8'-0" wide sidewalk.*

## Existing Conditions

### 5th Avenue N. (4th Street N to 5th Street N)

- Existing ROW along 5th Avenue N is 80' wide
- Current Street width is 48' wide
- 5th Avenue N is a designated MSA (Municipal State Aid) route
- ADT's for 5th Avenue are 3800
- There is currently on-street parking on portions of both sides of the street. A majority of this parking is metered parking.
- It is estimated about 30% of the historic street pavement is exposed and roughly about 70% of the pavement is covered with bituminous patching.
  - Very little visible areas of pavement settling
  - Estimated 70% of pavement appears to be in fair to good condition
- Sidewalk exists on both sides of the street.
  - Sidewalk on the west side of street is in poor condition based on visual analysis. Sidewalk is interrupted by numerous driveways to parking areas. Curbs also appear to be in fair condition.
  - Sidewalk on the east side of street is in poor condition based on visual analysis. Curbs also appear to be in fair condition.
- Existing on-street bike lane is defined on the east side of the street and a shared bike lane exists on the west side of the street.
- ADA accessibility
  - Accessibility on both sides of the street is considered fair.
  - Steep sidewalk slopes can cause issues in the winter months.

## Existing Photos



1 Multiple areas of patching along street  
 • pavers in good condition in center park of street



2 High point in street offers good views to neighborhood



3 Dark areas exist under highway overpass



4 Bike lane in poor condition

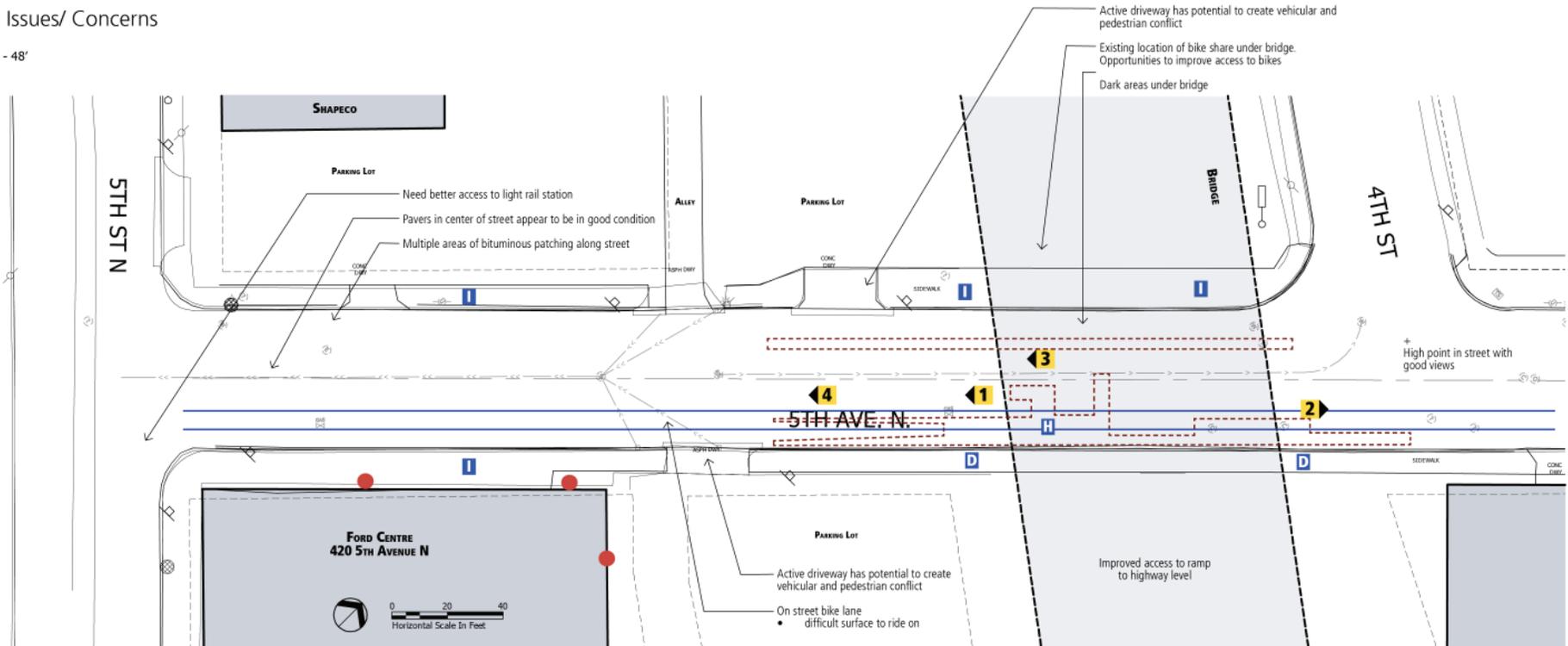
- A Concrete loading dock not ADA Accessible
- B No pedestrian ramp
- C No sidewalk connection
- D Sidewalk in poor condition
- E Areas of visible standing water

- F Multiple driveways create pedestrian/vehicular conflict
- G Damaged driveway
- H Bituminous patch overlay on road
- I Sidewalk in good/fair condition
- J Active delivery areas with trucks

- Pedestrian Door to Building
- Overhead delivery Door
- Active Loading Dock
- Active Loading Area

## Existing Issues/ Concerns

ROW - 80'  
 Street Width - 48'  
 ADT - 3800



Areas of street patching - Approximately 2,650 sf  
 23 parallel parking stalls

5th Avenue Existing Conditions  
**WAREHOUSE DISTRICT HERITAGE STREET PLAN**

## Existing Conditions

### 5th Avenue N. (3rd Street N to 4th Street N)

- Existing ROW along 5th Avenue N is 80' wide
- Current Street width is 48' wide
- 5th Avenue N is a designated MSA (Municipal State Aid) route
- ADT's for 5th Avenue are 3800
- There is currently on-street parking on portions of both sides of the street. All of this parking is metered parking.
- Historic pavements are visible only in half of the street. It is estimated about 55% of the historic street pavement is exposed and roughly about 45% of the pavement is covered with bituminous patching.
  - Very little visible areas of pavement settling
  - Estimated 85% of pavement appears to be in good condition
- Sidewalk exists on both sides of the street.
  - A short section of sidewalk along the west side is new. Remaining areas of sidewalk on the west side of street is in poor condition based on visual analysis. Sidewalk is interrupted by numerous driveways to parking areas and alleys. Curbs also appear to be in fair to poor condition.
  - Sidewalk on the east side of street is in fair condition based on visual analysis. Curbs also appear to be in fair condition.
- Existing on-street bike lane is defined on the east side of the street and a shared bike lane exists on the west side of the street.
- ADA accessibility
  - Accessibility on both sides of the street is considered fair.
  - Steep sidewalk slopes can cause issues in the winter months.

## Existing Photos



1 Long views at high point in road towards Washington Avenue



2 Long views towards Ford Center from intersection of 3rd Street



3 Pavers in streets in good condition. Shared bike lane and on-street parking conflicts



4 Sidewalks on the block appear in good condition but have steep grades.

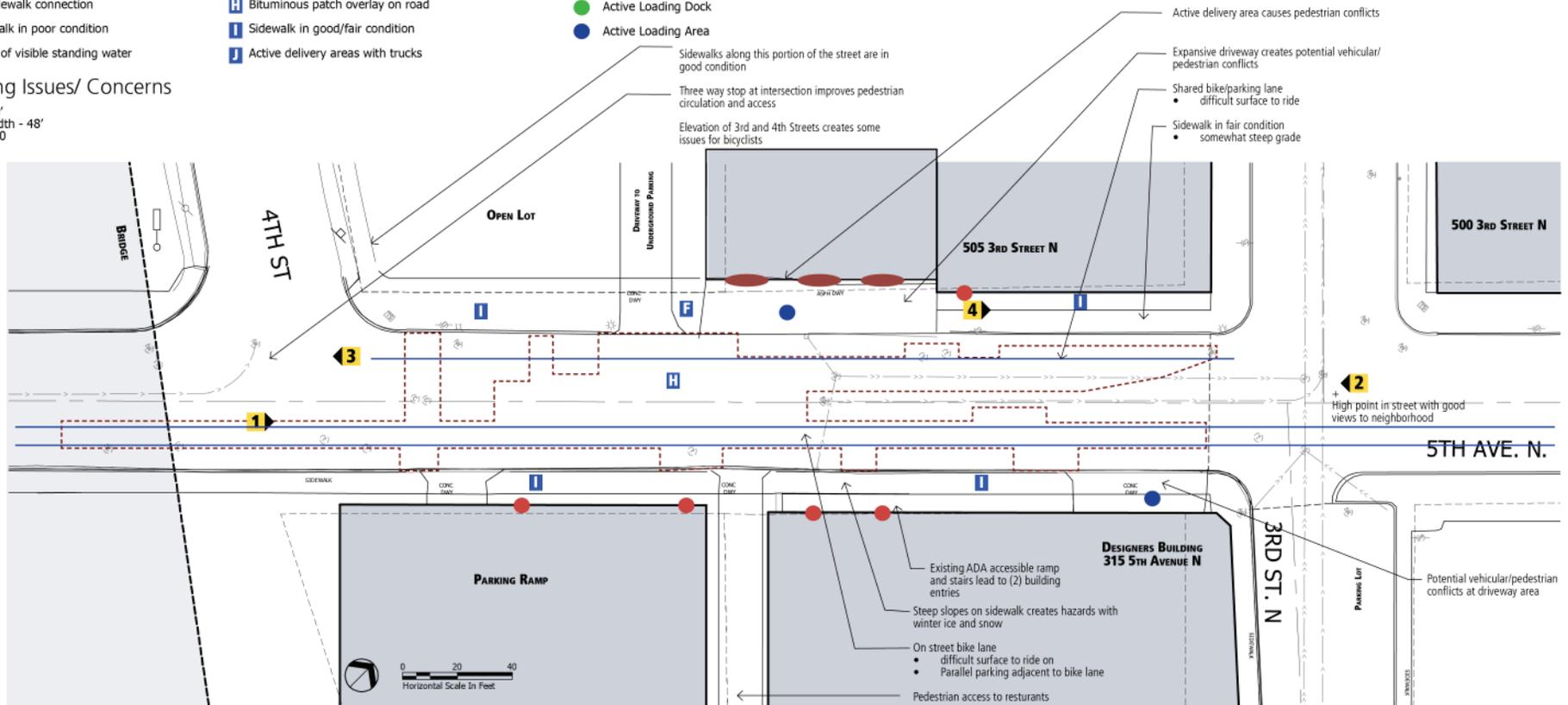
- A** Concrete loading dock not ADA Accessible
- B** No pedestrian ramp
- C** No sidewalk connection
- D** Sidewalk in poor condition
- E** Areas of visible standing water

- F** Multiple driveways create pedestrian/vehicular conflict
- G** Damaged driveway
- H** Bituminous patch overlay on road
- I** Sidewalk in good/fair condition
- J** Active delivery areas with trucks

- Pedestrian Door to Building
- Overhead delivery Door
- Active Loading Dock
- Active Loading Area

## Existing Issues/ Concerns

ROW - 80'  
Street Width - 48'  
ADT - 3800



   Areas of street patching - Approximately 10,400 sf  
12 parallel parking stalls

5th Avenue Existing Conditions  
**WAREHOUSE DISTRICT HERITAGE STREET PLAN**

## Existing Conditions

### 5th Avenue N. (Washington Avenue to 3rd Street N)

- Existing ROW along 5th Avenue N is 80' wide
- Current Street width is 48' wide
- 5th Avenue N is a designated MSA (Municipal State Aid) route
- ADT's for 5th Avenue are 3800
- There is currently on-street parking on portions of both sides of the street. All of this parking is metered parking.
- No historic pavements are visible in the street.
- New continuous sidewalk on the east side of the street. No sidewalk on SW side of street.
  - Sidewalk along the east side of the street is new. Sidewalk along the NW side of the street in very poor condition. Sidewalk is interrupted by numerous driveways to parking areas and alleys.
  - Curbs also appear to be in fair to poor condition.
- Existing on-street bike lane is defined on the east side of the street and a shared bike lane exists on the west side of the street.
- Loading dock occurs along the SW side of the street.
  - 500 N 3rd Street building has a loading dock that appears to be in poor condition. Loading dock contains a small ramp along the south edge that is not ADA compliant and provides a stair access along the north edge. Loading dock leads to building primary entrance.
  - Loading dock is not active.
- ADA accessibility
  - Accessibility on east side of street is good. Accessibility on west side of street is poor with existing sidewalk and loading dock in poor condition. It is common to see pedestrians walking in the street adjacent to the loading dock area.

## Existing Photos



1 Sidewalk in poor condition. Pedestrians prefer to walk on streets vs. loading dock



2 Loading dock appears private property so perceived no access



3 Edge of dock in fair to poor condition. Metal infrastructure in fair condition



4 Concrete surface of loading dock in fair condition



5 New sidewalk is wide with gentle slope. Potential area to add street trees

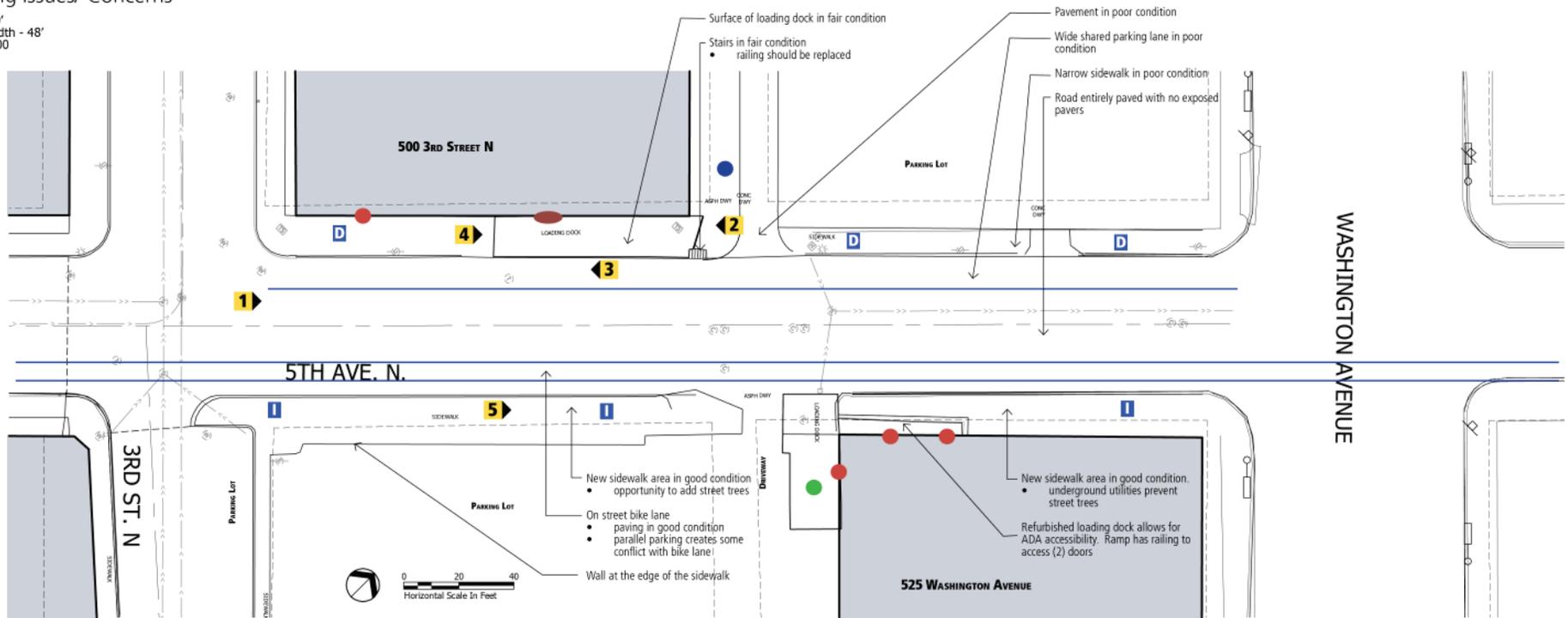
- A Concrete loading dock not ADA Accessible
- B No pedestrian ramp
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- D Sidewalk in poor condition
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- F Multiple driveways create pedestrian/vehicular conflict
- G Damaged driveway
- H Bituminous patch overlay on road
- I Sidewalk in good/fair condition
- J Active delivery areas with trucks

- Pedestrian Door to Building
- Overhead delivery Door
- Active Loading Dock
- Active Loading Area

## Existing Issues/ Concerns

ROW - 80'  
Street Width - 48'  
ADT - 3800



Areas of street patching  
27 parallel parking stalls

5th Avenue Existing Conditions  
**WAREHOUSE DISTRICT HERITAGE STREET PLAN**



## Primary Recommendations 5th Avenue N. (Washington Avenue to 5th Street N)

- ROW width: Existing ROW along 5th Avenue N is to remain at 80'-0" wide
- Street width: Narrow street width to 42'-0" wide. The proposed street section would consist of (2) 11'-0" drive lanes, (2) 5'-0" wide bicycle lanes (6'-0" wide bicycle lanes are preferred) and (1) 10' wide parallel parking lane on the south/east side of the street.
- Street design: Proposed street section should be reconstructed of concrete.
- On-street parking: Proposed parking lane is 10'-0" wide x 20'-0" long. There are approximately 12 parking stalls that will remain as part of the proposed street improvements.

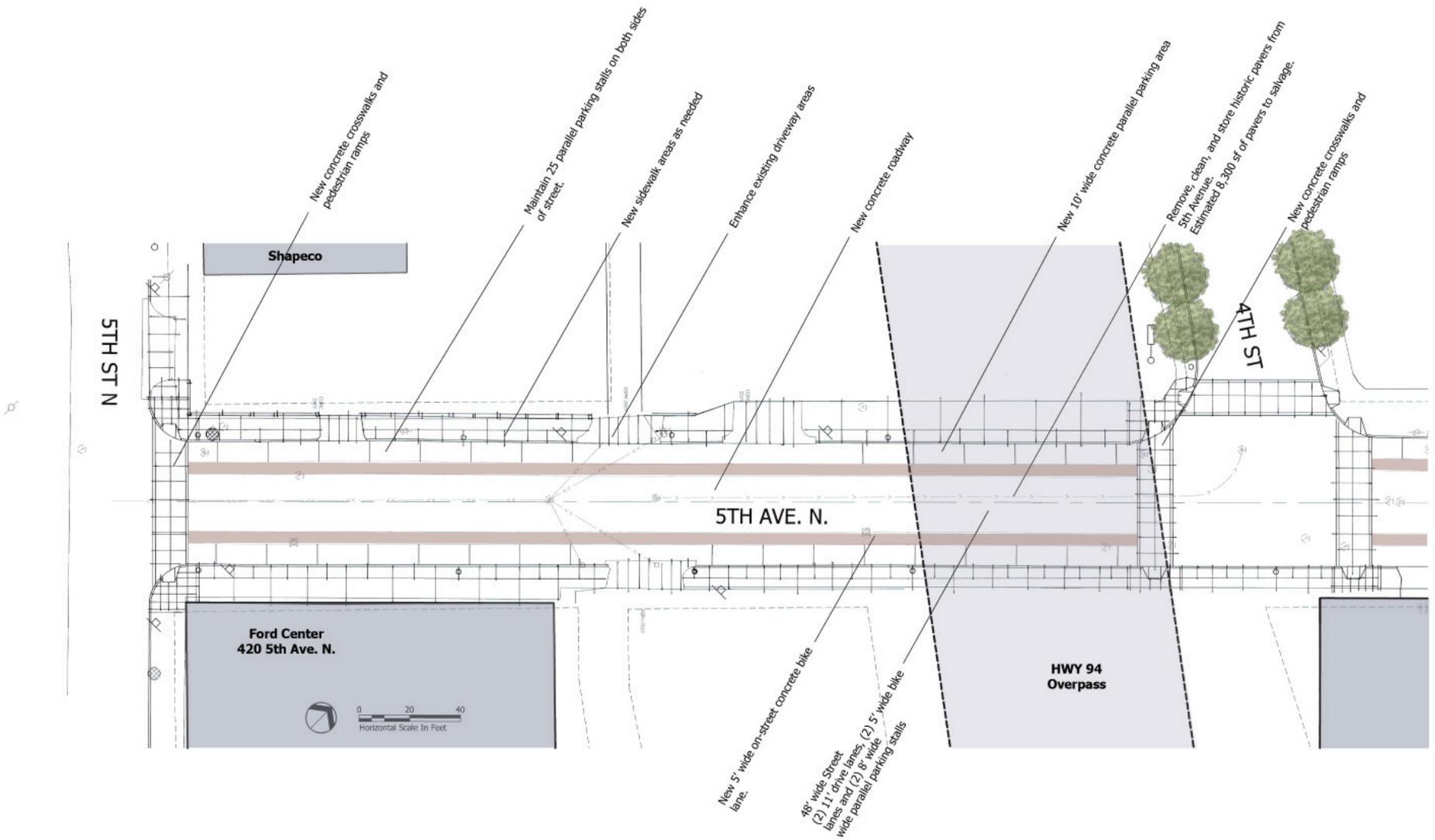
- Sidewalks: Widen sidewalk area on the north/east side of the street to approximately 6'-0" on the outside of the loading dock at the Western Container Building (500 3rd Street N.) The widened sidewalk should extend to Washington Avenue and provide a new sidewalk that will be approximately 16'-0" wide. The proposed sidewalk widening should be designed to meet City design standards and be ADA compliant.
- Driveways: Consolidate and narrow existing driveways, alleys and other curb cuts along the street in conjunction with future redevelopment opportunities.
- Crosswalks: Provide new concrete crosswalks and ADA accessible pedestrian ramps at all street intersections. Pedestrian ramps shall conform to City of Minneapolis, ADA Standards for Accessible Design, and PROWAAC standards.



Existing View of 5th Avenue



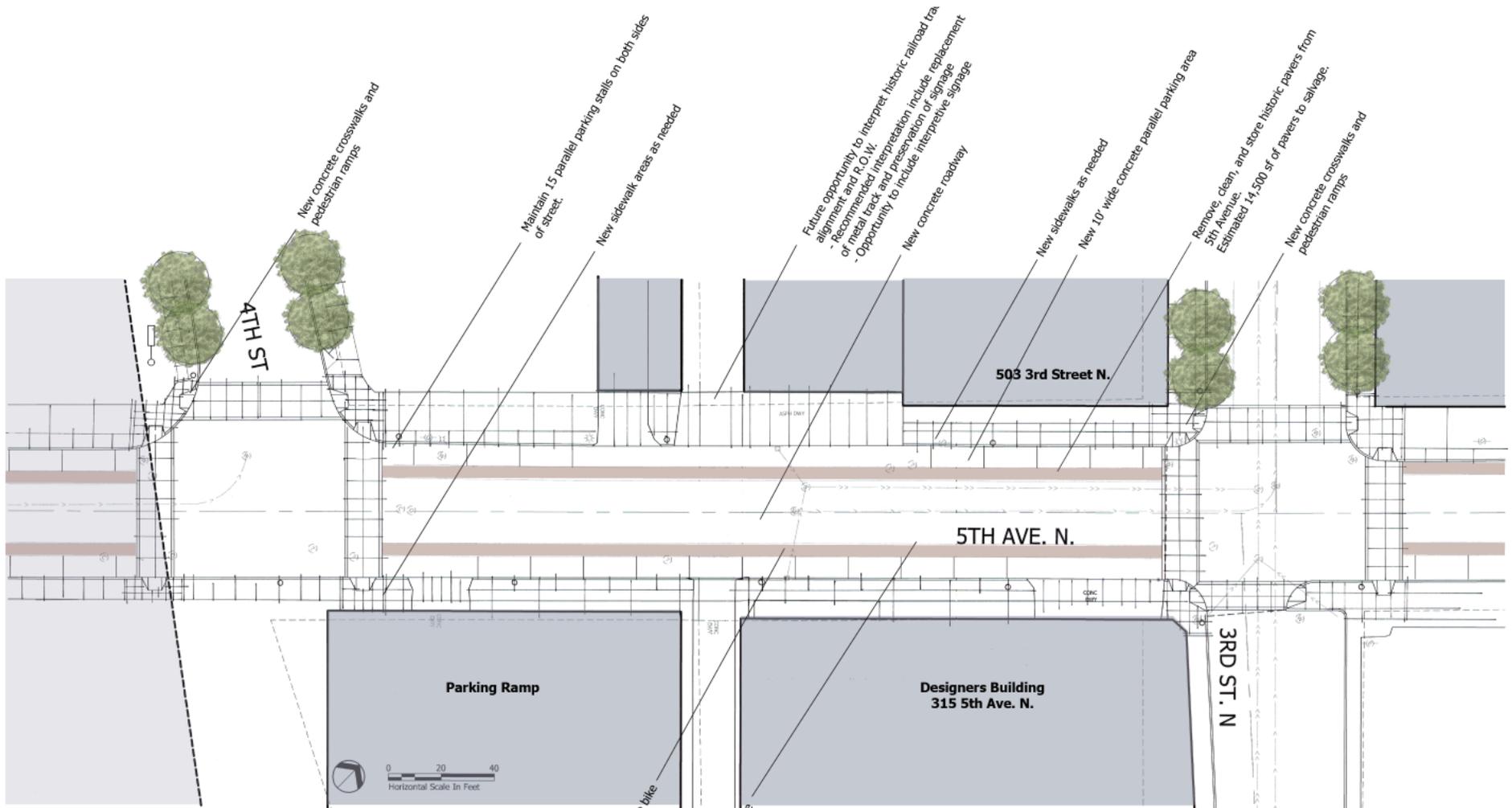
Proposed view looking north along 5th Avenue



Existing ROW: 80'-0"  
 Current Street Width: 48'-0"  
 Proposed Street Width: 48'-0"

- Key
- Historic Pavers
  - New Accessible Sidewalk  
-with City standard B6-24 curb
  - Existing Loading Docks
  - Active Loading Dock





Existing ROW: 80'-0"  
 Current Street Width: 48'-0"  
 Proposed Street Width: 48'-0"

- Key
- Historic Pavers
  - New Accessible Sidewalk  
-with City standard B6-24 curb
  - Existing Loading Docks
  - Active Loading Dock

New 5' wide on-street concrete bike lane.  
 48' wide Street lanes and (2) 11' drive lanes (2) 5' wide bike wide parallel parking stalls





View east towards 6th Avenue N. The current street is paved with asphalt and there are no visible pavers.



Wide sidewalks exist along 3rd Street adjacent to the Duffy Paper Building. The opportunity exists to enhance the existing ROW with street trees.



Wide sidewalks exist along both sides of 3rd Street N.

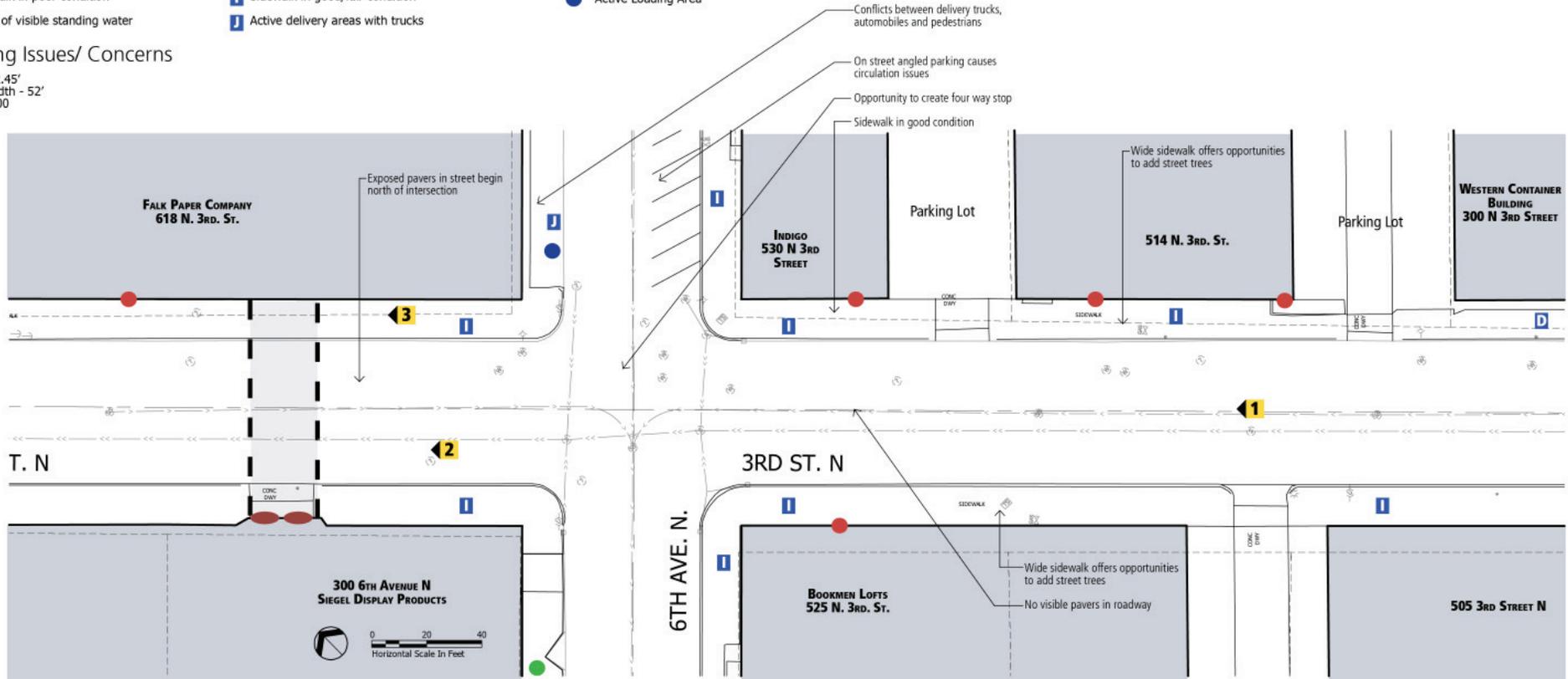
concrete loading dock not ADA Accessible  
 pedestrian ramp  
 sidewalk connection  
 sidewalk in poor condition  
 no visible standing water

- F** Multiple driveways create pedestrian/vehicular conflict
- G** Damaged driveway
- H** Bituminous patch overlay on road
- I** Sidewalk in good/fair condition
- J** Active delivery areas with trucks

- Pedestrian Door to Building
- Overhead delivery Door
- Active Loading Dock
- Active Loading Area

### Existing Issues/ Concerns

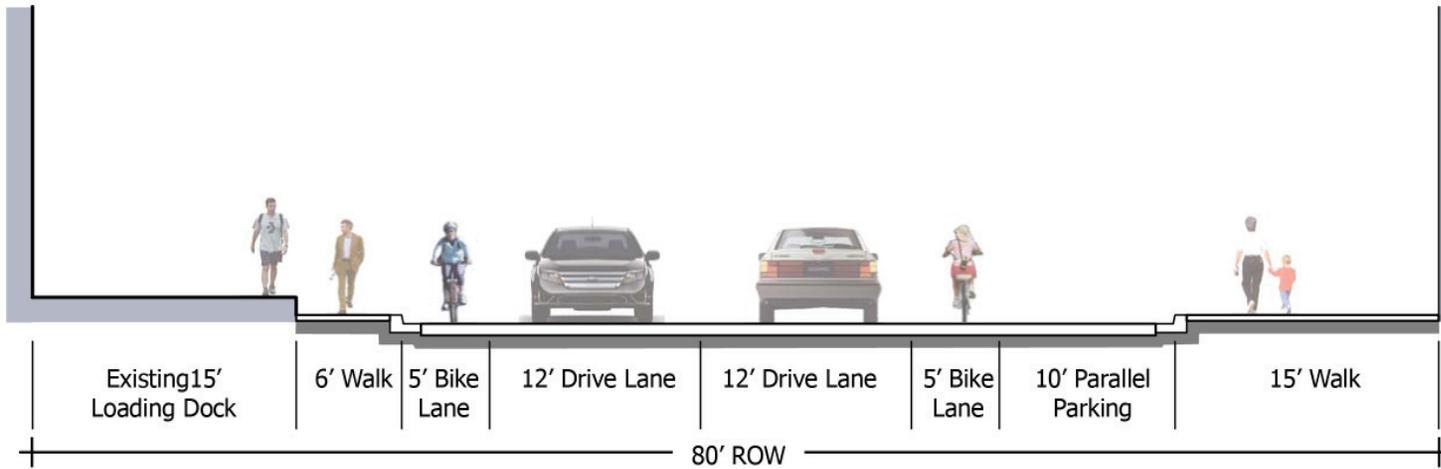
82.45'  
 Width - 52'  
 .700



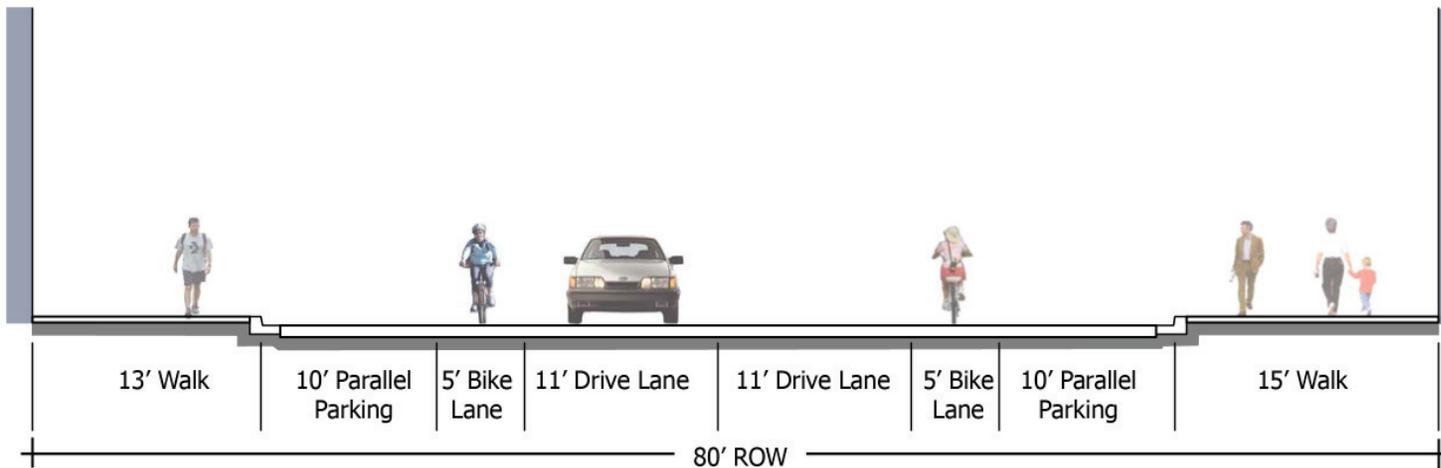
30 parallel parking stalls

3rd Street Existing Conditions  
**WAREHOUSE DISTRICT HERITAGE STREET PLAN**





Proposed Cross-Section  
5th Avenue North (5th Street North to 3rd Street North)



Proposed Cross-Section  
5th Avenue North (3rd Street North to Washington Avenue North)

## Existing Conditions Traffic Street

- Existing ROW along Traffic Street is 40' wide
- Current Street width varies between 22' and 25' wide
- Traffic Street is not a designated MSA (Municipal State Aid) route
- There is currently on-street parking on the south side of the street and parking lots along the north side.
- Historic granite pavements are visible in the street. Historic granite paving appears to be in good condition but there are visible areas of missing pavers. Some pavement settling has also occurred.
- No sidewalks exist along either side of the street.
- ADA accessibility
  - The street is not ADA accessible.

## Existing Photos



1 View north along Traffic Street



2 Missing granite cobbles



3 Road paving in fair to good condition

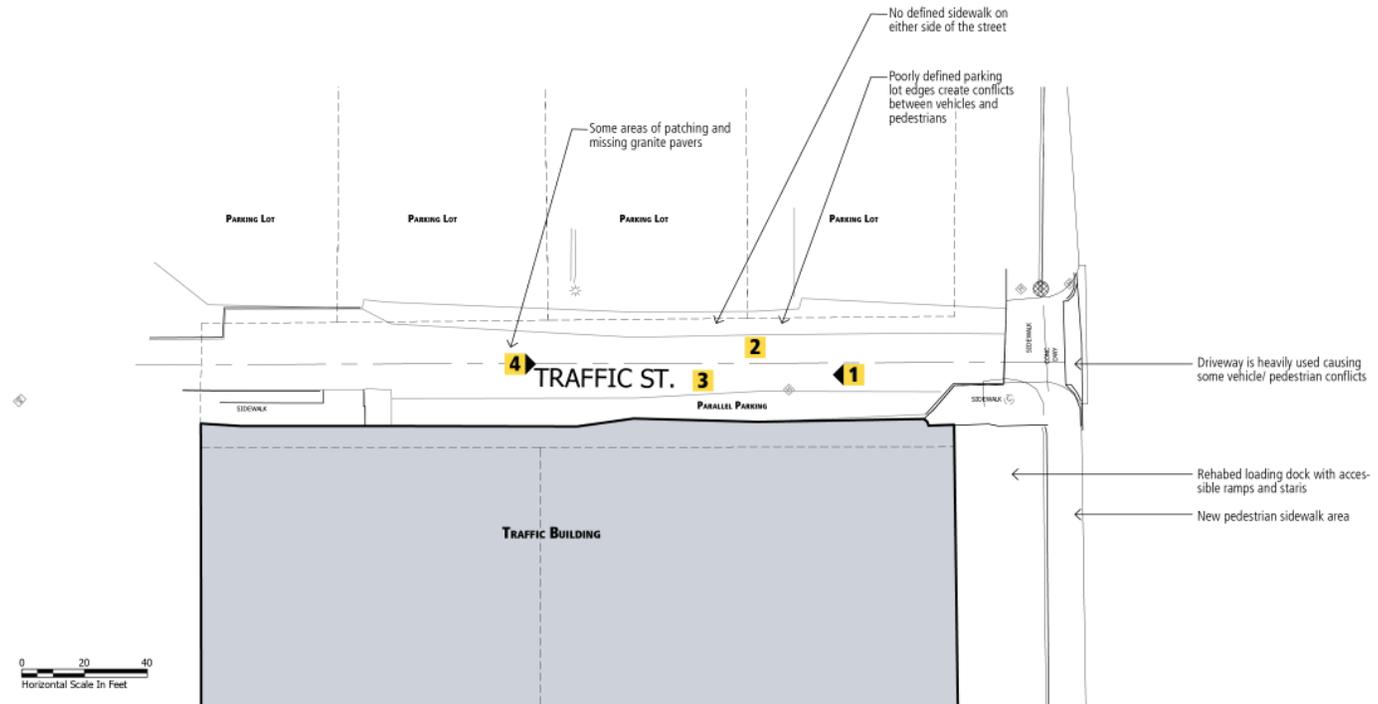


4 View towards Downtown along Traffic Street

- A** Concrete loading dock not ADA Accessible
- B** No pedestrian ramp
- C** No sidewalk connection
- D** Sidewalk in poor condition
- E** Areas of visible standing water
- F** Multiple driveways create pedestrian/vehicular conflict
- G** Damaged driveway
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- J** Active delivery areas with trucks

## Existing Issues/ Concerns

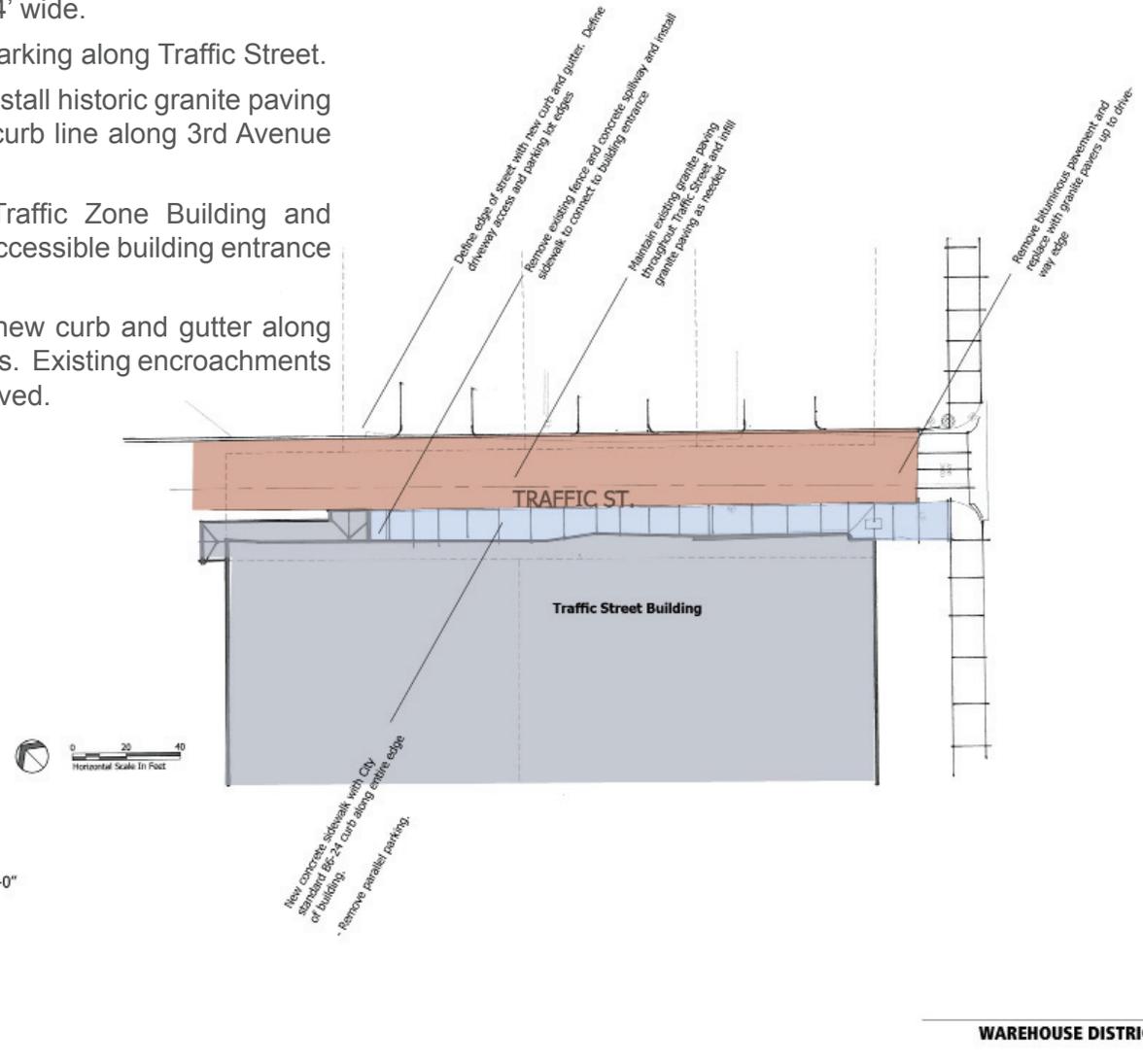
ROW - 40'  
 Street Width - Varies (22' - 25' wide)  
 ADT - Undefined (Not a Municipal State Aid Street)



Traffic Street Existing Conditions  
**WAREHOUSE DISTRICT HERITAGE STREET PLAN**

## Primary Recommendations Traffic Street

- ROW width: Existing ROW along Traffic Street is to remain at 40' wide.
- Street width: Proposed street width of 24' wide.
- On-street parking: Remove all parallel parking along Traffic Street.
- Granite paving: Remove, clean and re-install historic granite paving the entire length of the street from the curb line along 3rd Avenue North to the proposed Dock Street.
- Sidewalks: Enhance sidewalk along Traffic Zone Building and connect up to 3rd Avenue North and to accessible building entrance near proposed Dock Street.
- Street design: Define street edge with new curb and gutter along existing parking lots for adjacent buildings. Existing encroachments along Traffic Street should also be removed.



## Existing Conditions

### 2nd Avenue N. (1st Street N to Federal Reserve Parking Lot)

- Existing ROW along 2nd Avenue N is 80' wide
- Current Street width is 47' wide
- There is currently on-street parking on both sides of the street.
- Almost all historic pavements are visible. It is estimated about 97% of the historic street pavement is exposed and roughly about 3% of the pavement is covered with bituminous patching.
- Sidewalk exists on the north side of the street.
  - Sidewalk on the north side of street is in good/ fair condition based on visual analysis. Curbs also appear to be in fair condition.
- Loading dock occurs along the SE side of the street.
  - Loading dock contains a small ramp along the south edge that is not ADA compliant and provides a stair access along the north edge. Loading dock leads to building secondary entrance.
  - Loading dock is not active.
- ADA accessibility
  - Accessibility on the north side of the street is considered good

Existing Photos



1 View north west along 2nd Avenue N towards the River



2 View of historic paved street, sidewalk area and sandstone curb along 2nd Avenue N



3 View along loading dock on northwest edge of 2nd Avenue N



4 Close up view of historic Purington pavers placed in 2nd Avenue roadway



5 Wide view of 2nd Avenue N. Paving is generally in good condition and remains very level throughout the entire roadway



6 View towards 1st Street N along loading dock. Some pavers have heaved along the edge of the loading dock area

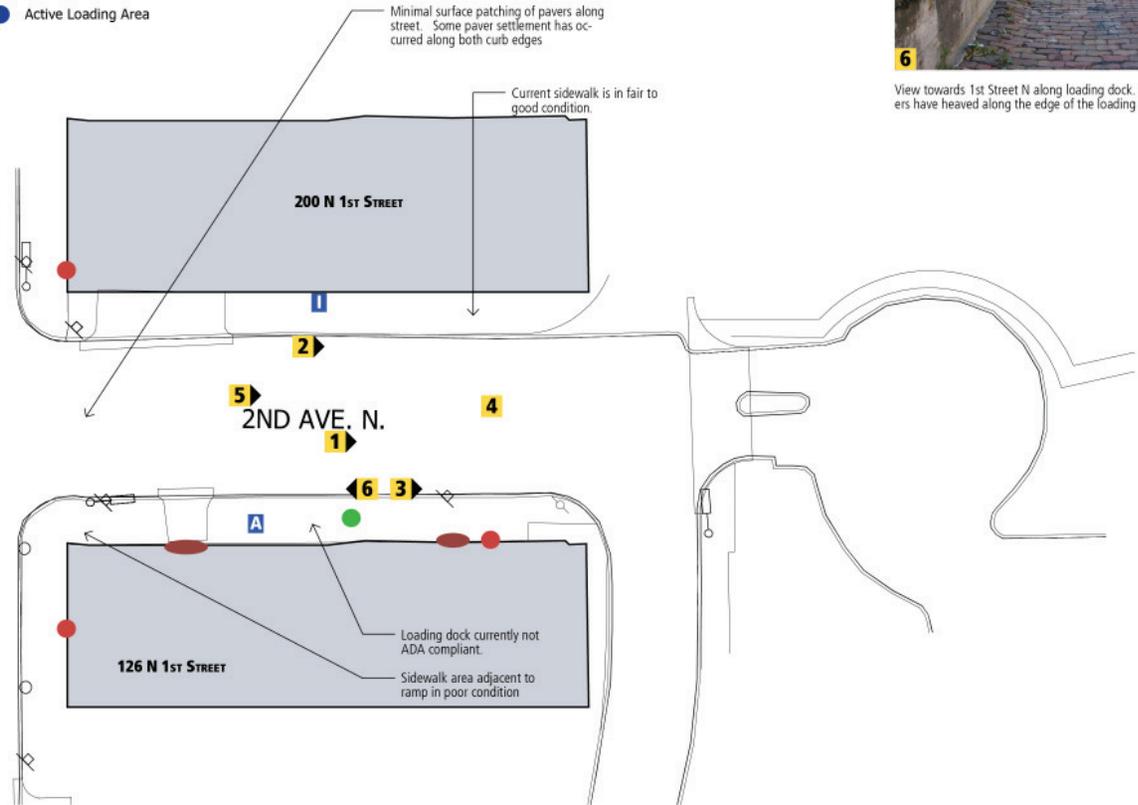
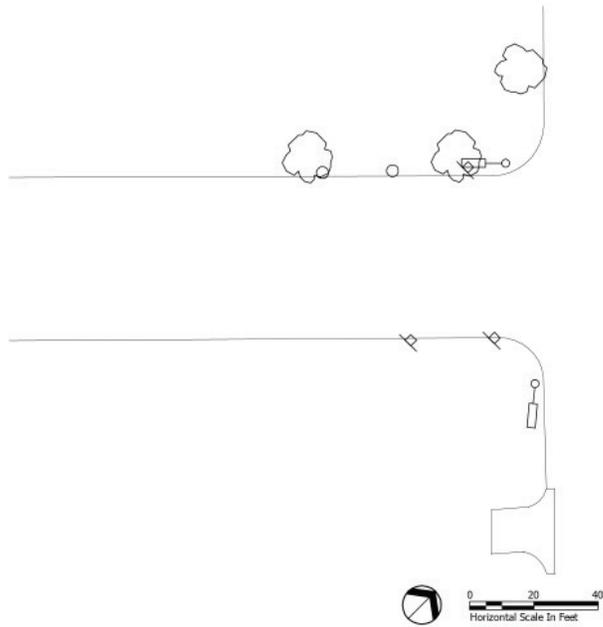
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- F Multiple driveways create pedestrian/vehicular conflict
- G Damaged driveway
- H Bituminous patch overlay on road
- I Sidewalk in good/fair condition
- J Active delivery areas with trucks

- Pedestrian Door to Building
- Overhead delivery Door
- Active Loading Dock
- Active Loading Area

Existing Issues/ Concerns

ROW - 80'-0"  
 Street Width - 48'-0" typ.  
 ADT - Undefined (Not a Municipal State Aid Street)



2nd Avenue Existing Conditions  
**WAREHOUSE DISTRICT HERITAGE STREET PLAN**

Warehouse District Heritage Street Plan



## Primary Recommendations

### 2nd Avenue N. (1st Street N to Federal Reserve Parking Lot)

- ROW width: Existing ROW along 2nd Avenue North to remain at 80' wide
- Street width: Street to remain at a width of 47'-0"
- On-street parking: Current on-street parking to remain.
- Brick pavers: Historic pavements in very good condition and it is recommended that the paving remain in place as is until there is a future need to repair/ replace.
- Street trees: Define future opportunities to add street trees and pedestrian level lighting along the north side of the street.
- Sidewalks: Replace the existing sidewalk as needed.



Existing ROW: 80'-0"  
 Current Street Width: 48'-0"  
 Proposed Street Width: 48'-0"

- Key**
- Historic Pavers
  - New Accessible Sidewalk  
-with City standard 86-24 curb
  - Existing Loading Docks
  - Active Loading Dock

**WAREHOUSE DISTRICT HERITAGE STREET PLAN**



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