



CPED STAFF REPORT
Prepared for the City Planning Commission

CPC Agenda Item #6
July 14, 2014
BZZ-6600

LAND USE APPLICATION SUMMARY

Property Location: 5355 34th Avenue South
Project Name: Steve's Tire and Auto
Prepared By: [Lisa Steiner](#), City Planner, (612) 673-3950
Applicant: APPRO Development
Project Contact: Jim Connelly
Request: To allow an addition to an existing minor automobile repair facility.
Required Applications:

Conditional Use Permit	To allow an expansion of an existing minor automobile repair facility in the CI District.
Variance	To increase the maximum size of a commercial use in the CI District from 4,000 square feet to approximately 4,495 square feet.
Site Plan Review	To allow a 2,701 square foot addition to an existing automobile service use.

SITE DATA

Existing Zoning	CI Neighborhood Commercial District AP Airport Overlay District
Lot Area	15,190 square feet
Ward	12
Neighborhood(s)	Minnehaha; adjacent to Keewaydin, Wenonah, and Morris Park
Designated Future Land Use	Mixed Use
Land Use Features	Neighborhood Commercial Node Community Corridor (34 th Avenue South)
Small Area Plan	None

Date Application Deemed Complete	May 30, 2014	Date Extension Letter Sent	Not applicable
End of 60-Day Decision Period	July 29, 2014	End of 120-Day Decision Period	Not applicable

BACKGROUND

SITE DESCRIPTION AND PRESENT USE. The site currently includes eight gas pumps with a large overhanging pump island canopy. An approximately 1,800 square foot automobile service station building with three service bays is located towards the rear of the site. The subject property has long been utilized for an automobile service station; it was rezoned in 1968 from R1A (Single-Family District) to B2S-1 (Neighborhood Service District) in order to allow for an automobile service station. The current owner has operated an automobile convenience facility and minor automobile repair facility at this site since the early 1990s. In 1997, the Zoning Board of Adjustment granted a front yard variance for this property from 20 feet to 8 feet 6 inches in order to permit a new pump island canopy at the existing automobile repair and service station.

SURROUNDING PROPERTIES AND NEIGHBORHOOD. The subject property is located at the northeast corner of 54th Street East and 34th Avenue South. A few small-scale neighborhood commercial uses are located at this intersection including a used books store, a dry cleaner, and another automobile repair facility. Residential uses are located directly north, east, and southeast of the subject property. 34th Avenue South is a neighborhood commercial corridor with many neighborhood-serving retail uses.

PROJECT DESCRIPTION. The applicant is proposing to remove the existing gasoline pumps, underground tanks, and pump island canopy, eliminating the automobile convenience facility use entirely. The existing 1,794 square foot one-story building is proposed to be remodeled with a 2,701 square foot addition proposed in order to accommodate three additional service bays and a larger waiting area. The proposed building addition will be one-story with a 593 square foot storage mezzanine above. Five of the service bays will be accessed through three garage doors along the west elevation of the building. A sixth service bay is proposed along 54th Street East. The applicant has indicated this will be their express service bay.

PUBLIC COMMENTS. The Nokomis East Neighborhood Association has indicated their support of the project. Any additional correspondence received prior to the public meeting will be forwarded on to the Planning Commission for consideration.

ANALYSIS

CONDITIONAL USE PERMIT

The Department of Community Planning and Economic Development has analyzed the application to allow the expansion of an existing minor automobile repair facility based on the following [findings](#):

1. *The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, comfort or general welfare.*

New minor automobile repair facilities are not permitted in the CI District. However, existing minor automobile repair facilities are permitted in the CI District with a Conditional Use Permit. This property has been utilized as an automobile convenience facility since the building was constructed in 1970 and has been under the current ownership for the last 21 years. The removal of the gas pumps will reduce the frequency of people coming to and from the site and should reduce traffic congestion on the site as well as odors and noise. Additionally, it will no longer be necessary for fuel delivery trucks to come to the site. CPED staff finds that the expansion of this minor

automobile repair facility will not be detrimental to or endanger the public health, safety, comfort or general welfare.

2. *The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.*

Although there are residential uses adjacent to this site, the property has been a minor automobile repair facility for over forty years. The applicant has stated that most of their business comes from the direct neighborhood. Although the facility is proposed to expand by three service bays, it is still anticipated to draw from the immediate neighborhood and is focused on being a neighborhood-serving business. The expanded automobile repair facility will not be injurious to the use or enjoyment of other property in the vicinity and will not impede the normal and orderly development or improvement of surrounding property.

3. *Adequate utilities, access roads, drainage, necessary facilities or other measures, have been or will be provided.*

There are currently three curb cuts leading into and out of this site. The applicant is proposing to retain all three curb cuts. However, the applicant is proposing to shift the location of the curb cut further south along 34th Avenue South to align with the new layout of the parking area. This will improve the public sidewalk along 34th Avenue South and allow for increased screening of the parking spaces on the north of the site. The proposed parking is adequate for the site as it meets its minimum off-street parking requirement. The parking requirement for a minor automobile repair facility is one space per 500 square feet of gross floor area plus two spaces per service bay. Approximately 865 square feet of the proposed building will be office space and six service bays are proposed. The parking requirement is 14 spaces and the applicant is proposing 14 spaces.

4. *Adequate measures have been or will be taken to minimize traffic congestion in the public streets.*

With the elimination of fuel sales, traffic congestion around this site is anticipated to decrease, with less frequent trips to the site. After the Preliminary Development Review report was created, the applicant redesigned the service bay on 54th Street East to prevent non-compliant vehicle maneuvers and the Public Works Department determined that the easternmost curb cut could therefore be allowed to remain. Maneuvering templates are provided on the site plan which ensure that all maneuvering can take place on site. The applicant has indicated that only the employees of the facility will be driving cars into and out of the service bays. Though the easternmost service bay along 54th is expected to be utilized as the express service bay, the applicant has indicated that this bay is not expected to have more frequent traffic than the other 5 service bays. Adequate measures have been taken to minimize traffic congestion in the public streets.

5. *The conditional use is consistent with the applicable policies of the comprehensive plan.*

The proposed development would be consistent with the following general land use policies of *The Minneapolis Plan for Sustainable Growth*:

Land Use Policy 1.1: Establish land use regulations to achieve the highest possible development standards, enhance the environment, protect public health, support a vital mix of land uses, and promote flexible approaches to carry out the comprehensive plan.

- 1.1.5 Ensure that land use regulations continue to promote development that is compatible with nearby properties, neighborhood character, and natural features; minimizes

pedestrian and vehicular conflict; promotes street life and activity; reinforces public spaces; and visually enhances development.

Land Use Policy 1.7: Limit new and expanded auto-oriented uses in the city so impacts on the form and character of commercial areas and neighborhoods can be minimized.

- 1.7.1 Discourage new and expanded high traffic, auto-oriented uses in neighborhood commercial nodes.

Land Use Policy 1.9: Through attention to the mix and intensity of land uses and transit service, the City will support development along Community Corridors that enhances residential livability and pedestrian access.

- 1.9.1 Support the continued presence of existing small-scale retail sales and commercial services along Community Corridors.

Land Use Policy 1.11: Preserve and enhance a system of Neighborhood Commercial Nodes that includes a mix of housing, neighborhood-serving retail, and community uses.

- 1.11.3 Discourage new or expanded uses that diminish the transit and pedestrian character of Neighborhood Commercial Nodes, such as some automobile services and drive-through facilities.

- 6. *The conditional use shall, in all other respects, conform to the applicable regulations of the district in which it is located.*

With the approval of the conditional use permit, variance, and site plan review, the proposal will comply with all provisions of the CI Neighborhood Commercial District.

VARIANCE

The Department of Community Planning and Economic Development has analyzed the application for a variance of the maximum size of a commercial use in the CI District based on the following [findings](#):

- 1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

In the CI district, all commercial uses are limited to 4,000 square feet. If there is no parking located between the principal structure and the street, the maximum allowed is 6,000 square feet for a one-story building or 8,000 square feet if within a 2 story building. Because the applicant's proposal maintains parking between the principal structure and the street, the maximum size allowed is 4,000 square feet. The existing building was constructed in 1970 towards the back of the lot to accommodate space for the gas canopy area. In considering the options for locating an addition, there would not be enough space to provide the required parking behind the existing structure, so the required parking must be located between the principal structure and the street. Therefore, the applicant is not able to qualify for the increased maximum floor area allowed by the zoning code. Practical difficulties for siting an addition exist because of the location of the existing structure.

- 2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

The proposal to increase the maximum size from 4,000 square feet to 4,495 square feet is reasonable and will be in keeping with the spirit and intent of the ordinance and the comprehensive

plan. The property will continue to be utilized as an automobile repair facility serving primarily patrons of the neighborhood. The use is allowed as a conditional use in the CI District because the minor automobile repair facility has existed on this site since 1970. The additional 495 square feet to allow for the proposed addition are found to be reasonable.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

The proposal to allow for a 495 square foot larger commercial space than allowed would not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. The variance will not be detrimental to the health, safety, or welfare of the general public or nearby properties. The size of the building will still allow the applicant to meet their required parking and exceed their landscaping requirement.

SITE PLAN REVIEW

The Department of Community Planning and Economic Development has analyzed the application based on the required [findings](#) and [applicable standards](#) in the site plan review chapter:

I. Conformance to all applicable standards of Chapter 530, Site Plan Review.

Building Placement and Design – *Requires alternative compliance*

- The placement of the building over 60 feet from the west property line and 19 feet from the south property line does not reinforce the street wall. The zoning code states that the first floor of a building shall not be more than eight feet from front lot line; building walls abutting each street on a corner are also subject to this requirement. Alternative compliance is requested.
- The area between the front lot line along 34th Avenue South would include nearly 12 feet of landscaping for the majority of the site. The area between the side lot line along 54th Street East would also contain landscaping.
- The building will be oriented so that the principal entrance will face the front lot line. Additional secondary entrances will be provided along the south and north elevations.
- The proposed building walls provide architectural detail and contain windows in order to create visual interest and increase security of adjacent outdoor spaces.
- Blank, uninterrupted walls that do not contain architectural elements will not exceed 25 feet in length.
- The primary exterior materials would be brick, fiber cement panels, glass, and rough-face concrete block, which are durable. Cast stone is proposed for the window sills.
- The materials and appearance of the side and rear walls are similar to and compatible with the front walls. Although the rear wall of the building is proposed to differ from the rest and will be primarily rough-face concrete block, the applicant is proposing to wrap the brick from the front and side walls around the rear corner for four feet.
- The applicant is proposing to use rough-face rather than plain-face concrete block along the rear wall of the building which is adjacent to a residential district.
- The principal entrance will be clearly defined with a canvas awning and a pitched roof to express the importance of the entrance.
- The window requirement for non-residential uses applies to both the 54th Street East and 34th Avenue South elevations. The requirements for windows are being met, although on 34th

Avenue South the exact minimum of 30.0% is being met, while 54th Street exceeds its requirement by 1.8%. See **Table I**.

- Required windows are vertical in proportion and are distributed in a more or less even manner.
- First floor windows are proposed to be tinted with a visible light transmittance ratio of only 0.4. Alternative compliance is necessary. Staff is recommending that the Planning Commission does not grant alternative compliance for the tinted windows.
- Parking, loading, storage, or mechanical equipment rooms will not take up more than 30 percent of the linear building frontage.
- The form and pitch of the proposed roof line is similar to the surrounding residential buildings of the neighborhood. Most commercial buildings in the area are flat roofed.

Table I. Percentage of Windows Required for Elevations Facing a Public Street, Sidewalk, Pathway, or On-Site Parking

	Code Requirement		Proposed	
Nonresidential Uses				
1 st Floor – 34 th Ave S	30% minimum	593 sq. ft.	30.0%	178 sq. ft.
1 st Floor – 54 th St E	30% minimum	484 sq. ft.	31.8%	154 sq. ft.

Access and Circulation – *Requires alternative compliance*

- Well-lit walkways at least 4 feet in width are required to connect buildings with an adjacent public sidewalk and any on-site parking. No walkways are provided between the building and the adjacent sidewalk or on-site parking. Alternative compliance is required.
- Transit shelters are not a part of this proposal.
- The applicant is retaining three curb cuts. Because the easternmost curb cut is necessary for the sixth service bay, the applicant has stated that it is not possible to consolidate the curb cuts. However, the applicant has proven that all maneuvering for the service bays can remain on-site. Vehicular access and circulation has been designed to minimize conflicts with pedestrians and surrounding residential properties. See further analysis in finding #4 of the conditional use permit portion of this report.
- The site is adjacent to the public alley on the block but there will be no access to it from this property.
- The proposed site plan minimizes the use of impervious surfaces. There is no maximum impervious surface requirement in the CI District. The applicant is proposing to reduce the area of impervious surfaces on the site by 1,928 square feet. The site as proposed will be 78.4% impervious surface, whereas it is currently 91.1% now.

Landscaping and Screening – *Requires alternative compliance*

- The overall composition and location of landscaped areas complement the scale of the development and its surroundings.
- The proposed project exceeds the requirement that not less than 20 percent of the lot not covered by buildings be landscaped. The applicant is proposing approximately 1,000 square feet of landscaping in excess of their requirement, or 29.9% landscaping. **See Table 2.**
- The zoning code requires that a 7-foot wide landscaped yard be provided along a public street, sidewalk or pathway when adjacent to a parking lot or a loading area. The applicant is exceeding this requirement and providing a 12 foot landscaped yard along most of the property’s 34th Avenue South frontage. A 9 foot 6 inch landscaped yard will be provided at the northwest corner in order to accommodate required parking.

- 7 feet of landscaping would be required in order to screen the single parking space south of the proposed building. This is an existing landscaped area with existing shrubs. An existing landscaped yard 6 feet in width is currently located in this area. Alternative compliance is required in order to retain the existing 6 foot landscaped yard.
- Screening that is 3 feet high and at least 65 percent opaque is required in the landscaped yards facing the public street and sidewalk in order to screen the parking area. Alpine currant shrubs are proposed that comply with the screening requirements.
- 1 tree must be provided for each 25 feet of linear parking frontage. This requirement is being met, as three trees are required along 54th Street East and four trees are required along 34th Avenue South.
- A 7 foot landscaped yard is provided along the northern property line which abuts a residential district. Screening provided on the applicant’s property does not meet the provision to be six feet in height and 95% opaque. This requires alternative compliance. An existing wood fence on the adjacent property currently provides screening. Staff is recommending alternative compliance be granted since the fence is already screening the area, but staff also recommends that as a condition of approval, if the existing fence is ever removed, the applicant will need to provide the required screening on their own property.
- All areas of the parking lot unavailable for parking or maneuvering will be landscaped.
- All parking is located within 50 feet from on-site deciduous tree.
- All areas not occupied by buildings, parking and loading facilities or driveways are covered with grasses, perennials, wood mulch, shrubs, or trees.
- Information on the landscape plan does not indicate that the maintenance of all landscape materials will comply with section 530.210. Staff is recommending as a condition of approval that landscape maintenance should incorporate environmentally sound management practices, as specified in 530.210.

Table 2. Landscaping and Screening Requirements

	Code Requirement	Proposed
Lot Area	--	15,190 sq. ft.
Building footprint	--	4,495 sq. ft.
Remaining Lot Area	--	10,695 sq. ft.
Landscaping Required	2,139 sq. ft.	3,201 sq. ft.
Canopy Trees (1: 500 sq. ft.)	4 trees	7 trees
Shrubs (1: 100 sq. ft.)	22 shrubs	30 shrubs

Additional Standards – Requires alternative compliance

- The applicant is proposing a rain garden to achieve some on-site retention and filtration of storm water.
- The proposed building would not block views of important elements of the city, and would be located and arranged to minimize shadowing on public spaces and adjacent properties and to minimize the generation of wind currents at ground level.
- The development includes some environmental design elements to prevent crime. Lighting will be provided at all building entrances and along the north, west, and south sides of the building without creating excessive glare or lighting of the site. However, CPED staff is recommending that the Planning Commission not grant alternative compliance for the applicant’s proposed tinted windows, as tinted windows will not support natural surveillance and visibility.
- The existing building is not a locally designated historic structure or historic resource.

2. Conformance with all applicable regulations of the zoning ordinance.

The proposed use is *conditional* in the CI Neighborhood Commercial District.

Off-street Parking and Loading – Meets requirements

- The size of the building is 4,495 square feet, with an additional 593 square feet of storage in the mezzanine level. The minimum vehicle parking requirement is 14 spaces. There is no bicycle parking requirement for a minor automobile repair facility. There is no loading requirement for the project.

Table 3. Vehicle Parking Requirements Per Use (Chapter 54I)

	Minimum Parking Requirement	Maximum Parking Allowed	Proposed
Auto repair, minor	14	17	14

Building Bulk and Height – Meets requirements

- For the purposes of calculating building bulk, the 593 square foot storage mezzanine was included. The proposed building is well below the maximum floor area ratio.

Table 5. Building Bulk and Height Requirements

	Code Requirement	Proposed
Lot Area	--	15,190 sq. ft.
Gross Floor Area (GFA)	--	5,088 sq. ft.
Maximum Floor Area Ratio (GFA/Lot Area)	1.7	0.33
Maximum Building Height	2.5 stories or 35 feet, whichever is less	27 ft. 6 in.

Residential Lot Requirements – Not applicable

Yard Requirements – Meets requirements

- Uses in commercial districts do not have minimum yard requirements unless the use is near a residential district or residential structure. A residential district exists to the north, east, and southwest of the subject property. See Table 7 for the yard requirements that apply to this property due to its location near a residential district.

Table 7. Minimum Yard Requirements

	Zoning District	Overriding Regulations	Total Requirement	Proposed
Front	20 ft. for first 25 ft. from RIA boundary	Front yard variance granted in 1997 from 20 ft to 8 ft 6 inches.	8 ft. 6 in.	9 ft. 6 in.
Interior Side (North)	5 ft.	--	5 ft.	5 ft. 6 in.

Signs – Not applicable

- Signs are subject to Chapter [543](#) of the zoning code.
- Sign plans were not submitted as part of this application. However, the applicant proposes the following modifications to the existing freestanding sign: removing the fuel price board and “Mobil” sign, moving the reader board to below the logo sign, and replacing the existing face with a new logo sign.
- Sign permits will need to be obtained separately from CPED.

Dumpster Screening – Meets requirements

- Refuse and recycling storage containers are required to be enclosed on all four sides by screening compatible with the principal structure not less than two feet higher than the refuse container or must be otherwise effectively screened from the street and adjacent residential uses.
- A six foot enclosure with matching brick veneer and sliding cedar plank gates is proposed which meets the requirements.

Screening of Mechanical Equipment – Meets requirements with Conditions of Approval

- The applicant has not indicated where the mechanical equipment will be located on the site. To meet the requirements of the zoning code, the mechanical equipment shall be screened with a fence, vegetation, or the building itself. CPED is recommending that the mechanical equipment be screened per these standards.

Lighting – Meets requirements

- The applicant has provided a lighting plan. Lighting will be provided at all building entrances and along the north, west, and south sides of the building without creating excessive glare or lighting of the site.

Impervious Surface Area – Not applicable

Specific Development Standards – Meets requirements

The proposed minor automobile repair facility is subject to the following development standards, per 536.20:

Automobile repair, minor.

- (1) All vehicles waiting for repair or pick-up shall be stored on the site in an enclosed building or in parking spaces in compliance with Chapter 541, Off-Street Parking and Loading.
- (2) Except in the I3 District, all repairs shall be performed within a completely enclosed building.
- (3) All vehicles parked or stored on-site shall display a current license plate with a current license tab. Outdoor storage of automotive parts or storage of junk vehicles is prohibited.
- (4) The sale of vehicles shall be prohibited.

(5) In the C1, C2 and C3S Districts, all service vehicles associated with the establishment shall be parked or stored in an enclosed structure after business hours.

(6) The use shall employ best management practices regarding the venting of odors, gas and fumes. Such vents shall be located a minimum of ten (10) feet above grade and shall be directed away from residential uses. All storage tanks shall be equipped with vaportight fittings to preclude the escape of gas vapors from the fill pipes.

(7) The premises, all adjacent streets, sidewalks and alleys, and all sidewalks and alleys within one hundred (100) feet shall be inspected regularly for purposes of removing any litter found thereon.

(8) Unattended, automated dispensing of gasoline or other engine fuel shall be prohibited.

AP Airport Overlay District Standards – Not applicable

3. Conformance with the applicable policies of *The Minneapolis Plan for Sustainable Growth*.

The Minneapolis Plan for Sustainable Growth identifies the site as Mixed Use on the future land use map. As conditioned, the proposed development is consistent with the following principles and policies outlined in the comprehensive plan, as well as those previously listed in the conditional use permit portion of this report:

Land Use Policy 1.4: Develop and maintain strong and successful commercial and mixed use areas with a wide range of character and functions to serve the needs of current and future users.

1.4.4 Continue to encourage principles of traditional urban design including site layout that screens off-street parking and loading, buildings that reinforce the street wall, principal entrances that face the public sidewalks, and windows that provide “eyes on the street.”

Environment Policy 6.3: Encourage sustainable design practices in the planning, construction and operations of new developments, large additions and building renovations.

6.3.2 Ensure that developments use storm water BMPs (Best Management Practices).

Environment Policy 6.8: Encourage a healthy thriving urban tree canopy and other desirable forms of vegetation.

6.8.2 Achieve, at a minimum, no net loss of the urban tree canopy by maintaining and preserving existing trees and planting new trees on public and private property.

Urban Design Policy 10.18: Reduce the visual impact of automobile parking facilities.

10.18.1 Require that parking lots meet or exceed the landscaping and screening requirements of the zoning code, especially along transit corridors, adjacent to residential areas, and areas of transition between land uses.

10.18.2 Parking lots should maintain the existing street face in developed areas and establish them in undeveloped areas through the use of fencing, walls, landscaping or a combination thereof along property lines.

Urban Design Policy 10.19: Landscaping is encouraged in order to complement the scale of the site and its surroundings, enhance the built environment, create and define

public and private spaces, buffer and screen, incorporate crime prevention principles, and provide shade, aesthetic appeal, and environmental benefits.

10.19.5 Landscaping plans should be designed to facilitate future maintenance including the consideration of irrigation systems, drought and salt-resistant species, ongoing performance of storm water treatment practices, snow storage, access to sun, proximity to buildings, paved surfaces and overhead utilities.

Urban Design Policy 10.22: Use Crime Prevention Through Environmental Design (CPTED) principles when designing all projects that impact the public realm, including open spaces and parks, on publicly owned and private land.

10.22.1 Integrate “eyes on the street” into building design through the use of windows to foster safer and more successful commercial areas in the city.

The proposed expanded minor automobile repair facility along a Community Corridor and within a Neighborhood Commercial Node is appropriate based on the future land use map and corresponding policies of the comprehensive plan.

4. Conformance with applicable development plans or objectives adopted by the City Council.

With the adoption of the staff recommendation, the site plan will be consistent with the applicable policies of the comprehensive plan. No small area plan exists for this area.

5. Alternative compliance.

The Planning Commission or zoning administrator may approve alternatives to any site plan review requirement upon finding that the project meets one of three criteria required for [alternative compliance](#). Alternative compliance is requested for the following requirements:

- **Reinforce street wall.** The first floor building wall is required to be located not more than 8 feet from the lot line. However, for this proposal, strict adherence to the requirement is impractical because of existing conditions. The existing building was constructed towards the back of the lot in order to accommodate space for fuel pumps. Options for remodeling the existing structure and locating an addition were limited due to the location of the existing building on the site. Required parking could not be accommodated to the rear of the building, but space for the addition could be accommodated in that space. The applicant is proposing to include additional improvements to address the adverse effects of the building’s location pulled back far from the property lines. They will be providing additional landscaping in excess of their requirement, with five feet of additional landscaping along most of 34th Avenue South to improve the pedestrian experience along 34th Avenue South.
- **Clear or lightly tinted window requirement.** First floor windows are required to have clear or lightly tinted glass with a visible light transmittance ration of 0.6 or higher. The applicant is requesting alternative compliance to allow for windows tinted with a 0.4 visible light transmittance ratio. Staff finds that it would be practical to have the windows be clear or lightly tinted glass with at least a 0.6 visible light transmittance ratio, as the provision requires. Overly tinted windows would prohibit views into and out of the building, which would be contradictory with the intent of the chapter and potentially negatively impact security. No additional amenities have been provided for mitigation of the adverse effects and the applicant is meeting only the absolute minimum window requirement along 34th Avenue South. Staff does not recommend that the Planning Commission grant alternative compliance for this provision.

- **Walkway requirement.** Well-lit walkways at least 4 feet in width are required to connect buildings with the adjacent public sidewalk and any on-site parking. No walkways are provided between the building and the adjacent sidewalk or on-site parking. Alternative compliance is required. Because of the need to locate the parking at the front of the lot, it would be impractical to provide a walkway through the parking lot to the front entrance. Staff is recommending that alternative compliance be granted, as this is due to difficulties caused by the location of the existing building.
- **Landscaped yard requirement.** The single parking space south of the building requires a landscaped yard seven feet in width and screening three feet in height and at least 65% opaque. A yard only six feet in width with shrubs that meet the screening requirement currently exists in this location. Increasing the yard to meet the required width would eliminate one parking space and the applicant would no longer meet their parking requirement. It is impractical to strictly adhere to this requirement since increasing the landscaped yard would require existing shrubs and curbing to be removed and one required parking space to be eliminated. Staff is recommending that the Planning Commission grant alternative compliance for this provision.
- **Screening adjacent to residential use requirement.** Because the parking area is adjacent to a residential district, screening six feet in height and at least 95% opaque is required. An existing wood fence on the adjacent property currently provides screening of the parking area. Staff is recommending alternative compliance be granted since the fence is already screening the area. However, because the fence is not located on the subject property, staff recommends that as a condition of approval, if the existing fence of the adjacent neighbor is ever removed, the applicant will need to provide the required screening on their own property.

RECOMMENDATIONS

Recommendation of the Department of Community Planning and Economic Development for the Conditional Use Permit:

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt the above findings and **approve** the application for a conditional use permit to allow the expansion of an existing minor automobile repair facility at the property located at 5355 34th Avenue South, subject to the following conditions:

1. The conditional use permit shall be recorded with Hennepin County as required by Minn. Stat. 462.3595, subd. 4 before building permits may be issued or before the use or activity requiring a conditional use permit may commence. Unless extended by the zoning administrator, the conditional use permit shall expire if it is not recorded within two years of approval.
2. All maneuvering associated with parking or entering and exiting the service bays shall take place on-site and not in the public right-of-way.

Recommendation of the Department of Community Planning and Economic Development for the Variance:

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt the above findings and **approve** the application for a variance to increase the maximum size of a commercial use in the C I District from 4,000 square feet to approximately 4,495 square feet at the property located at 5355 34th Avenue South.

Recommendation of the Department of Community Planning and Economic Development for the Site Plan Review:

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt the above findings and **approve** the site plan review application to allow an addition to an existing minor automobile repair facility at the property located at 5355 34th Avenue South, subject to the following conditions:

1. Approval of the final site plan, landscaping plan, and elevations by the Department of Community Planning and Economic Development.
2. All site improvements shall be completed by July 14, 2016, unless extended by the Zoning Administrator, or the permit may be revoked for non-compliance.
3. The proposed sign changes require a separate sign permit from CPED.
4. Mechanical equipment shall be screened as required by section 535.70 of the zoning code.
5. If the existing fence along the northern property line is ever removed, the applicant will provide on-site screening of the parking area that is at least six feet in height and 95 percent opaque.
6. Landscape maintenance should incorporate environmentally sound management practices, as specified in section 530.210.
7. Windows on the 54th Street East and 34th Avenue South elevations will be clear or lightly tinted glass with a visible light transmittance ratio of six-tenths (0.6) or higher as required by section 530.120 of the zoning code.

ATTACHMENTS

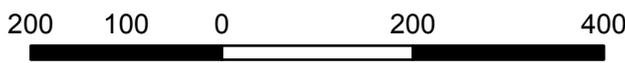
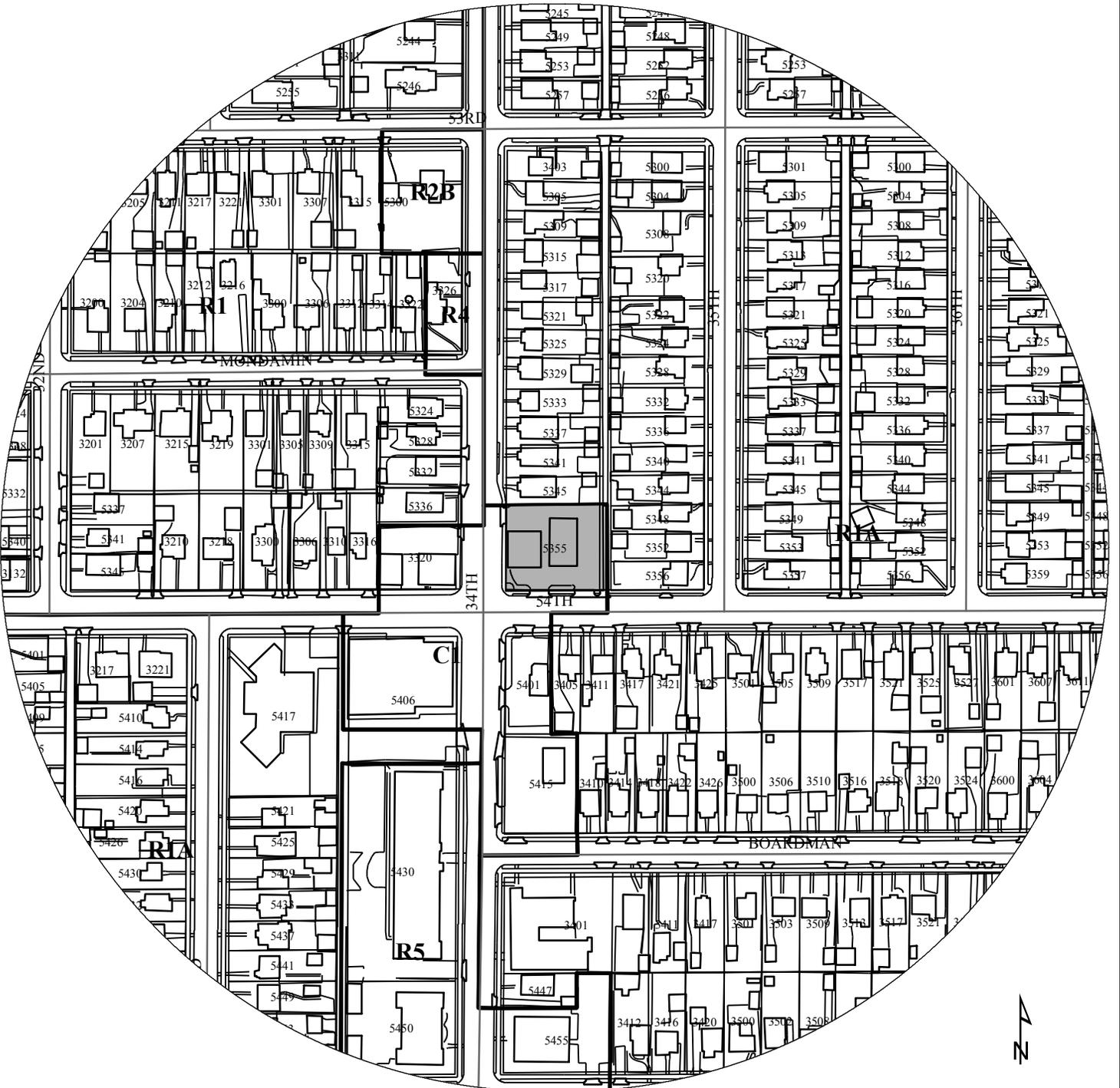
1. Zoning map
2. Aerial map
3. PDR report
4. Written description and findings submitted by applicant
5. Site survey
6. Plans
7. Building elevations
8. Renderings
9. Lighting plan and materials sheet
10. Photos
11. Correspondence

APPRO Development

12th

NAME OF APPLICANT

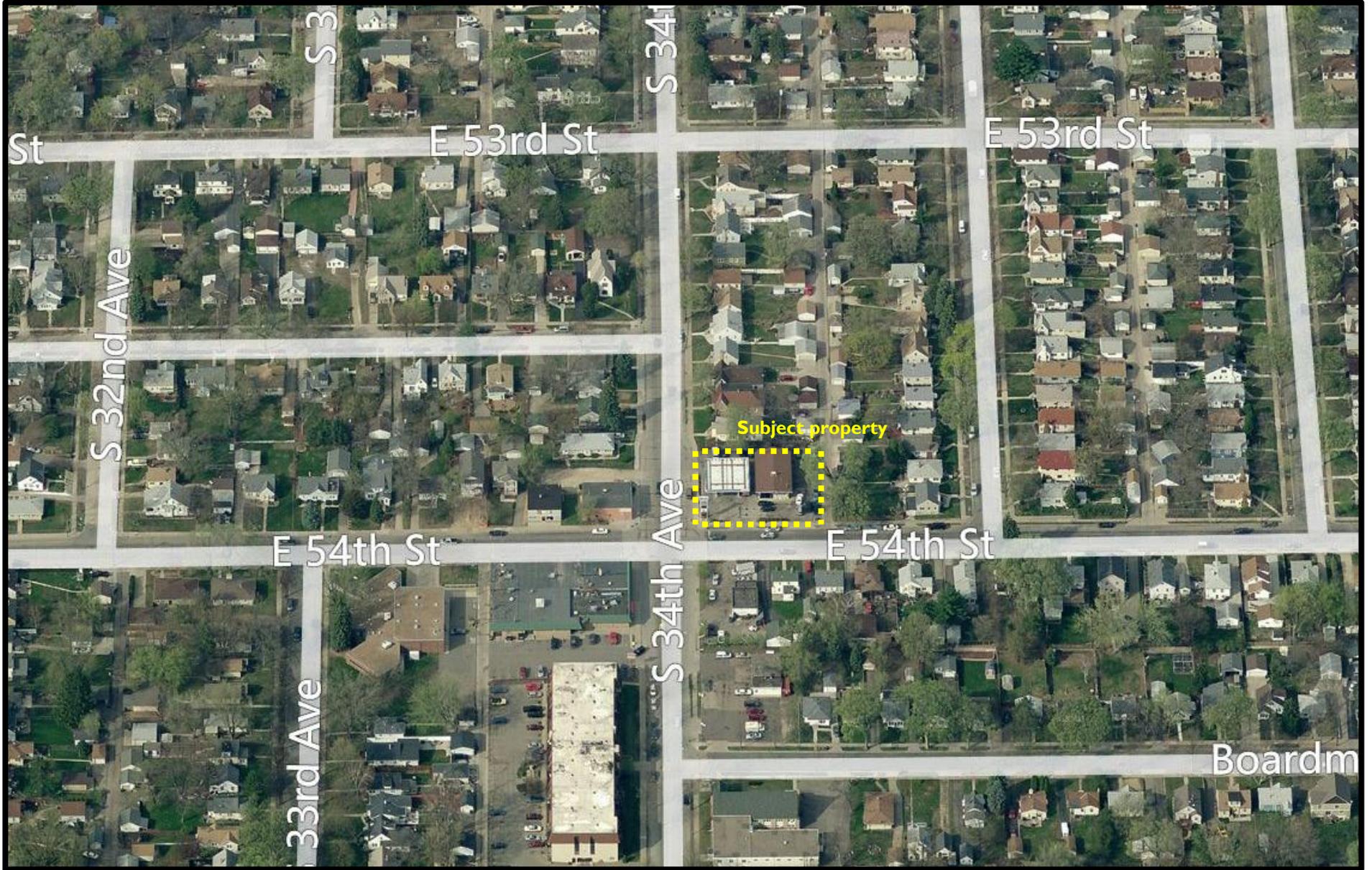
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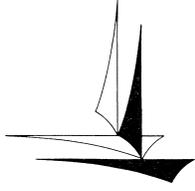


PROPERTY ADDRESS
5355 34th Avenue South

FILE NUMBER
BZZ-6600

BZZ-6600: 5355 34th Ave S





Minneapolis Development Review
250 South 4th Street
Room 300
Minneapolis, MN 55415

Preliminary Development Review Report

Development Coordinator Assigned: **DONALD ZART**
(612) 673-2726
don.zart@minneapolismn.gov

Status *
RESUBMISSION REQUIRED

Tracking Number:	PDR 1001207
Applicant:	ROBELI ENTERPRISES, LLP 9331 XYLON AVE BLOOMINGTON, MN 55438
Site Address:	5355 34TH AVE S
Date Submitted:	04-JUN-2014
Date Reviewed:	13-JUN-2014

Purpose

The purpose of the Preliminary Development Review (PDR) is to provide Customers with comments about their proposed development. City personnel, who specialize in various disciplines, review site plans to identify issues and provide feedback to the Customers to assist them in developing their final site plans.

The City of Minneapolis encourages the use of green building techniques. For additional information please check out our green building web page at: http://www.ci.minneapolis.mn.us/mdr/GreenBuildingOptions_home.asp.

DISCLAIMER: *The information in this review is based solely on the preliminary site plan submitted. The comments contained in this report are preliminary ONLY and are subject to modification.*

Project Scope

Proposed 2,701 sq ft addition to existing 1,794 sq ft building. Associated site work and removal of gas pumps and canopy.

Review Findings (by Discipline)

Zoning - Planning

- The following land use applications have been identified:
 - Conditional Use Permit: "Automobile repair, minor, existing on the effective date of this ordinance" is a Conditional Use in the C1 District. Persons wishing to establish or expand a conditional use shall obtain a conditional use permit for such use, as specified in Chapter 525, Administration and Enforcement. (548.30)

*Approved: You may continue to the next phase of developing your project.

*Resubmission Required: You cannot move forward or obtain permits until your plans have been resubmitted and approved.

- Site Plan Review: The establishment or expansion of automobile services uses are subject to site plan review. (530.30)
- Variance: To increase the maximum size of a commercial use in the C1 district from 4,000 square feet to approximately 4,495 square feet. (548.240)
- The applicant has submitted a complete land use application and intends to be on the July 14th City Planning Commission meeting.
- Based on staff's preliminary review, the following issues have been noted. Additional issues may be identified upon full analysis of the land use application.
 - South and west elevations are not meeting minimum window area requirements. First floor minimum window area is measured between 2 and 10 feet above the adjacent grade. Current proposal shows approximately 23.17% on the South elevation and 19.09% on the West elevation. Alternative compliance will be required if not meeting the minimum window area requirements. (530.120)
 - Plain face concrete block is prohibited where adjacent to residence district-currently proposed for East elevation. Exterior materials and appearance of the rear and side walls of any building shall be similar to and compatible with the front of the building. The use of plain face concrete block as an exterior material shall be prohibited where adjacent to a residence district. (530.120)
 - The Minnesota State Accessibility Code requires that access aisles for accessible spaces must be a minimum of 8 feet in width. The access aisle currently proposed is only 6 feet.
 - The parking spaces along 54th should be labeled as compact and included in site information as compact parking, as they only measure 16" feet by 8'6" feet. Please be aware that 75% of the required parking must be standard size spaces. (541.330)
 - The plan as proposed does not meet the general landscaping requirement that 20% of the site not occupied by buildings shall be landscaped. This site requires at least 2150 square feet of landscaping: 4 canopy trees, 22 shrubs, and the remainder of the landscaped area shall be covered with turf grass, native grasses or other perennial flowering plants, vines, shrubs or trees. (530.160) Large areas of only wood mulch do not count towards required landscaped areas.
 - There seem to be gaps in the screening along the north property line. Along the side interior lot line, screening must be 6 feet in height and at least 95% opaque. (530.160)
 - 3 trees are required along 54th and 4 trees are required along 34th. Not less than one tree shall be provided for each twenty-five linear feet or fraction thereof of parking or loading area lot frontage. (530.170)
 - A 7 foot landscaped yard is required along the proposed parking on 54th. The applicant must request alternative compliance for this standard in order to keep the existing 6 foot landscaped yard.
 - Please indicate an adequate water supply to maintain the landscape materials on the site plan, which is required in 530.210. Landscape maintenance should incorporate environmentally sound management practices such as the use of water and energy efficient systems such as drip irrigation. (530.210)
 - All maneuvering needs to take place in the off-street parking area, not in the public street. The service bay along 54th Street East does not appear to meet this requirement. (541.290)
 - Sign: The plans note an alternative to replace the existing reader board with an electronic reader board. Please note that dynamic changeable copy signs are not permitted in the C1 District without a sign variance. See Chapter 543 of the zoning code for sign regulations.
 - More information about the proposed tinted windows is required. First floor or ground floor windows shall have clear or lightly tinted glass with a visible light transmittance ratio of six-tenths (0.6) or higher. (530.120)
 - Curb cuts for vehicles shall be consolidated wherever possible. (530.150)
 - Please provide more details about the lighting proposed and provide a lighting plan. (535.590)
 - Please provide more detail about the proposed materials for the opaque trash gates.

❑ Addressing

- There is no change in address proposed or needed for this project.

❑ Parks - Forestry

- Contact Paul Martinson (612-499-9209) regarding removal or protection of trees during construction in the city right of way.

❑ Right of Way

- The plan as submitted meets the requirements of the Public Works Right-Of-Way Division.

❑ Street Design

- For detailed information related to City of Minneapolis standard specifications, details, and standard plates refer to the following: <http://www.minneapolismn.gov/publicworks/plates/index.htm>
- All driveway aprons shall be designed and constructed to City standards. Please refer to the following: http://www.minneapolismn.gov/publicworks/plates/public-works_road. Add the appropriate details from the ROAD-2000 Series: Driveways (ROAD-2000, ROAD-2001, and ROAD-2002) to the plans.
- All curb & gutter in the Public right-of-way shall be designed and constructed to City standards, curb & gutter to be City standard B624 Curb and Gutter. Please refer to the following: http://www.minneapolismn.gov/publicworks/plates/public-works_road. Add the appropriate details from the ROAD-1000 Series - Curbs and Gutters (ROAD-1003) to the plans.

❑ Sidewalk

- ADA compliant pedestrian ramps are required at each crosswalk at the intersection of E. 54th St. and 34th Ave. S. Construct two (2) ADA compliant pedestrian ramps at this location. Include the appropriate details and standard plates in the site plan, refer Mn/DOT Standard Plan 5-297.250 Pedestrian Curb Ramp Details at: <http://standardplans.dot.state.mn.us/stdplan.aspx>
- The boulevard space adjacent to the Bus Stop on 34th Ave. S. shall be removed and replaced with City standard concrete sidewalk.
- Note to the Applicant: Any currently defective public sidewalk, curb and gutter, or drive approaches in the public right of way must be removed and replaced as part of the construction work on the development site.

❑ Traffic and Parking

- Current ordinance states that all maneuvers associated with site operations, loading, parking or sanitation pick up for a private development shall occur on private property. The easterly curb cut along E. 54th St. allows for non-compliant site circulation of vehicles; vehicles entering or exiting the garage space cannot do so without maneuvering in the Public right-of-way, which is not allowed. Current Ordinance also recommends that curb cuts shall be reduced and consolidated as much as possible. In order to maintain vehicle circulation on site, the plan shall be redesigned to prevent non-compliant vehicle maneuvers or the easterly most curb cut shall be removed.

❑ Water

- The plan as submitted meets the requirements of the Public Works Water Maintenance & Distribution Division.

❑ Fire Safety

- The plan as submitted meets City of Minneapolis requirements for Fire Safety.

❑ Business Licensing

- Please contact Don Zart (612-673-2726) to go over the requirements of any Health Department Review that may be required.

❑ Environmental Health

- City and state records identify three operating underground storage tanks, two 10,000 used for gasoline and one 550 gallon waste oil tank. The proposed facility expansion should not restrict access for leak detection, maintenance and removal if necessary. If the tanks will be removed and replaced as part of this expansion see environmental service permit requirements listed below.
- If the project will be disturbing identified impacted soils on the site preapproval for removal, disposal and/or reuse of must occur from the MCPA and the City of Minneapolis prior to beginning excavation activities. If impacted soil is encountered during site activities call the MN State Duty officer at (615) 649-5451.
- If dewatering is required during site construction see below for city permit requirements. Subgrade structures should be designed to prevent infiltration of groundwater without the need for a permanent dewatering system being installed. If a continuously operating permanent dewatering system is needed it must be approved as part of the sanitary sewer and storm drain site plan approval prior to construction beginning.
- No construction, demolition or commercial power maintenance equipment shall be operated within the city between the hours of 6:00 p.m. and 7:00 a.m. on weekdays or during any hours on Saturdays, Sundays and state and federal holidays, except under permit. Contact Environmental Services at 612-673-3867 for permit information.
- Permits and approval are required from Environmental Services for the following activities: Temporary storage of impacted soils on site prior to disposal or reuse; Reuse of impacted soils on site; Dewatering and discharge of accumulated storm water or ground water, underground or aboveground tank installation or removal, well construction or sealing. Contact Tom Frame at 612-673-5807 for permit applications and approvals.

❑ Sewer Design**Stormwater Management:**

- Please identify the square footage of the area proposed to be disturbed with the project, and the existing and proposed impervious square footages within the disturbed area.
- It is recommended that the surface infiltration feature be planted with vegetation associated with infiltration practices, with native species being used as much as possible. It is also recommended that deep rooted plants such as prairie grass be used, as they increase the infiltration capacity of the underlying soils.
- It is also recommended that stormwater modeling calculations be provided and that an operations and maintenance plan be developed for the proposed rain garden features. These items will help ensure proper design of the features and ensure they continue to operate efficiently in the future. The site may be eligible for a stormwater utility fee credit, if the water quality BMP is constructed properly.
- The proposed project is located within the Minnehaha Creek Watershed District, which has a separate review process from that of the City. Please note it may be necessary to obtain approvals or permits from Minnehaha Creek Watershed District.

Surface Drainage:

- Please identify the location of the roof downspouts on the Grading & Drainage Plan.
- Erosion Control: Inlet protection should be provided on the catch basin within the alley on the east side of the property.

Non Stormwater Discharges:

- Detail all mechanical and non-stormwater discharges. Non-stormwater discharges are not permitted unless approved by the City of Minneapolis. Non-stormwater discharges not declared and approved will not be permitted. If there currently are none and nothing is proposed declare this status on the plans.
- For comments or questions on Public Works Surface Water & Sewers Division related requirements please contact Jeremy Strehlo, (Professional Engineer) at (612) 673-3973, or jeremy.strehlo@minneapolismn.gov

❑ Historical Preservation Committee

- The is not a historic flag on this property. No review required.

❑ Construction Code Services

- Access aisle for accessible parking stall required to be 8 ft in width - 6 ft shown on plan
- Locate accessible parking stall as close as possible to accessible entry
- Provide signage for accessible parking access aisle in addition to parking stall signage.
- A licensed architect and licensed engineer are required to prepare and sign complete construction plans for the building permit submittal.
- Contact the Met Council for a SAC determination.

http://www.minneapolismn.gov/www/groups/public/@regservices/documents/webcontent/convert_281675.pdf

END OF REPORT

May 28, 2014

Minneapolis Department of Zoning, Planning
Ms. Lisa Steiner
250 South 4th Street, Room # 300
Minneapolis, MN 55415-1316

RE: Property at 5355 34th Avenue South, Minneapolis

Dear Ms. Steiner:

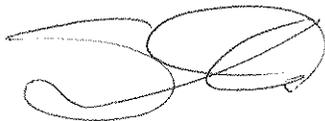
We are sending you this letter to address the statement of proposed use for our site as follows:

We plan to demolish the existing fueling canopy and remove the fueling units and underground tanks, we also plan to remodel the existing building as part of an expansion to the building. We will build an expansion onto the existing building on the east and south sides (3,136 SF, including storage mezzanine) for the intended use of maintaining and repairing automobiles and light trucks. The type of work being performed is considered minor automobile repair in the Minneapolis zoning ordinance as it does not include body work, reconditioning or painting. Included in the remodeling effort we will increase the waiting area in the building, upgrade the restrooms for accessibility, increase the amount of glass on the building and upgrade the overall appearance. We will also be increasing the amount of green space on the site.

Snow Removal:

We have identified an area on the northeast corner of the site for snow storage. Due to the limitations of the site, snow will be removed from the site when necessary.

Sincerely,

A handwritten signature in black ink, appearing to read "Steve Johnston". The signature is fluid and cursive, with a large loop at the end.

Steve Johnston President-CEO
Steve's Tire and Auto
Robeli Enterprises, LLP



May 28, 2014

Minneapolis Department of Zoning, Planning
Ms. Lisa Steiner
250 South 4th Street, Room # 300
Minneapolis, MN 55415-1316

RE: Property at 5355 34th Avenue South, Minneapolis
Conditional Use Permit Findings

Dear Ms. Steiner:

We are sending you this letter to address the Conditional Use Permit requirements you identified at our meeting on May 19, 2014. Please review the following statements:

1. *The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, comfort or general welfare.*

The proposed use is similar to the existing use of the site, which is a fuel sales and auto repair station. The use would not alter the essential character of the neighborhood. This minor auto repair facility and the service provided is aimed to satisfy the demand of its cliental which is the surrounding neighborhood. The station relies on the local community for its customer base. The use of the site has been, and will continue to be auto service related.

2. *The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.*

The current use includes 3 service bays and fuel (gasoline) sales. This business has served the neighborhood community for the past 21 years while being operated by the current ownership. The proposed use has 6 service bays with no gasoline sales or carwash. The proposed building would upgrade the architectural appearance of the site. All four sides of the building have a finished appearance. Also the amount of green space on the site has been increased and the planting area along 34th has been designed to enhance the street wall concept by creating a landscape hedge.

3. *Adequate utilities, access roads, drainage, necessary facilities or other measures, have been or will be provided.*

Our site is bordered by 34th Avenue South to the west and 54th Street East to the south. Auto circulation entering and leaving the building can all be contained within the parameters of the site. The pedestrian access to the site is improved by having a definitive entrance to the building and easy access to the entry. Deliveries to the building are typically by smaller vehicles. The circulation design is setup to provide a drive thru scenario for the delivery vehicles to eliminate backing in off the street. Fuel delivery trucks are no longer necessary for the proposed use.

4. *Adequate measures have been or will be taken to minimize traffic congestion in the public streets.*

The proposed use would significantly decrease the trip generation to the site due to the removal of the fuel canopy and islands. The amount of driveways shown disperses the traffic coming in and out of the site to accommodate traffic from multiple directions. Utilizing the existing curb cuts/driveways enables thru traffic for tow trucks, garbage trucks and parts delivery easing onsite congestion.

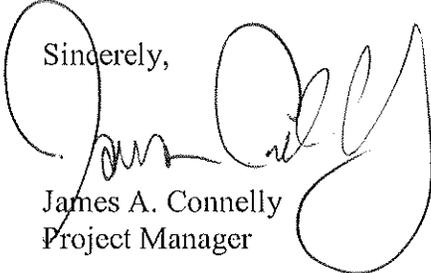
5. *The conditional use is consistent with the applicable policies of the comprehensive plan.*

The proposed use exists and is allowed in this district via a current conditional use permit. The proposed building complies with the standards contained in the zoning ordinance for exterior materials. The building size is far below the allowed floor area ratio amount and does not require any setback variances.

6. *The conditional use shall, in all other respects, conform to the applicable regulations of the district in which it is located.*

The plan would provide adequate parking spaces for our use. Landscaping on the site would be improved from the existing conditions and pervious surface area would be increased. The building exterior and site would be enhanced from its current condition. The ratio of building to lot size is less than the maximum allowed. The amount of glazing (or windows) in the building design along the street sides meets or exceeds the minimums within the ordinance.

Sincerely,


James A. Connelly
Project Manager



May 28, 2014

Minneapolis Department of Zoning, Planning
Ms. Lisa Steiner
250 South 4th Street, Room # 300
Minneapolis, MN 55415-1316

RE: Property at 5355 34th Avenue South, Minneapolis
Variance Application Findings

Dear Ms. Steiner:

We are sending you this letter to address the Variance requirements you identified via email on May 22, 2014. Thank you for clarifying that the maximum commercial building size in the C-1 district is 4,000 square feet. We feel the additional 459 feet that we are proposing is a minor increase above what the present ordinance allows for. Please review the following statements:

1. *Practical Difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by the persons directly involved in the property and are not based on economic considerations alone.*

The proposed project is an expansion to an existing building. The building is unique in nature given its original design incorporated a more suburban gas station layout than what is desired today per the ordinance. The placement of the building when it was constructed in 1970 did not take into consideration the placement of the building near the street corner as current ordinance would require. If it had and we could place the parking to the rear of the lot, in lieu of between the building and the street, and the building size for this lot would be 6,000 square feet and would comply with the ordinance being well under that amount. The proposed building size is 4,459 square feet. The existing building, when constructed, was placed in a traditional manner as far as fuel islands towards the street and building towards the back of the lot. The amount of space remaining towards the rear of the building is not enough to accommodate parking required onsite but does accommodate the need for additional building space. The business is intending to remain open during construction, having the majority of the work take place on the rear of the building enables that to occur.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

Minneapolis Department of Zoning, Planning

May 28, 2014

Page Two

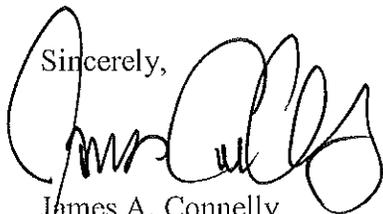
The current use includes 3 service bays and fuel (gasoline) sales this use has been in place since 1970. We are looking to modify the use to only complete automobile service, keeping the lot as an automobile related use. We propose that this is reasonable given that the service garage is frequented by many in the neighborhood and that C-1 zoning is a neighborhood commercial district. The focus of the business is to provide a desired service to the neighborhood. We have added additional landscaping to the street frontages of the lot to create a hedge row that promotes a street wall in keeping with the ordinance. We are upgrading the appearance of the building utilizing approved materials which will enhance the appearance of the street corner.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety or welfare of the general public or of those utilizing the property or nearby properties.*

The variance we are requesting is for an additional 459 square feet. The site in question consists of enough area to easily accommodate this amount of square footage and still provide landscaping and parking areas required by ordinance. The character of the neighborhood should remain unchanged for several reasons: the business that has serviced the neighborhood for many years is remaining, the service portion of the business is remaining, the area of the canopy removed is roughly the same size as the building expansion footprint we are proposing thereby not changing the amount of structure onsite by a great deal. The hours of operation would be less than the service station currently maintains and should not affect the adjacent residential areas due to late evening business operations. The newly remodeled building also enhances the area by upgrading what is currently in place.

Hopefully the above findings meet the purpose of your requirements. Please do not hesitate to contact me if additional information is required.

Sincerely,



James A. Connelly
Project Manager
APPRO Development, Inc.



May 28, 2014

Minneapolis Department of Zoning, Planning
Ms. Lisa Steiner
250 South 4th Street, Room # 300
Minneapolis, MN 55415-1316

RE: Property at 5355 34th Avenue South, Minneapolis
Alternative Compliance for Building Setback Statement

Dear Ms. Steiner:

We are sending you this letter to address the ordinance section 530.80 Alternative Compliance and the requirement you identified in your email dated May 20, 2014. Please review the following statements:

1. *The city planning commission or zoning administrator may approve alternatives to any site plan review requirement upon finding any of the following:*
2. *(1)The alternative meets the intent of this chapter and the site plan includes amenities or improvements that address any adverse effects of the alternative. Site amenities may include but are not limited to additional open space, additional landscaping and screening, green roof, decorative pavers, ornamental metal fencing, architectural enhancements, transit facilities, bicycle facilities, preservation of natural features, restoration of previously damaged natural environment, rehabilitation of existing structures that have been locally designated or have been determined to be eligible to be locally designated as historic structures, and design which is similar in form, scale and materials to existing structures on the site and to surrounding development.*

The site plan includes additional landscaping amenities and we are proposing to restore areas of impervious surfacing back to green space or landscape areas. To address the street wall concern due to the building being setback away from the street corner we have indicated a hedge row of landscape plantings along both 34th Avenue and 54th street where there is parking proposed. We also have widened the landscape area along 34th Avenue to accommodate this feature increasing the width from 3 feet to over 12 feet. We feel this will create a natural barrier along the sidewalk reminiscent of what the ordinance requirement is. We have also increased to overall green space on the site to accommodate the minimum requirements; whereas the site to date is well under the amount of green space required. We hope this description meets the intent of what is being asked for.

Sincerely,

James A. Connelly
Project Manager
APPRO Development, Inc.



June 20, 2014

Minneapolis Department of Zoning, Planning
Ms. Lisa Steiner
250 South 4th Street, Room # 300
Minneapolis, MN 55415-1316

RE: Property at 5355 34th Avenue South, Minneapolis
Alternative Compliance for Using Tinted Glass

Dear Ms. Steiner:

We are sending you this letter to address the ordinance section 530.80 Alternative Compliance and the requirement discussed in a phone message on June 18, 2014. Please review the following statement:

The city planning commission or zoning administrator may approve alternatives to any site plan review requirement upon finding any of the following:

(1)The alternative meets the intent of this chapter and the site plan includes amenities or improvements that address any adverse effects of the alternative. Site amenities may include but are not limited to additional open space, additional landscaping and screening, green roof, decorative pavers, ornamental metal fencing, architectural enhancements, transit facilities, bicycle facilities, preservation of natural features, restoration of previously damaged natural environment, rehabilitation of existing structures that have been locally designated or have been determined to be eligible to be locally designated as historic structures, and design which is similar in form, scale and materials to existing structures on the site and to surrounding development.

We are proposing to use bronze tinted glazing with a .40 visible light transmittance (VLT). To address the proposed use of tinted glazing (which would be slightly more than allowed per ordinance) in the windows in lieu of the .60 visible light transmittance requirements, we have provided additional glazing on both street-facing elevations (south and west). The building elevations themselves include additional amenities, including additional glazing, awning over the main entrance, and glass overhead garage doors (which have not been included in the calculations). Currently shown on the west is 32% glazing (190 sf / 593 total sf; between 2'-0" and 10'-0"). In addition to this glazing, we have glazing below 2'-0" and in all three garage doors, totaling an additional 227 sf. On the south elevation is 31.8% glazing (144 / 484 total sf; between 2'-0" and 10'-0"). In addition to this glazing, we have glazing below 2'-0" and in one garage door, totaling an additional 100 sf. We have also included windows on the north and east sides for visual interest and daylighting purposes. Because this is an open sales floor with a large amount of glass, we feel strongly that utilizing glazing with a lower VLT will be more energy efficient and will provide a more comfortable interior space. Although shades will be installed at the interior, having slightly tinted windows will help reduce the amount of time the shades are drawn. Please consider our request.

Sincerely,

James A. Connelly
Project Manager



June 20, 2014

Minneapolis Department of Zoning, Planning
Ms. Lisa Steiner
250 South 4th Street, Room # 300
Minneapolis, MN 55415-1316

RE: Property at 5355 34th Avenue South, Minneapolis
Alternative Compliance for Maintaining the Existing 6'-0" Landscaped Yard along 54th

Dear Ms. Steiner:

We are sending you this letter to address the ordinance section 530.80 Alternative Compliance and the requirement you identified in the Preliminary Development Review Report dated June 13, 2014. Please review the following statement:

*The city planning commission or zoning administrator may approve alternatives to any site plan review requirement upon finding any of the following:
(1)The alternative meets the intent of this chapter and the site plan includes amenities or improvements that address any adverse effects of the alternative. Site amenities may include but are not limited to additional open space, additional landscaping and screening, green roof, decorative pavers, ornamental metal fencing, architectural enhancements, transit facilities, bicycle facilities, preservation of natural features, restoration of previously damaged natural environment, rehabilitation of existing structures that have been locally designated or have been determined to be eligible to be locally designated as historic structures, and design which is similar in form, scale and materials to existing structures on the site and to surrounding development.*

The site and landscape plan indicates leaving in place a 6 foot wide x 35 foot long landscape island along the south property line adjacent to 54th street. It has been brought to our attention that landscape yards along this property line are to be 7 feet in width minimum. We are asking for alternative compliance for this landscape island so that it can be left in place at its current size (a reduction of one foot). We feel that removing the existing plantings, concrete curb and gutter and adjacent paved areas to gain one additional foot of space, only to replace all this with similar construction, seems excessive when the end result will be very similar in nature. The plantings are in good health in this landscaped area, the curb and gutter are in good repair so we view this as an area within the site to salvage the existing elements and reduce the amount of construction waste being produced onsite. Overall we are increasing the area of green space significantly onsite and would not need additional landscaped area in this specific location to meet the ordinance for greenspace. We do meet the 7 foot setback along 54th street where other work is being proposed. Please consider our request.

Sincerely,

James A. Connelly
Project Manager

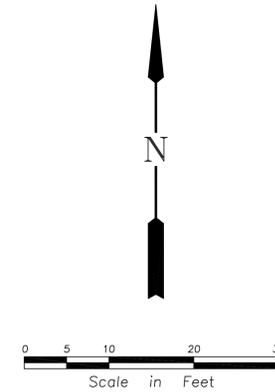
Survey for:
APPRO DEVELOPMENT, INC.
 at 5355 34th Avenue South – Minneapolis, Minnesota

NOTES

- * Bearings shown are based on Hennepin County Coordinates.
- * Utilities shown are from information furnished by the City of Minneapolis and Center Point Energy in response to Gopher State One Call Ticket No. 141404536.
- * Contact Gopher State 1 for utility locations before any construction shall begin. Phone 651-454-0002.
- * Area = 15,190 square feet (0.35 acre).

LEGEND

- Iron Monument Found
- Iron Monument Set
- ⊗ PK Nail Set
- S— Sanitary Sewer
- ST— Storm Sewer
- W— Watermain
- Hyd. ◊ Hydrant
- GV • Gate Valve
- MH ○ Manhole
- CB ○ Catch Basin
- HH ○ Handhole
- Inv. Invert Elevation
- Guard Post
- S • Sign
- PP ○ Power Pole
- LP ✱ Light Pole
- ▨ Concrete Surface
- ▨ Bituminous Surface
- G— Buried Gas



UTILITY STATEMENT

The underground utilities shown have been located from field survey information and existing drawings. The surveyor makes no guarantee that the underground utilities shown comprise all such utilities in the area, either in service or abandoned. The surveyor further does not warrant that the underground utilities shown are in the exact location indicated, although he does certify that they are shown as accurately as possible from information available. The surveyor has not physically located the underground utilities.

PROPERTY DESCRIPTION

Lots 16, 17 and 18, James G. Reid's Third Addition to Minneapolis, Hennepin County, Minnesota.

I hereby certify that this survey was prepared by me or under my direction and that I am a duly Licensed Land Surveyor under the laws of the State of Minnesota.

Dated this 28th day of May, 2014.

REHDER & ASSOCIATES, INC.

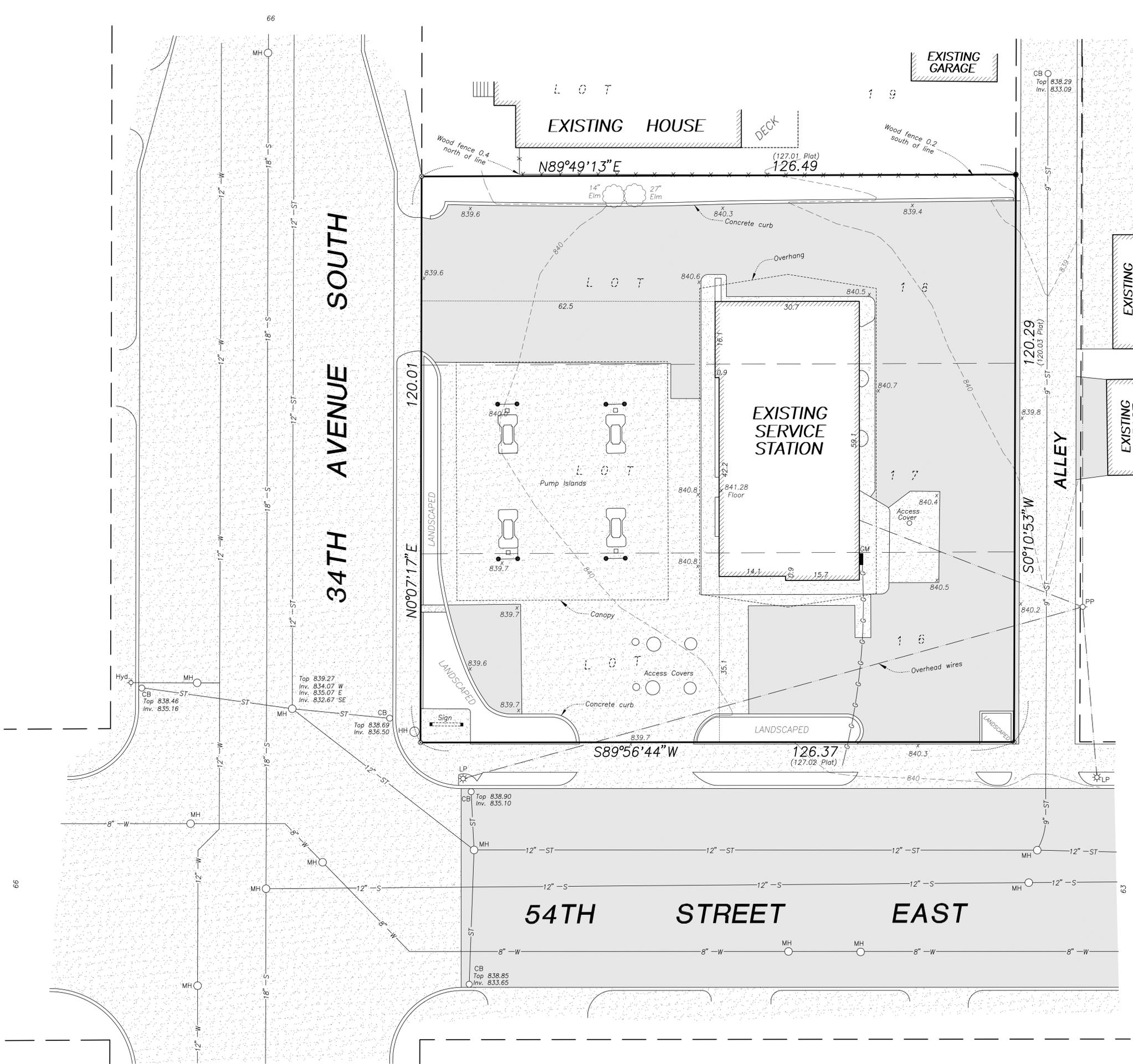
Thomas J. Aden
 Thomas J. Aden, Land Surveyor
 Minnesota License No. 43414

Rehder and Associates, Inc.

CIVIL ENGINEERS AND LAND SURVEYORS

3440 Federal Drive • Suite 110 • Eagan, Minnesota • Phone (651) 452-5051

JOB: 144-2187.017



CITY OF MINNEAPOLIS LANDSCAPE REQUIREMENTS:

SITE MINUS BUILDING AREA = REMAINDER OF WHICH 20% TO BE LANDSCAPED AREAS
 15,190 SF - 4,495 SF = 10,695 SF * 20% = 2,139 SF TO BE LANDSCAPED - 3,554 SF PROVIDED (OR 33%)

PLANTINGS:
 2,139/500 SF = 4 TREES (ONE TREE REQUIRED FOR EVERY 500 SF OF AREA) 1 EVERY 25' ALONG STREET
 1 NEW PROVIDED ALONG STREETS, 2 EXISTING TO REMAIN
 2,139/100 SF = 22 SHRUBS (ONE SHRUB REQUIRED FOR EVERY 100 SF OF AREA)
 30 NEW PROVIDED 1 41 PERENNIAL PLANTINGS PROVIDED

LANDSCAPED AREA BREAK DOWN:
 WOOD MULCH W/ PLANTINGS = 1,901 (17.1%)
 TURF = 349 SF (3.4%)
 RAIN GARDEN = 931 SF (8.7%)
 WOOD MULCH ONLY = 353 SF (3.3%)

TOTAL AREA LANDSCAPED = 3,201 SF (29.9%) *THIS EXCLUDES MULCH ONLY AREA ON EAST SIDE OF BUILDING

LANDSCAPED AREA:
 1,344 SF EXISTING
 3,554 SF PROPOSED

IMPERVIOUS AREA:
 13,833 SF EXISTING
 11,434 SF PROPOSED

DISTURBED AREA:
 13,413 SF

LANDSCAPE LEGEND AND NOTES:

- DECIDUOUS TREE: WOOD MULCH
- CONIFEROUS OR EVERGREEN TREE: TURF, NATIVE GRASS
- SYMBOL: EXISTING CONCRETE
- QUANTITY: PROPOSED CONCRETE
- DECIDUOUS SHRUB:

1. SEED MIX "A" SHALL BE - MNDOT MIX #240 (COMMERCIAL TURF GRASS MIX FOR ALL DISTURBED AREAS THAT ARE NOT SPECIFICALLY MARKED).
 PLACE SEED AT RATE OF 100 LBS. PER ACRE ALONG WITH MNDOT TYPE 1 MULCH AT A RATE OF 2 TONS PER ACRE.
2. PROVIDE CLEAN STRAW FREE OF WEED SEEDS FOR MULCH OVER THE SEEDED AREAS.
3. SHRUB BEDS TO HAVE "TYPAR" WEED FABRIC. OVERLAY FABRIC WITH WOOD MULCH. SHREDDED WESTERN RED CEDAR. POLY SHEETING WILL NOT BE USED WHERE PLANTINGS PROPOSED
4. FINELY SHREDDED MULCH SHALL BE INSTALLED UNDER ALL TREES, SHRUBS AND GENERAL PLANT BEDS (3"-4" DEEP).
5. SEED ALL OTHER AREAS THAT ARE DISTURBED DURING CONSTRUCTION WITH SEED MIX "A".
6. WOOD FIBER BLANKET TO BE PROVIDED AT ALL SEEDED AREAS ON ANY SLOPE 3:1 OR GREATER.
7. WOOD MULCH TO BE SHREDDED WESTERN RED CEDAR - 4" DEEP
8. SHRUB BEDS TO HAVE EDGER. EDGER TO BE: VALLEY VIEW BLACK DIAMOND 20' LENGTHS OR APPROVED EQUAL. SPLICES TO BE JOINED BETWEEN PIECES. ANCHORS/ STAKES TO BE USED- 4 MIN. PER LENGTH. EDGER NOT REQUIRED WHERE SHRUB BED MEETS CONCRETE WALK.
9. PLANTS TO BE MAINTAINED (BY LANDSCAPE CONTRACTOR) WITH MANUAL WATERING OR WATER TRUCK UNTIL OWNER OCCUPIES THE BUILDING.
10. UNDERGROUND UTILITIES TO BE LOCATED PRIOR TO ANY EXCAVATION.
11. LANDSCAPE CONTRACTOR TO DO STAKING AS REQUIRED FOR PLANTINGS.
12. LANDSCAPE CONTRACTOR TO STRIP TOPSOIL FROM MAINTENANCE EDGER. MULCH TO BE PLACED AND COMPACTED AS REQUIRED.

PLANT MATERIAL LEGEND

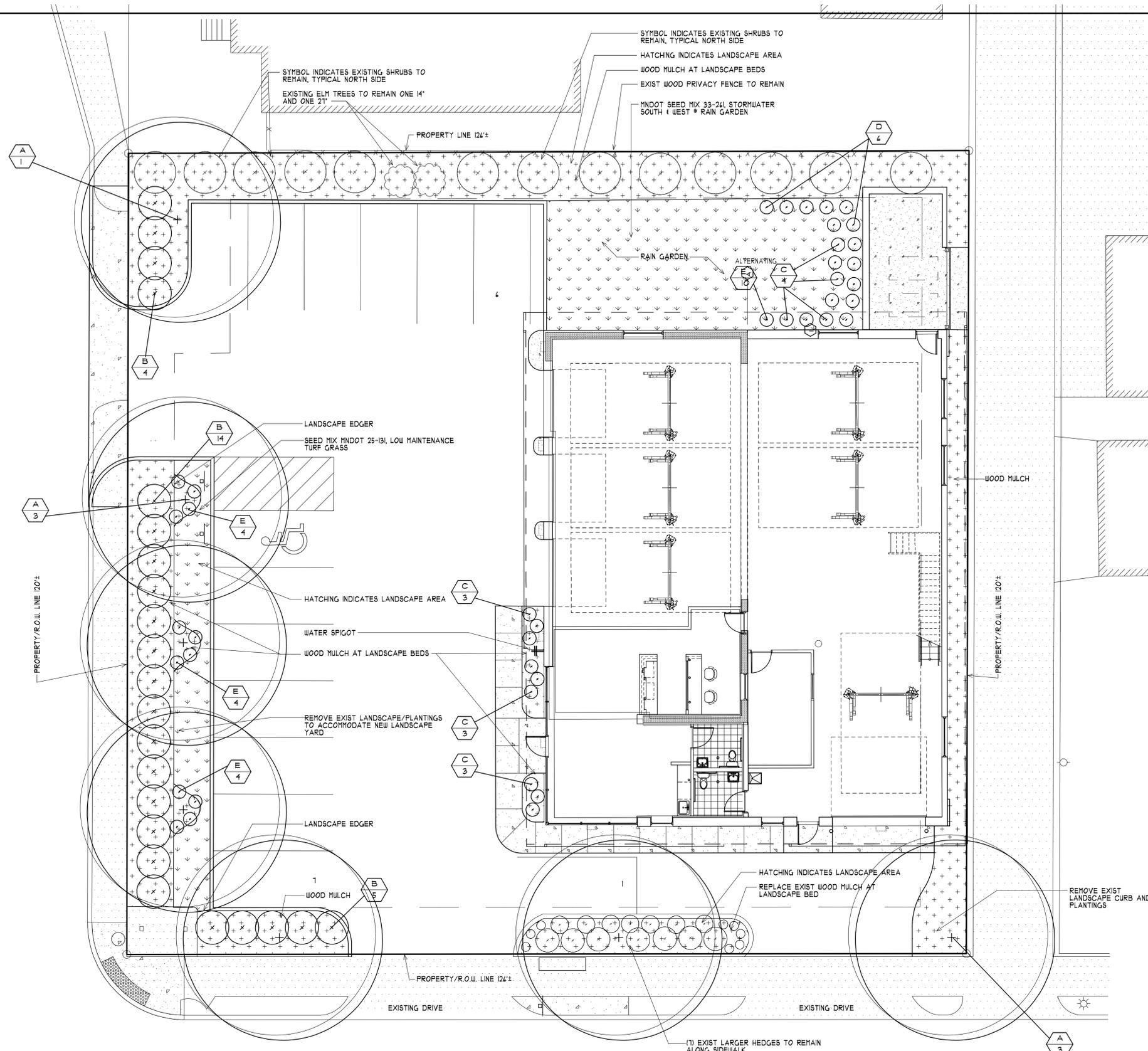
SYM	QTY	COMMON NAME	BOTANICAL NAME	SIZE/ROOT	REMARKS
A	1	SKYLINE HONEYLOCUST	GLIEDITSIA TRIACANTHOS VAR. INERMIS 'SKYCOLE'	2 1/2" BALLED AND BURLAPPED	-
B	23	CURRENT ALPINE	RIBES ALPINUM	#5 CONTAINER	HEDGE PLANTING
C	13	DAYLILLY	HEMEROCALLIS 'PARDON ME'	#1 CONTAINER	-
D	4	CORAL BELLS	HEUCHERA RICHARDSONII	#1 CONTAINER	-
E	22	HOSTA	HOSTA 'ADRIAN'S GLORY'	#1 CONTAINER	-
-	-	-	-	-	-

LANDSCAPE PLAN

SCALE: 1/8" = 1'-0"

54TH STREET EAST

SOUTH 34TH AVENUE



BY: _____ DATE: _____

REVISIONS:

NO.	DESCRIPTION	DATE
1	ISSUE	05/29/14

DRAWN BY: JAC
 CHECKED: CLR
 APPROVED BY: _____
 ISSUE DATE: 05/29/14

Prepared by: _____
 Registered Architect under the laws of the State of Minnesota
 Reg. No. 41634 Date: _____

PROJECT: AUTO SERVICE CENTER EXPANSION FOR: STEVE'S TIRE AND AUTO
 5355 34TH AVE. S MINNEAPOLIS, MN 55417

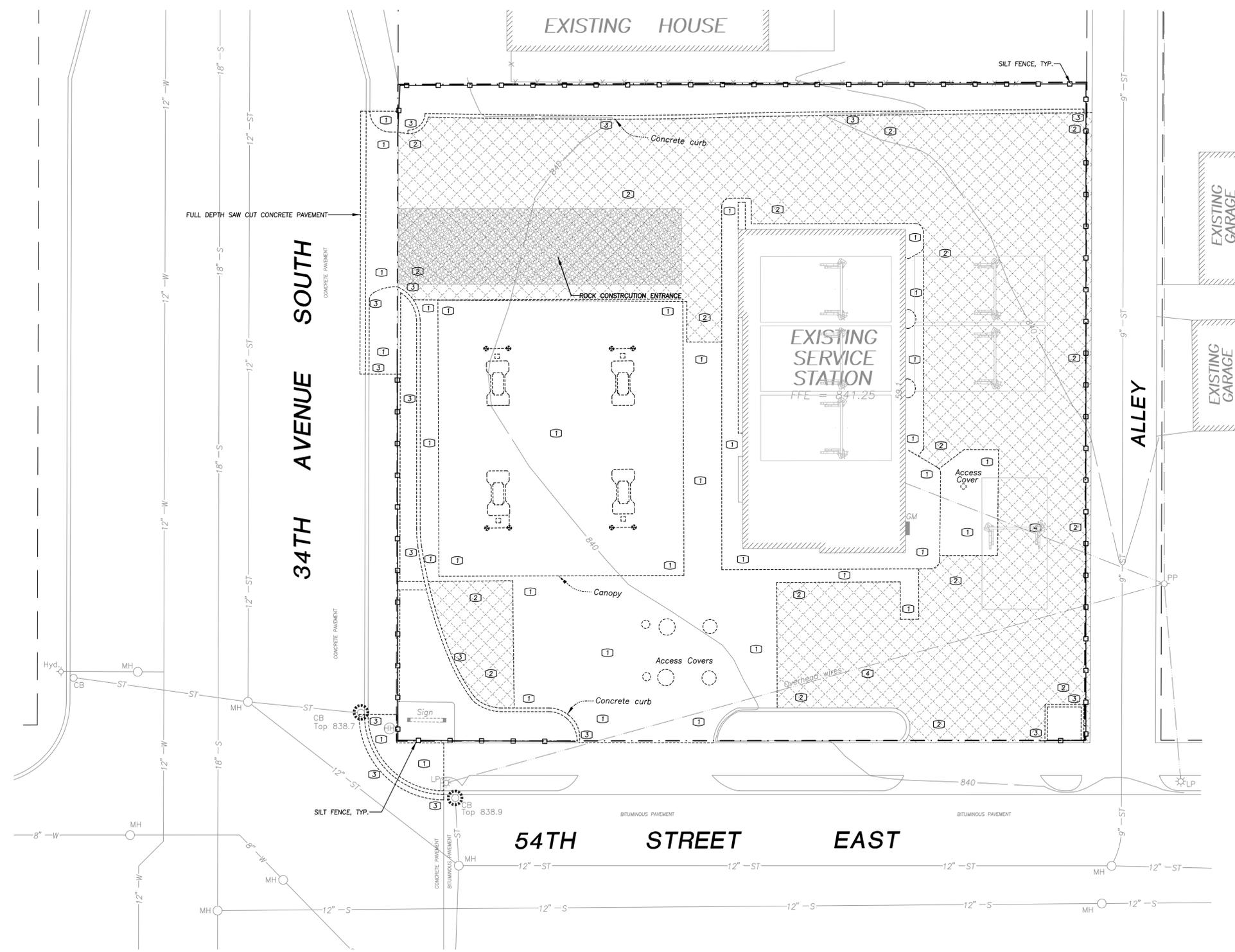
LANDSCAPE PLAN

2147L GRENADA AVE
 MINNEAPOLIS, MN 55444
 PHONE: 952-419-2111
 FAX: 952-419-2113
 EMAIL: office@approdevelopment.com

APPROVAL DEVELOPMENT

PROJECT #: 14-02-0010
 SHEET: AI-2

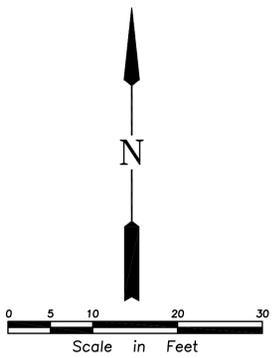
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A PORTABLE CONCRETE WASHOUT BASIN SHALL BE PROVIDED DURING CONSTRUCTION FOR CONCRETE WASHOUT OPERATIONS.

- DEMOLITION NOTES**
- 1 - Remove concrete pavement.
 - 2 - Remove bituminous pavement.
 - 3 - Remove concrete curb.
 - 4 - Coordinate relocation of overhead utility lines with appropriate Utility.

NOTE: SEE ARCHITECTURAL FOR DEMOLITION OF EXISTING CANOPY, PUMP EQUIPMENT AND UNDERGROUND FUEL TANKS



- LEGEND**
- INLET PROTECTION DEVICE
 - SILT FENCE
 - BOUNDARY/ROW/BLOCK LINE

GENERAL PLAN NOTES

1. State law: 48 hours before excavating, call gopher one-call, 651-454-0002, for field location of underground utility lines. This is a free service which locates utility company lines but does not locate private lines. Contractors excavating on site shall provide for locating of private lines, including privately owned lines, by contractor service or other suitable means. Hence company is familiar with the chs campus.
2. Contractor is responsible for providing all O.S.H.A. Safety measures during underground work. This includes providing and maintaining barricades and warning signs, and maintaining appropriate angle of repose on all exposed trenches to protect person and property. Provide shoring and sheeting as required.
3. Contractor shall be responsible for locating and protection of existing structures and utilities as required during excavation and underground work.
4. Contractor shall be responsible for disposal of all excavated material not suitable for backfill and all excess material remaining after backfilling has been completed.
5. Contractor shall be responsible for establishing existing site conditions including elevations, grades, finished surfaces and existing utilities. All areas affected during construction shall be returned to their original condition upon completion of underground work.
6. Contractor shall be responsible for obtaining and paying for all permits required prior to beginning work.
7. Field verify all elevations, dimensions and distances.
8. An obstruction permit is required anytime construction work is performed in the Public right-of-way. Please contact Scott Kramer at 612-673-2383 regarding details of sidewalk and lane closures. Log on to <http://minneapolis.mn.roadway.net/> for a permit.
9. Contact Tim Drew at 612-673-2152 prior to construction for the temporary removal/temporary relocation of any City of Minneapolis signal system that may be in the way of construction.
10. Contact Bill Prince at 612-673-3901 regarding impacts to the existing street lighting system.
11. Contact Doug Maday at 612-673-5755 prior to construction for the removal of any City of Minneapolis right-of-way signs that may be in the way of construction.
12. Permits and approval are required from Environmental Services for the following activities: After hours work, temporary storage of impacted soils on site prior to disposal or reuse, remediation of contaminated soil and groundwater, dewatering and discharge of accumulated storm water or ground water to City sewers, flammable waste traps, underground or above ground tank installation or removal, well construction or sealing, or on-site rock crushing. Contact Tom Frame at tom.frame@ci.minneapolis.mn.us for permit applications and approvals.
13. No construction, demolition or commercial power maintenance equipment shall be operated within the City between the hours of 6:00 a.m. on weekdays and 7:00 a.m. on weekends or during any hours on Saturdays, Sundays and state and federal holidays, except under permit. Contact Environmental Services at 612-673-3867 for permit information.

MINNEAPOLIS STANDARD EROSION CONTROL NOTES

1. Contractor must call a construction start 48 hours prior to any land disturbances 612-673-3867. Failure to do so may result in fines, the revocation of permit and a stop work order being issued.
2. Install perimeter erosion control at the locations shown on the plans prior to the commencement of any land disturbance or construction activities.
3. Before beginning construction, install a temporary rock construction entrance at each point where vehicles exit the construction site. Use 2 inch or greater diameter rock in a layer at least 6 inches thick across the entire width of the entrance. Extend the rock entrance at least 50 feet into the construction zone using a geo-textile fabric beneath the aggregate to prevent migration of soil into the rock from below.
4. Remove all soils and sediments tracked or otherwise deposited onto public and private pavement areas. Removal shall be on a daily basis when tracking occurs and may be ordered by Minneapolis inspectors at any time if conditions warrant. Sweeping shall be maintained throughout the duration of the construction and done in a manner to prevent dust being blown to adjacent properties.
5. Install inlet protection at all public and private catch basin inlets, which receive runoff from the disturbed areas. Contractor shall clean, remove sediment or replace storm drain inlet protection devices on a routine basis such that the devices are fully functional for the next rain event. Sediment deposited in and/or plugging drainage systems is the responsibility of the contractor. Hay bales or filter fabric wrapped grates are not allowed for inlet protection.
6. Locate soil or dirt stockpiles no less than 25 feet from any public or private roadway or drainage channel. If remaining for more than seven days, stabilize the stockpiles by mulching, vegetative cover, tarps, or other means. Control erosion from all stockpiles by placing silt barriers around the piles. Temporary stockpiles located on paved surfaces must be no less than two feet from the drainage/gutter line and shall be covered if left more than 24 hours.
7. Maintain all temporary erosion and sediment control devices in place until the contributing drainage area has been stabilized. Inspect temporary erosion and sediment control devices on a daily basis and replace deteriorated, damaged, or rotted erosion control devices immediately.
8. Temporarily or permanently stabilize all construction areas which have undergone final grading, and all areas in which grading or site building construction operations are not actively underway against erosion due to rain, wind and running water within 7-14 days. Use seed and mulch, erosion control matting, and/or sodding and staking in green space areas. Remove all temporary synthetic, structural, non-biodegradable erosion and sediment control devices after the site has undergone final stabilization with permanent vegetation establishment. Final stabilization for purposes of this removal is 70% established cover over denuded area.
9. Ready mixed concrete and concrete batch/mix plants are prohibited within the public right of way. All concrete related production, cleaning and mixing activities shall be done in the designated concrete mixing/washout locations as shown in the erosion control plan. Under no circumstance may washout water drain onto the public right of way or into any public or private storm drain conveyance.
10. Changes to approved erosion control plan must be approved by the erosion control inspector prior to implementation. Contractor to provide installation and details for all proposed alternate type devices.
11. If dewatering or pumping of water is necessary, the contractor is responsible for obtaining any necessary permits and/or approvals prior to discharge of any water from the site. If the discharge from the dewatering or pumping process is turbid or contains sediment laden water, it must be treated through the use of sediment traps, vegetative filter strips, or other sediment reducing measures such that the discharge is not visibly different from the receiving water. Additional erosion control measures may be required at the discharge point to prevent scour erosion. The contractor shall provide a dewatering/pumping plan to the Erosion Control Inspector prior to initiating dewatering activities.

PARTY RESPONSIBLE FOR MAINTENANCE OF ALL EROSION AND SEDIMENT CONTROL MEASURES

OWNER _____ DATE _____

DEVELOPER _____ DATE _____

CONTRACTOR _____ DATE _____

ALL CLEARING, GRADING, CONSTRUCTION OR DEVELOPMENT WILL BE DONE PURSUANT TO THE PLAN

OWNER _____ DATE _____

DEVELOPER _____ DATE _____

CONTRACTOR _____ DATE _____

Rehder & Associates, Inc.
 Civil Engineers, Planners and Land Surveyors
 3440 Federal Drive, Suite 110 • Eden, Minnesota, 55122
 651-452-5051 • Fax: 651-452-9797 • email: info@rehder.com
 PROJECT NO: 141-2187.018 DRAWING FILE: 2187018.DWG

I hereby certify that this plan was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer in the State of Minnesota.

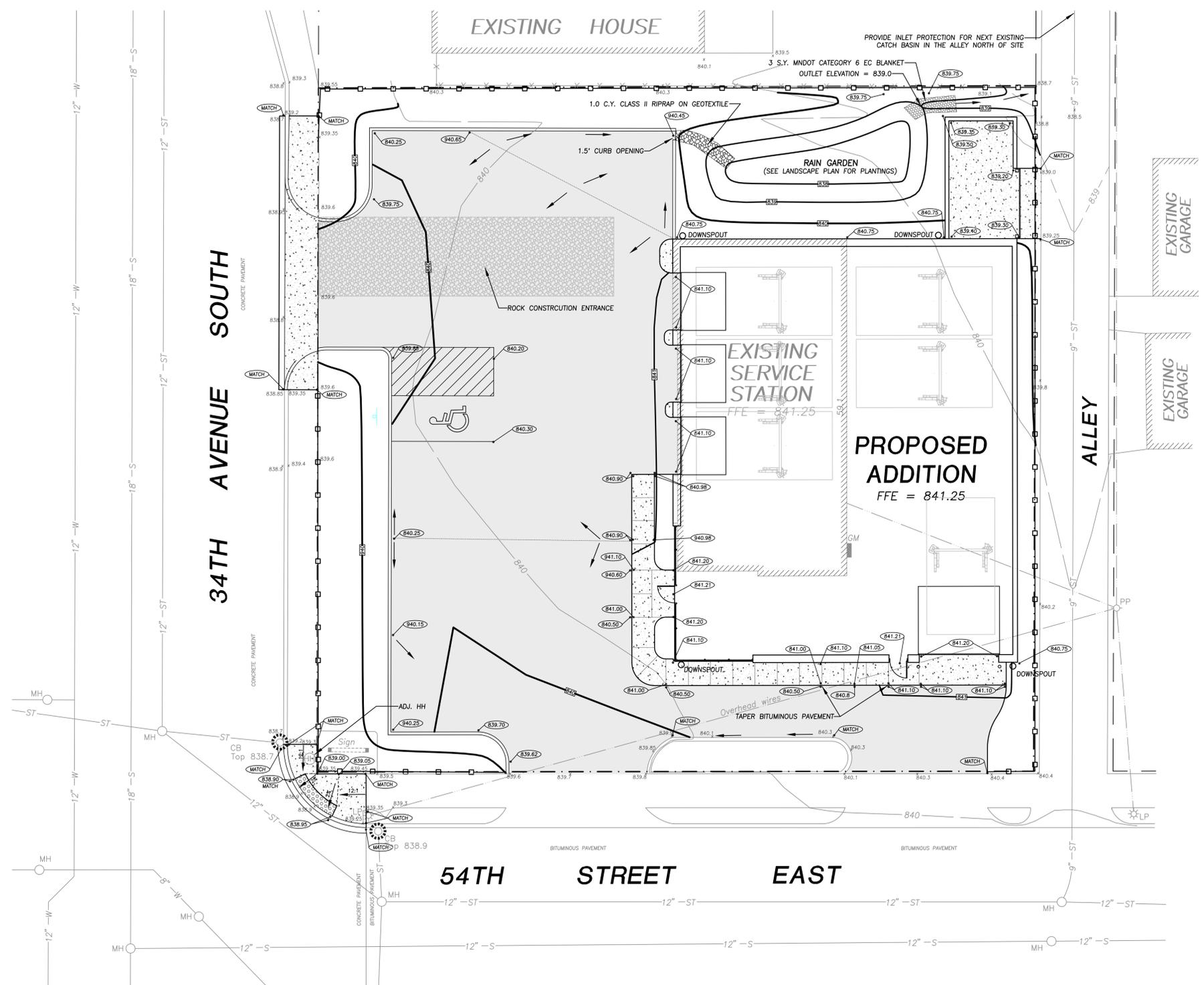
PRELIMINARY

Name _____ Date _____
 Reg. No. _____

Issued
 GENERAL LAND USE APP. & POR
 ADDRESS CITY COMMENTS - POR RESUBMITAL
 5-30-14
 6-20-14

DEMOLITION & EROSION CONTROL PLAN
STEVE'S TIRE & AUTO
 5355 - 34TH AVE SOUTH
 CITY OF MINNEAPOLIS

SHEET NUMBER
C1



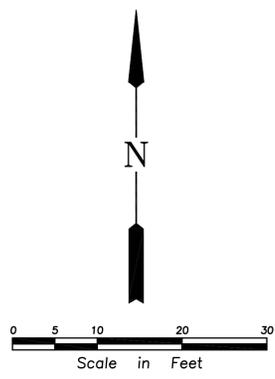
SITE DATA
 SITE AREA = 15,190 SQ.FT.
 EXISTING IMPERVIOUS AREA = 13,833 SQ.FT.
 PROPOSED IMPERVIOUS AREA = 11,905 SQ.FT.
 PROPOSED IMPERVIOUS AREA REDUCTION = 13.9%

NON-STORMWATER DISCHARGES
 The proposed structure and associated site improvements should not, to the best of our knowledge, produce any non-stormwater discharges. The soil borings do not reveal that the proposed structure will fall within a zone that produces continuous or regular groundwater discharges.
 As typical, the structure will, however, have a foundation dewatering system and an elevator pit tied to a sump pump. When that system does receive intermittent and/or nuisance flow, it is assumed that the flow will be the result of stormwater infiltrating the surface and entering the drain tile system. The sump pump for the system will discharge the stormwater at grade, adjacent to the building and, not, into a direct connection to the storm sewer system. Therefore, any flows generated by the drain tile/sump pump should be rare and be fully disconnected from the City's storm water infrastructure.

GRADING NOTES
 [1] - All elevations shown are to final surfaces.
 NOTE: REFER TO SHEET C1 FOR EROSION CONTROL NOTES

- CONSTRUCTION SEQUENCE SCHEDULE**
1. INSTALL TEMPORARY ROCK CONSTRUCTION ENTRANCE.
 2. INSTALL SILT FENCE & INLET PROTECTION AS SHOWN ON THE PLANS.
 3. PERFORM DEMOLITION OF SITE ITEMS.
 4. REMOVE TOPSOIL FROM THE SITE AND PLACE IN TEMPORARY STOCKPILE LOCATION OR HAUL OFF-SITE.
 5. ROUGH GRADE THE SITE.
 6. TEMPORARY SEED AND MULCH DISTURBED AREAS ON SITE.
 7. CONSTRUCT BUILDING ADDITION, PARKING LOT AND SIDEWALKS.
 8. FINAL GRADE SITE.
 9. STABILIZE SITE WITH SOD, SEED OR HARD SURFACE.
 10. REMOVE SILT FENCE, INLET PROTECTION AND ROCK CONSTRUCTION ENTRANCE.

- LEGEND**
- PROPOSED CONCRETE
 - PROPOSED STD. DUTY BITUMINOUS
 - PROPOSED CONTOUR
 - PROPOSED ELEVATION
 - SILT FENCE
 - INLET PROTECTION DEVICE
 - BOUNDARY/ROW/BLOCK LINE
 - EXISTING ELEVATION
 - EXISTING WATERMAIN
 - EXISTING SANITARY SEWER
 - EXISTING STORM SEWER



Rehder & Associates, Inc.
 Civil Engineers, Planners and Land Surveyors
 3140 Federal Drive, Suite 110 • Eagan, Minnesota 55122
 651-692-5051 • Fax: 651-692-9797 • email: info@rehder.com

PROJECT NO.: 141-2187-018 DRAWING FILE: 2187018.DWG

I hereby certify that this plan was prepared by me or a duly Licensed Professional Engineer under the laws of the State of Minnesota.

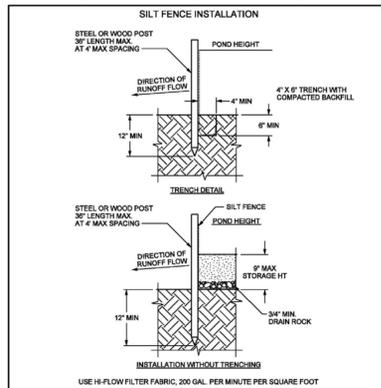
PRELIMINARY

Name _____ Date _____
 Reg. No. _____

Issued
 GENERAL LAND USE APP & PDR 5-30-14
 ADDRESS CITY COMMENTS - PDR RESUBMITAL 6-20-14

GRADING & DRAINAGE PLAN
 STEVE'S TIRE & AUTO
 5355 - 34TH AVE SOUTH
 CITY OF MINNEAPOLIS

SHEET NUMBER
C2



SILT FENCE INSTALLATION

STEEL OR WOOD POST
3/4" LENGTH MAX.
AT 4' MAX SPACING

4" X 6" TRENCH WITH
COMPACTED BACKFILL

12" MIN

4" MIN

6" MIN

DIRECTION OF
RUNOFF FLOW

4" MIN

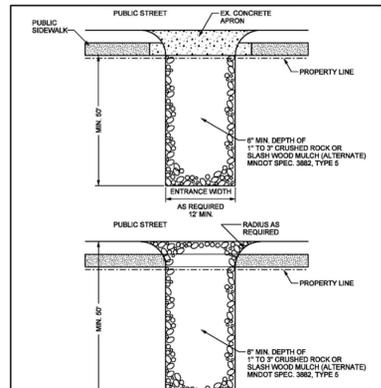
3/4" MIN
DRAIN ROCK

INSTALLATION WITHOUT TRENCHING

USE 1/2" FLOW FILTER FABRIC, 200 GAL. PER MINUTE PER SQUARE FOOT

MINNEAPOLIS
DEPARTMENT OF PUBLIC WORKS

STANDARD
PLATE
NO.
ROAD-3001



SILT FENCE INSTALLATION

STEEL OR WOOD POST
3/4" LENGTH MAX.
AT 4' MAX SPACING

4" X 6" TRENCH WITH
COMPACTED BACKFILL

12" MIN

4" MIN

6" MIN

DIRECTION OF
RUNOFF FLOW

4" MIN

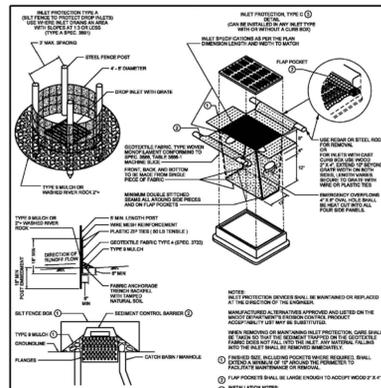
3/4" MIN
DRAIN ROCK

INSTALLATION WITHOUT TRENCHING

USE 1/2" FLOW FILTER FABRIC, 200 GAL. PER MINUTE PER SQUARE FOOT

MINNEAPOLIS
DEPARTMENT OF PUBLIC WORKS

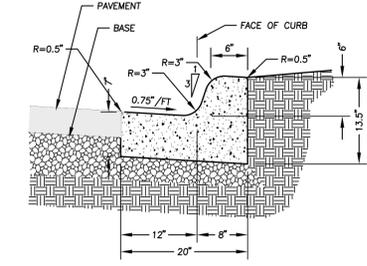
STANDARD
PLATE
NO.
SEWR-6002



INLET PROTECTION TYPE A & C

MINNEAPOLIS
DEPARTMENT OF PUBLIC WORKS

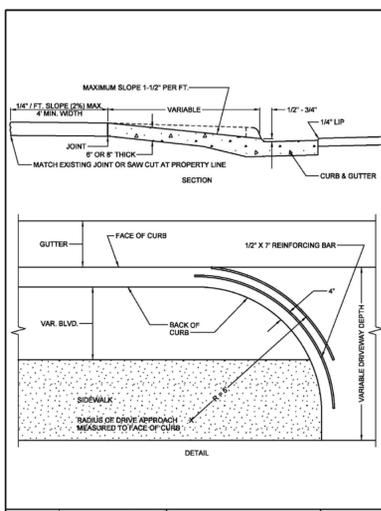
STANDARD
PLATE
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SEWR-6003



B612 CONCRETE CURB & GUTTER

NO SCALE

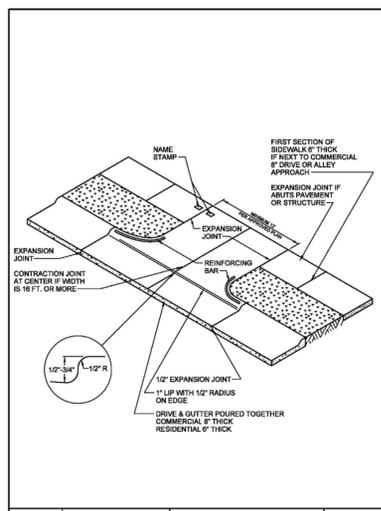
1
C3



TYPICAL DRIVEWAY CONSTRUCTION

MINNEAPOLIS
DEPARTMENT OF PUBLIC WORKS

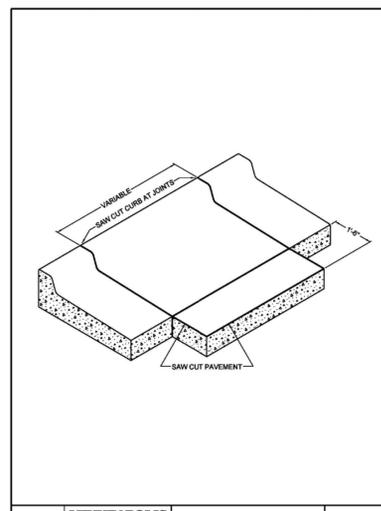
STANDARD
PLATE
NO.
ROAD-2001



TYPICAL DRIVEWAY

MINNEAPOLIS
DEPARTMENT OF PUBLIC WORKS

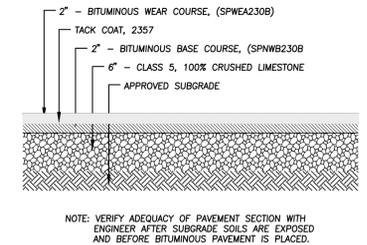
STANDARD
PLATE
NO.
ROAD-2002



SAW CUT AT CURB AND GUTTER REMOVAL

MINNEAPOLIS
DEPARTMENT OF PUBLIC WORKS

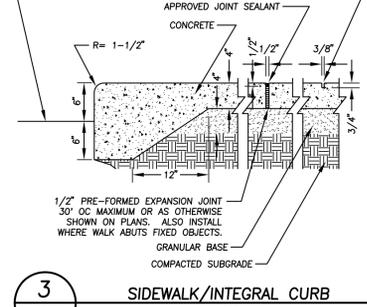
STANDARD
PLATE
NO.
ROAD-1010



PAVEMENT SECTION

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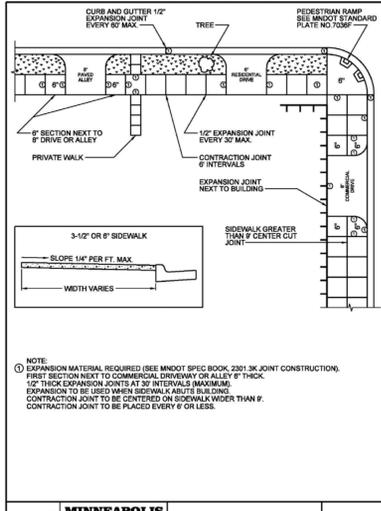
2
C3



SIDEWALK/INTEGRAL CURB

NO SCALE

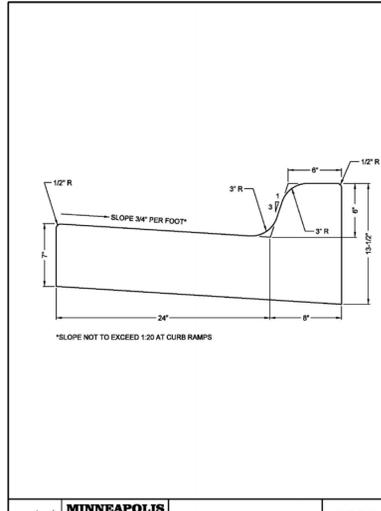
3
C3



TYPICAL SIDEWALK AND DRIVEWAY CONSTRUCTION

MINNEAPOLIS
DEPARTMENT OF PUBLIC WORKS

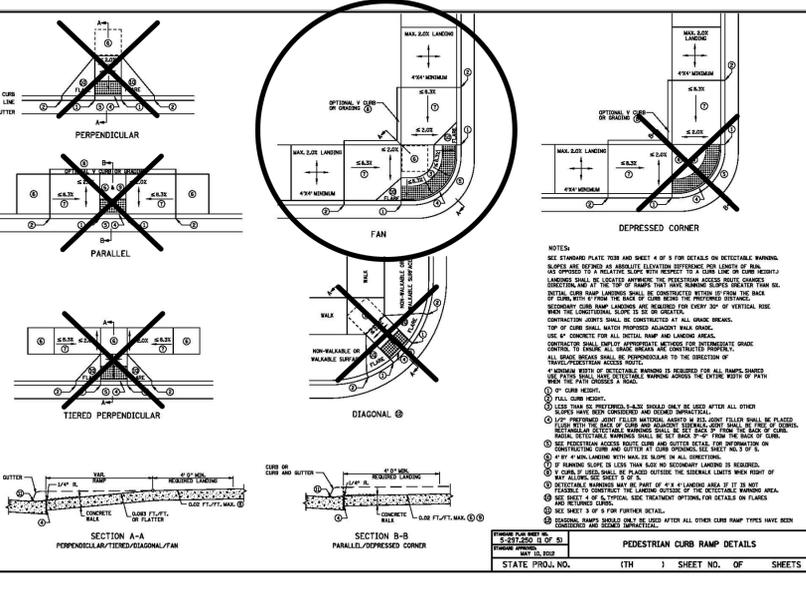
STANDARD
PLATE
NO.
ROAD-2003



B-624 CURB AND GUTTER

MINNEAPOLIS
DEPARTMENT OF PUBLIC WORKS

STANDARD
PLATE
NO.
ROAD-1003



PEDESTRIAN CURB RAMP DETAILS

MINNEAPOLIS
DEPARTMENT OF PUBLIC WORKS

STATE PROJ. NO. (TH) SHEET NO. OF SHEETS

SPECIFICATIONS

- GENERAL**
- GENERAL
 - Before construction begins, the Contractor will contact all utility companies, both public and private and have them locate all utilities within the construction limits.
 - The Contractor shall be responsible for arranging all required inspections with the governing authority that has jurisdiction over the work that is to be performed.
 - The Contractor shall stay within the construction limits unless approved otherwise by the Owner and/or Engineer. Construction limits are defined by the property boundary unless shown different on the plan.
 - The Contractor shall be responsible for protecting all existing structures, utilities, trees, etc. from damage during construction.
 - The Contractor shall be responsible for correcting any damage (at Contractor's expense).
 - Any discrepancies found on the site that affect the proposed work shall be reported to the Owner and/or Engineer before the completion of any additional work.

- SITE CLEARING**
- GENERAL
 - Remove trees, shrubs, grass, and other vegetation or obstructions, as required, to permit installation of improvements shown on the Plans.
 - EXECUTION
 - Trees and stumps shall be hauled from the site. Burial on-site or burning of trees and stumps will not be allowed.
 - Where existing trees are indicated to remain, leave existing topsoil in place within drip lines to prevent damage to root system.
 - Topsoil shall be stripped from disturbed areas and stockpiled in piles not exceeding 8-feet in depth.
 - Remove all back dirt and unsuitable material from under drives and roadways within 3-feet of final pavement subgrade.
 - Remove all waste materials and unsuitable or excess topsoil from Owner's property.

- GRADING, EROSION CONTROL, AND TURF ESTABLISHMENT**
- GENERAL
 - All grading, erosion control and turf establishment shall be according to the materials, workmanship, and other applicable requirements of the Minnesota Department of Transportation "Standard Specifications for Construction", latest edition, unless otherwise specified.
 - All erosion control measures shown on the plans must be installed prior to commencement of grading operations and maintained until all areas altered on the site have been restored.
 - All areas disturbed by construction shall be restored with seed and disked mulch, sod, wood fiber blanket, or be hard surfaced within two weeks of substantial completion of construction.
 - Provide approved borrow soil materials from off-site when sufficient approved soil materials are not available from excavations. Remove all excess and unsatisfactory material from the site.
 - Protect structures, utilities, sidewalks, pavements, and other facilities from damage caused by settlement, lateral movement, undermining, washout, and other hazards created by earthwork operations.
 - Compaction shall not be less than the following percentages of maximum dry density according to ASTM D 698:
 - Under structures, building slabs, steps, and pavements, compact the top 12 inches below subgrade and each layer of backfill or fill material at 100 percent maximum dry density.
 - Under walkways, compact the top 6 inches below subgrade and each layer of backfill or fill material at 100 percent maximum dry density.
 - Under lawn or unpaved areas, compact the top 6 inches below subgrade and each layer of backfill or fill material at 95 percent maximum dry density.
 - Grades as shown on the plan are to finished grade.
 - PRODUCTS
 - Satisfactory soils include ASTM D 2487 soil classification groups GW, GP, GM, SW, SP, and SM; free of rock or gravel larger than 2-inches in any dimension, debris waste, frozen materials, vegetation and other deleterious matter.
 - Unsatisfactory soils include ASTM D 2487 soil classification groups GC, SC, ML, MH, CL, CH, OL, OH, and PT.
 - All backfill and fill materials must be satisfactory soil materials.
 - Topsoil shall be per ASTM D 5288, free of stones 1" or larger.
 - Subbase and base material must be a naturally or artificially graded mixture of natural or crushed gravel, crushed stone, and natural or crushed sand meeting MNDOT Specifications for Class 5 gravel.
 - See landscape plan for turf establishment.

- BITUMINOUS PAVEMENT**
- GENERAL
 - Provide hot-mix asphalt pavement according to the materials, workmanship, and other applicable requirements of the Minnesota Department of Transportation "Standard Specifications for Construction", latest edition, unless otherwise specified.
 - Conform to applicable standards of authorities having jurisdiction for asphalt paving work on public property.
 - PRODUCTS
 - Use coarse and fine aggregate materials and gradations that have performed satisfactorily in previous installations.
 - Provide a base and wear course as indicated on the plan unless otherwise specified.
 - Provide a tack coat as indicated on the plan unless otherwise specified.
 - EXECUTION
 - Verify that the subgrade is dry and in suitable condition to support paving and imposed loads.
 - The Contractor shall furnish a tandem truck loaded with a minimum of 14-tons to check the completed subgrade and/or aggregate base for soft spots prior to placement.
 - Machine place hot-mix asphalt on prepared surface, spread uniformly, and strike off. Place asphalt mix by hand to areas inaccessible to equipment in a manner that prevents segregation of mix. Place each course to required grade, cross section, and thickness, when compacted.
 - Begin compaction as soon as placed hot-mix paving will bear roller weight without excessive displacement.
 - Provide an average density of 96 percent of reference laboratory density according to ASTM D 1559, but not less than 94 percent nor greater than 100 percent.
 - Tolerances: Base course thickness shall be plus or minus 0.5-inches and surface course shall be plus or minus 0.25-inches.

- PORTLAND CEMENT CONCRETE PAVEMENT**
- GENERAL
 - Provide Portland cement concrete pavement for roads, curbs, walks and exterior slabs according to the materials, workmanship, and other applicable requirements of the Minnesota Department of Transportation "Standard Specifications for Construction", latest edition, unless otherwise specified.
 - PRODUCTS
 - Portland cement concrete for curb and gutter and sidewalk shall be 4000 psi, 28-day compressive strength, 5.0% air entrainment, and 3-inch slump.
 - Curing compound shall be solvent-borne, liquid membrane-forming ASTM C309, Type I or approved equal.
 - EXECUTION
 - The Contractor shall furnish a tandem truck loaded with a minimum of 14-tons to check the completed subgrade and/or aggregate base for soft spots prior to pavement placement.
 - Comply with requirements and with ACI 304R for measuring, mixing, transporting, and placing concrete.
 - Preformed expansion joints using 0.5-inch thickness shall be placed at each end of curb radius, at intersections, and approximately every 200-feet.
 - Contraction joints shall be placed at minimum 10-foot intervals in the curb and gutter and at 5-foot for walks.
 - Provide a medium to fine broom finish perpendicular to traffic flow.
 - Protect freshly placed concrete from premature drying and excessive cold or hot temperatures using moisture curing, moisture-retaining-cover curing, curing compound or a combination of these.

Rehder & Associates, Inc.
 Civil Engineers, Planners and Land Surveyors
 3140 Federal Drive, Suite 110 • Eagan, Minnesota 55122
 651-192-5051 • Fax: 651-192-9797 • email: info@rehder.com

PROJECT NO: 141-2187.018 DRAWING FILE: 2187018.DWG

I hereby certify that this plan was prepared by
 a duly Licensed Professional Engineer
 in the State of Minnesota.

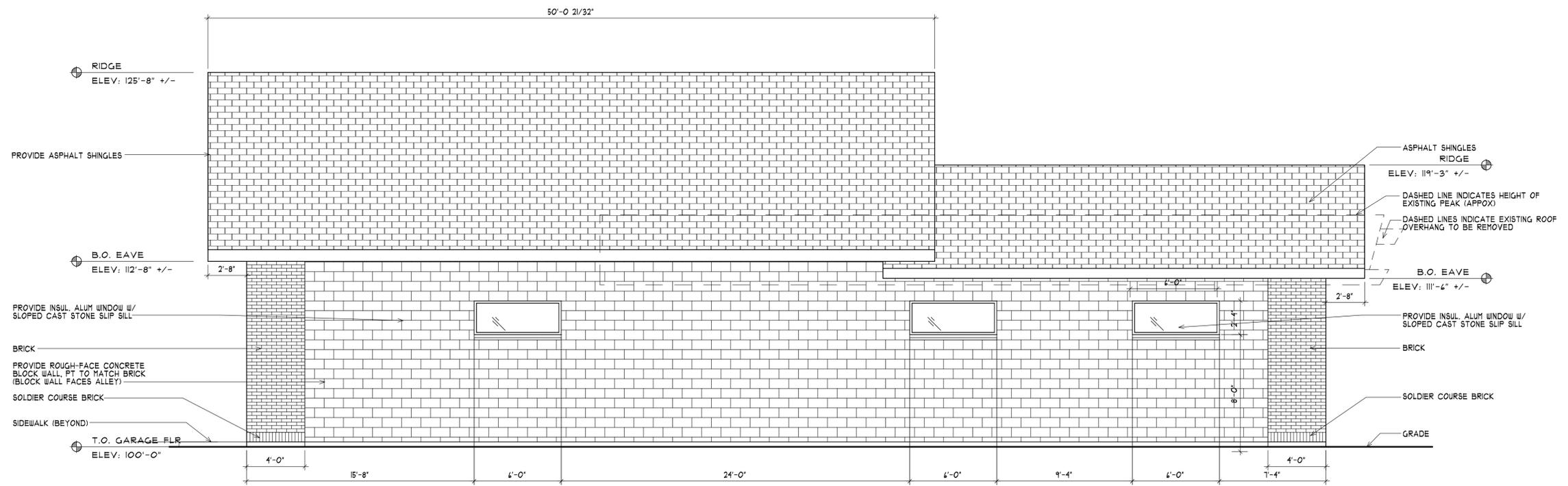
PRELIMINARY

Name _____ Date _____ Reg. No. _____

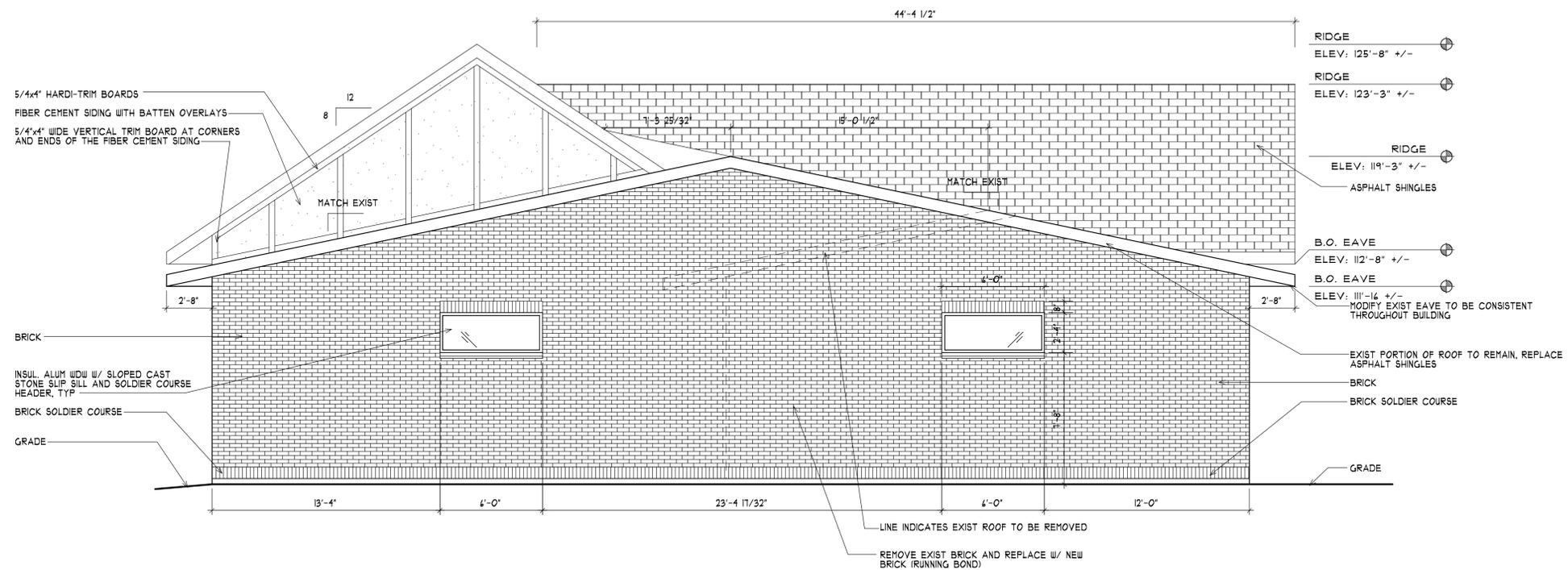
Issued
 GENERAL LAND USE APP. & FOR
 ADDRESS CITY COMMENTS - FOR RESUBMITAL
 5-30-14
 6-20-14

DETAILS & SPECIFICATIONS PLAN
STEVE'S TIRE & AUTO
5355 - 34TH AVE SOUTH
CITY OF MINNEAPOLIS

SHEET NUMBER
C3



EAST ELEVATION 2
SCALE: 1/4" = 1'-0"
A3-2



NORTH ELEVATION 1
SCALE: 1/4" = 1'-0"
A3-2



PROJECT	AUTO SERVICE CENTER EXPANSION FOR: STEVES TIRE AND AUTO		
PROJECT #	14-02-0010		
SHEET	A3-2		
PROJECT ADDRESS	5355 34TH AVE S MINNEAPOLIS, MN 55417		
PROJECT TYPE	EXTERIOR ELEVATIONS		
PROJECT CONTACT	21416 GRENADA AVE LAKEVILLE, MN 55044 PH: 952-449-2171 FAX: 952-449-2173 EMAIL: office@approdevelopment.com		
APPROVAL			
DRAWN BY	JAC	CHECKED BY	CLR
APPROVED BY	[Signature]		
ISSUE DATE	05/29/14		
REVISIONS	-		
BY	-	DATE	-

Steve's Tire and Auto - Colored Renderings/Elevations

5355 34th Avenue South
Minneapolis, MN



View from corner, looking northeast.



View toward southeast.



West elevation.



South elevation.



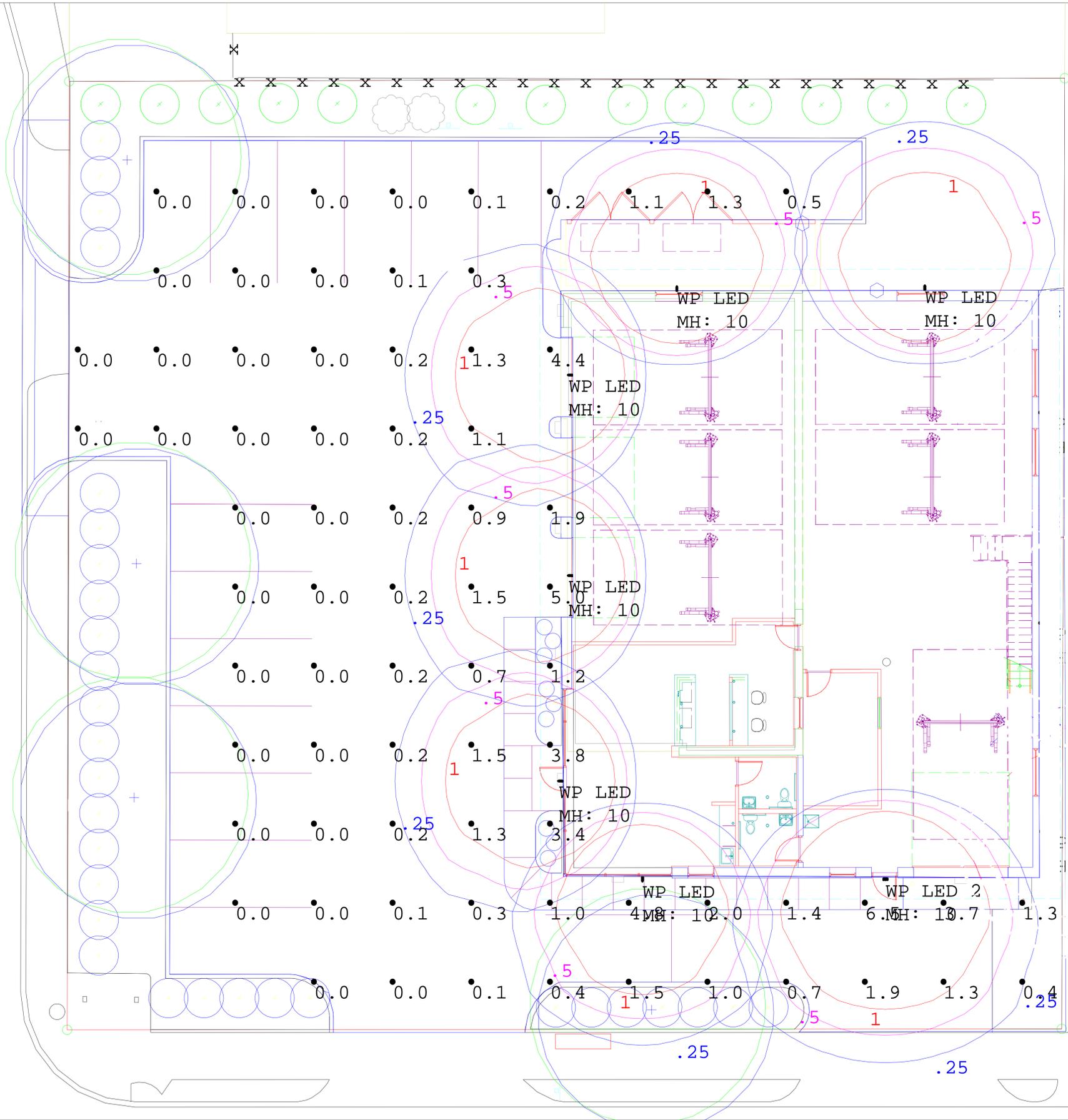
View looking northeast from south side of 54th.



View of main entry at southwest corner.



View from northwest corner of lot, looking southeast.



Calculation Summary

Label	CalcType	Units	Avg	Max	Min	Avg/Min	Max/Min
Parking Lot_Planar	Illuminance	Fc	0.84	6.5	0.0	N.A.	N.A.
Luminaire Schedule							
Symbol	Qty	Label	Arrangement	LLF	Description		
	6	WP LED	SINGLE	0.910	XTOR2A-N		
	1	WP LED 2	SINGLE	0.910	XTOR3A-N		
	3	WP LED VISOR	SINGLE	0.910	XTOR2A-N-WITH VISOR		

Steve's Tire & Auto

LED Option Without Pole

Drawn By: Dan Budke

Checked By:

Date: 6/18/2014

Scale:

Revisions

#	Date	Comments



DESCRIPTION

The IMPACT Elite Cylinder cutoff wall luminaire makes an ideal complement to site design. Rugged construction and full cutoff classified optics provide facade and security lighting for light restricted zones surrounding schools, office complexes, apartments, and recreational facilities. U.L. and cUL Listed for wet locations in down mount applications and damp locations in up mounted applications.

Catalog #		Type
Project		
Comments		Date
Prepared by		

SPECIFICATION FEATURES

Construction

Two-piece die-cast aluminum housing and removable hinged door frame nests securely for precise tolerance control and repeatability. Hinged door frame inset for clean mating with housing surface and secured via two [2] captive stainless steel fasteners. Optional tamper resistant torx-head fasteners [TR] offer vandal resistant access to the electrical compartment. 1/8" clear, heat and impact resistant tempered flat glass lens combine with molded silicone gasket to seal the integrated optical assembly while ensuring peak optical performance.

Electrical

Integral hard mount electrical components are secured and grounded within the die-cast aluminum housing for optimal heat sinking and extended component life. Minimum starting temperatures are -40°C (-40°F) for HPS and -30°C (-22°F) for MP. Compact fluorescent luminaires

feature electronic universal 120-277V high efficiency 50/60Hz ballast with -18°C (0°F) minimum starting temperature. Available in 50-175W HID or 26-84W CF. Emergency power pack available for 26-64W CF.

Optical

Premium anodized 95% reflective aluminum optical assemblies provide high efficiency Type II distribution. Optional silk screened house side flat glass shield provides decreased wall brightness. All Impact Elite Wall Series luminaires classify as IESNA full cutoff in downlight applications. Available with 10% or 50% uplight options for architectural highlighting of building details.

Mounting

Gasketed and zinc plated rigid steel mounting attachment fits directly to 4" j-box or wall with the Impact Elite "Hook-N-Lock" system for fast installation and mounting in both

traditional downlighting [wet location] or inverted uplighting [damp location] mounting positions. Secured via two [2] captive corrosion resistant allen head set screws concealed from view, but accessible from bottom of fixture.

Finish

Housing and door are protected with 5-stage premium TGIC polyester powder coat paint. Premium TGIC powder coat finishes withstand extreme climate changes while providing optimal color and gloss retention over the fixture's installed life. Standard colors include black, bronze, grey, white, dark platinum and graphite metallic. RAL and custom color matches available. Consult McGraw-Edison Architectural Colors Brochure for complete selection.



ISC IMPACT ELITE CYLINDER

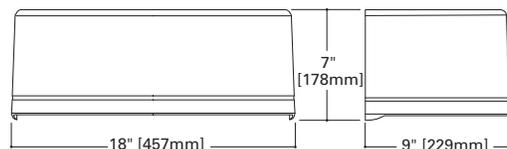
- 50 - 175W**
- Pulse Start Metal Halide
- Metal Halide
- High Pressure Sodium
- 26 - 84W**
- Compact Fluorescent

**FULL CUTOFF
WALL MOUNT LUMINAIRE**



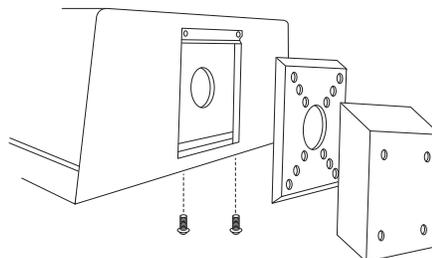
NOTE: In downlight applications only.

STANDARD DIMENSIONS

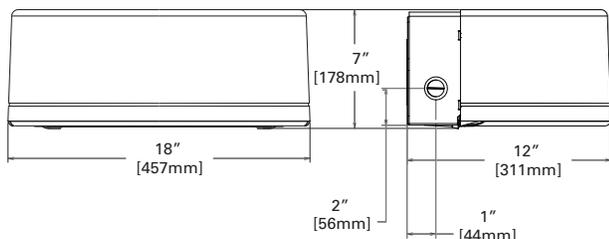


HOOK-N-LOCK MOUNTING

(Mounting attachment included. J-Box not included.)



DIMENSIONS WITH BACK BOX



TECHNICAL DATA

UL1598 Listed, CUL Listed
25°C Maximum Ambient Temperature
External Supply Wiring 90°C Minimum
Down Mounted—Wet Location
Up Mounted—Damp Location

ENERGY DATA

High Reactance Ballast Input Watts

50W HPS HPF (66 Watts)
50W MP HPF (72 Watts)
70W HPS HPF (91 Watts)
70W MP HPF (90 Watts)
100W HPS HPF (130 Watts)
100W MP HPF (129 Watts)
150W HPS HPF (190 Watts)
150W MP HPF (185 Watts)

Electronic Ballast Input Watts

26W CF HPF (29 Watts)
32W CF HPF (36 Watts)
42W CF HPF (46 Watts)
52W CF HPF (55 Watts)

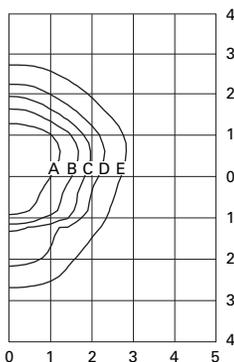
SHIPPING DATA

Approximate Net Weight:

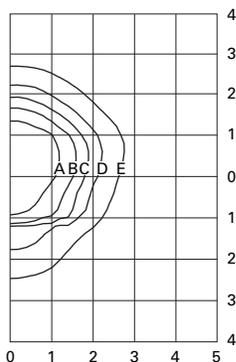
18 lbs. (8 kgs.)

IMPACT™

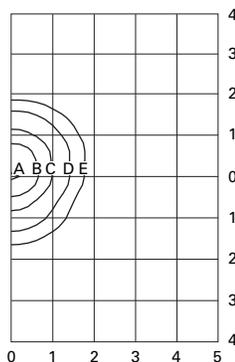
PHOTOMETRICS (Complete IES files available at www.cooperlighting.com)



ISC-150-MP-XX-2S
150—Watt MP
14,000—Lumen Clear Lamp



ISC-150-HPS-XX-2S
150—Watt HP
16,000—Lumen Clear Lamp



ISC-42-CF-XX-2S
42—Watt CF
3,200—Lumen Lamp

Footcandle Table

Select mounting height and read across for footcandle values of each isofootcandle line. Distance in units of mounting height.

Mounting Height	Footcandle Values for Isofootcandle Lines				
	A	B	C	D	E
150-MP / 150-HPS					
10'	7.20	2.88	1.44	0.72	0.29
12'	5.00	2.00	1.00	0.50	0.20
14'	3.65	1.46	0.73	0.37	0.07
42-CF					
18'	2.46	1.23	0.62	0.25	0.12
20'	2.00	1.00	0.50	0.20	0.10
25'	1.28	0.64	0.32	0.13	0.06

ORDERING INFORMATION

Sample Number: ISC-150-MP-MT-2S-BZ

Product Family ISC=Impact Elite Small Cylinder	Lamp Wattage ¹ MP 50=50W ² 70=70W ² 100=100W 150=150W HPS 50=50W ² 70=70W ² 100=100W 150=150W MH 175=175W ¹ CF 26=(1)26W 32=(1) 32W 42=(1) 42W 52=(2) 26W 64=(2) 32W 84=(2) 42W	Lamp Type MP=Pulse Start Metal Halide HPS=High Pressure Sodium MH=Metal Halide ¹ CF=Compact Fluorescent	Voltage ³ 120=120V 208=208V 240=240V 277=277V 347=347V 480=480V DT=Dual-Tap ⁴ MT=Multi-Tap ⁴ TT=Triple-Tap ⁴ E1=Electronic (120-277V) ⁵ Distribution 2S= Type II Segmented	Colors [add as suffix] AP=Grey BZ=Bronze BK=Black DP=Dark Platinum GM=Graphite Metallic WH=White	Options F=Single Fuse (120, 277, or 347V) ⁶ FF=Double Fuse (208, 240 or 480V) ⁶ P=Button Photocontrol (120, 208, 240 or 277V) ⁶ L=Lamp Included TR=Tamper Resistant Fasteners UPL10=10% Uplight UPL50=50% Uplight ⁷ HS=House Side Shield Glass ⁵ PL=Polycarbonate Lens ^{5, 8} Q=Quartz Restrike (1) T4 Lamp ⁹ EM=Emergency Quartz Restrike (1) T4 Lamp with Time Delay Relay ⁹ EM/SC=Emergency Separate Circuit (1) T4 Lamp ^{9, 10} QMR=Quartz Restrike (1) MR16 Lamp ^{11, 12} EMMR=Emergency Quartz Restrike (1) MR16 Lamp with Time Delay Relay ^{11, 12} EM/SC/MR=Emergency Separate Circuit (1) MR16 Lamp ^{10, 12, 13} EM/SC/12V=Emergency Separate Circuit 12V Low Voltage (1) MR16 Lamp ^{10, 14} 2EM/SC/12V=Emergency Separate Circuit 12V Low Voltage (2) MR16 Lamps ^{10, 14} EMI40-XXX=Emergency Cold Temperature (UL924 Listed) Power Pack -18°C/0°F, (1) CF Lamp ¹⁵ CFEM-XXX=Emergency CF Power Pack (UL924 Listed) (1) CF Lamp, 0°C/32°F ¹⁶ 2EMI40-XXX=Emergency Cold Temperature power pack (UL924 listed), -18°C/0°F, (2) CF Lamps with Back Box matching housing finish ^{15, 17} 2CFEM-XXX=Emergency CF power pack (UL924 listed) 0°C/32°F, (2) CF Lamps, with Back Box matching housing finish ^{16, 18}	Accessories ¹⁹ MA1255-XX=Thruway Back Box - Impact Elite Cylinder

Notes:

- HID lamps are medium base. 175W MH is available for non-U.S. markets only.
- Not available with 480V.
- Products also available in non-US voltages and 50HZ for international markets. Consult your Cooper Representative for availability and ordering information.
- Dual-tap ballast is 120/277V wired 277V. Multi-tap is 120/208/240/277V wired 277V. Triple-tap ballast is 120/277/347V wired 347V.
- Available with CF Option only. In cold temperatures, compact fluorescent lamps produce lower illumination levels.
- Must specify voltage.
- Not available with dual CF lamps. Not available with EMI40 or CFEM.
- Down lighting position only.
- (1) 120V Lamp, 100W maximum quartz lamp. Utility power may need to cycle to allow HID lamp to cool in warm climates. Not available with CF Option. Lamp supplied by others.
- Leads out of the back of the unit for auxiliary power.
- Not available with CF lamps.
- (1) or (2) 120V Lamps. GU10 base, 50W maximum each. Lamps supplied by others.
- Not Available with 52, 64, 84 CF wattages.
- (1) or (2) 12V bi-pin lamp, socket GU5.3 base, 35W maximum. Power supplied by low voltage DC source (supplied by others). Lamps supplied by others.
- Specify 120 or 277V, (-18°C) minimum temperature. Not available with UPL10 or UPL50.
- Specify 120 or 277V, (0°C) minimum temperature. Not available with UPL10 or UPL50.
- Not available in 84CF configuration.
- Available in 52CF only.
- Order separately. Replace "XX" with color designation.

Steve's Tire and Auto - Existing Photos

5355 34th Avenue South
Minneapolis, MN

















Steiner, Lisa

From: Rita Ulrich [<mailto:rulrich@nokomiseast.org>]
Sent: Monday, June 23, 2014 3:29 PM
To: Steiner, Lisa
Cc: Johnson, Andrew
Subject: Re: Steve's Tire & Auto

Thanks, Lisa.

NENA does support the project (I think most of us have used Steve's for repairs), even though people hate to lose the gas pumps. That puts more pressure on the Super America, which is already very busy.

I usually wait a week or so before the application goes to committee, in case any issues come up that we need to be aware of, but if that's not the case, you can consider this as notice of NENA's official support.

Thank you,
Rita

Rita Ulrich, Executive Director
Nokomis East Neighborhood Association
3000 E 50th St, Minneapolis MN 55417
612-724-5652 rulrich@nokomiseast.org
www.nokomiseast.org

Casie Radford

From: Steve Johnston <sjohnston12@comcast.net>
Sent: Tuesday, May 20, 2014 4:24 PM
To: Jim Connelly; Casie Radford
Subject: Fwd: [FWD: RE: 5355 34th Ave S]

From: steve@stevesmn.com
To: "me" <sjohnston12@comcast.net>
Sent: Tuesday, May 20, 2014 4:23:13 PM
Subject: [FWD: RE: 5355 34th Ave S]

----- Original Message -----

Subject: RE: 5355 34th Ave S
From: "Staff" <nenal@nokomiseast.org>
Date: Tue, May 20, 2014 4:02 pm
To: <steve@stevesmn.com>
Cc: "Rita @ NENA" <rulrich@nokomiseast.org>

Steve,

Thank you for sending the initial elevations and site plan.

I have made, and will supply 11x17" copies of both for our Board members on Thursday night.

Doug Walter
Associate Director
Nokomis East Neighborhood Association (NENA)
3000 East 50th Street, Mpls MN 55417
612-724-5652 fax: 612-724-2770
dwalter@nokomiseast.org
www.nokomiseast.org

From: steve@stevesmn.com [<mailto:steve@stevesmn.com>]
Sent: Tuesday, May 20, 2014 2:04 PM
To: nenal@nokomiseast.org
Cc: lisa.steiner@minneapolismn.gov
Subject: 5355 34th Ave S

Good Afternoon,

My name is Steve Johnston, the owner of Steve's Tire & Auto located at 5355 34th Ave S In Minneapolis. I am contacting you today to make you aware of a building project we are planning for my business at this location. We currently sell gasoline and service vehicles at this location. We are planning to remove the gasoline component of the business and focus more on the vehicle service and repair component.

To do this we are planning on adding onto the existing building to the east and south adding to the number of service bays and increasing the size of the office and waiting area. City planner

Lisa Steiner has informed us we will need a new Conditional Use Permit and a site plan review for this work to move forward.

The applicant will be:
APPRO Development Inc.
c/o Jim Connelly
21476 Grenda Avenue
Lakeville, MN55044
Phone 952-469-2171
Email: jimc@approdevelopment.com

Appro is working on the building design and construction for me.

I have attached 2 initial site plan images for your review.

Thank you taking the time to speak with me last week. I look forward to meeting with the NENA board on Thursday May 22, 2014.

Thank you

Steve Johnston
Steve's Tire & Auto
Robeli Enterprises

Cell 612-508-7535
Fax 612-767-7399
www.stevesmn.com