

LAND USE APPLICATION SUMMARY

Property Location: 1300 West Lake St., 1300 ½ West Lake St. and 2928 Fremont Ave. South
Project Name: 1300 West Lake Street
Prepared By: Aaron Hanauer, City Planner, (612) 673-2494
Applicant: Daniel Oberpriller, CPM Development, LLC
Project Contact: Bob Loken of ESG Architects, Inc.
Request: To build a seven-story mixed use building with residential units, office space, and commercial space.

Required Applications:

Conditional Use Permit	To allow for increase in the maximum height from 4 stories, 56 feet to 7 stories, approximately 91.6 feet at tallest point in the C3A/Community Activity Center District.
Variance	To increase maximum allowable floor area from 3.8 to 4.3.
Variance	To increase the size of a commercial storefront from 8,000 square feet to 21,700 square feet in the C3A/Community Activity Center District.
Variance	To reduce the interior setback from 17 feet to 0 feet to allow the building and port cochere.
Variance	To increase the distance the first floor will be located from the front property lines in the Pedestrian Oriented Overlay District from 8 feet to 15 feet (from Fremont Avenue) and 29 feet (from Lagoon Avenue).
Variance	To reduce the off-street loading requirement from two spaces to one space.
Site Plan Review	To build a seven-story, mixed use building with 125 units, 5,300 square feet of office space, and 21,700 square feet of commercial space.

SITE DATA

Existing Zoning	C3A/Community Activity Center District PO/ Pedestrian Overlay District
Lot Area	33,490 square feet / 0.77 acres
Ward(s)	10
Neighborhood(s)	Lowry Hill East; adjacent to Calhoun Area Residents Action Group (CARAG)
Designated Future Land Use	Mixed Use
Land Use Features	Adjacent to the Uptown Activity Center Commercial Corridors: West Lake Street and Lagoon Avenue
Small Area Plan(s)	<u>Uptown Small Area Plan</u> (2008)

Date Application Deemed Complete	July 23, 2015	Date Extension Letter Sent	N/A
End of 60-Day Decision Period	September 21, 2015	End of 120-Day Decision Period	N/A

BACKGROUND

SITE DESCRIPTION AND PRESENT USE.

The project site is 33,490 square feet (134.8' x 248.5') and consists of three lots: 1300 West Lake Street, 1300 ½ West Lake Street and 2928 Fremont Avenue South. Located two blocks to the east of Hennepin Avenue, the project site has street frontage along West Lake Street, Fremont Avenue South, and Lagoon Avenue. It is a flat lot with less than two feet of variation in topography throughout the entire site. There are two utility easements on the project site.

Currently, the project site currently contains a one-story, concrete block building (approximately 23,200-square foot footprint) built in 1949 as well a 23-space surface parking area. It is the location of Cheapo Records and Booksmart. The project site currently has two access points to the surface parking area on the north end of the block: one vehicular access point on Fremont Avenue South and one access point on Lagoon Avenue. There is not a public alley between the project site and the site to the west. Bike parking posts are currently located in the right-of-way along West Lake Street and Lagoon Avenue. There is no landscaping on the project site; however there are street trees with tree grates along West Lake Street, Fremont Avenue, and Lagoon Avenue.

SURROUNDING PROPERTIES AND NEIGHBORHOOD.

The immediate surrounding properties are a combination of new apartment, mixed use, and commercial buildings. Nearby major development projects built or recently approved by the City Planning Commission (CPC) include The Walkway, a seven-story mixed use building at 1320 West Lake Street ([BZZ-5455](#)), and the Mozaic a 10-story, mixed use building with ground floor retail, above-grade parking, and office space ([BZZ-4954](#)). In 2014, the CPC approved Mozaic East; an eight-story building with 199,000 square feet of office space and ground floor commercial space ([BZZ-6767](#)).

The project site is located just outside of the Uptown Activity Center; the adjacent parcel to the west is within the activity center. The project site is located within the Hennepin Lake Pedestrian Oriented Overlay District. West Lake Street and Lagoon Avenue are commercial corridors. The project site is surrounded by commercial zoning. With the exception of the C2-zoned property that is located to the southeast (1221 West Lake Street), the adjacent properties are zoned C3A/Community Activity Center District.

The surrounding area is a dynamic area in terms of alternative transportation. It is one of the most bike, pedestrian, and transit friendly areas in the city with a walking score of 97, bike score of 96, and a transit score of 75 (source: Walk Score). There are no bike lanes along the project site. However, the Midtown Greenway Bike Trail is located a block to the north of the project site. In addition, there are three Nice Ride stations within five blocks of the project site. Furthermore, the project site is within two blocks of transit stops for at least five bus lines (#6, #12, #17, #21, and #23) including two high frequency transit lines (#6 and #21).

Vehicle traffic on West Lake Street is one-way eastbound, one-way westbound on Lagoon Avenue, and one-way northbound on Fremont Avenue. The annual average daily traffic counts are 17,800 on West Lake Street and 11,400 on Lagoon Avenue (2012). There is on-street metered parking available along all three streets.

PROJECT DESCRIPTION.

The applicants state that the purpose of the project is to redevelop an underutilized half-block site in the heart of Uptown into a vibrant, pedestrian-oriented, mixed use development. The project is a seven-story, mixed use building that would include a single-retail tenant on the first floor, approximately 6,000 square feet of office space on the second floor, and 125 residential units on floors two through seven. The bedroom counts would be 70 studios, 25 1-bedrooms, and 30 2-bedrooms; 202 total bedrooms. There would be two outdoor areas; a 3,000 square foot terrace above the second floor along Lake Street and a 730 square foot terrace on the upper floor. Off-street parking would consist of two floors of underground parking that would provide parking for 161 vehicles and 125 bikes. The proposed project would require the removal of the existing building. The demolition of the existing building would likely be able to be approved administratively as it has not been identified as a historic resource.

COMMITTEE OF THE WHOLE REVIEW

The project was reviewed at the June 18, 2015, City Planning Commission-Committee of the Whole meeting. Overall, comments were favorable. The City Planning Commission did encourage the applicant to incorporate a second entrance for the commercial establishment at the Fremont Avenue and Lake Street intersection, have bump-outs at Lake Street and Lagoon Avenue to help with pedestrian access, look to have the rear elevation be more similar to the front and side elevations, and explore a different color of cement board where there is a combination of stucco and cement board, especially on the rear elevation. With the large gap between the proposed building and The Walkway Building to the west, the applicant was encouraged to explore the possibility of including a port cochere along the Lagoon Avenue elevation similar to the one on Lake Street to help continue the building wall.

PUBLIC COMMENTS. No public comments were submitted prior to the publication of the staff report. Any correspondence received prior to the public meeting will be forwarded on to the Planning Commission for consideration.

ANALYSIS

CONDITIONAL USE PERMIT

The Department of Community Planning and Economic Development has analyzed the application to allow for an increase in the maximum height from 4 stories, 56 feet to 7 stories, approximately 91.6 feet at tallest point in the C3A/Community Activity Center District based on the following findings:

- 1. The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, comfort or general welfare.*

The applicants are proposing the construction of a seven-story, 91.6-foot tall mixed use building. The height of the building was measured to the top of the elevator penthouse since the penthouse is greater than 14 feet in height (16.75 feet). However, a majority of the building is six stories and 76.5 feet tall. The seventh floor mezzanine levels are 86.5 feet high (see site plan review section for a specific review of how height was measured).

The proposed building will not be detrimental to the public health, safety, comfort or general welfare provided the development complies with all applicable building code, site development codes, life safety ordinances as well as Public Works Department standards.

2. *The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.*

The conditional use to allow an increase in height will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district. As stated in Finding I, the proposed seven-story, 91.6-foot tall structure will be shorter than the 10-story Mozaic Building to the northwest and the eight-story Mozaic East Building that was approved by the City Planning Commission in 2014. The Walkway Apartment Building to the west, like the proposed building, is seven stories. As the applicant points out, the *Uptown Small Area Plan* supports tall, dense, mixed use development in the Core of Uptown, which is considered the area between Hennepin Avenue and Bryant Avenue, the Greenway and Lake Street.

3. *Adequate utilities, access roads, drainage, necessary facilities or other measures, have been or will be provided.*

Adequate utilities and drainage will be provided as part of the project. The applicant received initial City of Minneapolis department comments (including Public Works) at a preliminary development review meeting on July 24, 2015. The applicant will be required to continue to work closely with Public Works, Construction Code Services, Land Use, Design and Preservation staff and the various utility companies during the duration of the development should the applications be approved to meet these requirements. This would be required to ensure that all procedures are followed and that the development complies with all City and other applicable requirements. Adequate access roads are also provided. Vehicles are proposed to enter the project site on Lake Street and exit the site on Lagoon Avenue. This configuration received initial Public Works support.

4. *Adequate measures have been or will be taken to minimize traffic congestion in the public streets.*

Adequate measures have been taken to minimize traffic congestion in the public streets. The mixed use development will exceed minimum vehicle and bicycle parking requirements. In addition, all loading will occur on site in the service and loading area behind the building. As required by the Lake and Hennepin Pedestrian Oriented Overlay District, the applicant submitted a transportation development management plan (TDMP). The draft TDMP states that traffic from the proposed mixed use building is not anticipated to have a significant impact on area traffic operations. To help with traffic generated from the new development, the TDMP report recommends designating a transportation coordinator for the building, steps to promote walking and biking, and steps to minimize the impact of truck traffic. The applicant is committed to implementing recommendations in the TDMP that promotes alternative transportation by residents, employees, and customers. The final TDMP will require Public Works approval.

5. *The conditional use is consistent with the applicable policies of the comprehensive plan.*

The *Minneapolis Plan for Sustainable Growth* designates the project site for mixed use development. The project site is located along two designated commercial corridors, Lake Street and Lagoon Avenue and is immediately adjacent to the Uptown Activity Center. The conditional use request to allow an increase in height is consistent with the following land use and urban design policies of *The Minneapolis Plan for Sustainable Growth*.

Land Use Policy 1.2: Ensure appropriate transitions between uses with different size, scale, and intensity.

- I.2.1 Promote quality design in new development, as well as building orientation, scale, massing, buffering, and setbacks that are appropriate with the context of the surrounding area.

Land Use Policy I.5: Promote growth and encourage overall city vitality by directing new commercial and mixed use development to designated corridors and districts.

Land Use Policy I.8: Preserve the stability and diversity of the city's neighborhoods while allowing for increased density in order to attract and retain long-term residents and businesses.

- I.8.1 Promote a range of housing types and residential densities, with highest density development concentrated in and along appropriate land use features.

Land Use Policy I.10: Support development along Commercial Corridors that enhances the street's character, fosters pedestrian movement, expands the range of goods and services available, and improves the ability to accommodate automobile traffic.

- I.10.4 Encourage a height of at least two stories for new buildings along Commercial Corridors, in keeping with neighborhood character.
- I.10.5 Encourage the development of high-density housing on Commercial Corridors.

Land Use Policy I.13: Support high density development near transit stations in ways that encourage transit use and contribute to interesting and vibrant places.

- I.13.1 Encourage pedestrian-oriented services and retail uses as part of higher density development near transit stations.
- I.13.5 Concentrate highest densities and mixed use development adjacent to the transit station and along connecting corridors served by bus.

Urban Design Policy 10.5: Support the development of multi-family residential dwellings of appropriate form and scale.

- 10.5.2 Medium-scale, multi-family residential development is more appropriate along Commercial Corridors, Activity Centers, Transit Station Areas and Growth Centers outside of Downtown Minneapolis.

Urban Design Policy 10.6: New multi-family development or renovation should be designed in terms of traditional urban building form with pedestrian scale design features at the street level.

- 10.6.1 Design buildings to fulfill light, privacy, and view requirements for the subject building as well as for adjacent properties by building within required setbacks.

The *Uptown Small Area Plan* (2008), which provides additional guidance for the project site and surrounding area, identifies the project site as a prime area for redevelopment. The project site is within the Core of Uptown. The small area plan states that a majority of new development should be directed to the Core, which is considered the area between Hennepin Avenue and Bryant Avenue, the Greenway and Lake Street. The *Uptown Small Area Plan* was divided into character areas. The project site is located within the Urban Village Character Area. The small area plan states that there are opportunities for buildings to be up to 84 feet in height in the Urban Village Character Area on major corridors (e.g. Lake Street and Lagoon Avenue), which is the case with the project site. A majority of the project is below the 84 foot height guidelines (76.5'). Although,

the seventh floor mezzanine levels are 2.5 feet taller than the recommended height, they provide height variation in the building which enhances the building design. The elevator penthouse is the only portion of the building that will exceed 86.5 feet in height. This is a 750 square foot area that is placed near the middle of the project site. The proposed height is consistent with the applicable policies of the comprehensive plan.

6. *The conditional use shall, in all other respects, conform to the applicable regulations of the district in which it is located.*

The proposed development will conform to the applicable regulations of the district in which it is located upon the approval of the conditional use permit, variances, and site plan review.

Additional Standards to Increase Maximum Height

In addition to the conditional use permit standards, the Planning Commission shall consider, but not be limited to, the following factors when determining the maximum height of principal structures in commercial districts:

1. *Access to light and air of surrounding properties.*

The increase in height would have little effect on the access to light and air of surrounding properties. With the exception of the two-story port cochere, the building will be setback 16 feet from the western interior property line and 30 feet from The Walkway Building to the west. Given the design and placement of the proposed building and The Walkway Building, the separation of the residential units of the two buildings will be even greater than 30 feet. All other surrounding buildings will be separated an even greater distance.

2. *Shadowing of residential properties, significant public spaces, or existing solar energy systems.*

The applicant has provided shadow studies, which are attached to this report. Allowing the proposed building to have the additional height than what is allowed in the C3A Zoning District will not have a significant impact in terms of shadowing of nearby residential properties. As mentioned in the finding above, there will be at least 30 feet of separation between the proposed building and the building to the west which will help reduce the shadowing. Allowing the additional height will also not have a major shadowing impact on significant public spaces or existing solar energy systems. The project will be more than a full city block away from the nearest public space (Midtown Greenway) and three blocks away from permitted solar energy system (3021 Holmes Avenue South, BINB 2002664 and 2845 Colfax Avenue South, BIRE 3086585).

3. *The scale and character of surrounding uses.*

The proposed development will fit in with the scale and character of the surrounding uses. The applicants are proposing a seven-story, 91.6-foot tall building at the tallest point with a majority of the building six stories and 76.5 feet tall. Neighboring buildings that are equivalent or taller include the seven-story/82 foot tall Walkway Apartment Building at 1320 West Lake Street, and the 10-story/112-foot tall Mozaic building at 1320 Lagoon Avenue. In addition, the eight-story/111-foot tall Mozaic East at 2900 Fremont Avenue was approved by the CPC in 2014. The applicants are proposing a seven-story, 91.6-foot tall building at the tallest point with a majority of the building six stories and 76.5 feet tall.

4. *Preservation of views of landmark buildings, significant open spaces or water bodies*

The proposed project will not have an impact of views of landmark buildings, significant open spaces or water bodies. The project will be located two blocks to the west of the nearest local historic

landmark (Uptown Theater, 2900 Hennepin Avenue), nearly two blocks away from the nearest significant open space (Midtown Greenway) and more than eight blocks from the nearest water body (Lake Calhoun).

VARIANCE

The Department of Community Planning and Economic Development has analyzed the application for a variance to increase maximum allowable floor area from 3.8 to 4.3; based on the following findings:

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

The maximum floor area ratio (FAR) before density bonuses in the C3A District is 2.7. The project qualifies for two density bonuses for enclosed parking and a mixed use development as outlined in Section 548.130-Density Bonuses. Therefore, the allowed FAR of the project is 3.8. A variance is being requested to increase the FAR to 4.3 (140,686 square feet of floor area on a 33,490 square foot lot). Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The project site is in a unique area in Minneapolis with the concentration of designated land use features that support high density and very high density buildings. The project site is located adjacent to the Uptown Activity Center which is supportive of very high density (120-200 dwelling units per acre) when it is contextually sensitive. The proposed project would have 160 dwelling units per acre, which is comparable to The Walkway Apartment Building that was built to a density of 120 units per acre. In addition, the project site is located along the Lake Street and Lagoon Avenue Commercial Corridors which are supportive of intensive commercial uses.

Furthermore, the *Uptown Small Area Plan* reinforces the comprehensive plan's identification of the subject property and surrounding area of having the most dense and intense buildings within the area. The project site is located within the Core of Uptown. The Core of Uptown is the area recommended by the plan to concentrate density and intensity. The project will provide the density and intensity encouraged by the plan while also having a built form supported by the small area plan. This includes maximizing sunlight along Lake Street and Lagoon Avenue by stepping the building back and having a storefront that will have frontage along both Lake Street and Lagoon Avenue.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

The purpose of the floor area ratio requirement is to measure the intensity of the site being developed. The request to increase the FAR from 3.8 to 4.3 is reasonable and is in keeping with the spirit and intent of the ordinance and the comprehensive plan. Even though the building exceeds floor area ratio requirement, the applicant has taken measures to reduce the building's impact by having all of the off-street parking underground and stepping the upper stories back from the street along Lake Street, Fremont Avenue, and Lagoon Avenue. In addition, having the portions of the building with a seventh/mezzanine floor provides greater variation in the building that will be situated on half a city block. As stated in Finding 1 above, the *Minneapolis Plan for Sustainable Growth* and the *Uptown Small Area Plan* are supportive of larger buildings at the project site as it is adjacent to the Uptown Activity Center, within the zoning district that supports activity center preferred development, C3A/Community Activity Center District, along two commercial corridors, and is within the Core of Uptown.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property. Recent development in the area has been of a similar or taller height and with comparable or greater floor area than the proposed project. The Walkway Building just to the west of the project site is seven stories and 122,000 square feet and the Mozaic building to the northwest is 10-stories. In addition, the Mozaic East office building just to the north was approved to be eight stories and 199,000 square feet. The proposed variance will also not be injurious to the use or enjoyment of other property in the vicinity nor be detrimental to the health, safety, or welfare of the general public. As the applicant points out, the development will be beneficial to the public health and welfare and to neighboring properties by filling a gap in the urban fabric and replacing a one-story building and surface parking with a seven-story, mixed use building.

VARIANCE

The Department of Community Planning and Economic Development has analyzed the application increase the size of a commercial storefront from 8,000 square feet to 21,700 square feet in the C3A/Community Activity Center District; based on the following findings:

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

Per 548.360-General District Regulations for the C3A/Community Activity Center District, the maximum floor area for a retail sales and service use is 8,000 square feet when parking is not located between the principal structure and the street, and the structure in which the commercial use is located is at least two stories. The proposed retail use will be 21,700 square feet.

The applicant is seeking a variance to increase the size of a commercial storefront from 8,000 square feet to approximately 21,700 square feet. Practical difficulties exist in complying with the ordinance because of circumstances unique to the property and area. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic consideration alone. The project site is located adjacent to the Uptown Activity Center. *The Minneapolis Plan for Sustainable Growth* identifies activity centers as supporting a wide range of commercial uses (as well as office and residential uses). The Uptown Activity Center is one of the most commercially-vibrant and commercially-eclectic activity centers in Minneapolis with large and small storefronts coexisting. The *Uptown Small Area Plan* recognizes the major national retailing trends that have taken place since the 1990s of more people purchasing goods in big box retailers located in the suburbs. The small are plan is supportive of additional larger national retailers locating within the Core of Uptown alongside the smaller establishments.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

The purpose of limiting the maximum floor area of retail sales and services in the C3A/Community Activity Center District is to support neighborhood scale retail sales and services and to have a wide range of commercial uses with a busy street life and a traditional urban form and scale. The property owner proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and comprehensive plan. The proposed project will bring a

greater commercial storefront presence to Lake Street, Fremont Avenue, and Lagoon Avenue, which is a recommendation of the *Uptown Small Area Plan*, and an improvement over existing conditions. The proposed commercial storefront will also make the street life more robust over existing conditions. In addition, the project is built in a traditional urban form and scale with no surface parking, entrances on both Lake Street and Lagoon Avenue, and a large amount of glazing along all street frontage. As the applicant points out, allowing for the larger retail establishments is also in keeping with the comprehensive plan guidance for Commercial Corridors that are intended to accommodate intensive commercial uses, strengthen surrounding urban neighborhoods, and expand the range of goods and services available. As a condition of approval, the retail establishment shall have an entrance to the retail establishment on Lake Street and Lagoon Avenue as currently shown.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. There are numerous other commercial establishments in the C3A/Community Activity Center District that exceed the 8,000 square foot floor area requirement. This includes multiple establishments in Calhoun Square (BZZ-2558), Victoria Secret (approximately 10,000 square feet), Urban Outfitters (approximately 11,000 square feet), and the Cheapo Records/Booksmart establishments on the project site (approximately 17,000 square feet each). In addition, as stated in finding 2, the building will be built in a traditional urban form and scale with no surface parking, entrances on both Lake Street and Lagoon Avenue, have a large amount of glazing along all street fronts, and have storefront entrances on Lake Street and Lagoon Avenue. The proposed variance will not be detrimental to the health, safety or welfare of the general public as it will provide neighborhood-serving retail goods and services, which will benefit the use and enjoyment of other property in the vicinity, as well as the health, safety and welfare of area residents, visitors and customers.

VARIANCE

The Department of Community Planning and Economic Development has analyzed the application To reduce the interior setback from 17 feet to 0 feet to allow the building and port cochere; based on the following findings:

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

Per 548.410 (b) (2), the interior setback for the seven-story building is 17 feet (5 feet plus two feet for every story above the first. The building at its closest point will be 16 feet from the shared interior property line and the port cochere will be at located at the interior property line near the Lake Street vehicular access point. Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The project site is unique in that it is located in a dense urban environment, adjacent to a designated activity center, along two commercial corridors, and within the Core of Uptown. A continuous building wall is typically encouraged from one end of a property line to the other in these areas. Given the placement of The Walkway Building to the west of the project site and the proposed building, there will be 42 feet of separation between the buildings along Lake Street and 45 feet of separation between the buildings along Lagoon Avenue; the buildings at their closest point will be 31 feet apart. This is a greater than desired separation

between buildings within the Uptown Activity Center. The proposed placement of the building a foot closer than allowed and the port cochere at the property line will help reduce this gap between the buildings.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

The intent of the interior setback requirement is to minimize conflict among the land uses and to buffer residential uses from adjacent commercial uses to ensure access to light and air. The property owner proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinances and the comprehensive plan. The neighboring building to the west is a similar use to the proposed building; commercial on the first floor with residential units on the upper floors. The proposed building design and location is looking to balance the larger than usual interior setback yard variance requirement of 17 feet with good urban design in a commercial area. As stated in finding 1 above, the actual building will be 16 feet from the property line at its closest point. Given the location of the neighboring building there will be over 30 feet of separation between the building to the west and the proposed building, which is an adequate separation to minimize conflict among the land uses. The proposed port cochere with terrace at the interior property line along Lake Street will help reduce the wider than usual gap that will exist between two buildings in the Core of Uptown, which is a dense urban environment. By allowing the port cochere at the property line, the open area between the two buildings will be reduced from 42 feet to 13 feet, which is close to the typical distance between two buildings in this area if an alley was present.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

Granting the proposed variance will not alter the essential character of the locality or be injurious to the use and enjoyment of other property in the vicinity. Historically, buildings within the Core of Uptown were built out to the property lines which typically created a continuous building wall. Granting of the proposed variance will allow a better street wall presence on the project site by allowing the building and port cochere to be closer to the interior property line. The proposed placement of the port cochere and terrace at the property line will help mitigate the larger than usual distance between the two buildings on this city block. In addition, granting of the variance will also not be detrimental to the health, safety, and welfare of the general public or those utilizing the property or nearby properties. The neighboring building has similar uses to the proposed building; residential and commercial. The port cochere portion of the building will be more than 13 feet from The Walkway building to the west and there will be more than 30 feet of separation between the actual buildings.

VARIANCE

The Department of Community Planning and Economic Development has analyzed the application increase the increase the distance the first floor will be located from the front property lines in the Pedestrian Oriented Overlay District from 8 feet to 15 feet and 29 feet; based on the following findings:

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

Per Section 551.110, the placement of buildings shall reinforce the street wall, maximize natural surveillance and visibility, and facilitate pedestrian access and circulation. The first floor of buildings shall be located not more than eight feet from the front lot line. In the case of a corner lot, the building wall abutting each street shall be located not more than eight feet from the lot line, except where a greater yard is required by this zoning ordinance. A small portion of the building at the intersection of Lagoon Avenue and Fremont Avenue will not be in compliance with this requirement as the entrance to the retail establishment will be recessed 15 feet from the Fremont Avenue property line and 29 feet from the Lagoon Avenue property line. Practical difficulties exist in complying with the ordinance because of circumstances unique to the property that were not created by persons presently having an interest in the property and are not based on economic consideration alone. With a walk score of 97 out of 100, the project site is located in one of the most pedestrian-friendly and pedestrian-active areas in Minneapolis. The 2014 Public Works pedestrian count confirms that the pedestrian activity within the area is one of the busiest outside of Downtown and the University of Minnesota (Source: [Public Works 2014 Pedestrian Traffic Map](#)). The proposed recessed entrance at the intersection of Fremont Avenue and Lagoon Avenue for a small portion of the building wall (11 percent of the building wall along Fremont Avenue and 13 percent of the building wall along Lagoon Avenue) will help provide a covered area from weather elements for the pedestrian rich environment area.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

The intent of the building placement requirement of having buildings built within eight feet of the property line is to enhance the pedestrian character of commercial areas, promote street life, and eliminate having off-street parking in front of a building. The property owner proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and comprehensive plan. The applicant is proposing a building that will be built out to the property lines with the exception of the entrance area at Lagoon/Fremont; the other three entrances that face the public street will be built within eight of the property line. The building intersection at Lagoon Avenue and Fremont Avenue will be setback a greater than allowed distance from the property line for only a small portion of the overall building wall; 11 percent of the building wall along Fremont Avenue and 13 percent of the building wall along Lagoon Avenue. This is done in order to provide ample room for pedestrians to enter the building in a busy pedestrian area. In addition, the design of the building with the upper floors built at the property line or within five feet of the property line creates an enclosed alcove area that provides protection for pedestrians from weather elements. The applicant will be complying with the building placement requirement in 551.100 in terms of providing a landscaping amenity between the building and the lot line as they are proposing to plant a raised plant bed with perennial plantings, such as Feather Reed Grass, Purple Flame Grass, and Purple Cone Flowers.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

Granting the proposed variance will not alter the essential character of the locality or be injurious to the use and enjoyment of other property in the vicinity. The applicant is proposing that only one of four of the building entrances facing the street be setback further than eight feet. In addition, only 11 percent of the building wall along Fremont Avenue and 13 percent of the overall building wall along Lagoon Avenue will be setback further than what is allowed in the pedestrian overlay district. The design of the building with the upper floors extending near the property lines along the street will give the building a strong street presence at the Lagoon and Fremont intersection. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public

or of those utilizing the property or nearby properties. The proposed building setback will help provide additional real estate for pedestrians in the area.

VARIANCE

The Department of Community Planning and Economic Development has analyzed the application to reduce the off-street loading requirement from two spaces to one space; based on the following findings:

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

Per Section 541.560 and Table 541-8, the proposed 125-unit, 5,883 square foot office, and 21,700 square foot retail requires two small loading spaces that are 10 feet wide by 25 feet deep exclusive of aisle and maneuvering space. The applicant is proposing to have one loading space that will exceed the 10' x 25' requirement. Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The project site does not have an alley, which increases the amount of lot area devoted to accessing the project site. In addition, the project site is located within a dense urban activity center where it is important to have a strong building wall presence along the street. By requiring a second loading space it will likely require reducing the building street wall along Lake Street and/or Lagoon Avenue, which would be an adverse impact to the building to the Core of Uptown.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

The property owner proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and comprehensive plan. The intent of the off-street loading requirement is to reduce traffic congestion in the public streets. The project includes one small loading space. Even though the loading space that meets City of Minneapolis zoning code requirements will be dedicated to the retail establishment, the applicant has designed a separate off-street loading area for the residential portion of the building at the back of the building. This area does not meet the zoning code dimension requirements for a loading space; however, it is located near the building elevators that will provide easy on-site access to the residential units. In addition, the applicant states that property management will coordinate shared use of the loading area to prevent congestion.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

Granting the proposed variance to reduce the off-street loading requirement from two spaces to one space will not alter the essential character of the locality or be injurious to the use and enjoyment of other property in the vicinity. Granting of the variance will also not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties. The applicant has designed a loading area for the building that is anticipated to meet all building use needs. This includes a small off-street loading space for the retail establishment and an off-street loading and unloading area for the residential portion of the building. Even though the loading/unloading area for the residential portion of the building does not meet City of Minneapolis

zoning code size requirements, it will likely be able to accommodate most of the loading and unloading needs of the residential units.

SITE PLAN REVIEW

The Department of Community Planning and Economic Development has analyzed the application based on the required findings and applicable standards in the site plan review chapter:

I. Conformance to all applicable standards of Chapter 530, Site Plan Review.

Building Placement and Design – *Requires alternative compliance*

- The placement of building at or near the property lines along Lake Street, Fremont Avenue and Lagoon Avenue will reinforce the street building wall in the immediate area. It should be noted that there will be approximately 30 feet of separation between the proposed building and The Walkway Building to the west, which is a greater distance than typical in this area; the building will be located 29 feet from the western property line along Lake Street and 16 feet from the western property line along Lagoon Avenue. CPED encouraged the applicant to look at a shared driveway with the neighboring property to reduce this distance between buildings; however, the applicant stated that this was not possible for this project due to financing constraints of the neighboring building. To improve the street wall condition along Lake Street the applicant has installed a covered driveway entrance along Lake Street that spans from the main building to the western property line.
- The project will maximize natural surveillance and visibility as the project exceeds the minimum window requirements outlined in Chapter 530-Site Plan Review and the Pedestrian Oriented Overlay District glazing requirements outlined in Chapter 551 (see Table 1 and 2). It will also facilitate pedestrian access and circulation as all building entrances lead directly to the public sidewalk.
- Per Chapter 530-Site Plan Review and the Pedestrian Oriented Overlay District building placement requirements outlined in Chapter 551, the first floor of the proposed building is required to be not more than eight feet from the front lot line for all portions of the building. Since the project site is a corner lot, the building wall abutting each street shall be located not more than eight feet from the lot line. The building complies with this requirement with the exception of two areas: the portion of the building at the Fremont/Lagoon intersection which is setback 29 feet from Lagoon and 15 feet from Fremont and the portion of the building at the Fremont/Lake intersection which is setback 46 feet from Fremont Avenue and 7.6 feet from Lake Street. Alternative compliance and a variance are required for the building placement at the Fremont/Lagoon intersection. However, alternative compliance and a variance are not required for the placement of the building at Fremont/Lake given it is only a small portion of the building wall (7.6 linear feet).
- Most of the building is located up to the property lines along Lake Street, Fremont Avenue, and Lagoon Avenue. For the two areas where the building is setback from the street (the Lake/Fremont intersection and the Lagoon/Fremont intersection), the project site will include a raised plant bed with perennial plantings, such as Feather Reed Grass, Purple Flame Grass, and Purple Cone Flowers. It should also be noted that the applicant is proposing other landscaping in the public right-of-way.
- The building is oriented so that at least one principal entrance faces the public street. The commercial use will have two main entrances: one at the intersection of Lagoon/Fremont that will face Lagoon Avenue and one at the intersection of Lake/Fremont that will face Lake Street. The building's residential and office uses will have their primary entrances facing Lake Street.
- The building walls provide architectural detail to create visual interest. This includes breaking up the larger building into separate modules, having projecting volume around individual balconies on the Fremont Avenue elevation, and larger building projections at the two main intersections: Lake/Fremont and Lagoon/Fremont.

- The building contains windows as required by Chapter 530-Site Plan Review and the Pedestrian Oriented Overlay District glazing requirements outlined in Chapter 551 (see Table 1 and 2 below). The windows create visual interest and will increase the security of outdoor spaces by maximizing visibility. The amount of eyes on the street will increase significantly compared to existing conditions.
- The building is a larger building as it will span a half city block. The building includes recesses and projections on all elevations. All elevations include projecting balconies. The building is also broken into different modules that help break up the massing. The upper floors of the Fremont and Lagoon Avenue elevations are recessed approximately five feet from the building wall on the first floor. The Lake Street elevation is also broken up with the building stepping down to two stories along the western half of the building.
- The building will not have a blank, uninterrupted wall that does not include windows, entries, recesses or projections, or other architectural elements for more than 25 feet. As a condition of approval, the applicant shall install two decorative metal screens on the first floor of the western elevation where there is mechanical equipment proposed on the northern end of the site in order to maintain compliance with the blank wall requirement outlined in Section 530.120-Building Design.
- The exterior building materials are considered durable as outlined in Chapter 530-Site Plan Review and the *City of Minneapolis Guide to Exterior Building Walls and Materials*. The eight materials proposed to be used are brick, stone, wood, colored concrete masonry unit (CMU), burnished face CMU, metal panel, stucco, fiber cement board, and glazing. To be in compliance with the *City of Minneapolis Guide to Exterior Building Walls and Materials*, CPED is recommending that the applicant use a standing seam metal panel (or metal panel of efficient thickness) for exterior cladding in order to discourage an oil-canning effect.
- The exterior building walls that face a public street are in compliance with the percentage of durable materials allowed on an elevation (see Table 3). However, the elevations that face the internal courtyard exceed the percentage of fiber cement allowed. Alternative compliance is required.
- As stated in *The City of Minneapolis Guide to Exterior Building Walls and Materials*, buildings should not include more than three exterior materials on each elevation. This excludes windows, doors, foundation, and accent materials. The Lagoon Avenue elevation and the courtyard elevations will have three materials. However, the south elevation (Lake Street) would have four exterior materials, the eastern elevation (Fremont Avenue) would have four materials, and the western elevation (alley elevation) would have five materials (see Table 3). Alternative compliance is required. To help reduce the visibility of the mechanical equipment, the heating, ventilation and air conditioning screens on the outside walls shall be matched to paint the surrounding wall finish as is currently proposed.
- The exterior materials and appearance of the rear (east elevation) is similar and compatible with the street-facing elevations. It is recognized that there is more fiber cement board used on the rear elevation than the other elevations, however, the building continues the building materials from the Lake Street and Lagoon Avenue elevations onto the alley elevations and has projecting balconies on the rear elevation like those used on the elevations facing the street.
- All on-site accessory parking will be below grade.
- The project includes multiple entrances. The principal entrance to the residential portion of the building and the office space are located on Lake Street. Both of these entrances are clearly defined and emphasized through the use of a large amount of clear glazing and a second floor overhang that extends further towards Lake Street than any other portions of this elevation. There are two main pedestrian commercial entrances; one at the Lagoon/Fermont intersection and the other at the Lake/Fremont intersection. Both entrances have a large amount of glazing surrounding the entrance and are designed to accommodate signage that will further demarcate the entrance

locations. It also important to note that the building entrances are located at busy pedestrian intersections.

- The project is subject to two window requirements. Section 530.120-Building Design outlines the requirements for the windows on all floors for residential and nonresidential uses that face a public street, public sidewalk, public pathway, or on-site parking lot. Section 551.120-Building Façade outlines the window requirements for the first-floor façade of any nonresidential use that faces a public street or sidewalk. The project will be in compliance with the window requirements of both zoning code sections (see Table 1 and 2). To be in compliance with Section 530.120-Building Design and Section 551.120-Building Façade, all windows or doors that are intended to meet the window requirement shall be lightly tinted glass that allow views into and out of the building at eye level and have a visible light transmittance ratio of 0.6 or higher.
- CPED requested a shelving plan for the retail establishment to ensure the retail establishment will be in compliance with the requirement for views into and out of the building at eye level and the ground floor active function requirement of the site plan review chapter. However, the applicant stated that the shelving plan has not been finalized. As a condition of approval, the first floor shall allow views into and out of the building at eye level for at least 40 percent of the window area as defined by Section 551.120-Building Design. To be in compliance with Section 551.120, shelving, mechanical equipment or other similar fixtures shall not block views into and out of the building in the area between four and seven feet above the adjacent grade. In addition, the first floor of the building shall be designed to accommodate active functions by ensuring that storage, or mechanical equipment rooms are limited to no more than 30 percent of the linear building frontage along each wall facing a public street, public sidewalk, or public pathway.
- The building’s flat roof is similar to the surrounding buildings.
- The parking garage is located below ground.

Table 1. Chapter 530-Site Plan Review: Percentage of Windows Required for Elevations Facing a Public Street, Sidewalk, Pathway, or On-Site Parking

	Code Requirement		Proposed	
Residential Uses				
1st Floor (Lake Street)	20% minimum	30 sq. ft.	20%	30 sq. ft.
2nd Floor (Lake Street)	10% minimum	50 sq. ft.	36%	176 sq. ft.
3 rd -6 th Floors (Lake Street)	10% minimum	127 sq. ft.	20%	248 sq. ft.
7 th Floor (Lake Street)	10% minimum	452 sq. ft.	34%	452 sq. ft.
2 nd -6 th Floors (Fremont Ave)	10% minimum	267 sq. ft.	11%	301 sq. ft.
7 th Floor (Fremont Ave)	10% minimum	83 sq. ft.	36%	301 sq. ft.
2 nd -6 th Floors (Lagoon Ave)	10% minimum	127 sq. ft.	22%	273 sq. ft.
7 th Floor (Lagoon Ave)	10% minimum	85 sq. ft.	24%	201 sq. ft.
Nonresidential Uses-				
Office-1st Floor (Lake Street)	30% minimum	62 sq. ft.	66%	137 sq. ft.
Retail-1 st Floor (Lake Street)	30% minimum	170sq. ft.	49%	281 sq. ft.
Retail-1 st Floor (Fremont Ave)	30% minimum	595 sq. ft.	72%	1,428 sq. ft.
Retai-1 st Floor (Lagoon Ave)	30% minimum	283 sq. ft.	64%	608 sq. ft.
Office-2 nd Floor (Lake Street)	10% minimum	77 sq. ft.	57%	444 sq. ft.

Table 2. Chapter 55I-Pedestrian Oriented Overlay District: Percentage of Clear Glazing in Windows or Doors on 1st Floor Facing a Public Street or Sidewalk

	Code Requirement		Proposed	
Nonresidential Uses				
Office-1st Floor (Lake Street)	40% minimum	83 sq. ft.	80%	167 sq. ft.
Retail-1 st Floor (Lake Street)	40% minimum	227 sq. ft.	62%	354 sq. ft.
Retail-1 st Floor (Fremont Ave)	40% minimum	794 sq. ft.	72%	1,428 sq. ft.
Retail-1 st Floor (Lagoon Ave)	40% minimum	378 sq. ft.	64%	608 sq. ft.

Table 3. Percentage of Exterior Materials per Elevation

Material	Maximum allowed	North (Lagoon)	South (Lake)	East (Fremont)	West (alley)	North courtyard	South courtyard
Number of materials	3	3	4	4	5	3	3
Brick (face)	100%	14%	-	6%	8%	-	-
Stone	100%	-	7%	6%	-	-	-
Glazing	100%	-	-	-	-	-	-
Metal panel	75%	32%	-	15%	10%	13%	29%
Stucco	75%	16%	17%	32%	9%	-	-
Wood	75%	-	12%	-	-	-	-
Burnished CMU	50%	-	-	-	8%	7%	6%
Fiber cement board	30%	-	18%	-	30%	61%	48%

Access and Circulation – Meets requirements

- Building entrances along Lake, Lagoon, and Fremont will either lead directly to the public sidewalks or have a walkway that is at least four feet that connects them to the public sidewalks. In addition, entrances along the alley will have walkways that connect to the public sidewalks (see Sheet L1.1). It should also be noted that the applicant is proposing curb bulbouts at the Lake/Fremont Avenue and Lagoon/Fremont intersections. If approved by Public Works, this will help with the pedestrian experience in Uptown by reducing the distance pedestrians will have to travel from neighboring intersections.
- There are no existing or planned transit shelters on the project site.
- Vehicular access and circulation is designed to minimize conflicts with pedestrian traffic and with surrounding residential uses. Pedestrian traffic will be separated from vehicle traffic at all access/exit points of the building. The project site does not have an alley. The project will have one vehicular access point and vehicular exiting point. Currently there is an access point along Fremont Avenue near the Fremont/Lagoon intersection and another access point at Lagoon Avenue midblock. The project would change this to have an access point midblock at Lake Street and an exit point midblock at Lagoon Avenue. This is the same location and traffic-flow configuration as the neighboring building to the west. In order to reduce the length of a wide, unobstructed access points with the two buildings on this block face, there will be raised curbs along the interior property line to separate the individual driveway areas of the two buildings.

- The project includes onsite loading that does not conflict with pedestrian traffic. Access for service vehicles will not be located near residence and office residence districts.
- There will be a slight reduction in impervious surface with the project. The current site does not have any pervious surfaces. The proposed project will reduce the impervious surface from 33,490 square feet to 33,368 square feet (122 square foot difference).

Landscaping and Screening – Requires alternative compliance

- Overall composition and location of landscaped areas will complement the scale of the development and its surroundings. The main area of the project’s landscaping is within the public realm, which is consistent with the adjacent Walkway Building to the west and other newer development within the Core of Uptown. The project will have contiguous planting areas along Lake, Lagoon, and Fremont Avenue.
- The zoning code requires that at least 20 percent of the site not occupied by buildings be landscaped and that one canopy tree be planted for each 500 square feet of required green space and at least one shrub be planted for each 100 square feet of required green space. Since the below-grade parking garage would occupy the entire site no on-site landscaping, trees, or shrubs are required (see Table 4). Although no on-site landscaping is required, the applicant is proposing to plant some perennials and grasses on-site and a variety of perennials, grasses, shrubs and trees within the public right-of-way. The existing 11 street trees would be replaced with 11 new street trees.
- There will be no parking and loading fronting along a public street. The only portion of the project that is required to provide a landscaped yard and screening is the loading area along the back of the building since it faces a permitted residential use to the west. To be in compliance with Section 530.170, this area shall have a landscaped yard that is seven feet in width and have screening that is six feet in height. The applicant is not proposing landscaping at this area. Alternative compliance is required.
- There will be no vehicle surface parking as part of the project.
- The portions of the site that are not covered by building, parking and loading facilities, driveways and sidewalks will be landscaped areas. There is a 50 square foot area near the Fremont/Lagoon intersection and a 74-square foot area landscaped near the Lake/Fremont intersection.
- As a condition of approval, the proposed landscaping onsite and within the public right-of-way shall comply with Section 530.200-Plant Material Standards and 530.210-Installation and Maintenance of Materials.

Table 4. Landscaping and Screening Requirements

	Code Requirement	Proposed
Lot Area	--	33,490 sq. ft.
Building footprint (including underground parking garage)	--	33,490 sq. ft.
Remaining Lot Area	--	0 sq. ft.
Landscaping Required	0 sq. ft.	0 sq. ft.
Canopy Trees (1: 500 sq. ft.)	0 trees	0 trees
Shrubs (1: 100 sq. ft.)	0 shrubs	0 shrubs

Additional Standards – Meets requirements

- The perimeter of the driveway is defined by a six inch by six inch continuous concrete curb or greater.

- The site plan will not have an impact on the blocking of views of important elements of the city including parks, greenways, significant buildings and water bodies. The project site is located at least 350 feet from the Midtown Greenway, parks, significant buildings and water bodies.
- The building is located and arranged to minimize shadowing on public spaces and adjacent properties. As previously stated, the nearest public space (Midtown Greenway) is more than 350 feet away. Given the placement of the building and the u-shaped design the habitable building area will be more than 16 feet from the shared property line and 30 feet from The Walkway Building to the west. This building placement will help reduce the shadowing on the nearest buildings.
- The building will not have a major impact on the generation of wind currents at ground level.
- The site plan includes crime prevention design elements as specified in Section 530.260. This includes natural surveillance and visibility with a greater amount of windows than required for the building that helps people observe adjacent spaces and public sidewalks, territorial reinforcement and space delineation with landscaping near the commercial entrances, and natural and well defined access points that incorporate architectural elements to highlight where the entrances are as well as good commercial storefront location near the street intersections.
- The project site does not include a locally designated historic resource or structure that has been determined to be eligible to be locally designated.

2. Conformance with all applicable regulations of the zoning ordinance.

The proposed residential, office, and commercial uses are *permitted uses* in the C3A/ Community Activity Center District.

Off-street Parking and Loading – Requires variance(s)

- A total of 161 off-street parking spaces will be provided on two floors of underground parking. The project with residential, office, and retail uses requires a total of 91 off-street parking spaces (see Table 5).
- As required by Table 541, each dwelling unit is required to provide one parking space (125 spaces). However, the project is able to take advantage of the transit incentive that reduces the off-street parking requirement by 50 percent for a multifamily dwelling building that is within a quarter mile of a bus stop with midday service headways of 15 minutes or less; a reduction of 63 spaces. A qualifying bus stop (#42036) is located on the northeast side of the Lagoon/Fremont Avenue intersection; 100 feet away from the project site. Therefore, the residential portion of the building has a minimum off-street parking requirement of 62 spaces. The applicant is proposing that the residential portion of the project have 128 parking spaces.
- As required by Table 541, an office use is required to provide one parking space per 500 square feet of gross floor area in excess of 4,000 square feet. The office use will have 5,883 square feet of gross floor area. Therefore, this will require three spaces. However, per Section 541.170-Specific Off-Street Parking Requirements, all uses over 1,000 square feet, other than those specified under the heading "Residential Uses" shall provide a minimum of four parking spaces. Per 551.140-Accessory Parking, the minimum off-street parking requirement for nonresidential uses shall be 75 percent of the minimum requirement specified in Chapter 541-Off-Street Parking and Loading. Therefore, the minimum off-street parking requirement for the office use is three spaces.
- As required by Table 541, a general retail sales and service use is required to provide one space per 500 square feet of gross floor area in excess of 4,000 square feet. The general retail sales and service use will have 21,700 square feet of gross floor area. Therefore, this use will require 35 off-street parking spaces. Like the office use, the retail use qualifies for the 25 percent reduction outlined in Section 551.140-Accessory Parking. Therefore, the minimum off-street parking

requirement for the retail use is 26. Combined, the nonresidential uses will require 29 off-street parking spaces. The applicant is proposing 33 spaces.

- Per Table 541-1, there is not a maximum off-street parking requirement for a residential use. However, Section 541.450-Maximum Number of Vehicles for Dwellings does limit the number of surface vehicle parking spaces to two vehicles per dwelling unit. The project is not proposing any surface parking and will be in compliance with this requirement. Per Table 541-1, the maximum allowed parking for an office and retail establishment is one space per 200 square feet of gross floor area. Therefore, the maximum off-street parking allowance for the 5,883 square feet office space is 29 spaces and the maximum off-street parking allowance for the 21,700 square foot retail establishment is 108 spaces. Combined, the maximum off-street parking for the nonresidential uses is 137 spaces. The project is proposing 33 off-street parking spaces for the nonresidential uses and therefore will not be exceeding the maximum allowance (see Table 5).
- As required by Section 541.330-Size, 75 percent of the required parking shall comply with the minimum dimensions for standard spaces. Therefore, 47 parking spaces for the residential use and 22 spaces for the nonresidential uses shall be standard parking spaces. As proposed, the project will have 115 standard sized parking spaces for the residential use and 25 standard sized parking spaces. As a condition of approval, all parking areas shall be marked to be in compliance with Section 541.320-Marking of Spaces and the compact parking stalls shall be labeled as such as required by Section 541.330-Size.
- As required by Section 541.330-Size, the two-way drive aisles in the underground parking area will be at least 22 feet in width.
- The driveways and curb cuts are in compliance with 541.240 as they are at least 12 feet in width and do not exceed 25 feet in length. At the property lines, the driveways will be 12 feet in length. To comply with 541.280-Access to Parking Spaces, each off-street parking space will open directly to an aisle or driveway of such width and design as to provide safe and efficient means of vehicular access to such parking spaces. To comply with 541.300-Surfacing, all driveways leading to parking areas, and all other areas upon which motor vehicles may be located will be surfaced with a dustless all-weather hard surface material capable of carrying a wheel load of 4,000 pounds.
- To be in compliance with Section 541.370 – General Maintenance snow falling in the alley and maneuvering area will be removed from the site.
- As required by Section 541.180-Bicycle Parking, a multiple-family dwelling with five or more units is required to provide one bike parking space for every two dwelling units; and 90 percent of the required bicycle parking shall meet the standards for long-term bicycle parking. Required long-term bicycle parking spaces shall be located in enclosed and secured or supervised areas providing protection from theft, vandalism and weather and shall be accessible to intended users. Required long-term bicycle parking for residential uses shall not be located within dwelling units or within deck or patio areas accessory to dwelling units. Therefore, the 125 residential unit portion of the building is required to provide 63 bike parking spaces and to have 56 of those spaces in an enclosed area as outlined in Section 541.180. As proposed, the project will have 125 enclosed bike parking spaces for the residential portion of the building (see Table 6).
- As required by Section 541.180-Bicycle Parking, an office use is required to provide three bike parking spaces or one space per 15,000 sq. ft. of gross floor area, whichever is greater. In addition, not less than 50 percent of the required bicycle parking shall meet the standards for long-term bicycle parking. Required long-term bicycle parking spaces shall be located in enclosed and secured or supervised areas. Therefore, the 5,883 square foot office space is required to provide three bike parking spaces and to have at least two of the spaces meet the long term bike parking requirement. The project is proposing to have at least three bike parking stalls for the office use and that all will meet the long-term bicycle parking standards (see Table 6). As a condition of approval, the bike parking dedicated to the office uses shall be labeled on plans issued for a final building permit to show compliance with Section 541.180.

- As required by Section 541.180-Bicycle Parking, a retail establishment is required to provide three bike parking spaces or one space per 5,000 sq. ft. of gross floor area, whichever is greater. In addition, not less than 50 percent of the required bicycle parking shall meet the standards for short-term bicycle parking. Required short-term bicycle parking spaces shall be located in a convenient and visible area within 50 feet of a principal entrance and shall permit the locking of the bicycle frame and one wheel to the rack and shall support a bicycle in a stable position without damage to the wheels, frame or components. With the permission of the city engineer, required bicycle parking may be located in the public right-of-way. Therefore, the 21,700 square retail establishment is required to provide four bike parking spaces and have at least two of the spaces meet the short term bike parking requirement. The project is proposing to have 18 bike parking stalls for the retail establishment and that all will meet the short-term bicycle parking standards (see Table 6).
- Per Section 541.560 and Table 541-8, the proposed 125-unit, 5,883 square foot office, and 21,700 square foot retail requires two small loading spaces that are 10 feet wide by 25 feet deep exclusive of aisle and maneuvering space. The applicants are proposing one small loading space. A variance is required (see Table 6).

Table 5. Vehicle Parking Requirements Per Use (Chapter 541)

	Minimum Vehicle Parking	Applicable Reductions	Total Minimum Requirement	Maximum Parking Allowed	Proposed
Residential	125	63	62	N/A	128
Office	4	1	3	29	3
Commercial/Retail	35	9	26	108	30
Total	164	73	92	137*	161

* There is not a maximum number of off-street parking spaces for residential uses.

Table 6. Bicycle Parking and Loading Requirements (Chapter 541)

	Minimum Bicycle Parking	Minimum Short-Term	Minimum Long-Term	Proposed	Loading Requirement	Proposed
Residential	63	--	56	125	1	0
Office	3	--	2	3	0	0
Retail	4	2	--	18	1	1
Total	52	2	58	146	2	1

Building Bulk and Height – Requires conditional use permit

- The proposed project requires a conditional use permit for the proposed height. The maximum building height allowed in the C3A Zoning District is four stories or 56 feet, whichever is less. The applicants are proposing a seven-story building that is 91.6 feet tall measured to the top of the elevator penthouse. The natural grade 10 feet from the center of the building (along Fremont Avenue) is 879.8 feet above sea level. The proposed first floor elevation is 881.25 feet (1.45 feet above natural grade). As outlined in 520.160-definitions, building height is measured to the top of the elevator penthouse for a project when it is greater than 14 feet in height; as proposed it is 16.75 feet tall. The height of the building from the top of the first floor slab to the top of the

elevator penthouse is 90.1 feet (971.35 feet above sea level). Therefore, the building is 91.6 feet tall as defined by the Minneapolis zoning code (see Table 7). It should be noted that the building floor heights will be in compliance with the definition of a story in order to not be considered additional stories. Per 520.160-definitions, the first floor shall not exceed 20 feet in height and the upper floors 14 feet in height in order to be considered a single floor. The project is in compliance with these ceiling height requirements as the first floor will be 20 feet and all upper floors will be 10.75 feet tall.

- The proposed project requires a variance for the proposed floor area ratio. The base maximum floor area ratio in the C3A Zoning District is 2.7. However, the project qualifies for two 20-percent density bonuses as outlined in Section 548.130. The multiple-family dwelling building qualifies for the bonus for enclosed parking by having all required parking provided entirely below ground. The project also qualifies for the bonus in having a mixed commercial-residential building by having residential uses above a ground floor and more than 50 percent of the ground floor devoted to commercial uses. The project is proposing to have 80 percent of the ground floor devoted to commercial uses (21,669/27,132). With the bonuses, the allowed floor area ratio for the project is 3.78. The applicant is proposing to have a floor area ratio of 4.3: 143,173 square feet of floor area on a lot that is 33,490 square feet (see Table 7).

Table 7. Building Bulk and Height Requirements

	Code Requirement	Proposed
Lot Area	--	33,490 sq. ft. / 0.77 acres
Gross Floor Area (GFA)	--	143,173 sq. ft.
Minimum Floor Area Ratio (GFA/Lot Area)	N/A	4.3
Maximum Floor Area Ratio (GFA/Lot Area)	3.8	4.3
Maximum Building Height	4 stories or 56 feet, whichever is less	7 stories or 91.6 ft.

Lot Requirements – Not applicable

- Per Section 548.400-Lot Dimension Requirements, the minimum lot area for a multiple-family dwelling is 5,000 square feet and the minimum lot width requirement is 40 feet. The proposed project will be in compliance with these requirements (see Table 8). There is not a maximum impervious surface area requirement, maximum lot coverage requirement, or density requirement for the C3A/Community Activity Center District. In addition, there is not a minimum lot area or lot width requirement for office or retail establishments in the C3A Zoning District outlined in Table 548-2.

Table 8. Lot Requirements Summary

	Code Requirement	Proposed
Density (DU/acre)	--	163 DU/acre
Minimum Lot Area-Residential	5,000	33,490
Minimum Lot Width-Residential	40	134 ft.

Yard Requirements – Requires variance(s)

- Per Section 548.140-Yard Requirements, the project site has an established front yard requirement along Lake Street and Lagoon Avenue because the adjacent use to the west has a residential use. However, since the neighboring building with residential units is located at the property lines along Lake Street and Lagoon Avenue, the front yard setback along both of these streets is 0 feet. Per 548.410 (b) (2)-Yard Requirements, the interior setback for the seven-story building is 17 feet (5 feet plus two feet for every story above the first). The building at its closest point will be 16 feet from the shared interior property line and the port cochere will be at located at the interior property line near the Lake Street vehicular access point (see Table 9). Therefore, the project requires a variance to reduce the interior side yard setback from 17 feet to 0 feet.
- Portions of the 6th and 7th floor mezzanine along Lagoon Avenue extend over the property line. The building extension in the right of way, the landscaping planting in the boulevard, and the bike parking in the boulevard will require an encroachment permit.

Table 9. Minimum Yard Requirements

	Zoning District	Overriding Regulations	Total Requirement	Proposed
Fremont Avenue (Front)	N/A	N/A	0 ft.	0 ft.
Lake Street (Front)	N/A	0	0 ft.	0 ft.
Lagoon Avenue (Front)	N/A	0	0 ft.	0 ft.
West (Interior)	N/A	17	17 ft.	0 ft.

Signs – Not applicable

- The applicant did not submit a sign plan with their applications. All signs are subject to the requirements outlined in Chapter 543-On-Premise Signs and all proposed sign require permits.

Refuse Screening – Meets requirements

- The project will be in compliance with Section 535.80-Screening of refuse and recycling storage containers as the refuse for the residential, office, and retail use will be stored within the building.

Screening of Mechanical Equipment – Meets requirements

- For the residential units, the applicant is proposing to have heating, ventilation and air conditioning (HVAC) equipment on the building wall in the form of mechanical louvers. The mechanical equipment for the second floor office space and retail space (distributed air handling and condensing units) will be located within their respective spaces and/or the parking garage. The applicant is proposing to screen a backup generator and mechanical equipment located within the first floor of western building wall with decorative metal screens. As a condition of approval, the electric transformer located next to the western building wall and any other mechanical equipment on roofs or other locations shall be screened to be in compliance with Section 535.70-Screening of Mechanical Equipment.

Lighting – Not applicable

- A lighting plan was not submitted with the application. As conditioned, a lighting plan shall be submitted prior to building permit issuance that shows the project is in compliance with the lighting requirements outlined in section 535.590-Lighting.

Commercial Zoning District Requirements – *Requires variance(s)*

- Per Section 548.180-Enclosed Building Requirement, the commercial retail establishment on the first floor will be subject to the Enclosed Building Standards for outdoor sales and display.
- Per Section 548.360-General District Regulations for the C3A/Community Activity Center District, the maximum floor area for a retail sales and service use is 8,000 square feet when parking is not located between the principal structure and the street, and the structure in which the commercial use is located is at least two stories. The proposed retail use will be 21,700 square feet. A variance is required to allow this additional floor area.
- Per Section 548.360 -General District Regulations, fast food restaurants are prohibited in new storefront buildings.

Other General Development Standards – *Meets requirements*

- The project will be in compliance with Section 535.90-General Standards for Residential Uses as all units will exceed the minimum 350 square foot gross floor area for an efficiency unit and 500 square foot floor area for a dwelling unit.

Specific Development Standards – *Not applicable*

- Per Table 548-1- Principle Uses in Commercial Districts, multiple-family dwellings, office, and general retail uses do not have specific development standards.

PO Overlay District Standards – *Requires variance(s)*

- The project site is within the Lake and Hennepin Pedestrian Oriented Overlay District and subject to the general and specific pedestrian oriented overlay district requirements outlined in Chapter 551. Per 551.100, fast food restaurants are prohibited in this new storefront building.
- Per 551.120-Building Facade , at least 40 percent of the first floor façade of any nonresidential use that faces a public street or sidewalk shall be windows or doors of clear or lightly tinted glass that allow views into and out of the building at eye level. Windows shall be distributed in a more or less even manner. Minimum window area shall be measured between the height of two (2) feet and ten (10) feet above the finished level of the first floor. The project will be in compliance with this requirement along Lake Street, Fremont Avenue, and Lagoon Avenue (see Table 2). To be in compliance with Section 551.120-Building Facade, no backlit awnings and canopies are allowed.
- Per 551.150-, a travel demand management plan was required to be submitted. The applicant submitted a draft TDMP on July 20, 2015 and is committed to implementing recommendations in the TDMP that promotes alternative transportation by residents, employees, and customers. Implementing steps that promote alternative transportation will help mitigate the negative impact on air quality, parking and roadway infrastructure.

3. Conformance with the applicable policies of *The Minneapolis Plan for Sustainable Growth*.

The Minneapolis Plan for Sustainable Growth identifies the site as mixed use on the future land use map. This site is adjacent to Lake Street and Lagoon Avenue, which are both designated as commercial corridors. It is also within the Uptown activity center. The proposal would be consistent with the applicable principles and policies of the comprehensive plan including the following:

Land Use Policy 1.3: Ensure that development plans incorporate appropriate transportation access and facilities, particularly for bicycle, pedestrian, and transit.

- I.3.1 Require safe, convenient, and direct pedestrian connections between principal building entrances and the public right-of-way in all new development and, where practical, in conjunction with renovation and expansion of existing buildings.
- I.3.2 Ensure the provision of high quality transit, bicycle, and pedestrian access to and within designated land use features.

Land Use Policy I.4: Develop and maintain strong and successful commercial and mixed use areas with a wide range of character and functions to serve the needs of current and future users.

- I.4.1 Support a variety of commercial districts and corridors of varying size, intensity of development, mix of uses, and market served.
- I.4.2 Promote standards that help make commercial districts and corridors desirable, viable, and distinctly urban, including: diversity of activity, safety for pedestrians, access to desirable goods and amenities, attractive streetscape elements, density and variety of uses to encourage walking, and architectural elements to add interest at the pedestrian level.
- I.4.4 Continue to encourage principles of traditional urban design including site layout that screens off-street parking and loading, buildings that reinforce the street wall, principal entrances that face the public sidewalks, and windows that provide “eyes on the street”.

Land Use Policy I.12: Support Activity Centers by preserving the mix and intensity of land uses and by enhancing the design features that give each center its unique urban character.

- I.12.1 Encourage a variety of commercial and residential uses that generate activity all day long and into the evening.
- I.12.2 Encourage mixed use buildings, with commercial uses located on the ground floor and secure entrances for residential uses.
- I.12.3 Encourage active uses on the ground floor of buildings in Activity Centers.
- I.12.4 Discourage uses that diminish the transit and pedestrian character of Activity Centers, such as automobile services, surface parking lots, and drive-through facilities.
- I.12.5 Encourage a height of at least two stories for new buildings in Activity Centers, in keeping with neighborhood character.
- I.12.9 Encourage architectural design, building massing and site plans to create or improve public and semi-public spaces in Activity Centers.

Housing Policy 3.2: Support housing density in locations that are well connected by transit, and are close to commercial, cultural and natural amenities.

- 3.2.1 Encourage and support housing development along commercial and community corridors, and in and near growth centers, activity centers, retail centers, transit station areas, and neighborhood commercial nodes.

Urban Design Policy 10.9: Support urban design standards that emphasize traditional urban form with pedestrian scale design features at the street level in mixed use and transit-oriented development.

- 10.9.1 Encourage both mixed use buildings and a mix of uses in separate buildings where appropriate.

- 10.9.2 Promote building and site design that delineates between public and private spaces.
- 10.9.4 Coordinate site designs and public right-of-way improvements to provide adequate sidewalk space for pedestrian movement, street trees, landscaping, street furniture, sidewalk cafes and other elements of active pedestrian areas.

Urban Design Policy 10.10: Support urban design standards that emphasize a traditional urban form in commercial areas.

- 10.10.1 Enhance the city's commercial districts by encouraging appropriate building forms and designs, historic preservation objectives, site plans that enhance the pedestrian environment, and by maintaining high quality four season public spaces and infrastructure.
- 10.10.3 Enhance pedestrian and transit-oriented commercial districts with street furniture, street plantings, plazas, water features, public art and improved transit and pedestrian and bicycle amenities.
- 10.10.4 Orient new buildings to the street to foster safe and successful commercial nodes and corridors.
- 10.10.6 Require storefront window transparency to assure both natural surveillance and an inviting pedestrian experience.

Urban Design Policy 10.16: Design streets and sidewalks to ensure safety, pedestrian comfort and aesthetic appeal.

- 10.16.1 Encourage wider sidewalks in commercial nodes, activity centers, along community and commercial corridors and in growth centers such as Downtown and the University of Minnesota.
- 10.16.2 Provide streetscape amenities, including street furniture, trees, and landscaping, that buffer pedestrians from auto traffic, parking areas, and winter elements.
- 10.16.3 Integrate placement of street furniture and fixtures, including landscaping and lighting, to serve a function and not obstruct pedestrian pathways and pedestrian flows.
- 10.16.4 Employ pedestrian-friendly features along streets, including street trees and landscaped boulevards that add interest and beauty while also managing storm water, appropriate lane widths, raised intersections, and high-visibility crosswalks.

Urban Design Policy 10.19: Landscaping is encouraged in order to complement the scale of the site and its surroundings, enhance the built environment, create and define public and private spaces, buffer and screen, incorporate crime prevention principles, and provide shade, aesthetic appeal, and environmental benefits.

Urban Design Policy 10.23 Promote climate-sensitive design principles to make the winter environment safe, comfortable and enjoyable.

- 10.23.1 Consider solar access, shelter from wind and snow storage and removal in site design.
- 10.23.2 Locate pedestrian places on the sunny sides of streets and buildings to shelter from the wind and utilize the sun's warmth.
- 10.23.3 Consider building context, placement, and height to manage wind speeds.
- 10.23.4 Encourage snow removal and storage practices that promote pedestrian and bicycle activity and safety.

- 10.23.6 Encourage street tree plantings to reduce wind speed and provide separation between pedestrians and cars.

4. Conformance with applicable development plans or objectives adopted by the City Council.

The property is also located within the boundaries of the *Uptown Small Area Plan* that was adopted by the City Council in 2008. The recommended land use for the site is mixed use with transit-oriented density. The plan calls for concentrating retail activities at key locations on Hennepin Avenue and along Lake Street and Lagoon Avenue east of Hennepin. In the activity center, it also recommends vertically mixing land uses with ground floors occupied by entertainment, hotels, restaurants, shopping, and destination uses, as well as retailers (local and national) while upper floors should contain offices or residential uses. A goal of having mixed use blocks along Lake Street is to improve walkability and connectivity between Uptown and Lyn/Lake. In addition to the principles and polices outlined in the conditional use permit and variance sections of this report, the following guidance from the *Uptown Small Area Plan* applies to this proposal:

Land Use Recommendations.

- Encourage mixed use blocks along Lake Street with the goal of improving walkability and connectivity between Uptown and Lyn/Lake.
- Encourage office and employment uses in the Core as means of boosting daytime population.

Built Form Recommendations.

- Concentrate density and intensity in the Core.
- Encourage buildings in the Core to fit within a sculpted envelope that maximizes sunlight to the Greenway, Lake Street and Lagoon Avenue.
- Encourage all buildings on Lake Street and Lagoon Avenue, east of Hennepin Avenue, to contain storefronts.

Pedestrian and Bicycle Recommendations.

- Shorten the walk distance for pedestrians crossing streets in Uptown by providing bump outs at signalized intersections.
- Ensure that new development (residential and commercial) provides an adequate number of bicycle parking stalls.

5. Alternative compliance.

The Planning Commission or zoning administrator may approve alternatives to any site plan review requirement upon finding that the project meets one of three criteria required for alternative compliance. Alternative compliance is requested for the following requirements:

- **Building placement.** A portion of the first floor of the building is proposed to be setback 29 feet from Lagoon Avenue and 15 feet from Fremont Avenue. Per Chapter 530-Site Plan Review (and the Pedestrian Oriented Overlay District building placement requirements outlined in Chapter 551), the first floor of buildings is required to be not more than eight feet from the front lot line. Since this is a corner lot, both the building wall along Lagoon and Fremont are required to meet this requirement. Alternative compliance and a variance are required for the placement of the first floor building wall further than 8 feet from the property line. As outlined in the variance section above, the proposed setback provides some additional room for a building entrance to a larger retailer. In addition, the proposed recessed design entrance with the upper floors built out to the property line creates an enclosed alcove area that provides protection from weather elements for pedestrians, while having a strong building presence at the property line. The proposed alternative of recessing a small portion of the building on the first floor for an entrance while having upper

floors extend near the front property lines is consistent with development objectives adopted by the city council (*Minneapolis Plan for Sustainability* and the *Uptown Small Area Plan*)

- **Percentage of exterior building materials allowed:** The applicant is proposing to have 61 percent of the north courtyard elevation be fiber cement board and 48 percent of the south courtyard elevation be fiber cement. Per the [City of Minneapolis, Guide to Exterior Building Walls and Materials](#), buildings should not include more than 30 percent of an elevation with fiber cement board on an elevation. Alternative compliance is required. Given that there will be minimal to no visibility from the street of these elevations, these elevations contain architectural enhancements with projecting balconies, and all other elevations comply with the percentage of allowed materials, the proposed alternative is consistent with the applicable development objectives adopted by the city council.
- **More than three materials on an elevation (south, east, and west).** The applicant is proposing to have four exterior materials on the south elevation (Lake Street): four materials on the east elevation (Fremont Avenue) and five materials on the west elevation (alley). Per the [City of Minneapolis, Guide to Exterior Building Walls and Materials](#), buildings should not include more than three exterior materials on an elevation (excluding windows, doors, and foundation and accent materials). Having three or fewer durable exterior building materials for an elevation is a traditional building design practice for mixed use buildings like that being proposed. The applicant has made an effort to come closer to compliance with the standard of having three building materials on an elevation since the Committee of the Whole meeting. However, CPED is recommending that the Planning Commission not provide alternative compliance for having more than three materials on the elevations in order to be in compliance with the [City of Minneapolis, Guide to Exterior Building Walls and Materials](#). By reducing the number of materials, it will simplify the design and further unify all building elevations.
- **Required landscaping and screening:** The loading area along the back of the building is required to provide a seven-foot wide landscaped yard and a six-foot tall screening since it abuts a permitted residential use. The applicant is not proposing landscaping or screening in this area. Alternative compliance is required. Strict adherence to the landscaping and screening requirements in this instance are impractical because of site conditions. The project is in a dense urban part of Minneapolis where it is encouraged to limit the amount of open space between buildings, limit the amount of area devoted to driveways, and to continue to have a strong building wall presence. By requiring a seven-foot wide landscape buffer, it would likely reduce the building street presence along Lake Street and Lagoon Avenue and significantly alter the maneuvering in and out of the project site. In addition, it should be noted that the neighboring building to the west on the first floor where the landscaping and screening are required currently has a transformer, trash area, and a retail establishment that does not windows.

RECOMMENDATIONS

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt staff findings for the applications by CPM Development for the properties located at 1300 West Lake Street, 1300 ½ West Lake Street, and 2928 Fremont Avenue South:

A. Conditional Use Permit to allow an increase in the maximum permitted height in the C3A/Community Activity Center District.

Recommended motion: **Approve** the conditional use permit for an increase in height from 4 stories or 56 feet to 7 stories or 91.6 feet in the C3A/Community Activity Center District, subject to the following conditions:

- I. The conditional use permit shall be recorded with Hennepin County as required by Minn. Stat. 462.3595, subd. 4 before building permits may be issued or before the use or activity requiring a conditional use permit may commence. Unless extended by the zoning

administrator, the conditional use permit shall expire if it is not recorded within two years of approval.

B. Variance to increase the allowable floor area ratio.

Recommended motion: **Approve** the variance request to increase the maximum allowable floor area for the proposed building from 3.8 to 4.3.

C. Variance to increase the size of a commercial storefront.

Recommended motion: **Approve** the variance request to increase the size of a commercial storefront from 8,000 square feet to 21,700 square feet in the C3A/Community Activity Center District, subject to the following conditions:

1. The retail establishment shall maintain not less than two public entrances facing the public street, and shall maintain a storefront entrance on Lagoon Avenue and Lake Street as proposed.

D. Variance to reduce the interior yard setback.

Recommended motion: **Approve** the variance to reduce the interior yard setback from 17 feet to 0 feet to allow the building and port cochere.

E. Variance to increase the distance the first floor will be located from the front property line in the pedestrian oriented overlay district.

Recommended motion: **Approve** the variance request to increase the distance the first floor will be located from the front property lines in the Pedestrian Oriented Overlay District from 8 feet to 15 feet (from Fremont Avenue) and 29 feet (from Lagoon Avenue), subject to the following conditions:

1. As required by Section 551.100, the area between the building and the lot line shall include the raised bed landscape areas as proposed.

F. Variance to reduce the off-street loading requirement.

Recommended motion: **Approve** the variance to reduce the off-street loading requirement from two small loading spaces to one small loading space.

G. Site Plan Review for a new commercial building.

Recommended motion: **Approve** the site plan review for the construction of a seven-story, mixed use building with 125 residential units, 5,883 square feet of office space, and 21,700 square feet of commercial space, subject to the following conditions:

1. A detailed floor plan and shelving plan shall be submitted prior to building permit issuance that shows full compliance with the requirements of Section 530.120-Building Design and Section 551.120-Building Façade.
2. All mechanical equipment shall be screened as required by Section 535.70-Screening of Mechanical Equipment. In addition, the mechanical equipment recessed into the building wall along the western elevation (rear), shall be screened with a decorative metal screen to be in compliance with the blank wall requirement in Section 530.120-Building Design.
3. As outlined in the [City of Minneapolis, Guide to Exterior Building Walls and Materials](#), buildings shall not include more than three exterior materials on an elevation (excluding windows, doors, and foundation and accent materials). To be in compliance with the *City of Minneapolis Guide to Exterior Building Walls and Materials*, a standing seam metal panel (or metal panel of efficient thickness) shall be used for exterior cladding in order to discourage an oil-canning effect. To help reduce the visibility of the mechanical equipment, the heating,

- ventilation and air conditioning screens on the outside walls shall be matched to paint the surrounding wall finish.
4. As required by Section 541.180-Bicycle Parking, the bike parking dedicated to the office uses shall be labeled on plans submitted for building permit issuance.
 5. A lighting plan shall be submitted prior to building permit issuance that shows the project is in compliance with the lighting requirements outlined in Section 535.590-Lighting.
 6. All parking areas shall be marked as required by Section 541.320-Marking of Spaces and the compact parking stalls shall be labeled as required by Section 541.330-Size.
 7. Any outdoor sales and display shall be in compliance with Section 548.180-Enclosed Building Standards. Fast food restaurants are prohibited in the proposed building as required by Section 548.360-General District Standards and 551.100. And as required by Section 551.120-Building Facades, no backlit awnings and canopies are allowed.
 8. The landscaping onsite and within the public right-of-way shall comply with Section 530.200-plant material standards and 530.210-installation and maintenance of materials.

ATTACHMENTS

1. BZZ Map, aerial, and future land use map
2. PDR report
3. Written description and statement to the findings submitted by applicant
4. Zoning analysis submitted by applicant
5. Plan set: civil plan set, landscape plan, architectural plan set
6. Hydrology report
7. Travel demand management plan
8. 6.18.15 COW elevations and renderings

CPM Development

10th

NAME OF APPLICANT

WARD

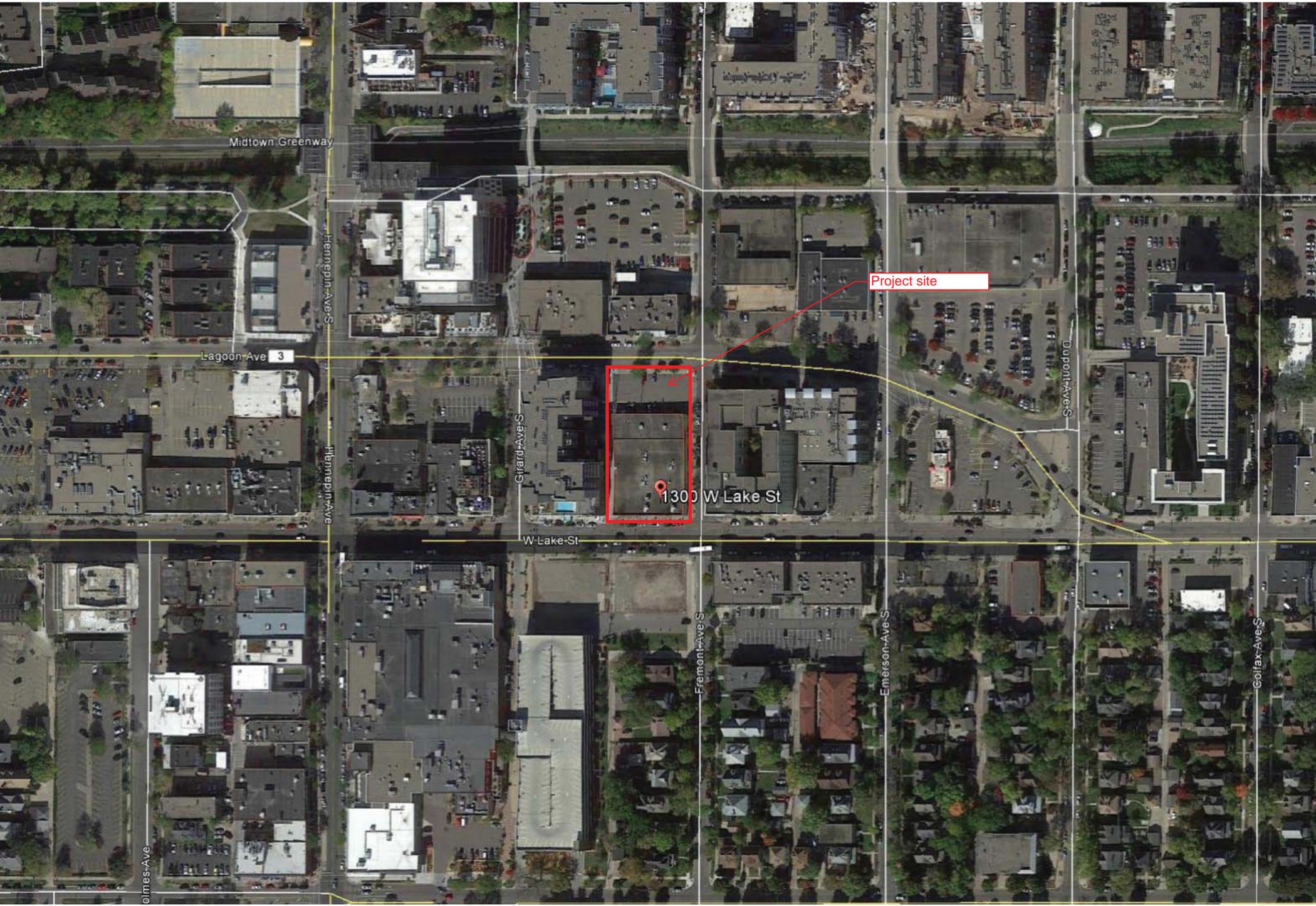


PROPERTY ADDRESS

1300 West Lake Street and 2929 Fremont Avenue South

FILE NUMBER

BZZ-7296



Midtown Greenway

Lagoon Ave 3

Hennepin Ave S

Hennepin Ave

Girard Ave S

W Lake St

1300 W Lake St

Project site

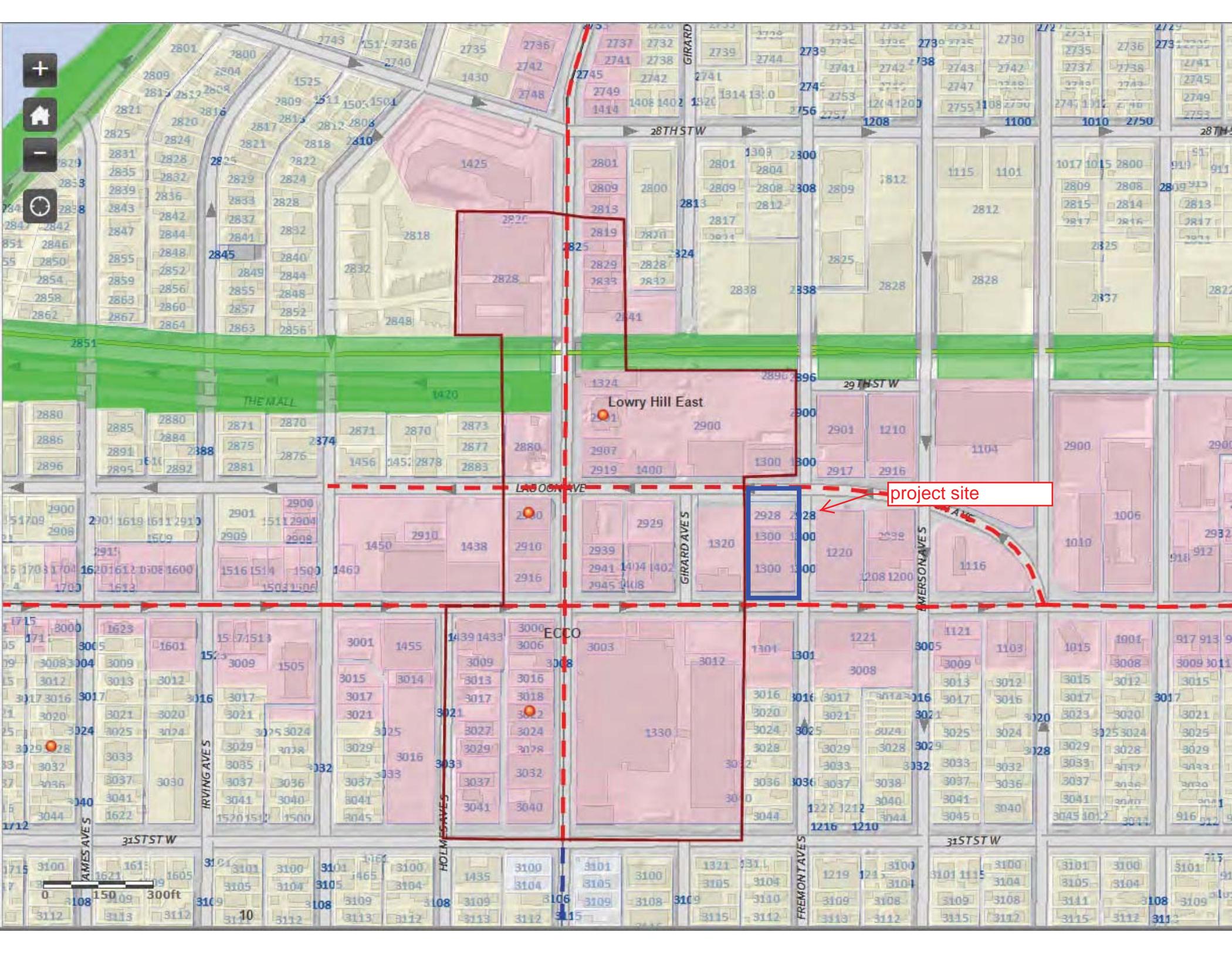
DuPont Ave S

Fremont Ave S

Emerson Ave S

Golfax Ave S

Olmes Ave



project site

Lowry Hill East

LAOON AVE

ECCO

THE MALL



0 108 150 300ft



Minneapolis Development Review
250 South 4th Street
Room 300
Minneapolis, MN 55415

Preliminary Development Review Report

Development Coordinator Assigned: **MATTHEW JAMES**
(612) 673-2547
matthew.james@minneapolismn.gov

Status *
RESUBMISSION REQUIRED

Tracking Number:	PDR 1001335
Applicant:	CPM DEVELOPMENT 2919 KNOX AVE S. SUITE #200 MINNEAPOLIS, MN 55408
Site Address:	1300 LAKE ST W 1300 LAKE ST W 2928 FREMONT AVE S
Date Submitted:	10-JUL-2015
Date Reviewed:	24-JUL-2015

Purpose

The purpose of the Preliminary Development Review (PDR) is to provide Customers with comments about their proposed development. City personnel, who specialize in various disciplines, review site plans to identify issues and provide feedback to the Customers to assist them in developing their final site plans.

The City of Minneapolis encourages the use of green building techniques. For additional information please check out our green building web page at: http://www.ci.minneapolis.mn.us/mdr/GreenBuildingOptions_home.asp.

DISCLAIMER: *The information in this review is based solely on the preliminary site plan submitted. The comments contained in this report are preliminary ONLY and are subject to modification.*

Project Scope

Mixed-used development with 26,500 sq. ft of commercial/retail space and 123 dwelling units.

Review Findings (by Discipline)

Zoning - Planning

- The demolition of the existing building will likely be able to be approved administratively. The project is scheduled to go to the August 18 CPC meeting. The following land use applications have been identified:

*Approved: You may continue to the next phase of developing your project.

*Resubmission Required: You cannot move forward or obtain permits until your plans have been resubmitted and approved.

- Conditional use permit to allow for increased height
- Variance to increase the maximum floor area ratio ("FAR")
- Variance to increase the allowed floor area for a retail use in the C3A District;
- Variance to reduce the off-street loading requirements
- Site plan review.

□ Addressing

- Per City of Minneapolis Street Naming and Address Standard V1.22, the City of Minneapolis holds authority for assignment of all addresses, verification, change, and/or additions. Each assigned address number uses the street that provides the best/direct access for life safety equipment and best/direct access to the occupants.
- The address for the proposed Lake and Fremont mixed-use building will be 1300 W. Lake St. This address meets the City of Minneapolis Street Naming and Address Standard requirements. The commercial establishment at the corner of Lake and Fremont will be 1300 W. Lake. The apartment complex entrance will be addressed 1312 W. Lake. The commercial establishment at the corner of Lagoon and Fremont will be 1301 Lagoon.
- When assigning suite sequences the following guidelines are as follows:
 - The first one to two digits of the suite sequence number will designate the floor number of the site.
 - The last two digits of the suite sequence number will designate the unique ID for the unit (condo, suite, unit, or apartment).
 - Suite sequence digit numbers will be assigned to dwelling, commercial and retail units, not common areas. For example, laundry rooms, saunas, workout rooms, etc., would not be assigned numbers.
 - Please provide each condo, suite, unit or apartment number.

□ Parks - Forestry

- Contact Craig Pinkalla (612-499-9233 cpinkalla@minneapolisparcs.org) regarding any questions related to planting, removal or the process for protection of trees during construction in the city right of way.
- Effective January 1, 2014, the City of Minneapolis and the Minneapolis Park and Recreation Board adopted an update to the existing Parkland Dedication Ordinance. The adopted City of Minneapolis Parkland Dedication ordinance is located in Section 598.340 of the City's Land Subdivision ordinance:
 - <http://library.municode.com/index.aspx?clientId=11490>
- As adopted, the fee in lieu of dedication for new residential units is \$1,521 per unit (affordable units excluded per ordinance) and for commercial and industrial development it is \$202.80 per development employee (as defined in ordinance). Any dedication fee (if required) must be paid at the time of building permit issuance.
- There is also an administration fee that is 5% of the calculated park dedication fee.
- As proposed, for your PROJECT, the calculated dedication fee is as follows:
 - Park Dedication Fee Calculation =
 - Residential (123 units x \$1,521 per unit) = \$187,083
 - Non-Residential Commercial Space = \$ 00
 - 5% of \$XX (Administration Fee) = \$ 1,000
 - Total Park Dedication Administrative Fee: = \$188,083
- This is a preliminary calculation based on your current proposal; a final calculation will be made at the time of building permit submittal.
- For further information, please contact Matthew D. James at (612) 673-2547.

□ Right of Way

- An encroachment permit shall be required for all streetscape elements in the Public right-of-way such as: plants & shrubs, planters, tree grates and other landscaping elements, sidewalk furniture (including bike racks and bollards), and sidewalk elements other than standard concrete walkways such as pavers, colored concrete, stairs, raised landings, retaining walls, access ramps, and railings (NOTE: railings may not extend into the sidewalk pedestrian area). Please contact Bob Boblett at (612) 673-2428 for further information.

- Note to the Applicant: Any elements of an earth retention system and related operations (such as construction crane boom swings) that fall within the Public right-of-way will require an encroachment permit application. If there are to be any earth retention systems which will extend outside the property line of the development then a plan must be submitted showing details of the system. All such elements shall be removed from the Public right-of-way following construction with the exception of tie-backs which may remain but must be uncoupled and de-tensioned. Please contact Bob Boblett at (612) 673-2428 for further information.
- In addition, any elements of an earth retention system and related excavations that fall within the Public right-of-way will require a "Right-of-Way Excavation Permit". This permit is typically issued to the General Contractor just prior to the start of construction. However, it is the Applicant's responsibility to insure that all required permits have been acquired by its consultants, contractors, sub-contractor's prior to the start of work.
- Contact Paul Cao at (612) 673-2943 for position and alignment of bike racks proposed in the Public right-of-way. If the racks are privately owned, they will require an encroachment permit.
- The Project limits fall within the boundaries of the Uptown Special Service District. Any improvements, modifications, and alterations to the streetscape are subject to the review and approval of the Uptown Special Service District, and will require the approval of the Special Service District Advisory Board. Please coordinate contact with the Advisory Board through Andy Carlson (612) 673-2836.

❑ Street Design

- The plan as submitted meets the requirements of the Public Works Street Design Division.
- Note to the Applicant: Site Plan revisions based upon Traffic and Parking comments may require further review by Street Design.

❑ Sidewalk

- Planters and trees placement should be considered carefully, shifted as far toward the curb as possible to maximize pedestrian through space to meet ADA guidelines and also achieve appropriate planter sizes for their proposed plantings. Bicycle rack placement should also be considered; contact Paul Cao at (612) 673-2943 for position and alignment of bike racks proposed in the Public right-of-way. Bike bollard details shall be provided with the plans. A minimum 8'-0" clear zone (un-obstructed) sidewalk shall be maintained for the length of any block in a straight line. For further clarification, site plans shall be fully dimensioned in relation to the property lines, Public right-of-way, sidewalks, street furniture, landscaping, utilities, and other obstructions. Sidewalk layouts and landscaping in the Public right-of-way shall follow the established design standards of Access Minneapolis; refer to the following:
 - <http://www.minneapolismn.gov/publicworks/transplan/index.htm>
- The symbolism for proposed sidewalk depicted on Sheet C4.0 of the plans would seem to indicate a distinctive joint pattern that is different from the sidewalk pattern depicted on the Landscaping Plans (Sheet L1.1); the Applicant shall clarify the proposed sidewalk joint pattern and depict it consistently throughout the plans. Note that minimum joint spacing of 4' is typical for Minneapolis standard sidewalk.
- Open planters proposed along Fremont Ave. S. shall be fully detailed; planter cross-sections shall be included in the Landscaping Plans. The 6" planter curb is not a preferred alternative. Three sided tree guard or tree rail- This is called out as an "ornamental" fence, but it is unclear what "metal fence flush at street curb" means. In general it is recommended that open planting beds be considered, not tree grates - reference streetscape across Fremont Ave. at this location. The tree detail on Sheet L1.2 is not accurate for an enclosed planting bed.
- ADA compliant pedestrian ramps are required at each crosswalk at the intersections of Fremont Ave. S. and W Lake St. and Fremont Ave. S. Lagoon Ave. Unless site conditions are restrictive; two (2) ADA compliant pedestrian ramps should be constructed at each location. Include the appropriate details and standard plates in the site plan; refer Mn/DOT Standard Plan 5-297.250 Pedestrian Curb Ramp Details at: <http://standardplans.dot.state.mn.us/stdplan.aspx>

□ Traffic and Parking

- The nature of the proposed development is such that traffic impacts will be an issue; please contact Allan Klugman at (612) 673-2743 to discuss the requirements of a Travel Demand Management Plan (TDMP).
- The proposed bump outs on Fremont Ave. S. will not be approved at this time. Any bump-out would limit the possibility of changing the roadway configuration in the future (and there has been a recent push for more access throughout the Uptown area). The City does view bump-outs as a positive amenity; however, in order to continue the discussion, Public Works requests that the TDMP studies Fremont Ave. S. (as a one-way roadway) and how future configurations to that street (with bump-outs) could be restricted. Note to the Applicant: The bump-out into Lake St. will need Hennepin County approval. Hennepin County has been contacted for comment and a response is pending.
- Current ordinance states that all maneuvers associated with loading, parking or sanitation pick up for a private development shall occur on private property. Please provide a narrative explaining the trash removal operations and show turning maneuvers for all truck type vehicles that will be using the loading dock/parking entrance areas.
- Per City Ordinance the Applicant shall provide for (and identify) a solid waste collection point (SWCP) on the site plans. The location of the SWCP is subject to the review and approval of the Public Works Department.
- The Applicant shall note the location of any existing Metro Transit "bus stops" on the site plan.
- Current ordinance states that all maneuvers associated with loading, parking or sanitation pick up for a private development shall occur on private property. Please provide a narrative explaining the trash removal operations and show turning maneuvers for all vehicles that will be using the loading dock/parking entrance areas. Per City Ordinance the Applicant shall provide for (and identify) a solid waste collection point (SWCP) on the site plans. The location of the SWCP is subject to the review and approval of the Public Works Department.
- Street Lighting: Please contact Bill Prince at (612) 673-3901 regarding existing and proposed street lighting. All street lighting (existing and proposed) shall be shown clearly on the site plan. Prior to site plan approval, the Applicant shall contact Bill Prince at (612) 673-3901 to determine street lighting requirements. Note: If decorative street lighting exists on the proposed site it must be preserved or replaced at existing levels. Street lighting will be strongly encouraged in areas immediately adjacent to existing lighting systems, in high density areas such as Uptown and the University of Minnesota, and along major pedestrian corridors and business nodes as identified in the Minneapolis Street Lighting Policy.
- Note to the Applicant: The construction of this development will likely require the use of Public right-of-way (roadway and sidewalks) for construction purposes.

□ Water

- The plan as submitted meets the requirements of the Public Works Water Maintenance & Distribution Division.

□ Fire Safety

- Provide required fire suppression system throughout building.
- Fire department connection must be located on the address side of building and within 150 feet of a fire hydrant.
- Provide required fire alarm system throughout building.
- Maintain fire apparatus access at all times.

□ Business Licensing

- Continue to work with Matthew D. James at (612) 673-2547 concerning a Food Plan Review, SAC determination and any Business License application submittal that may be required for this proposed project.

□ Environmental Health

- In 1930 the City of Minneapolis issued construction permit for a filling station. A Special Council Permit that accompanied the construction permit indicates 2 - 1000 gallon gasoline and 2 pumps were to be installed. City fire records indicate a station was operated by Howard Wise from 1941 with 9600 gallon gas tank which was removed in 1947. In 1948 a 6000 gallon indoor fuel oil tank was installed for the Velie Motor Company. Except for the fire department reference of the removal of the 9600 gallon gasoline tank no other information is available; on the disposition of the other identified tanks.
- Documentation on the current tank status must be submitted to environmental services on the 2-1,000 gallon gas tanks and 1 - 6,000 fuel oil tanks. If no documentation exists a contingency plan needs to be submitted to Tom Frame (tom.frame@minneapolismn.gov) on actions that will be taken these tanks are found during site development activities.
- The identification of the site as former service station and motor company identifies a potential problem of contaminated soils being present. It is recommended the developer enter the Minnesota Pollution Control Petroleum Brownfield program and request a letter of no association for any contaminated soils that may be present on site. If impacted soil is encountered during site activities call the MN State Duty officer at (615) 649-5451.
- If dewatering is required during site construction see below for city permit requirements. Subgrade structures should be designed to prevent infiltration of groundwater without the need for a permanent dewatering system being installed. If a continuously operating permanent dewatering system is needed it must be approved as part of the sanitary sewer and storm drain site plan approval prior to construction beginning.
- No construction, demolition or commercial power maintenance equipment shall be operated within the city between the hours of 6:00 p.m. and 7:00 a.m. on weekdays or during any hours on Saturdays, Sundays and state and federal holidays, except under permit. Contact Environmental Services at (612) 673-3867 for permit information.
- Permits and approval are required from Environmental Services for the following activities: Temporary storage of impacted soils on site prior to disposal or reuse; Reuse of impacted soils on site; Dewatering and discharge of accumulated storm water or ground water, underground or aboveground tank installation or removal, well construction or sealing. Contact Tom Frame at (612) 673-5807 for permit applications and approvals.

□ Historical Preservation Committee

- There is no HPC flag on this property. HPC review is not required at this time. HPC review is required for any wrecking permits pertaining to the removal of existing structures.

□ Sewer Design

- Groundwater: Please provide a copy of any geotechnical reports for the site. It must be adequately demonstrated that pumping of groundwater will not be necessary in order to keep the below grade areas dry.
- Stormwater Management: City of Minneapolis Utility Notes 3 and 4 on Sheet C3.0 should be removed.
- Surface Drainage: The existing and proposed spot elevations along the west property line do not match. Is a retaining wall proposed along the property line? Please clearly identify what is proposed for grading in this area. The drainage of the adjacent property should not be negatively impacted and an emergency overflow for the low point along the drive aisle should be provided so accumulated runoff does not enter the building.
- Please include the proposed spot elevations on the gutter line on the plan view of Sheet C2.2, similar to Sheet C2.1. There appears to be a discrepancy between some of the elevations along the profiles between Sheets C2.1 and Sheet C2.1. For instance the elevation of the angle point of the bump out at Sta. 1+19.05 on Sheet C2.2 is identified in the profile as 879.94, while on the plan view of sheet C2.1 it is 879.40. The elevation of Sta. 0+50.71 on Sheet C2.2 is 879.95 while the plan view of Sheet C2.1 shows a spot elevation of 879.69 in close proximity.
- Utility Connections: Please clearly identify what is proposed for the sanitary sewer connection. Multiple connection scenarios should not be included on the plans. If the existing connections are proposed to be re-

used as the first preference, please only include this on the plans. If it is determined the existing connections will not be used later, the plan can be amended.

- If the new sanitary connections are proposed, it is preferred that the sanitary sewer connections be made at the existing manholes just upstream and downstream of the current proposed connection locations. Please evaluate if this is feasible, if not please contact Jeremy Strehlo, 612-673-3973, for further requirements for the proposed connection directly to the main.
- The locations of the existing service connections to the property should be identified on the plans. Existing utility service location information is available through the City's Utility Connections Office, 612-673-2451.
- The proposed storm sewer connection to the main in Fremont, north of Lake, should be core-drilled and a saddle tee fitting installed, per City of Minneapolis Standard Supplemental Specifications. A new manhole is not permitted for the connection. Please revise the plans accordingly. The same comment applies to the proposed storm sewer connection to the main in Lake St.
- Utility: It appears the City catch basin at the corner of Lagoon and Fremont would need to be relocated to accommodate the proposed bump out. This is not identified as being removed on the Demolition Plan. Please clearly identify all catch basins and catch basin leads proposed to be removed and relocated.
- The proposed catch basins in Fremont and Lake should correspond to the low point along the curb line. The profile view of the streets indicates the low points are in different locations from the CBs.
- Please include the relevant City standard detail plates for the construction of the City catch basins on Lake St and Fremont Ave. These include: SEWR-1003, SEWR-1008-R1, SEWR-1009, SEWR-1011-R1, SEWR-2006, SEWR-2007, SEWR-2008, SEWR-3000 and SEWR-3001.
- Please include the pipe material for the existing City storm and sanitary sewer adjacent to the property. For City sanitary and storm sewer infrastructure records contact (612) 673-2405.
- Non Stormwater Discharges: Detail all mechanical and non-stormwater discharges. Non-stormwater discharges are not permitted unless approved by the City of Minneapolis. Non-stormwater discharges not declared and approved will not be permitted. If there currently are none and nothing is proposed declare this status on the plans.
- For comments or questions on Public Works Surface Water & Sewers Division related requirements please contact Jeremy Strehlo, (Professional Engineer) at (612) 673-3973, or jeremy.strehlo@minneapolismn.gov.

❑ Construction Code Services

- Excavation earth retainage design will be necessary.
- If future doors are required along Fremont for smaller tenants, they cannot swing over property line.
- Two exits are required from the roof terrace.
- A Service Availability Charge (SAC) determination will need to be submitted to the Metropolitan Council for the proposed project. Please refer to this link for more information or Contact Karon Cappaert at (651) 602-1118 or karon.cappaert@metc.state.mn.us .
http://www.ci.minneapolis.mn.us/mdr/docs/sac_availability_charge.pdf

END OF REPORT

1300 West Lake Street Mixed Use Development
Statement of Proposed Use and Project Description
July 31, 2015

Purpose and Vision

The purpose of the project is to redevelop an underutilized half-block site in the heart of Uptown into a vibrant, pedestrian-oriented mixed use development. The site currently houses a one-story, suburban-style big box retail store and surface parking lot. The existing store contains two retail uses of roughly 23,000 sf each – one at street level and another in the basement. The proposed new development will maximize retail uses at the street level with a single 21,700 sf retail store featuring prominent entrances facing both Lagoon and Lake. 5,900 sf of boutique office space will occupy level 2 facing Lake Street and 125 dwelling units in levels 2 through 6 will comprise the remainder of the development. While no public alley exists on the site, vehicular access will be restricted to two narrow one-way curb cuts on Lake St and Lagoon Ave, mimicking a traditional alley arrangement. Loading for all uses will be accommodated in the interior of the site, out of view of the street. The result is that the three street frontages will maximize active uses, helping to animate the street.

Architecture and Urban Design

The site straddles two prominent commercial streets, Lake St and Lagoon Ave which are built out with commercial buildings that form strong street walls. The urban fabric in this area is among the most complete in the city, outside of downtown. The design responds to this context by maintaining the established street wall on all three public street faces. Prominent metal-clad volumes anchor the two primary corners of the site. Between these strong corners, the building steps back to accommodate balconies and a southwest-facing rooftop terrace for the use of the building's residents. Durable materials such as stone, brick, natural cedar and glass form the base of the building with lighter materials such as metal, stucco and fiber cement comprising the upper portion. In response to the limited pedestrian space, the building incorporates recesses at each of the corner retail entrances. In addition, curb bulb outs are proposed at the corner of Lake and Fremont and Lagoon and Fremont. The result will be significantly expanded pedestrian space at each corner, as well as shortened pedestrian crossing distances across Lake Street and Fremont Avenue.

Required Applications

The applications required for the project are:

1. Conditional use permit for increased height;
2. Variance to increase the maximum floor area ratio (“FAR”);
3. Variance to increase the allowed floor area for a retail use in the C3A District;
4. Variance of interior yard setback;
5. Variance of the building placement standard of the Pedestrian Oriented Overlay District;
6. Variance to reduce the loading requirements; and
7. Site plan review

CONDITIONAL USE PERMIT FOR INCREASED HEIGHT REQUIRED FINDINGS

A conditional use permit is requested to increase the height for the project from the 4-story/56’ limit of the C3A District to 7 stories, 89’-4” to the top of the elevator penthouse. The building is 7 stories instead of 6 because the corner units on the 6th floor have a mezzanine level. The majority of the building will be 6 stories, 73’-4” high. The 7th-story mezzanine levels that anchor the corners of the building will be 84’ high. Only the elevator penthouse that provides access to a roof terrace will be 89’-4” high and it will be set back toward the middle of the roof.

1) The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety comfort or general welfare.

Construction of a mixed use building that is 89’-4” in height will not be detrimental to or endanger the public health, safety, comfort or general welfare, and granting the CUP will allow a development that will benefit the neighborhood and reinforce goals of the comprehensive and small area plan. The project will continue the positive trend in Uptown of replacing surface parking and obsolete 1 and 2-story buildings with dense, mixed-use, multi-story buildings. The building and site design and the addition of commercial uses and residents will activate the pedestrian realm and enhance the Uptown Activity Center. The new construction will comply with all building and site development codes.

2) The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property of uses permitted in the district.

The proposed height will not be injurious to the use and enjoyment of other property or impede development and improvement of surrounding property. The height is virtually the

same as the 7 story / 84'-8" height of the Walkway project that was recently constructed on the west side of the block. Other nearby existing and planned development include a 5-story apartment building across Fremont, the 10-story existing and the approved 8-story MoZaic buildings across Lagoon, and anticipated redevelopment of the vacant Calhoun Square lot across Lake with a building of similar height. The *Uptown Small Area Plan* (USAP) supports tall, dense, mixed use development in the Core of Uptown.

3) Adequate utilities, access roads, drainage, necessary facilities or other measures, have been or will be provided.

Adequate utilities, access, drainage and other facilities will be provided. The development team will continue to work closely with Public Works, Plan Review and Planning staff to comply with City and other applicable requirements. Vehicular access will be restricted to two narrow one-way curb cuts on Lake St and Lagoon Ave, mimicking a traditional alley arrangement. Loading for all uses will be accommodated in the interior of the site, out of view of the street.

4) Adequate measures have been or will be taken to minimize traffic congestion in the public streets.

A Travel Demand Management Plan is being prepared and will require approval by Public Works. The Developer will make commitments in the TDMP that will promote use of alternative transportation by residents, employees and customers. All loading will occur on site in the service and loading area behind the building. The parking garage will provide all parking required for the residential and commercial uses.

5) The conditional use is consistent with the applicable policies of the comprehensive plan.

The *Minneapolis Plan for Sustainable Growth* designates the site for Mixed Use development. The site is located between two designated Commercial Corridors, Lake St and Lagoon Ave, and is immediately adjacent to the Uptown Activity Center. The project and proposed height are consistent with the following policies and implementation steps from the comprehensive plan.

Land Use Policy 1.2: Ensure appropriate transitions between uses with different size, scale, and intensity.

1.2.1 Promote quality design in new development, as well as building orientation, scale, massing, buffering, and setbacks that are appropriate with the context of the surrounding area.

Land Use Policy 1.5: Promote growth and encourage overall city vitality by directing new commercial and mixed use development to designated corridors and districts.

Land Use Policy 1.8: Preserve the stability and diversity of the city's neighborhoods while allowing for increased density in order to attract and retain long-term residents and businesses.

1.8.1 Promote a range of housing types and residential densities, with highest density development concentrated in and along appropriate land use features.

Land Use Policy 1.10: Support development along Commercial Corridors that enhances the street's character, fosters pedestrian movement, expands the range of goods and services available, and improves the ability to accommodate automobile traffic.

1.10.1 Support a mix of uses – such as retail sales, office, institutional, high-density residential and clean low impact light industrial – where compatible with the existing and desired character.

1.10.2 Encourage commercial development, including active uses on the ground floor, where Commercial Corridors intersect with other designated corridors.

1.10.4 Encourage a height of at least two stories for new buildings along Commercial Corridors, in keeping with neighborhood character.

1.10.5 Encourage the development of high-density housing on Commercial Corridors.

Urban Design Policy 10.5: Support the development of multi-family residential dwellings of appropriate form and scale.

10.5.2 Medium-scale, multi-family residential development is more appropriate along Commercial Corridors, Activity Centers, Transit Station Areas and Growth Centers outside of Downtown Minneapolis.

Urban Design Policy 10.6: New multi-family development or renovation should be designed in terms of traditional urban building form with pedestrian scale design features at the street level.

10.6.1 Design buildings to fulfill light, privacy, and view requirements for the subject building as well as for adjacent properties by building within required setbacks.

The USAP identifies the site as falling within the Urban Village, South Sub-Area and within the Core of Uptown and recommends redevelopment of the site with mixed use, transit-oriented density development with a height of up to 84 feet. The majority of the building will be 6 stories, 73'-4" high. The 7th-story mezzanine levels that anchor the corners of the building will be 84' high. Only the elevator penthouse that provides access to a roof terrace will be 89'-4" high and it will be set back toward the middle of the roof. The proposed height is consistent with the guidance of the USAP.

6) The conditional use shall in all other respects, conform to the applicable regulations of the district in which it is located.

Upon approval of the submitted applications, the project will conform to the applicable regulations of the C3A zoning district.

Additional factors to be considered when determining the maximum height per §548.110:

(1) Access to light and air of surrounding properties.

Public right-of-way separates the proposed development from surrounding properties on three sides. Mid-block driveways that serve the Walkway and the proposed building will separate the buildings by at least 28 feet. Much greater separation will occur in the interior of the block due to additional setbacks of the upper floors of both buildings in that location. Allowing the requested increase in height will not impede access to light and air for the surrounding properties.

(2) Shadowing of residential properties, significant public spaces, or existing solar energy systems.

A shadow study has been submitted that shows the degree of shadowing of other residential properties by the project. The shadows cast by the new building will be similar to those cast by the adjacent residential buildings to the east and west of the site. The project does not shadow any significant public spaces. No existing solar energy systems are known to be shadowed by the project.

(3) The scale and character of surrounding uses.

The scale and character of the proposed building is compatible with surrounding development and consistent with the guidance in the USAP. The height is virtually the same as the 7 story / 84'-8" height of the Walkway project that was recently constructed on the west side of the block. Other nearby existing and planned development include a 5-story apartment building across Fremont, the 10-story existing and approved 8-story MoZaic buildings across Lagoon, and anticipated redevelopment of the vacant Calhoun Square lot across Lake with a building of similar height.

(4) Preservation of views of landmark buildings, significant open spaces or water bodies.

The project will not block views of landmark buildings, significant open spaces or water bodies.

VARIANCE FOR FLOOR AREA RATIO
REQUIRED FINDINGS

The maximum FAR before density bonuses in the C3A District is 2.7. With two density bonuses for enclosed parking and mixed use, the allowed FAR of the project is 3.78. A variance is being requested to increase the FAR to 4.28.

1) Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.

Practical difficulties exist for building within the FAR limits without a variance and complying with the City's goals for development in this part of Uptown. The site is located immediately adjacent to the Uptown Activity Center, which is intended to support, and be supported by, high and very high-density housing. The site is located on two Commercial Corridors and is within 2 blocks of the Uptown Transit Center and Midtown Greenway bike trail. The USAP also calls for the greatest new density to be concentrated in the Core of Uptown. These land use features and transit amenities converge to support the mix of uses and higher residential density that can be achieved on this site with the requested FAR variance. These are unique circumstances, not created by the developer.

2) The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.

The proposed FAR is reasonable and consistent with the City's land use guidance for high density development in this area. The upper floors of the building are sculpted with stepbacks and balconies, which, along with variations in building materials, colors and roof height, mitigate the perception of the mass of the building from the pedestrian realm, in keeping with the spirit of the ordinance.

3) The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.

The granting of the variance will not alter the essential character of the area or be injurious to the use or enjoyment of other properties. The height and massing of the project is similar to surrounding mid-rise developments. If above grade parking was included in the calculation of floor area, the FAR of the fully-built out MoZaic development, which has 6 levels of above grade parking, would be comparable to the proposed building. The development will be beneficial to the public health and welfare and to neighboring properties by filling a gap in the urban fabric and replacing a suburban-style 1-story building and surface parking with a 7-story building of unique and high quality design.

VARIANCE TO INCREASE FLOOR AREA OF RETAIL USE
REQUIRED FINDINGS

A variance is requested to increase the floor area limit on individual retail uses from 8,000 sf to 21,700 sf to allow a single retail tenant on the ground floor of the building.

1) Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.

The site is uniquely situated to accommodate a larger retail use that will be a convenient and economical source of goods for the burgeoning residential community in the Uptown/Midtown Greenway area. Due to the exceptional concentration of residential developments in and within walking distance of Uptown, there is an emerging need for larger retail uses that can provide a reasonably-priced source of household, health and/or grocery needs, in addition to the eclectic, smaller scale stores that enliven the Uptown retail environment. The project site is one of the few infill sites remaining in Uptown that can accommodate this type of retail use. These are unique circumstances not created by the Developer that create practical difficulties in complying with the ordinance and providing for the retail needs of the neighborhood.

2) The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.

The C3A District regulations generally promote smaller-scale, “neighborhood-oriented” retail uses. In light of the expansive and expanding residential density within walking distance of the project site, some larger retail uses are needed to serve the needs of the surrounding neighborhood. The retail store will have entries on both Lagoon Ave and Lake St, and extensive windows engaging with the pedestrian realm along Fremont Ave. The proposed variance is, therefore, in keeping with the spirit and intent of the ordinance. It is also in keeping with the comprehensive plan guidance for Commercial Corridors that are intended to accommodate intensive commercial uses, strengthen surrounding urban neighborhoods, and expand the range of goods and services available.

3) The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.

The property currently contains two retail tenants of approximately 23,000 sf each in a single-story building. The proposed 21,700 sf retail use will enhance the character of Uptown because it will be incorporated in a mixed-use building. It will provide neighborhood-serving retail

goods and services, which will benefit the use and enjoyment of other property in the vicinity, as well as the health, safety and welfare of area residents, visitors and customers.

VARIANCE OF INTERIOR YARD SETBACK
REQUIRED FINDINGS

A variance is requested to allow the interior setback of the building to be reduced from 17' to 0'. With certain exceptions, there is generally no setback requirement in the commercial districts. One exception is that residential uses must be set back 5 + 2x, but no more than 15', from interior lot lines. The majority of the building meets this requirement. The other exception that applies in this case is that a setback of 5 + 2x (with no upper limit) is required from an interior lot line that abuts a lot used for residential use, even when, as here, the abutting residential use is also in the commercial district. The Walkway project that was recently developed on the west half of the block contains residential units. The Walkway was not subject to this setback requirement because it was the first residential use to be constructed on the block. Because a portion of the proposed building at the eastern corners is 7 stories, the setback requirement is 17'. The majority of the building will be set back 16' from the shared interior lot line. The porte cochere and amenity terrace at the southwest corner will be at the property line.

1) Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.

Practical difficulties exist for complying with both the ordinance and principles of good urban design. The driveway for the Walkway runs between Lake and Lagoon through the center of the block, along the shared lot line. Site design and vehicular access considerations dictate that the driveway for the proposed development be similarly situated through the block. This creates a wide gap in the street wall along Lake that the porte cochere is intended to mitigate. As noted above, a greater set back is required for this development than for the first-in-time residential project on the west half of the block. These are unique circumstances not created by the Developer.

2) The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.

The proposed setbacks are reasonable and in keeping with the spirit and intent of the ordinance and comprehensive plan. The purpose of the setback requirement is to buffer residential uses from adjacent commercial uses and to ensure access to light and air. In this case, both developments are residential and the buildings will be separated by approximately

30' along most of the shared lot line. The porte cochere will be adjacent to the driveway for the Walkway. The design is consistent with the land use and urban design policies of the comprehensive plan noted in the findings for the conditional use permit above.

3) The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.

The requested variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity, or be detrimental to the public or surrounding properties. The proposed setbacks are similar to those of the Walkway. The new building will replace a 1-story building and surface parking lot, improving the urban design of the locality by placing building walls along all three street frontages of the site.

VARIANCE OF BUILDING PLACEMENT STANDARD
IN PEDESTRIAN ORIENTED OVERLAY DISTRICT
REQUIRED FINDINGS

A variance is requested to allow the ground-floor facades framing the entry into the retail store at the corner of Lagoon and Fremont to be more than 8 feet from the property lines. The entry facade will be approximately 29' from Lagoon and 15' from Fremont.

1) Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.

Practical difficulties exist for complying with the ordinance because of the topography of the site and the need to provide a sufficiently-wide entry to accommodate a high-level of pedestrian traffic into a major retail store. The elevation of the site slopes down from Lagoon to Lake. The north entry to the retail space must be setback further from Lagoon in order to have sufficient length for the slope of the entry walk down to the level of the entry door and retail floor. These are unique circumstances not created by the Developer.

2) The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.

The proposed setbacks of the ground-floor facades for the north retail entry are reasonable. In addition to accommodating the required slope of the entry sidewalk, the setbacks provide

additional area off the public sidewalk for pedestrians entering and exiting the store. The building facades above the ground floor will be built close to the property line, reinforcing the street wall and providing a sheltered entry, in keeping with the spirit and intent of the ordinance and comprehensive plan.

3) The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.

The requested variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity, or be detrimental to the public or surrounding properties. The new building will replace a surface parking lot on this half of the development site, improving the urban design of the locality by placing building walls along all three street frontages of the site.

VARIANCE TO REDUCE LOADING REQUIREMENT REQUIRED FINDINGS

A variance is being requested to reduce the loading requirement for the project from two small loading spaces to one loading space that will be located in the interior of the site and accessed from the driveway that will run along the west side of the site.

1) Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.

Practical difficulties exist for providing two off-street loading spaces because of the excessive amount of maneuvering area that would be required to accommodate two trucks at once and because, as a practical matter, two spaces are not needed to serve the uses in the building. One small loading space is required for the retail use and one for the residential use. The loading space is expected to be used daily to serve the retail use. After initial lease up of the residential units, however, residential use of the space will be minimal and sporadic. Property management will coordinate shared use of the loading area to prevent congestion and make more efficient use of space in a dense urban environment. These circumstances are unique to the property and have not been created by the applicant.

2) The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.

A single loading space for the project is reasonable. As noted, the residential use of the loading area will be sporadic and shared use can be managed. It is in keeping with the spirit and intent of the City's ordinances and comprehensive plan policies to not require dedicated loading areas in excess of the actual demand when off-street loading can be provided in a way that makes more efficient use of the site.

3) The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.

The requested loading variance will not alter the character of the locality or be detrimental to the public or surrounding properties because loading activity will be accommodated on site.

Zoning Analysis: Lake & Fremont
ESG - 7/10/15

Replatted properties	Lot Size	GFA	FAR
Residential		115,714	
Retail		21,683	
Office		6,000	
Total	33,490	143,397	4.28

Zoning District	C3A
Overlay District	Pedestrian Overlay
Max FAR	2.70
20% bonus - Underground Parking	3.24
20% bonus - mixed use	3.78
Proposed FAR	4.28
Max Height	4 Stories, 56' (increase allowed with Conditional Use Permit)
Proposed height - Residential	7 stories, 89'-4"

13.27% variance

Parking Analysis (no restaurant)	Proposed DUs or GSF	Base Zoning (Table 541-1)		PO reductions (551.140e)		Transit Reduction (541.200)		Applied Zoning	
		Min	Max	Min	Max	Min	Max	Min	Max
Residential	123	123	none			90%		111	none
Office	6,000	4.00	30.00	75%	75%			3	23
Retail (single, non-restaurant)	21,683	35.37	108.42	75%	75%			27	81
Total								140	None

Parking Analysis (restaurant)	Proposed DUs or GSF	Base Zoning (Table 541-1)		PO reductions (551.140e)		Transit Reduction (541.200)		Applied Zoning	
		Min	Max	Min	Max	Min	Max	Min	Max
Residential	123	123	none			90%		111	none
Office	6,000	4.00	30.00	75%	75%			3	23
Restaurant (no entertainment)	21,683	69.61	289.11	75%	75%			52	217
Total								166	None

Parking Provided	Stalls
Minus 2	83
Minus 1	78
Total	161

Required applications:
Site Plan Review
CUP to increase max allowable height
Variance to increase max allowable FAR
Variance for single retail use over 8,000 sf
TDMP (required for Henn/Lake PO Overlay)

Parking requirements	Min	Max
General Retail Sales & Services	1 space per 500 sf of GFA in excess of 4,000 sf	1 space per 200 sf of GFA
Office	1 space per 500 sf of GFA in excess of 4,000 sf	1 space per 200 sf of GFA
Restaurant, Sit Down	1 space per 500 sq. ft. of GFA up to 2,000 sq. ft. + 1 space per 300 sq. ft. of GFA in excess of 2,000 sq. ft.	1 space per 75 sq. ft. of GFA

LAKE & FREMONT

1300 W LAKE ST

MINNEAPOLIS, MN



elness swenson graham architects
500 WASHINGTON AVENUE SOUTH
MINNEAPOLIS, MINNESOTA 55415
P: 612.339.5508
F: 612.339.5382
WWW.ESGARARCH.COM

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly licensed architect under the laws of the State of Minnesota.

Signature: _____
Typed or Printed Name: _____
Title: _____

NOT FOR CONSTRUCTION

LAKE & FREMONT

MINNEAPOLIS, MN

Purpose and Vision

The purpose of the project is to redevelop an underutilized half-block site in the heart of Uptown into a vibrant, pedestrian-oriented mixed use development. The site currently houses a one-story, suburban style big box retail store and surface parking lot. The proposed new development will maximize retail uses at the street level with a single 21,500 sf retail store featuring prominent entrances facing both Lake St and Lagoon Ave. 5,000 sf of boutique office space will occupy level 2 facing Lake Street and 123 dwelling units in levels 2 through 5 will comprise the remainder of the development. While no public alley exists on the site, vehicular access will be restricted to two narrow one-way curb cuts on Lake St and Lagoon Ave, mimicking a traditional alley arrangement. Loading for all uses will be accommodated in the interior of the site, out of view of the street. The result is that the three street frontages will maximize active uses, helping to animate the street.

Architecture and Urban Design

The site straddles two prominent commercial streets, Lake St and Lagoon Ave which are built out with commercial buildings that form strong street walls. The urban fabric in this area is among the most complete in the city, outside of downtown. The design responds to this context by introducing prominent, metal-clad towers that project out toward the street at each of the two primary corners of the site. In addition to providing visual interest, the towers serve to anchor the corners of the site, reinforcing the traditional street wall. Between these strong corners, the building steps back to accommodate balconies and a southwest-facing rooftop terrace for the use of the building's residents. Durable materials such as stone and brick (accented with natural cedar) form the base of the building with lighter materials such as metal, stucco and fiber cement comprising the upper portion. In response to the limited pedestrian space available, the building incorporates recesses at each of the corner retail entrances. In addition, curb bulb outs are proposed at the corner of Lake and Fremont, and Lagoon and Fremont. The result will be significantly expanded pedestrian space at each corner of the site, as well as shortened pedestrian crossing distances across Lake and Fremont.

Minneapolis Plan for Sustainable Growth & Uptown Small Area Plan

The site is located between two designated Commercial Corridors, Lake St and Lagoon Ave, and is immediately adjacent to the Uptown Activity Center. The Uptown SAP identifies the site as falling within the Urban Village, South Sub-Area and within the Core of Uptown.

The design responds to several guiding principals of both of these planning documents:

- Elimination of one-story building and surface parking lot
- Mix of uses (retail, office and residential)
- Active uses and engaging facades at street level help fill the existing gap in urban fabric, provides connectivity
- Continuous retail uses on Lake St and Lagoon Ave.
- All parking below grade
- All loading and vehicle movements accommodated on site, out of public view
- Highest intensity development on Commercial Corridors and within the Core of Uptown.
- Sculpted building envelope breaks down scale and provides visual interest



PROJECT LOCATION

Vicinity

Site Location

PROJECT TEAM

Owner/Developer: CPM Companies
2919 Knox Ave. S, Suite 200
Minneapolis, MN 55408
Ph: 612-843-2490

Architect: Elness Swenson Graham Architects Inc.
500 Washington Ave. South, Suite 1080
Minneapolis, MN 55415
Ph: 612-339-5508
Fx: 612-339-5382

Contractor: Reuter Wallon Commercial
2919 Knox Avenue S., Suite 200
Minneapolis, MN 55408
612-876-4380

Civil Engineer: BKBM Engineers
5500 Brooklyn Blvd
Minneapolis, MN 55429
763-843-0420

Structural Engineer: BKBM Engineers
5500 Brooklyn Blvd
Minneapolis, MN 55429
763-843-0420

Landscape Architect: Calyx Design Group
1583 Berkeley Ave
Saint Paul, MN 55105

Mechanical, Electrical, Plumbing Engineers: DesignBuild by Contractor

SYMBOLS LEGEND

	SECTION AND ELEVATION REF		EXTERIOR ELEVATION KEYNOTE REF
	INTERIOR ELEVATION REF		ROOM NAME AND NUMBER
	DETAIL REF		DOOR NUMBER
	WALL TYPE AND U/I		SECTION KEYNOTE REF
	ROOM TYPE AND SHEET A1(2)		REVISION NUMBER
	ELEVATION REF		FIRE EXTENSION/CHIMNEY
	SPOT ELEVATION REF		FIRE EXTENSION/CHIMNEY - WALL MOUNTED
	FLOOD FINISH TRANSITION REF		

UNIT MIX & SQUARE FOOTAGES

Sale and Leasing Office: 606 Walker
606 Walker - 55161/2013

Level	Use	Total GFA	Parking	Total GFA	Office GFA	Residential GFA	Garage GFA	Units	Residential Parking	Commercial Parking
MINUS 1	Parking	12,850	1,843						86	0
Level 1	Residential	14,898	2,076	28,752	0	28,752	0	123	0	0
Level 2	Residential	14,898	2,076	28,752	5,218	18,734	0	0	0	0
Level 3	Residential	14,898	2,076	28,752	0	18,734	0	0	0	0
Level 4	Residential	14,898	2,076	28,752	0	18,734	0	0	0	0
Level 5	Residential	14,898	2,076	28,752	0	18,734	0	0	0	0
Total	Residential	59,792	8,171	118,212	5,218	87,234	0	123	0	0

* Parcel 1: Retail gfa includes 1,823 of residential above parking entry
* Parcel 2: Includes 1,823 of residential above parking entry

Level	Bicycle Count
MINUS 2	81
MINUS 1	84
TOTAL	165

Description	Count	DIMENSIONS
MINUS 2 STANDARD STALL	65	8'-6" X 18'-0" MIN
MINUS 2 POSSIBLE STALL	16	8'-0" X 18'-0" MIN
MINUS 2 COMPACT STALL	12	8'-0" X 15'-0" MIN
MINUS 1 STANDARD STALL	84	8'-6" X 18'-0" MIN
MINUS 1 POSSIBLE STALL	4	8'-0" X 18'-0" MIN
MINUS 1 COMPACT STALL	12	8'-0" X 15'-0" MIN
TOTAL	91	

PARKING STALL TOTAL: 161

SHEET INDEX

SHEET NUMBER	SHEET NAME
TT1	TITLE SHEET
01/1	CIVIL
02/1	02/1 SITE SURVEY
03/1	03/1 SELECTIVE SITE DEMOLITION PLAN
04/1	04/1 GRADING, DRAINAGE, AND EROSION CONTROL PLAN
05/1	05/1 LAKE STREET AND LAGOON AVENUE PLAN AND PROFILE
06/1	06/1 FURNISHING, FINISH, PLAN AND PROFILE
07/1	07/1 UTILITY PLAN
08/1	08/1 PARKING AND LOADING PLAN
09/1	09/1 STORM WATER DRAINAGE PLAN
10/1	LANDSCAPE
11/1	11/1 LANDSCAPE LAYOUT PLAN
12/1	12/1 LANDSCAPE DETAILS
13/1	ARCHITECTURAL
14/1	14/1 CONTEXT AERIAL
15/1	15/1 EXISTING SITE PHOTOGRAPHS
16/1	16/1 SIGNAGE SCHEDULE
17/1	17/1 MATERIALS
18/1	18/1 MINUS 2 & MINUS 1 PLANS
19/1	19/1 STREET LEVEL PLAN
20/1	20/1 LEVEL 2 & LEVEL 3 PLANS
21/1	21/1 LEVEL 4, UPPER ROOF PLANS
22/1	22/1 EXTERIOR ELEVATIONS - COLOR
23/1	23/1 EXTERIOR ELEVATIONS BEW
24/1	24/1 EXTERIOR ELEVATIONS - COLOR
25/1	25/1 EXTERIOR ELEVATIONS BEW
26/1	26/1 EXTERIOR ELEVATIONS - COLOR
27/1	27/1 EXTERIOR ELEVATIONS BEW
28/1	28/1 EXTERIOR ELEVATIONS - COLOR
29/1	29/1 EXTERIOR ELEVATIONS BEW
30/1	30/1 SD VIEWS
31/1	31/1 SD VIEWS
32/1	32/1 SD VIEWS

7702015 - LAND USE APPLICATION / PDR
7702015 - LUM REVISIONS

LAND USE APPLICATION UPDATES 8/7/15

ORIGINAL ISSUE

REVISIONS

No.	Description	Date

215524
PROJECT NUMBER

ESC
DRAWN BY

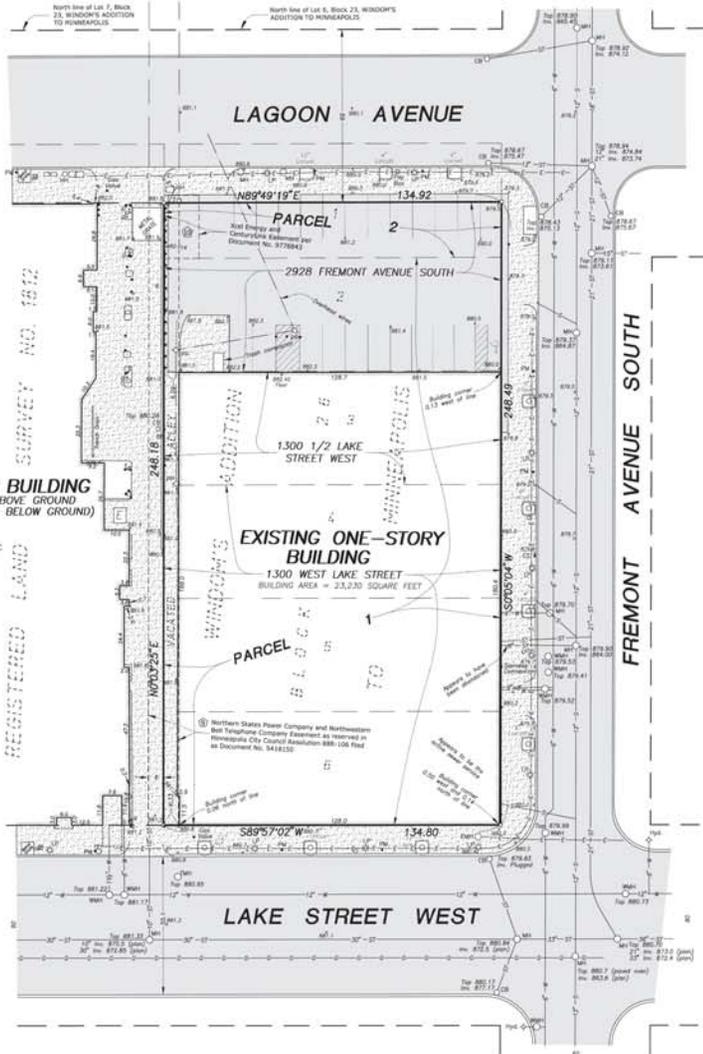
ESC
CHECKED BY

KEY PLAN

1300 W LAKE ST

TITLE SHEET
T1.1

Survey for: CPM DEVELOPMENT



NOTES

- * Bearings shown are based on Hennepin County Coordinates.
- * Utilities shown are from information furnished by the City of Minneapolis, Excel Energy and Center North Energy in response to Gopher State One Call Ticket No. 151464826 and are verified where possible.
- * Contact Gopher State 1 for utility locations before any construction shall begin. Phone: 651-654-3000.
- * Area: 33,490 square feet (0.77 acre).

EASEMENT NOTES CORRESPONDING TO SCHEDULE B, SECTION TWO, FIRST AMERICAN TITLE INSURANCE COMPANY COMMITMENT NO. NCS-724063-MPLS

Item 9 - Reservation of existing easement, right-of-way and authority of Northern States Power Company and Northwestern Bell Telephone Company in the vacated alley, as contained in Resolution 888-106, a certified copy of which was recorded June 8, 1988, as Document No. 5418:50 affects the property and is shown on the survey.

Item 10 - Reservation of existing easement rights and authority of Xcel Energy and CenturyLink in vacated Lagoon Avenue, as contained in Resolution 2012B-207, a certified copy of which was recorded April 23, 2012, as Document No. 80778813, as amended by Resolution 2012B-444, a certified copy of which was recorded May 7, 2013, as Document No. A9946597 affects the property and is shown on the survey.

PROPERTY DESCRIPTION CONTAINED IN FIRST AMERICAN TITLE INSURANCE COMPANY COMMITMENT NO. NCS-724063-MPLS

- Parcel 1:**
Lots 3, 4, 5, and 6, Block 26, Winthrop's Addition to Minneapolis, Hennepin County, Minnesota, and the east half of the adjoining vacated alley.
(Abstract Property)
- Parcel 2:**
That part of vacated Lagoon Avenue described as Lot 1, Block 26, and east half of adjacent alley, Winthrop's Addition, lying south of a line parallel with and 69 feet south of the north line of Lots 6 and 7, and adjacent alley, Block 23, said Winthrop's Addition to Minneapolis.
(Abstract Property)



UTILITY STATEMENT

The underground utilities shown have been located from field survey information and existing drawings. The surveyor makes no guarantee that the underground utilities shown comprise all such utilities in the area, either in service or abandoned. The surveyor further does not warrant that the underground utilities shown are in the exact location indicated, although the surveyor certifies that they are shown as accurately as possible from information available. The surveyor has not physically located the underground utilities.

I hereby certify that this survey was prepared by me or under my direction and that I am a duly Licensed Land Surveyor under the laws of the State of Minnesota.
Dated this 25th day of June, 2015.

REHDER & ASSOCIATES, INC.
Scott
Gary C. Rehder, Land Surveyor
Minnesota License No. 22036

Rehder and Associates, Inc.
CIVIL ENGINEERS AND LAND SURVEYORS
3440 Federal Drive • Suite 110 • Eden, Minnesota • Phone (855) 452-3001



class room plan architect
500 Washington Avenue South
Minneapolis, Minnesota 55415
P: 612.339.5501
F: 612.339.5182
www.esgarch.com

BK&W
3800 Douglas Boulevard
Minneapolis, MN 55425-2518
Phone: (763) 848-0400
Fax: (763) 848-0421
www.bkw.com

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

Engineer
THOMAS J. CLARE
Typed or Printed Name
27931 July 10, 2015
Drawn: T. Clare

NOT FOR CONSTRUCTION

Project Status

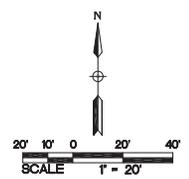
ORIGINAL ISSUE	07-10-15	
REVISIONS		
No.	Description	Date

15394
PROJECT NUMBER
WH: KAM
CHECKED BY
KEY PLAN

LAKE & FREMONT

SELECTIVE SITE DEMOLITION PLAN

C1.0



PROPOSED PLAN SYMBOLS

CONSTRUCTION LIMITS	-----
FILTER LOG	--- ---
PROPERTY LINE	---
SWIMOUT LINE (APPROX.)	- - - - -
ROCK CONSTRUCTION ENTRANCE	-----
BITUMINOUS REMOVAL	▨
CONCRETE REMOVAL	▩
PIPE REMOVAL	▧
CURB REMOVAL	▦
TREE REMOVAL	⊗

ABBREVIATIONS

BEDC	Bench Mark
CB	Center Point
CONC	Concrete
CP	Center Point Pipe
ELEV	Elevation
EX	Existing
FR	Front
MAX	Maximum
MIN	Minimum
MB	Manhole
PM	Post Mark
PKC	Parking Marking
PCP	Reinforced Concrete Pipe

APPROXIMATE DISTURBED AREA IS 0.77 ACRES

NOTE: OTHER SHEETS NOT SHOWN ON PLAN MAY RECEIVE RUNOFF FROM CONSTRUCTION ACTIVITIES. RETAIN SLOPE PROTECTION PER DETAIL 5/C1.0 ON ALL STORM INLETS THAT MAY RECEIVE RUNOFF.

DEMOLITION AND REMOVAL NOTES:

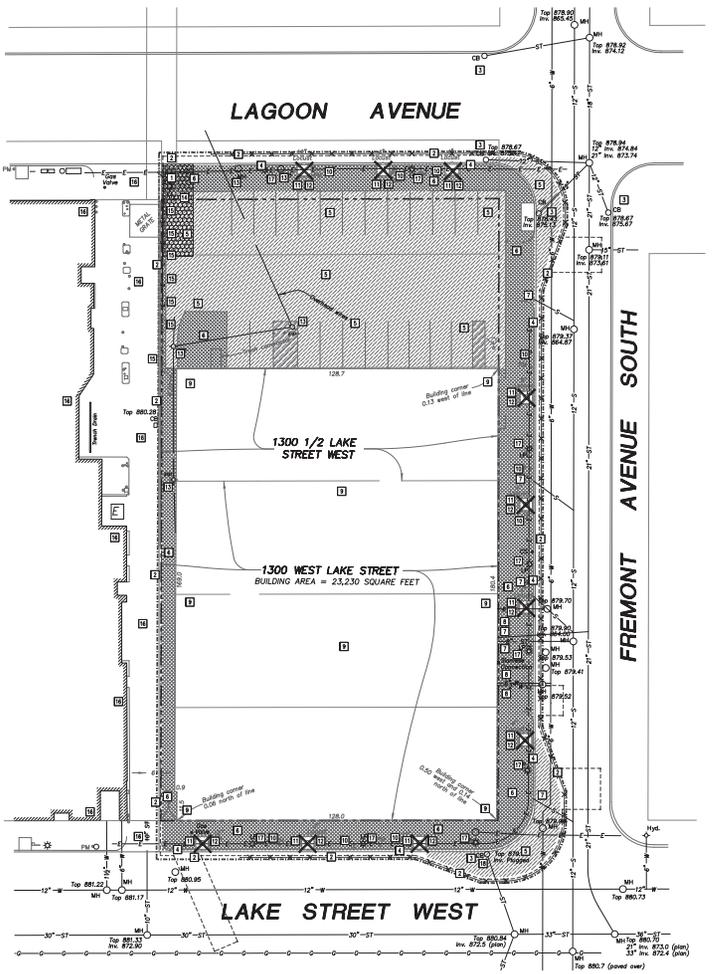
- PRIOR TO START OF DEMOLITION, ALL EROSION AND SEDIMENT CONTROL MEASURES TO BE INSTALLED BY THE CONTRACTOR AND INSPECTED BY THE CITY OF MINNEAPOLIS AND MISSOURI WATERWAYS MANAGEMENT ORGANIZATION. ALL SILT FENCES SHALL BE INSTALLED AND INSPECTED PRIOR TO ANY CONSTRUCTION ACTIVITY. SILT FENCES SHALL BE INSTALLED ALONG THE CONTOR.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN THE LOCATION OF ALL EXISTING UTILITIES. THE CONTRACTOR SHALL VERIFY THE LOCATION, DEPTH AND MARK ALL EXISTING UTILITIES 48 HOURS BEFORE CONSTRUCTION STARTS. THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS FROM THE CITY OF MINNEAPOLIS. IF UTILITIES ARE MARKED OR IF MARKS ARE SHOWN INCORRECTLY, CONTACT THE CITY OF MINNEAPOLIS AT 612-673-4444 FOR FIELD LOCATING EXISTING UTILITIES. CONTACT UTILITIES OWNERS IF DAMAGE OCCURS TO CONSTRUCTION.
- THERE MAY BE MISCELLANEOUS ITEMS TO BE REMOVED THAT ARE NOT IDENTIFIED ON THESE PLANS. THE CONTRACTOR SHALL FIRST DO A VISUAL SURVEY OF THE SITE AND REVIEW THE DOCUMENTS TO OBTAIN A CLEAR UNDERSTANDING OF THE INTENDED SCOPE OF WORK.
- PRIOR TO START OF CONSTRUCTION, DISCONNECT ALL GAS AND ELECTRIC SERVICES. COORDINATE DISCONNECTION OF EACH UTILITY WITH THE UTILITY OWNER. REMOVE ALL GAS AND ELECTRIC LINES UNDER PROPOSED BUILDING FOOTPRINT.
- ANY UTILITIES NOT NEEDED FOR REMOVAL OR MAINTENANCE ARE TO BE PROTECTED AT ALL TIMES.
- ALL EXISTING CURB AND GUTTER IS TO BE REMOVED WITHIN THE SCOPE OF THE PROJECT FROM THE SAW CUT LINES TO THE NEAREST JOINT.
- THE BACKGROUND INFORMATION WAS PREPARED BY ANDER & ASSOCIATES, INC., GREG GENTZ (651) 432-9051.
- ALL WORK IN THE PUBLIC RIGHT OF WAY IS TO BE COORDINATED WITH THE CITY OF MINNEAPOLIS, ROADWAY REPAIRS, BOLLARDING, AND TRAFFIC CONTROL ARE TO BE FOR CITY OF MINNEAPOLIS STANDARDS AND SPECIFICATIONS.

KEYED NOTES

- KEYED NOTES ARE DENOTED BY [] ON PLAN.
- INSTALL ROCK CONSTRUCTION ENTRANCE. REFER TO DETAIL 5/C1.0.
 - INSTALL SEDIMENT CONTROL - FILTER LOG. REFER TO DETAIL 4/C1.0.
 - INSTALL SLOPE PROTECTION. REFER TO DETAIL 3/C1.0.
 - REMOVE CURB AND GUTTER IN ITS ENTIRETY TO THE EXTENT SHOWN. SAWCUT AND REMOVE AT NEAREST JOINT. REFER TO DETAIL 5/C1.0. FOLLOW ALL CITY OF MINNEAPOLIS STANDARDS FOR REMOVAL AND TRAFFIC/VEHICULAR CONTROL.
 - REMOVE BITUMINOUS PAVEMENT IN ITS ENTIRETY TO THE EXTENTS SHOWN.
 - REMOVE CONCRETE PAVEMENT IN ITS ENTIRETY TO THE EXTENTS SHOWN.
 - PRIOR TO REMOVING EXISTING BITUMINOUS PAVEMENT, THE CONTRACTOR IS TO DETERMINE WHICH BITUMINOUS PAINTS ARE NORMALLY APPLIED TO DRIVEWAYS. THE CONTRACTOR IS TO REMOVE THE APPLICABLE BITUMINOUS PAINTS AND RELAY THE PAVEMENT SURFACE. BITUMINOUS PAINTS AND THERMOPLASTIC MATERIALS TO BE REMOVED AND CHIPPED. THE CHIPPED MATERIAL IS TO BE RECYCLED OR REUSED. ALL CHIPPED MATERIAL IS TO BE REMOVED FROM THE SITE IF NOT USING IT FOR THIS PROJECT. FOLLOW ALL CITY OF MINNEAPOLIS STANDARDS AND SPECIFICATIONS FOR UTILITY REMOVAL/REINSTALLATION.
 - PRIOR TO START OF CONSTRUCTION, THE CONTRACTOR IS COORDINATING WITH THE CITY OF MINNEAPOLIS AND MISSOURI WATERWAYS MANAGEMENT ORGANIZATION FOR THE POSSIBLE REUSE OF THE EXISTING WATER SERVICES AND/OR WATER MAINS. CONTACT THE CITY OF MINNEAPOLIS AND MISSOURI WATERWAYS MANAGEMENT ORGANIZATION ABOUT THE POSSIBILITY OF REUSING THESE SERVICES. IF DEEMED UNDESIRABLE, THEN REMOVE ALL OTHER WATER SERVICES TO EXISTING BUILDING PER CITY OF MINNEAPOLIS STANDARDS AND SPECIFICATIONS.
 - REMOVE BUILDING IN ITS ENTIRETY, INCLUDING EXTERIOR STAIRS, OVERHANGS, FOOTINGS, AND SMALL UTILITIES. COORDINATE REMOVAL OF SMALL UTILITIES WITH LOCAL UTILITY COMPANIES.
 - REMOVE AND SALVAGE EXISTING PARKING METERS AND PAY BOXES. COORDINATE WITH CITY OF MINNEAPOLIS.
 - REMOVE TREE IN ITS ENTIRETY INCLUDING STUMP.
 - REMOVE AND SALVAGE EXISTING TREE GRATE. COORDINATE WITH CITY OF MINNEAPOLIS.
 - REMOVAL AND RELOCATION OF EXISTING POWER POLE AND LINES TO BE COORDINATED WITH LOCAL UTILITY COMPANY PRIOR TO START OF CONSTRUCTION.
 - REMOVAL AND RELOCATION OF EXISTING TELEPHONE BOB AND LINES TO BE COORDINATED WITH LOCAL TELEPHONE COMPANY.
 - EXISTING BOLLARDS TO BE REMOVED.
 - EXISTING BUILDING AND MARKERS/DEVICES ARE TO REMAIN UNDISTURBED. INGRESS/EGRESS TO EXISTING FACILITY IS TO REMAIN FUNCTIONAL AT ALL TIMES.
 - EXISTING STREET LIGHTS ARE TO BE REMOVED AND SALVAGED. COORDINATE RELOCATION/REINSTALLATION WITH THE CITY OF MINNEAPOLIS.
 - EXISTING CATCH BASINS AND STORM SEWERS TO BE REMOVED. REFER TO C3.0 FOR APPROXIMATE LIMITS OF REMOVAL AND NEW CATCH BASIN LOCATION. COORDINATE ALL WORK WITH CITY OF MINNEAPOLIS AND MISSOURI WATERWAYS MANAGEMENT ORGANIZATION.
 - EXISTING CATCH BASIN AND STORM SEWER TO BE REMOVED. REFER TO C3.0 FOR APPROXIMATE LIMITS OF REMOVAL AND NEW CATCH BASIN LOCATION. COORDINATE ALL WORK WITH CITY OF MINNEAPOLIS AND MISSOURI WATERWAYS MANAGEMENT ORGANIZATION.

GENERAL NOTES

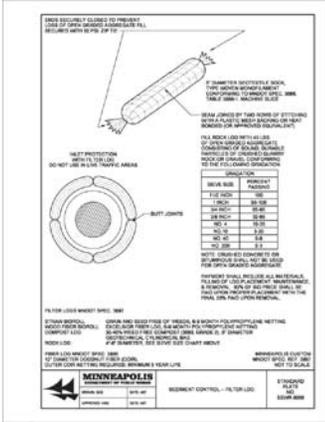
- CONCRETE CURB AND GUTTER REMOVAL, PAVEMENT REMOVAL, AND UTILITY REMOVAL LIMITS ARE TO BE COORDINATED WITH THE CITY OF MINNEAPOLIS AND UTILITY OWNER. REFER TO ALL CONSTRUCTION DOCUMENTS.
- BEFORE BEGINNING CONSTRUCTION, INSTALL A TEMPORARY TRAFFIC CONTROL PLAN WHILE WORKING WITHIN THE RIGHT-OF-WAY. THE TRAFFIC CONTROL PLAN SHALL BE APPROVED BY THE CITY ENGINEERING DEPARTMENT PRIOR TO STREET DEMOLITION.
- CONTRACTOR SHALL VISIT SITE PRIOR TO BIDDING AND REVIEW ALL CONSTRUCTION DOCUMENTS AND GEOGRAPHICAL RECORDS OF ADJACENT CONSTRUCTION. CONSTRUCTION SHALL BE ALLOWED FOR ITEMS THAT SHOULD HAVE BEEN ANTICIPATED BY THE CONTRACTOR.
- THE ROCK CONSTRUCTION ENTRANCE INDICATED ON THE PLAN IS SHOWN IN AN APPROXIMATE LOCATION. PRIOR TO START OF CONSTRUCTION, THE CONTRACTOR IS TO COORDINATE WITH THE CITY OF MINNEAPOLIS FOR THE EXACT ROCK CONSTRUCTION ENTRANCE LOCATION.



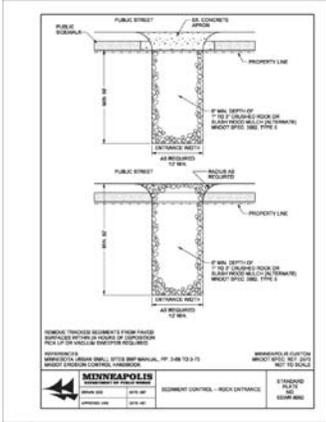
1 SELECTIVE SITE DEMOLITION PLAN
1"=20'

MINNEAPOLIS STANDARD EROSION CONTROL NOTES

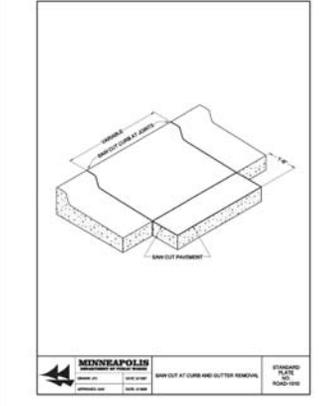
- CONTRACTOR MUST CALL A CONSTRUCTION PERMIT AS BEING PRIOR TO ANY LAND DISTURBANCES 612-673-8867. FAILURE TO DO SO MAY RESULT IN FINES, THE REVOCATION OF PERMIT AND A STOP WORK ORDER BEING ISSUED.
- INSTALL PERMANENT EROSION CONTROL AT THE LOCATIONS SHOWN ON THE PLANS PRIOR TO THE COMMENCEMENT OF ANY LAND DISTURBANCE OR CONSTRUCTION ACTIVITIES.
- BEFORE BEGINNING CONSTRUCTION, INSTALL A TEMPORARY ROCK CONSTRUCTION ENTRANCE AT EACH POINT WHERE VEHICLES EXIT THE CONSTRUCTION SITE. USE 2 INCH OR GREATER DIAMETER ROCK IN A LAYER AT LEAST 6 INCHES THICK ACROSS THE ENTIRE WIDTH OF THE ENTRANCE. EXTEND THE ROCK ENTRANCE AT LEAST 10 FEET INTO THE CONSTRUCTION ZONE USING A 60-PILE PERMANENT FABRIC BOLLARD. THE AGGREGATE TO BE USED MUST BE 3/4" OR LARGER.
- REMOVE ALL SOILS AND SEDIMENTS TRACKED OR OTHERWISE DEPOSITED ONTO PUBLIC AND PRIVATE DRIVEWAYS AREAS. REMOVAL SHALL BE ON A DAILY BASIS WHEN TRACKING OCCURS AND MAY BE ORDERED BY MINNEAPOLIS INSPECTORS AT ANY TIME IF CONDITIONS WARRANT. SWEEPING SHALL BE MAINTAINED THROUGHOUT THE DURATION OF THE CONSTRUCTION AND DONE IN A MANNER TO PREVENT TRACKING PROPERTIES.
- INSTALL INLET PROTECTION AT ALL PUBLIC AND PRIVATE CATCH BASIN INLETS, WHICH RECEIVE RUNOFF FROM THE DISTURBED AREAS. CONTRACTOR SHALL CLEAN, REPAIR, OR REPLACE EXISTING CURB AND GUTTER INLET PROTECTION DEVICES ON A ROUTINE BASIS SUCH THAT THE DEVICES ARE FULLY FUNCTIONAL FOR THE NEXT RAIN EVENT. SEDIMENT DEPOSITED IN AND/OR PLUGGING DRAINAGE SYSTEMS IS THE RESPONSIBILITY OF THE CONTRACTOR. **SLAY BARRER OR PETER FABRIC TRAPPED FILTERS ARE NOT ALLOWED FOR INLET PROTECTION.**
- LOCATE SOIL OR SILT STOCKPILES NO LESS THAN 25 FEET FROM ANY PUBLIC OR PRIVATE ROADWAY OR DRAINAGE CHANNEL. IF REMAINING FOR MORE THAN SEVEN DAYS, STAKE THE STOCKPILES TO MAINTAIN VEGETATIVE COVER, TAMPS, OR OTHER MEANS CONTROL EROSION FROM ALL STOCKPILES BY PLACING SILT BARRIERS AROUND THE PILES. TEMPORARY STOCKPILES LOCATED ON PAVED SURFACES MUST BE NO LESS THAN TWO FEET FROM THE DRAINAGE/GUTTER LINE AND SHALL BE COVERED IF LEFT MORE THAN 24 HOURS.
- MAINTAIN ALL TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES IN PLACE UNTIL THE CONTRIBUTING DRAINAGE AREA HAS BEEN STABILIZED. INSPECT TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES ON A DAILY BASIS AND REPLACE DETERIORATED, DAMAGED, OR NOTED EROSION CONTROL DEVICES IMMEDIATELY.
- TEMPORARILY OR PERMANENTLY ESTABLISH ALL CONSTRUCTION AREAS WHICH HAVE UNDERGONE FINAL GRADING, AND ALL AREAS IN WHICH GRADING OR SITE BUILDING CONSTRUCTION OPERATIONS ARE NOT ACTIVELY UNDERWAY AGAINST EROSION DUE TO RAIN, WIND AND RUNNING WATER WITHIN 7-14 DAYS. USE SEED AND MULCH, EROSION CONTROL MATTING, AND/OR EROSION CONTROL STAKES IN OPEN SPACE. REMOVE ALL TEMPORARY SYNTHETIC, STRUCTURAL, NON-Biodegradable EROSION AND SEDIMENT CONTROL DEVICES AFTER THE SITE HAS UNDERGONE FINAL STABILIZATION WITH PERMANENT VEGETATION ESTABLISHMENT. FINAL STABILIZATION FOR PURPOSES OF THIS REMOVAL IS THE ESTABLISHED COLOR CODE DESIGNATED AREA.
- READY MIXED CONCRETE AND CONCRETE BATCH/PAK PLANTS ARE PROHIBITED WITHIN THE PUBLIC RIGHT OF WAY. ALL CONCRETE RELATED PRODUCTION, CLEANING AND WASHING ACTIVITIES SHALL BE DONE IN THE DESIGNATED CONCRETE WASH/ROUTING LOCATIONS AS SHOWN IN THE EROSION CONTROL PLAN. UNDER NO CIRCUMSTANCES MAY WASHOUT WATER DRAIN ONTO THE PUBLIC RIGHT OF WAY OR INTO ANY PUBLIC OR PRIVATE STORM DRAIN CONVEYANCE.
- CHANGES TO APPROVED EROSION CONTROL PLAN MUST BE APPROVED BY THE EROSION CONTROL INSPECTOR PRIOR TO IMPLEMENTATION. CONTRACTOR TO PROVIDE INSTALLATION AND DETAILS FOR ALL PROPOSED ALTERNATE TYPE DEVICES.
- IF EROSION CONTROL PLANNING IS NECESSARY, THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ANY NECESSARY PERMITS AND/OR APPROVALS PRIOR TO DISCHARGE OF ANY WATER FROM THE SITE. IF THE DISCHARGE FROM THE DRAINING OR PUMPING PROCESS IS TURBID OR CONTAINS EXCESSIVE LOADS OF SOLIDS, IT MUST BE TREATED THROUGH USE OF SEDIMENT TRAPS, WEEDING FILTERS OR OTHER MEANS INCLUDING MEASURES SUCH THAT THE DISCHARGE IS NOT VISIBLY DIFFERENT FROM THE RECEIVING WATER. ADDITIONAL EROSION CONTROL MEASURES MAY BE REQUIRED AT THE DISCHARGE POINT TO PREVENT SCOUR PROBLEMS. THE CONTRACTOR SHALL PROVIDE A DRAINING/PUMPING PLAN TO THE EROSION CONTROL INSPECTOR PRIOR TO INITIATING DRAINING ACTIVITIES.



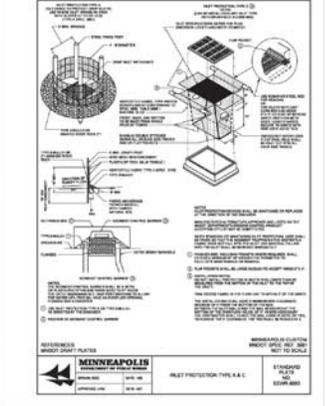
4 SEDIMENT CONTROL - FILTER LOG
NOT TO SCALE



2 SEDIMENT CONTROL - ROCK ENTRANCE
NOT TO SCALE

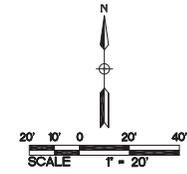


5 SAW CUT AT CURB AND GUTTER REMOVAL
NOT TO SCALE



3 INLET PROTECTION TYPE A AND C
NOT TO SCALE

ABBREVIATIONS	
BEDS	Building
BM	Bench Mark
CB	Catch Basin
CONC	Concrete
ELDV	Elevation
EX	Existing
FFE	Finished Floor Elevation
INV	Invert
LFE	Lower Floor Elevation
MW	Manhole
MH	Manhole
MN	Minimum
MB	Man Box
PM	Paving Marking
PVC	Polyvinyl Chloride
RCP	Rusticated Concrete Pipe



esg
 550 Washington Avenue South
 Minneapolis, Minnesota 55415
 P: 612.339.5501
 F: 612.339.5182
 www.esgarch.com

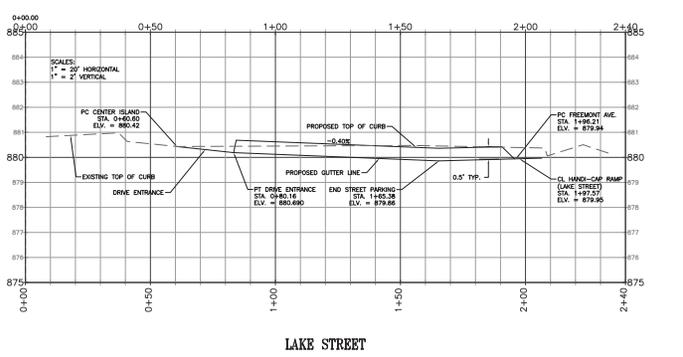
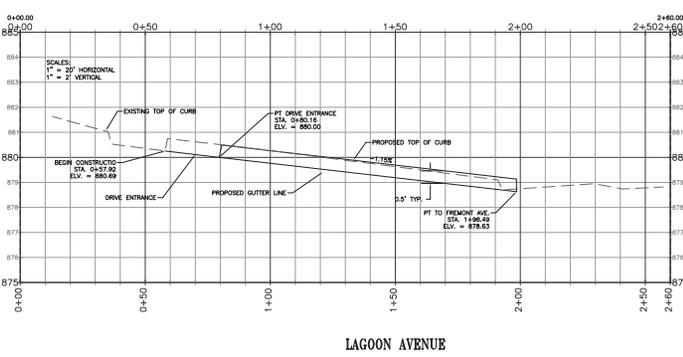
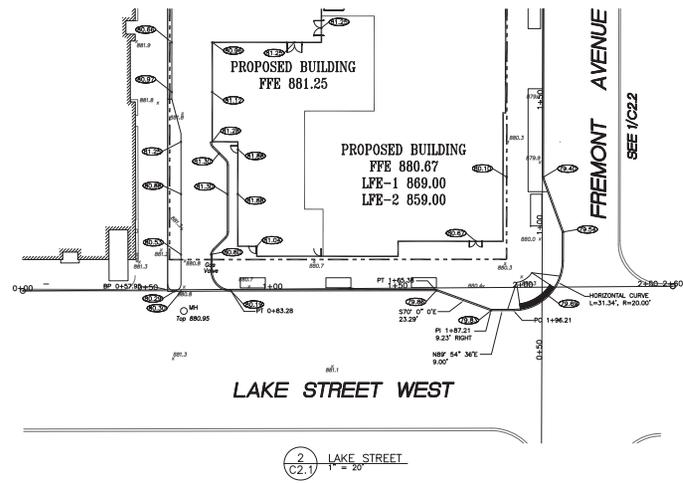
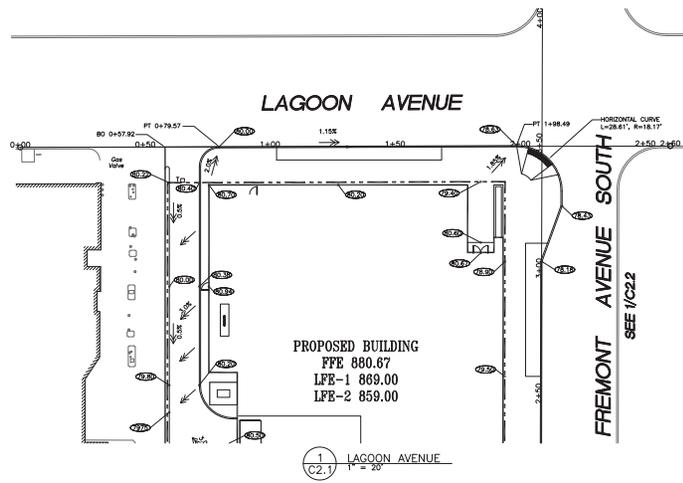


BK&M
 3800 Broadway Boulevard
 Minneapolis, MN 55425-2518
 Phone: (763) 848-0420 Fax: (763) 848-0421
 www.bkm.com

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

Signature:
 THOMAS J. CELARE
 Typed or Printed Name
 27031 July 10, 2015
 License No. 5066

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Project Status

ORIGINAL ISSUE	07-10-15	
REVISIONS		
No.	Description	Date

15394
 PROJECT NUMBER
 WH: KAM
 DRAWN BY: KAM
 CHECKED BY:
 KEY PLAN

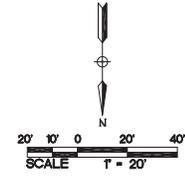


LAKE & FREMONT

LAKE STREET AND
 LAGOON AVENUE
 PLAN AND PROFILE

C2.1

ABBREVIATIONS	
BUILD	Building
BM	Bench Mark
CB	Catch Basin
CONC	Concrete
ELV	Elevation
EX	Existing
FFE	Finished Floor Elevation
FIN	Finish
LFE	Lower Floor Elevation
MN	Manhole
MN	Manhole
MN	Manhole
MB	Manhole
MB	Manhole
MB	Manhole
PC	Parking
PVC	Polyvinyl Chloride
RCP	Rusticated Concrete Pipe



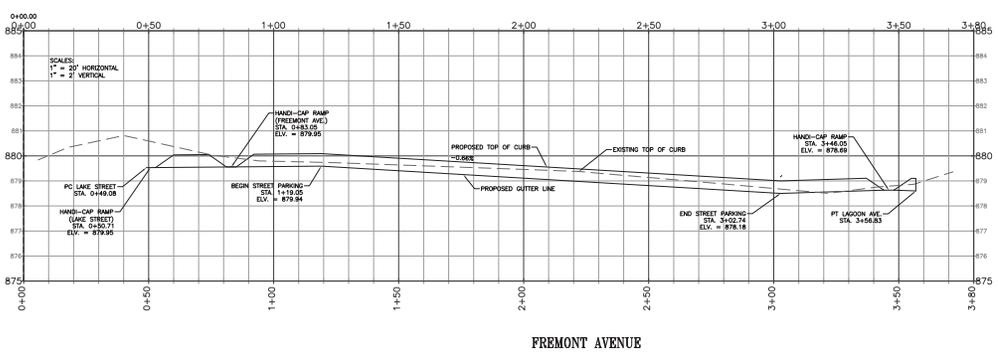
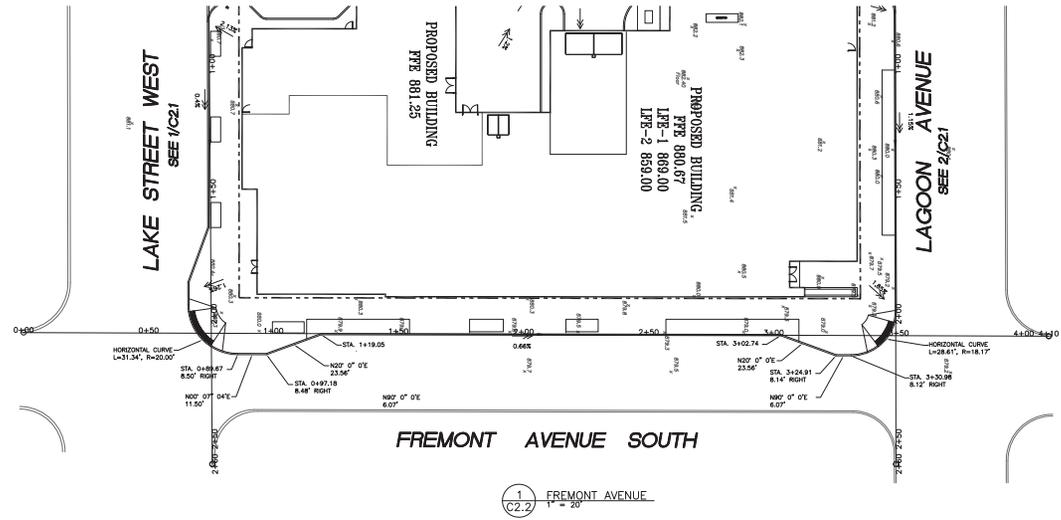
esg
 esser senner graham architects
 550 Washington Avenue South
 Minneapolis, Minnesota 55415
 P: 612.339.5501
 F: 612.339.5182
 www.esgarch.com

BK&M
 8800 Brooklyn Boulevard
 Minneapolis, MN 55425-2518
 Phone: (763) 848-0420
 Fax: (763) 848-0421
 www.bkm.com

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THOMAS J. CEDARE
 27031 July 10, 2015
 Designer

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Project Status		
ORIGINAL ISSUE	07-10-15	
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15394 PROJECT NUMBER		
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DESIGNED BY	CHECKED BY	
KEY PLAN		

LAKE & FREMONT

FREMONT AVENUE
 PLAN AND PROFILE

C2.2



chess venison graham architects
 500 Washington Avenue South
 Minneapolis, Minnesota 55411
 P: 612.339.5501
 F: 612.339.5382
 www.esgarch.com



8800 Douglas Boulevard
 Minneapolis, MN 55425-2118
 Phone: (763) 848-0420
 Fax: (763) 848-0421
 www.bkm.com

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Signature:
 THOMAS J. CLARKE
 Title of Printed Name
 27031 July 10, 2015
 License # 1000

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Project Status

ORIGINAL ISSUE	07-10-15
REVISIONS	
No. Description Date	

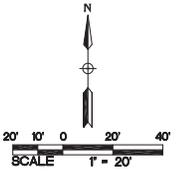
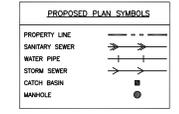
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 PROJECT NUMBER
 WH KAM
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 KEY PLAN



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UTILITY PLAN

C3.0



ABBREVIATIONS

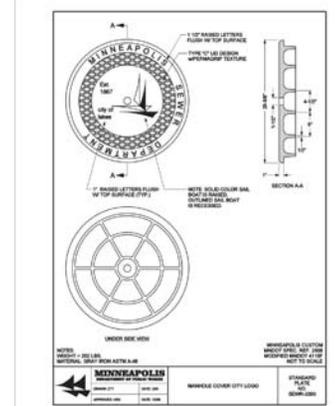
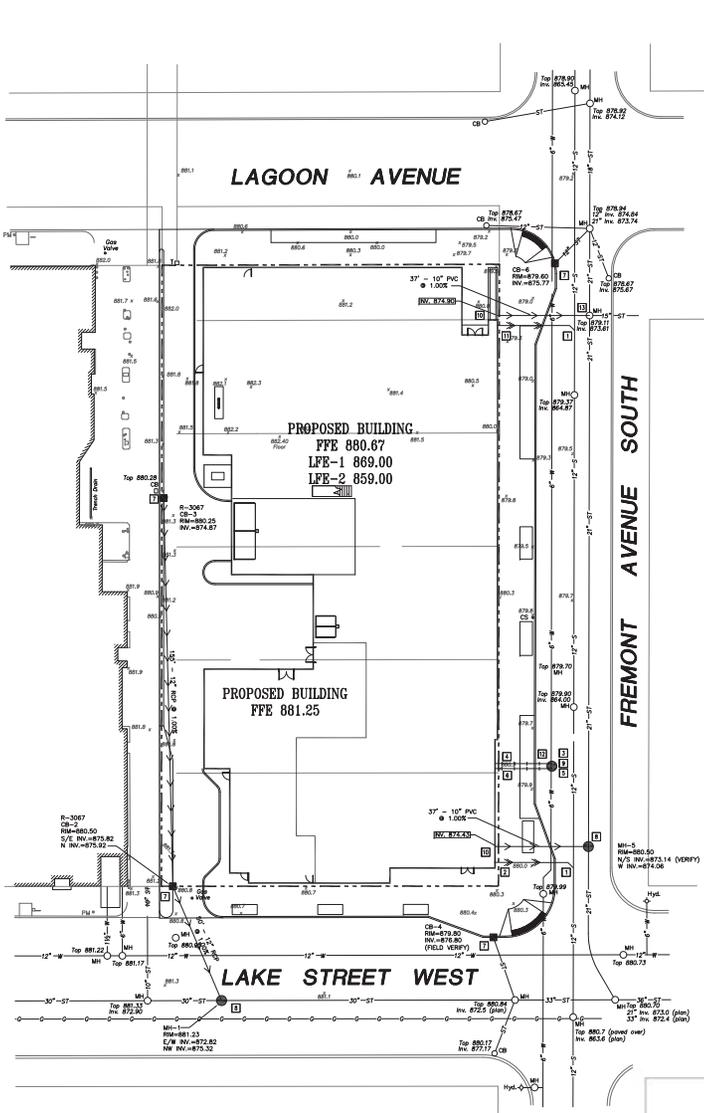
BLO	Block
BM	Bench Mark
CB	Catch Basin
CONC	Concrete
CP	Cast Iron Pipe
ELEV	Elevation
EX	Existing
FF	Finished Floor Elevation
INVT	Invert
LFE	Lower Floor Elevation
MEN	Manhole
MN	Manhole
PCF	Reinforced Concrete Pipe
RFM	Rim Elevation
RD	Road Right

UTILITY NOTES FOR WORK IN PUBLIC RIGHT-OF-WAY

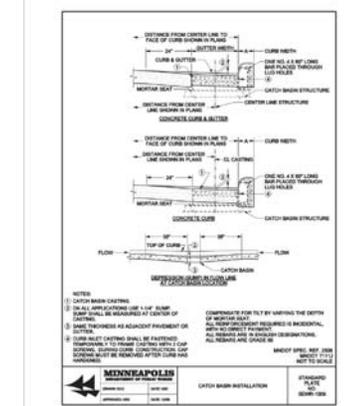
- FOLLOW ALL CITY OF MINNEAPOLIS AND HENNEPIN COUNTY STANDARDS AND SPECIFICATIONS.
- PRIOR TO CONSTRUCTION, CONTRACTORS ARE TO COORDINATE ALL WORK WITH RIGHT OF WAY AND OPEN ALL APPLICABLE PERMITS.

- KEY: NOTES ARE DENOTED BY [] ON PLAN.
- PRIOR TO START OF CONSTRUCTION THE CONTRACTOR IS TO REVISION EXISTING SANITARY SEWER SERVICES TO THE SITE FOR INVERT NOT 7' ON SHARED C.T.S. IF THE EXISTING SANITARY SEWER SERVICE LINE NOT GOING TO BE USED THEN CONDUIT TO EXISTING 12-INCH SANITARY SEWER WITH 12-INCH BY 8-INCH WYE. FOLLOW ALL CITY OF MINNEAPOLIS STANDARDS AND SPECIFICATIONS.
 - INSTALL 24-FEET OF 8-INCH SCH 40 PVC PIPE AT 1.0% TO INVERT OF 864.11; STUB TO WITHIN 5-FEET OF PROPOSED BUILDING; COORDINATE EXACT LOCATION AND INVERT ELEVATION WITH MECHANICAL CONTRACTOR PRIOR TO THE START OF CONSTRUCTION.
 - INSTALL 8-INCH WATER MAIN FOR 8-INCH FIRE SERVICE; THE CITY OF MINNEAPOLIS WILL PROVIDE AND INSTALL THE WATER MAIN AND VALVE AS PART OF THE PERMIT FEE. THE CONTRACTOR IS TO EXISTING FOR THE BETTER; CONTRACT THE WATER VALVE MANHOLE; CONNECT TO THE VALVE AND BACKFILL THE TRENCH PER CITY OF MINNEAPOLIS STANDARDS AND SPECIFICATIONS; REFER TO DETAIL 7/C3.0. PROPOSED WATER MAINS ARE POSSIBLY LOCATED IN CURB LINE. THE INVERT IS TO BE ONE FOOT ABOVE THE FINISH GRADE OF CURB LINE. COORDINATE ALL WORK WITH CITY OF MINNEAPOLIS. CONTRACTOR SHALL SUBMIT TRAFFIC CONTROL PLANS TO MATCH PRIOR TO CONSTRUCTION.
 - STUB 6-INCH FIRE PROTECTION LINE TO WITHIN 5-FEET OF PROPOSED BUILDING; FOLLOW ALL CITY OF MINNEAPOLIS STANDARDS AND SPECIFICATIONS.
 - INSTALL 4-INCH WATER MAIN FOR 4-INCH DOMESTIC SERVICE; THE CITY OF MINNEAPOLIS WILL PROVIDE AND INSTALL THE WATER MAIN AND VALVE AS PART OF THE PERMIT FEE. THE CONTRACTOR IS TO EXISTING FOR THE BETTER; CONTRACT THE WATER VALVE MANHOLE; CONNECT TO THE VALVE AND BACKFILL THE TRENCH PER CITY OF MINNEAPOLIS STANDARDS AND SPECIFICATIONS; REFER TO DETAIL 7/C3.0. PROPOSED WATER MAINS ARE POSSIBLY LOCATED IN CURB LINE. THE INVERT IS TO BE ONE FOOT ABOVE THE FINISH GRADE OF CURB LINE. COORDINATE ALL WORK WITH CITY OF MINNEAPOLIS. CONTRACTOR SHALL SUBMIT TRAFFIC CONTROL PLANS TO MATCH PRIOR TO CONSTRUCTION.
 - STUB 2-INCH DOMESTIC WATER SERVICE TO WITHIN 5-FEET OF PROPOSED BUILDING; FOLLOW ALL CITY OF MINNEAPOLIS STANDARDS AND SPECIFICATIONS.
 - INSTALL CATCH BASIN REFER TO DETAIL 3/C3.0 AND 4/C3.0.
 - INSTALL MANHOLE REFER TO DETAIL 2/C3.0.
 - CONTRACTOR TO INSTALL WATER VALVE MANHOLE FOR 8-INCH FIRE SERVICE AND 2-INCH DOMESTIC SERVICE; REFER TO DETAIL 7/C3.0.
 - LOCATION OF PROPOSED ROOF DRAIN LEAD, COORDINATE EXACT LOCATION AND INVERT ELEVATION WITH MECHANICAL CONTRACTOR PRIOR TO THE START OF CONSTRUCTION.
 - INSTALL 24-FEET OF 8-INCH SCH 40 PVC PIPE AT 1.0% TO INVERT OF 863.31; STUB TO WITHIN 5-FEET OF PROPOSED BUILDING; COORDINATE EXACT LOCATION AND INVERT ELEVATION WITH MECHANICAL CONTRACTOR PRIOR TO THE START OF CONSTRUCTION.
 - EXISTING WATER SERVICES AND/OR VALVE MANHOLES MAY REMAIN. PRIOR TO START OF CONSTRUCTION THE CONTRACTOR IS TO REFER TO NOTES 8 OR DOMESTIC C.T.S. NOTED NOTES 5, 6, 8, AND 9 ON THIS SHEET ONLY APPLY IF EXISTING WATER SERVICES AND/OR VALVE MANHOLES ARE IN USE.
 - CONNECT EXISTING WATER MAIN WHO EXISTING MANHOLE AT INVERT ELEVATION 874.53; FOLLOW ALL CITY OF MINNEAPOLIS STANDARDS AND SPECIFICATIONS FOR CONNECTION.

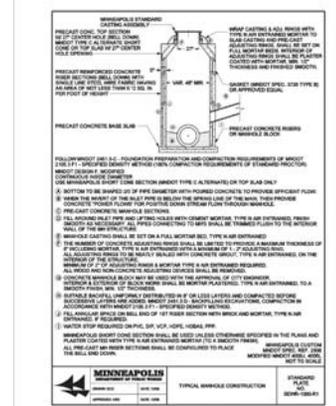
- CITY OF MINNEAPOLIS UTILITY NOTES:
- ALL WORK DONE OUTSIDE OF THE LOT LINE ON PUBLIC RIGHT-OF-WAY MUST BE DONE BY A CONTRACTOR LICENSED TO WORK IN THE CITY OF MINNEAPOLIS WHO HAS THE \$100,000.00 SIDEWALK CONTRACTOR'S BOND ON FILE WITH THE SIDEWALK DIVISION.
 - ALL UTILITIES MUST BE INSTALLED BY A MINNEAPOLIS LICENSED CONTRACTOR.
 - THE CONTRACTOR, PROPERTY OWNER OR RESPONSIBLE PARTY SHALL CONTACT MINNEAPOLIS SURVEY SERVICES AND SERVICE AS REGARDS PRIOR TO ANY EXCAVATION OR CONSTRUCTION RELATED OPERATION, 912-753-3434. FULL CONTACT INFORMATION: 912-753-3434.
 - UPON THE PROJECT'S COMPLETION, THE GENERAL CONTRACTOR SHALL PROVIDE TO THE DEPARTMENT OF PUBLIC UTILITIES MANAGEMENT SERVICE BUILDING RECORD DRAWINGS. THIS REPORT WILL SERVE AS A MEANS OF VERIFICATION THAT THE INTENT OF THE APPROVED STANDARD DESIGN HAS BEEN MET. THE PLAN REPORT SHALL SUBSTANTIATE THAT ALL ASPECTS OF THE ORIGINAL DESIGN HAVE BEEN ADEQUATELY PROVIDED FOR BY THE CONSTRUCTION OF THE PROJECT.
- UTILITY NOTES:
- CONDUIT SERVICE CONNECTION LOCATIONS AT THE BUILDING WITH THE MECHANICAL CONTRACTOR PRIOR TO CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR UNCOORDINATED WORK.
 - ALL SERVICE CONNECTIONS WITH LESS THAN 5 FEET OF COVER OVER THE TOP OF PIPE ARE TO BE INSTALLED. PROTECTION SHALL BE INSTALLED FROM THE CONNECTION OF THE SERVICE AS THE BUILDING TO THE POINT WHICH THE SERVICE ATTAINS 5 FEET OF COVER. CONTRACTOR SHALL OBTAIN WRITTEN PERMISSION FROM ARCHITECT OR ENGINEER PRIOR TO INSTALLATION OF INSULATION.
 - PROTECT ALL EXISTING STRUCTURES AND UTILITIES WHICH ARE NOT SCHEDULED TO BE REMOVED.
 - ALL SEWER AND WATER CROSSINGS SHALL HAVE A MINIMUM VERTICAL SEPARATION OF 1.5 FEET AND HORIZONTAL SEPARATION OF 1 FEET. FOLLOW ALL HEALTH DEPARTMENT AND CITY OF MINNEAPOLIS AND HENNEPIN COUNTY STANDARDS.
 - ALL WATER MAIN SHALL BE DUCTILE IRON PIPE, CLASS 50, UNLESS NOTED OTHERWISE.
 - ALL WATER MAIN SHALL HAVE A MINIMUM DEPTH OF COVER OF 7.5 FEET OVER TOP OF WATER MAIN.
 - PROVIDE THURST BLOSSOM ON ALL WATER MAIN PER CITY OF MINNEAPOLIS. PROVIDE MECHANICAL JOINT RESTRAINTS ON ALL BENDS, VALVES, TEES, FLANGES AND HYDRANT LEADS.
 - SANITARY SEWER PIPING SHALL BE SCH 40 PVC FOR MINNEAPOLIS UNLESS NOTED OTHERWISE. STORM SEWER PIPING SHALL BE REINFORCED CONCRETE PIPE (RCP), UNLESS NOTED OTHERWISE. ALL 18-INCH THROUGH 24-INCH RCP SHALL BE CLASS 3 UNLESS NOTED OTHERWISE. ALL STORM SEWER PIPE THAT IS EXPOSED TO ROAD TRAFFIC SHALL BE CLASS 5 UNLESS NOTED OTHERWISE.
 - CONTRACTORS ARE TO COORDINATE ALL WORK WITH GAS, ELECTRIC, TELEVISION AND TELEPHONE COMPANIES PRIOR TO START OF CONSTRUCTION.
 - ALL PORTIONS OF THE STORM SEWER SYSTEM LOCATED WITHIN 10- FEET OF THE BUILDING OR WATER SERVICE LINE SHALL BE TESTED IN ACCORDANCE WITH ALL PLUMBING AND WATER MAINS USED TO MAKE WATER TIGHT CONNECTIONS TO MANHOLES, CATCH BASINS, AND OTHER STRUCTURES. GREAT CARE IS AN EXCEPTIONAL ALTERNATE. GREAT CARE JOINTS ARE PERMITTED ONLY FOR REPAIRS AND CONNECTIONS OF EXISTING LINES CONSTRUCTED WITH SUCH JOINTS.



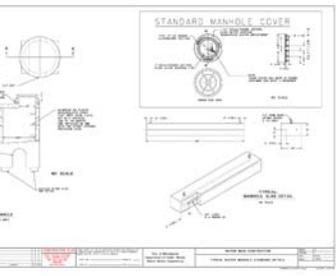
6 MANHOLE COVER CITY LOGO NOT TO SCALE



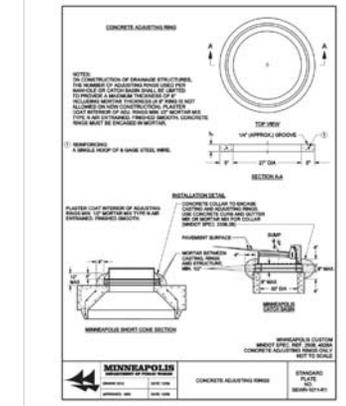
4 CATCH BASIN INSTALLATION NOT TO SCALE



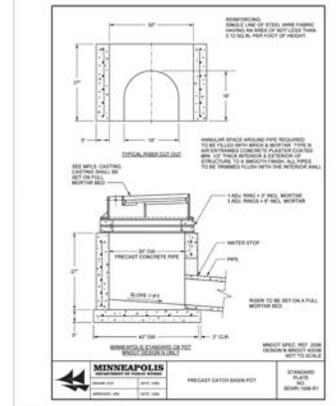
2 TYPICAL MANHOLE CONSTRUCTION NOT TO SCALE



7 WATER MAIN CONSTRUCTION NOT TO SCALE



5 CONCRETE ADJUSTING RINGS NOT TO SCALE



3 PRECAST CATCH BASIN POT NOT TO SCALE

1 UTILITY PLAN 1" = 20'



esg
 elness swanson graham architects
 500 washington avenue south
 minneapolis minnesota 55415
 P: 612.339.1508
 F: 612.339.5382
 www.esgarch.com

BKBM
 8800 Brooklyn Boulevard
 Minneapolis, MN 55425-2918
 Phone: (763) 845-2400
 Fax: (763) 845-0421
 www.bkbn.com

800 888 8888, 1-800-888-8888
 800 888 8888, 1-800-888-8888
 800 888 8888, 1-800-888-8888

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

Signature
 THOMAS J. CESARE
 PE/Professional Engineer
 27931 July 16, 2015
 CESARE Date

NOT FOR CONSTRUCTION

Project Status

ORIGINAL ISSUE: 07-10-15

REVISIONS

No. Description Date

15394
 PROJECT NUMBER

WH KAM
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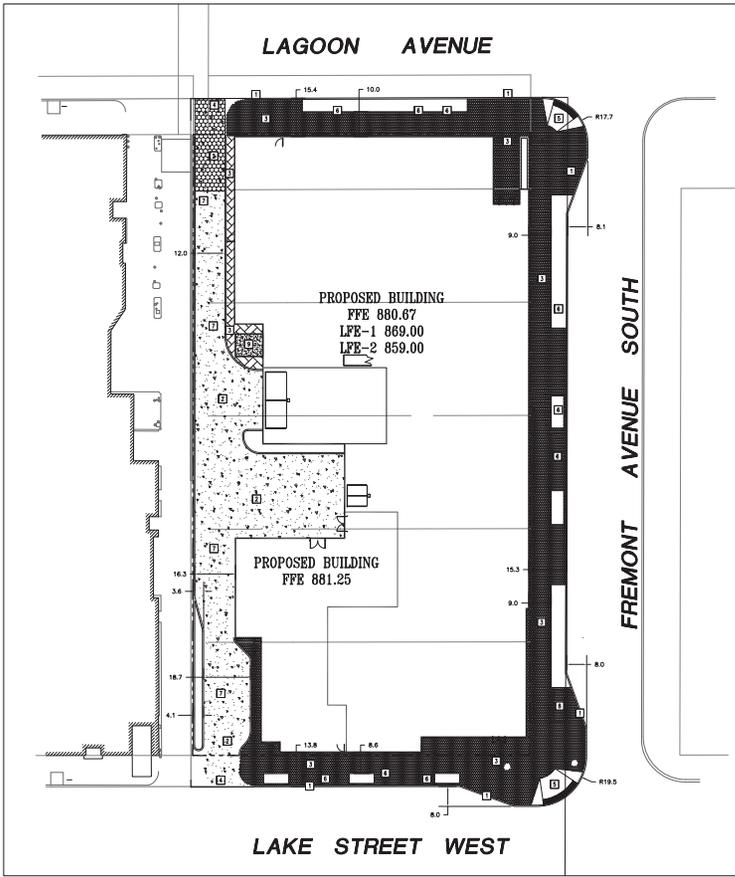
KEY PLAN



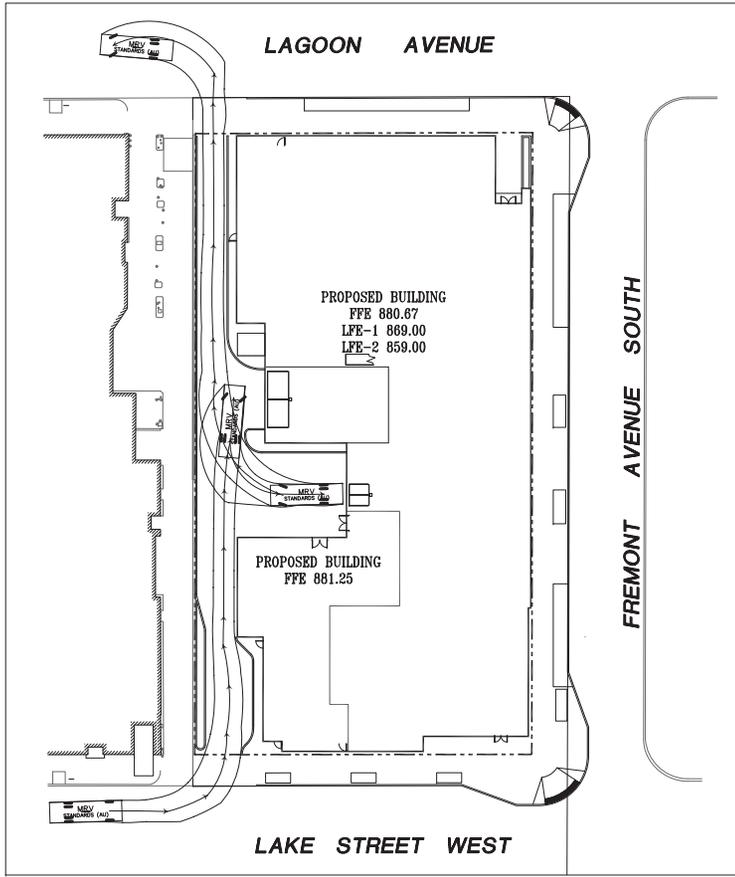
LAKE & FREMONT

PAVING AND GEOMETRIC PLAN

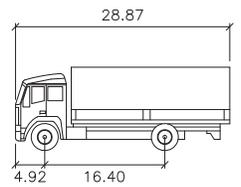
C4.0



1 PAVING AND GEOMETRIC PLAN
 C4.0 1" = 20'



2 TRUCK TURNING MOVEMENT PLAN
 C4.0 1" = 20' - 28' DELIVERY VEHICLE



MRV
 feet
 Width : 8.20
 Track : 8.20
 Lock to Lock Time : 6.0
 Steering Angle : 34.0

3 DELIVERY TRUCK VEHICLE PROFILE
 C4.0 NOT TO SCALE

PROPOSED PLAN SYMBOLS

PROPERTY LINE	---
BITUMINOUS PAVEMENT	[Symbol]
CONCRETE PAVEMENT/SIDEWALK	[Symbol]
SEE LANDSCAPE PLANS FOR PAVEMENT DETAILS	[Symbol]
PEDESTRIAN CURB RAMP	[Symbol]
SIGN	[Symbol]

NOTE: CONSTRUCTION LIMITS ARE ASSUMED TO BE PROPERTY LINE UNLESS OTHERWISE SHOWN.

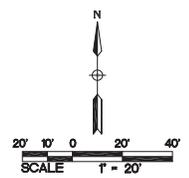
ABBREVIATIONS

FIELD	Benchmark
BM	Benchmark
CONC	Concrete
ELEV	Elevation
EX	Existing
FTE	Finished Floor Elevation
LFE	Lower Floor Elevation
MAX	Maximum
PS	Pay Box
PM	Parking Meter
R	Radius
Typical	Typical
MINUTCO	Minnesota Manual on Uniform Traffic Control Devices

- GENERAL NOTES:**
1. ALL DIMENSIONS ARE TO FACE OF CURB UNLESS NOTED OTHERWISE.
 2. ALL CURB AND GUTTER IS TO BE BREA CONCRETE CURB AND GUTTER UNLESS NOTED OTHERWISE.
 3. NO SIDEWALK IS TO HAVE MORE THAN A 2% CROSS SLOPE OR MORE THAN A 5% LONGITUDINAL SLOPE.
 4. REFER TO ARCHITECTURAL PLANS FOR PROPOSED BUILDING LAYOUT.
 5. FOLLOW ALL CITY OF MINNEAPOLIS AND HENNING COUNTY RULES, REGULATIONS AND SPECIFICATIONS WHEN WORKING IN PUBLIC RIGHT OF WAY.
 6. NO PONDING OF WATER OR ABRUPT TRANSITIONS WILL BE ALLOWED WHERE NEW PAVEMENT/CURB/SIDEWALK MATCHES INTO EXISTING PAVEMENT/CURB/SIDEWALK.
 7. THE CONTRACTOR IS TO CONTACT THE CITY OF MINNEAPOLIS FIRE MARSHAL FOR THE EXACT PLACEMENT OF FIRE LANE, YELLOW-PAINTED CURBING AND NO PARKING AREAS FOR FIRE PROTECTION PURPOSES.

- PAVING NOTES:**
1. REFER TO STRUCTURAL PLANS FOR SToop DETAILS. ALL WALKS ARE TO BE CENTERED ON THE DOORS.
 2. INSTALL APPROPRIATE EXPANSION MATERIAL WHERE CONCRETE IS ADJACENT TO BUILDING FACE.
 3. SAWCUT AND MATCH NEW BITUMINOUS PAVEMENT INTO EXISTING PAVEMENT. NO ABRUPT DRIVE TRANSITIONS OR PONDING OF WATER WILL BE ALLOWED.
 4. MATCH NEW CONCRETE CURB AND GUTTER INTO EXISTING. FOLLOW ALL CITY OF MINNEAPOLIS STANDARDS AND SPECIFICATIONS FOR CURB TYPE, MATERIAL, AND INSTALLATION METHODS.
 5. SAWCUT EXISTING BITUMINOUS PAVEMENT AND CURB AND GUTTER TO NEAREST JOINT. COORDINATE MATCH LINES WITH SITE EMISSION CONTRACTOR AND CONSTRUCTION MANAGER. INSTALL DRIVE ENTRANCE FOR CITY OF MINNEAPOLIS STANDARDS AND SPECIFICATIONS. FOLLOW ALL CITY OF MINNEAPOLIS AND HENNING COUNTY REQUIREMENTS FOR TRAFFIC CONTROL.

- NOTED NOTES**
- NOTED NOTES ARE DENOTED BY [Symbol] ON PLAN.
- [Symbol] INSTALL MESH CONCRETE CURB AND GUTTER. REFER TO DETAIL 1/C/S.1.
 - [Symbol] INSTALL CONCRETE PAVEMENT. REFER TO DETAIL 7/C/S.1.
 - [Symbol] INSTALL CONCRETE MALK. REFER TO DETAIL 8/C/S.1 AND 8/C/S.1. REFER TO LANDSCAPE PLANS FOR CONCRETE FINISH AND JOINTING.
 - [Symbol] INSTALL CONCRETE DRIVE ENTRANCE. REFER TO DETAIL 2/C/S.1.
 - [Symbol] INSTALL PEDESTRIAN CURB RAMP. REFER DETAIL 10/C/S.1, 11/C/S.1, 12/C/S.1, 13/C/S.1, AND 14/C/S.1.
 - [Symbol] INSTALL SAWCUT PARKING METER AND PAY BOXES. COORDINATE WITH CITY OF MINNEAPOLIS.
 - [Symbol] INSTALL CONCRETE ALLEY. REFER TO DETAIL 6/C/S.1.
 - [Symbol] INSTALL BIKE BOLLARDS. REFER TO LANDSCAPE PLANS.
 - [Symbol] INSTALL CONCRETE TRANSFORMER PAD. REFER TO DETAIL 8/C/S.1.



NOTE: ALL SIDEWALK, CURB AND GUTTER, DRIVEWAY ENTRANCES, AND ALLEYS ARE TO BE CONSTRUCTED FOR:
 STANDARD SUPPLEMENTAL SPECIFICATIONS FOR CONSTRUCTION OF PUBLIC INFRASTRUCTURE IN THE CITY OF MINNEAPOLIS 2011 EDITION
 DEPARTMENT OF PUBLIC WORKS
 MINNEAPOLIS, MINNESOTA
 FEBRUARY 11, 2011



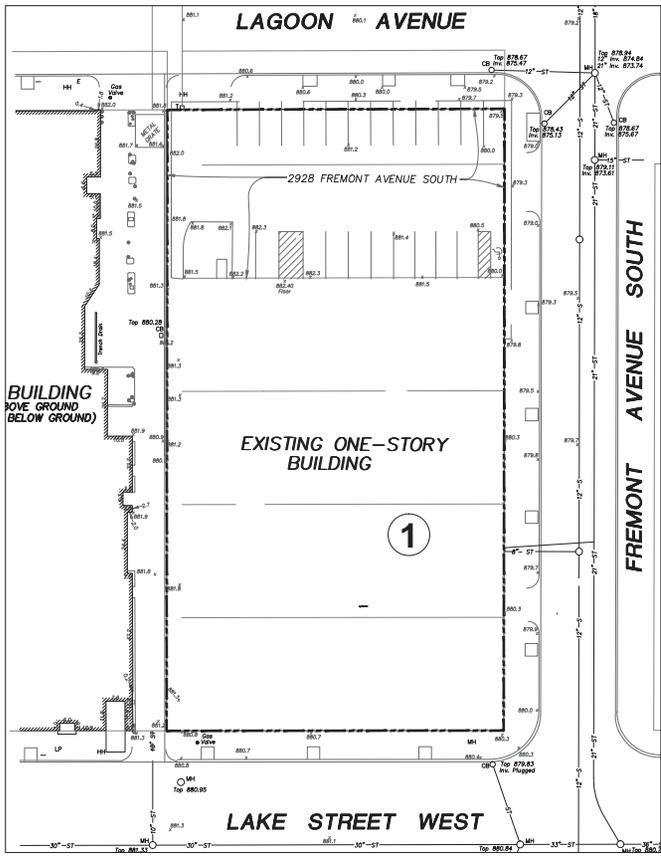
dhess (evanston) gluhm architects
 500 washington avenue south
 minneapolis, minnesota 55415
 phone: (612) 339-5508
 f: (612) 339-3382
 www.esgarch.com



2020 Franklin Boulevard
 Minneapolis, MN 55405-2518
 Phone: (763) 864-0421
 Fax: (763) 864-0421
 www.bkm.com

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.
 Signature: THOMAS J. CELAR
 Title or Printed Name: 27931 July 10, 2015
 License #, Date

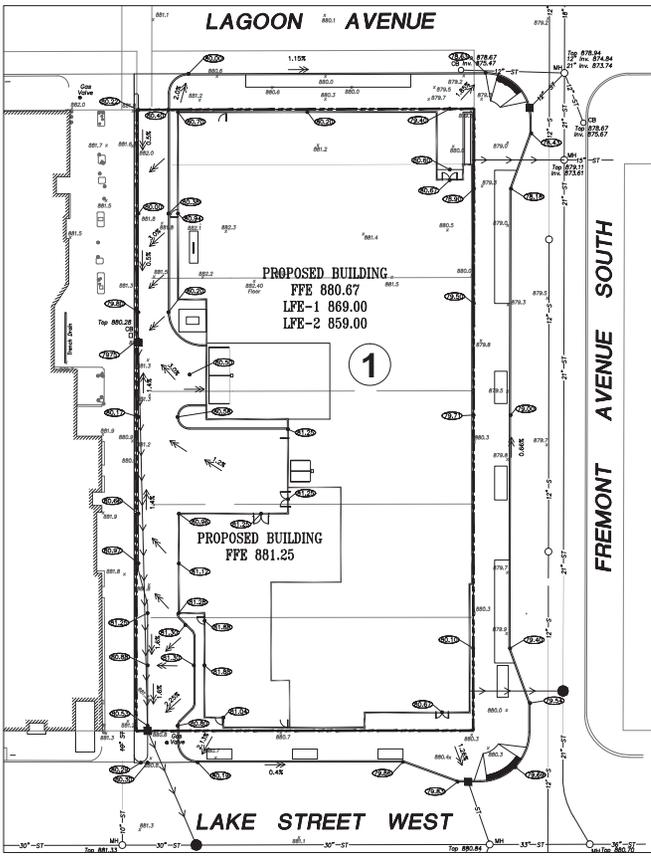
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1 EXISTING CONDITIONS
 1" = 20'

EXISTING DRAINAGE AREAS					
DRAINAGE AREA	IMPERVIOUS AREA (ACRES)	PERVIOUS AREA (ACRES)	TOTAL AREA (ACRES)	2-YEAR (2.85") 100-YEAR (7.49")	ROUTING
1	0.77	0.00	0.77	3.19	4.83
TOTAL	0.77	0.00	0.77	3.19	4.83

STORMWATER RUNOFF SUMMARY			
	2-YR STORM (2.85") RUNOFF (CFS)	10-YR STORM (4.28") RUNOFF (CFS)	100-YR STORM (7.49") RUNOFF (CFS)
EXISTING SITE	3.19	4.83	8.48
PROPOSED SITE	3.19	4.83	8.48



2 PROPOSED CONDITIONS
 1" = 20'

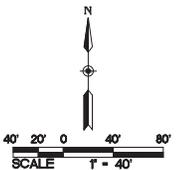
PROPOSED DRAINAGE AREAS					
DRAINAGE AREA	IMPERVIOUS AREA (ACRES)	PERVIOUS AREA (ACRES)	TOTAL AREA (ACRES)	2-YEAR (2.85") 100-YEAR (7.49")	ROUTING
1	0.77	0.00	0.77	3.19	4.83
TOTAL	0.77	0.00	0.77	3.19	4.83

MINNEAPOLIS VICINITY MAP
 COPYRIGHT GOOGLE MAPS



ABBREVIATIONS

BLDG	Building
BMP	Best Management Practice
CB	Catch Basin
CONC	Concrete
ELEV	Elevation
EX	Existing
FTE	Finished Floor Elevation
FIN	Finish
LMF	Lower Floor Elevation
MAX	Maximum
MIN	Minimum
MPC	Minnesota Pollution Control Agency
PVC	Polyvinyl Chloride
RCP	Reinforced Concrete Pipe



STORMWATER AND EROSION CONTROL MAINTENANCE:
 EXPOSED SOIL AREAS: ONCE EVERY 7 DAYS AND WITHIN 24 HOURS FOLLOWING A 1/2 INCH STABILIZED AREAS: ONCE EVERY 30 DAYS.
 FROSTEN GROUND: AS SOON AS RESUMING OCCURS OR PRIOR TO RESUMING CONSTRUCTION.
 RECORDS: A COPY OF THE GRADING, DRAINAGE EROSION CONTROL PLAN AND WATERFIED DATA & BMPPP PLANS AS WELL AS THE INSPECTION/MAINTENANCE LOGS ARE TO BE KEPT EITHER IN THE FIELD OFFICE, INSPECTOR'S VEHICLE OR CONTRACTOR'S VEHICLE.

FINAL STABILIZATION:
 STABILIZATION BY UNIFORM PERENNIAL VEGETATIVE COVER (50% DENSITY).
 DRAINAGE DITCHES STABILIZED.
 ALL TEMPORARY SYNTHETIC AND STRUCTURAL BMP'S REMOVED.
 CLEAN OUT SEDIMENT FROM CONDUITS AND STABILIZATION BASINS (RETURN TO DESIGN CAPACITY).

STOCKPILES:
 TEMPORARY STOCKPILES MUST HAVE SELF FENCE OR OTHER SPECIFIC SEDIMENT CONTROL. CANNOT BE PLACED IN ANY NATURAL BUFFERS OR SURFACE WATER. INCLUDE STOCKPILER CONTAINERS SUCH AS CURB AND GUTTER SYSTEMS, OR CONCRETE AND DITCHES UNLESS STOCKPILER BYPASS IN PLACE FOR THE STOCKPILER.

HANDLING AND STORAGE OF HAZARDOUS MATERIALS:
 IF THE CONTRACTOR INTENDS TO USE POLYMERIC FLUOCALCANS OR OTHER SEMI-DURABLE TREATMENT CHEMICALS ON THE PROJECT SITE, THE CONTRACTOR MUST COMPLY WITH THE FOLLOWING MINIMUM REQUIREMENTS:

1. THE CONTRACTOR MUST USE CONVENTIONAL EROSION AND SEDIMENT CONTROL PRIOR TO CHEMICAL APPLICATION TO INSURE EFFECTIVE TREATMENT. CHEMICALS MAY ONLY BE APPLIED WHERE TREATED STOCKPILER IS DIRECTED TO A SEDIMENT CONTROL SYSTEM WHICH ALLOWS FOR FILTRATION OR SETTLEMENT OF THE FLOC PRIOR TO DISCHARGE.
2. CHEMICALS MUST BE SELECTED THAT ARE APPROXIMATELY SUITED TO THE TYPES OF SOILS LIKELY TO BE EXPOSED DURING CONSTRUCTION AND TO THE EXPECTED LIABILITY. PH AND FLOW RATE OF STOCKPILER FLOWING INTO THE CHEMICAL TREATMENT SYSTEM OR AREA.
3. CHEMICALS MUST BE USED IN ACCORDANCE WITH ACCEPTED ENGINEERING PRACTICES AND WITH OODING SPECIFICATIONS AND SEDIMENT REMOVAL DESIGN SPECIFICATIONS PROVIDED BY THE MANUFACTURER OR PRODUCER/OUTLETER OF THE APPLICABLE CHEMICALS.

AGENCY CONTACTS:
 CITY OF MINNEAPOLIS
 SURFACE WATERS AND SEWERS
 7811 EAST 28TH STREET
 MINNEAPOLIS, MN 55407
 PHONE: (612) 673-2400

OWNED:
 OWNER NAME, COMPANY NAME, ADDRESS AND PHONE NUMBER

PROJECT NARRATIVE:
 EXISTING SITE DESCRIPTION --- THE PROJECT IS LOCATED BETWEEN LAKE STREET WEST AND LAGOON AVENUE ALONG FREMONT AVENUE IN THE CITY OF MINNEAPOLIS, MINNESOTA. THE SITE HAS A TOTAL APPROXIMATE AREA OF 0.77 ACRES. CURRENTLY THE SITE CONTAINS A ONE STORY COMMERCIAL BUILDING COVERING 23,320 SF WITH AN AREA OF 10,000 SF OF UNDERGROUND PARKING. THERE ARE NO STOCKPILERS ON THE SITE. THE STORMWATER SHEET FLOWS TO STORM SEWER STRUCTURES LOCATED IN LAGOON AVENUE, FREMONT AVENUE AND LAKE STREET WEST.

PROPOSED SITE DESCRIPTION:
 THE PROPOSED SITE WILL HAVE AN APPROXIMATE IMPERVIOUS AREA OF 0.77 ACRES. THE PROJECT CONSISTS OF A 6 STORY MULTI USE BUILDING, WITH 2 LEVELS OF UNDERGROUND PARKING. STORMWATER ON THIS SITE SHEET FLOWS TO STORM SEWER STRUCTURES LOCATED IN LAGOON AVENUE, FREMONT AVENUE AND LAKE STREET WEST. STORMWATER FROM THE BUILDING WILL BE COLLECTED THROUGH ROOF DRAINS AND OUTLET INTO THE EXISTING STORM SEWER ALONG FREMONT AVENUE.

INSPECTIONS:
 EXPOSED SOIL AREAS: ONCE EVERY 7 DAYS AND WITHIN 24 HOURS FOLLOWING A 1/2 INCH STABILIZED AREAS: ONCE EVERY 30 DAYS.
 FROSTEN GROUND: AS SOON AS RESUMING OCCURS OR PRIOR TO RESUMING CONSTRUCTION.
 RECORDS: A COPY OF THE GRADING, DRAINAGE EROSION CONTROL PLAN AND WATERFIED DATA & BMPPP PLANS AS WELL AS THE INSPECTION/MAINTENANCE LOGS ARE TO BE KEPT EITHER IN THE FIELD OFFICE, INSPECTOR'S VEHICLE OR CONTRACTOR'S VEHICLE.

CONSTRUCTION ACTIVITY EROSION PREVENTION PRACTICES:
 CONTRACTOR SHALL STABILIZE ALL EXPOSED SOIL AREAS (INCLUDES STOCKPILER). STABILIZATION MUST BE INITIATED IMMEDIATELY TO LIMIT SOIL EROSION. PREVENTER OR TEMPORARY CONSTRUCTION ACTIVITY IS PERMANENTLY OR TEMPORARILY CEASED. ALL PORTION OF THE SITE AND WILL NOT RESUME FOR A PERIOD EXCEEDING 7 CALENDAR DAYS. STABILIZATION MUST BE COMPLETED NO LATER THAN 7 CALENDAR DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.

FOR PUBLIC WATER THAT THE DNR HAS PROMULGATED "WORK IN WATER RESTRICTIONS" DURING SPECIFIED FISH SPawning THE FRAMES, ALL EXPOSED SOIL AREAS THAT ARE WITHIN 200 FEET OF THE WATER'S EDGE AND ADJACENT TO THESE WATERS MUST COMPLETE THE STABILIZATION ACTIVES WITHIN 24 HOURS DURING THE RESTRICTION PERIOD.
 PIPE OUTLETS MUST BE PROVIDED WITH TEMPORARY OR PERMANENT CONTROL STRUCTURE AFTER CONNECTION TO A SURFACE WATER.
 SEDIMENT CONTROL MEASURES MUST BE INSTALLED ON ALL LOW-SPREADING PERIMETERS TO PREVENT ADJACENT LAND DISTURBING ACTIVITIES BEGIN.

POLLUTION PREVENTION MANAGEMENT MEASURES:
 SOLID WASTE DISPOSED PROPERLY; COMPLY WITH MPCA REQUIREMENTS.
 HAZARDOUS WASTE STORED (SECONDARY CONTAINMENT, RESTRICTED ACCESS) AND DISPOSED IN COMPLIANCE WITH MPCA REQUIREMENTS.

EXTERNAL WASHING OF TRUCKS AND OTHER CONSTRUCTION VEHICLES MUST BE LIMITED TO A DEFINED AREA OF THE SITE. RINSEWATER MUST BE CONTAINED AND WASTE PROPERLY DISPOSED. NO ENGINE OILDRAGING ALLOWED ON-SITE. CONCRETE WASHOUT ON-SITE ALL LIQUID AND SOLID WASTE GENERATED BY CONCRETE WASHOUT OPERATIONS MUST BE CONTAINED IN A LEAK-PROOF CONTAINMENT FACILITY OR THAT DOES NOT ALLOW LIQUIDS TO ENTER GROUND WATER IS CONSIDERED AN IMPERMISSIBLE USE. THE LIQUID AND SOLID WASTES MUST NOT CONTACT THE PERMANENT AND MUST NOT BE TRANSPORTED FROM THE CONCRETE WASHOUT OPERATIONS OF AREAS. LIQUID AND SOLID WASTES MUST BE DISPOSED OF PROPERLY IN COMPLIANCE WITH MPCA REGULATIONS. A SIGN MUST BE INSTALLED ADJACENT TO EACH WASHOUT FACILITY TO INFORM CONCRETE EQUIPMENT OPERATORS TO UTILIZE THE PROPER FACILITIES. THE CONCRETE WASHOUT AREA INDICATED ON THE PLANS IS SHOWN IN AN APPROXIMATE LOCATION. PRIOR TO THE START OF CONSTRUCTION, THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION IN ACCORDANCE WITH MPCA REQUIREMENTS.

MECHANICAL AND NON STORMWATER DISCHARGES, EXISTING AND PROPOSED:
 1. WATER LINE FLUSHING
 2. LANDSCAPE IRRIGATION
 3. DISCHARGE FROM POTABLE WATER SOURCES
 4. FOUNDATION DRAIN
 5. AIR CONDENSING CONDENSATION
 6. FOOTING DRAINING
 7. FLOORING WASH WATERS WITHOUT DETERGENTS.

Project Status

ORIGINAL ISSUE: 07-10-15

REVISIONS
 No. Description Date

15394 PROJECT NUMBER
 WH KAM
 DRAWN BY CHECKED BY



LAKE & FREMONT

STORM WATER DRAINAGE PLAN

C5.0

Landscape Notes:

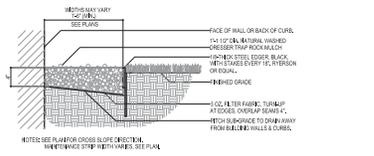
- Landscape contractor to provide nursery pull list (all of bedding) including plant species and sizes shipped to the site. Additionally, the landscape contractor shall provide nursery stock traceability, proving none of the materials provided contain or are genetic strains of the neonicotinoid family including acetamiprid, clothianidin, imidacloprid, nitenpyram, nithiazine, thiodiazinon and thiamethoxam.
- Refer to civil plans for grading, drainage, site dimensions, survey, tree removal, proposed utilities & erosion control.
- All plant material shall comply with the latest edition of the American Standard for Nursery, American Association of Nurserymen. Unless noted otherwise, deciduous shrubs shall have at least 5 canes at the specified shrub height. Plant material shall be delivered as specified.
- Plan takes precedence over plant schedule if discrepancies in quantities exist.
- All proposed plants shall be located and staked as shown.
- Adjustment in location of proposed plant material may be needed in field. Should an adjustment be required, the client will provide field approval. Significant changes may require site review and approval.
- The project landscape contractor shall be held responsible for watering and properly handling all plant materials brought on the site both before and after installation. Schedule plant deliveries to coincide with expected installation time within 36 hours.
- All plant materials shall be fertilized upon installation as specified.
- The landscape contractor shall provide the owner with a watering schedule appropriate to the project site conditions and to plant material growth requirements.
- If the landscape contractor is concerned or perceives any deficiencies in the plant selections, soil conditions, drainage or any other site condition that might negatively affect plant establishment, survival or guarantee, they must bring these deficiencies to the attention of the landscape architect & client prior to bid submission. Plant bed drainage concerns during plant installation shall be brought to the attention of the Owner and General Contractor immediately.

Planting Palette:

DECIDUOUS OVERSTORY TREES:					
Qty.	Key	Scientific Name	Common Name	Size	Root / Notes
3	A	Quercus bicolor	Swamp White Oak	2.5' Cal.	B&B
5	B	Ulmus americana 'Princeton'	Princeton Elm	2.5' Cal.	B&B

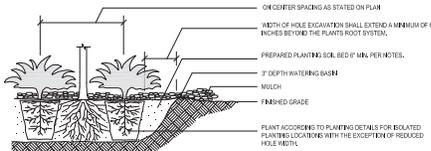
DECIDUOUS UNDERSTORY TREES:					
Qty.	Key	Scientific Name	Common Name	Size	Root / Notes
3	C	Syringa reticulata	Japanese Tree Lilac	2' Cal.	B&B

SHRUBS + GRASSES:					
Qty.	Key	Scientific Name	Common Name	Size	Root / Notes
5	L	Miscanthus sinensis 'Purpureus	Purple Flame Grass	#5/30' Hgt.	POT
58	M	Echinacea purpurea	Purple Cone Flower	#1/12' Hgt.	POT
8	N	Calamagrostis x acutifolia 'Karl Foerster'	Feather Reed Grass	#5/30' Hgt.	POT
64	O	Broussiera micrantha 'Palace Purple'	Palace Purple Coral Bells	#1/12' Spt.	POT
29	P	Nepeta racemosa 'Walker's Low'	Walker's Low Catmint	#1/12' Spt.	POT



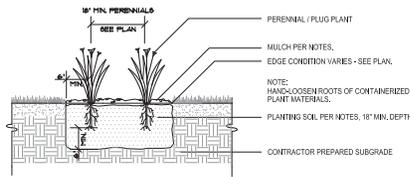
1 MINERAL MULCH MAINTENANCE STRIP

L1.2 NOT TO SCALE



2 TYP. SHRUB PLANTING - SECTION

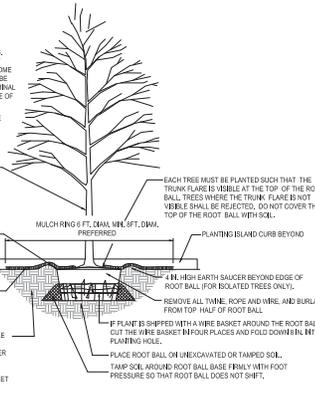
L1.2 NOT TO SCALE



3 TYP. PERENNIAL/PLUG PLANTING - SECTION

L1.2 NOT TO SCALE

NOTES:
DO NOT HEAVILY PRUNE THE TREE AT PLANTING. PRUNE ON 1 CROSSOVER LIMBS, DISJOINTED LEADERS, AND BROKEN OR DEAD BRANCHES. SOME INTERIOR TWIGS AND LATERAL BRANCHES MAY BE PRUNED; HOWEVER, DO NOT REMOVE THE TERMINAL BUDS OF BRANCHES THAT EXTEND TO THE EDGE OF THE CROWN.
FIELD STAKE TREES FOR THE APPROVAL OF THE LANDSCAPE ARCHITECT
WRAP TREE TRUNKS BY NOV. 15TH AND REMOVE BY MAY 1ST.
MARK THE NORTH SIDE OF THE TREE IN THE NURSERY AND NOTATE TREE TO FACE NORTH AT THE SITE WHENEVER POSSIBLE.



4 DECIDUOUS TREE PLANTING - PLANTING ISLAND

L1.2 NOT TO SCALE

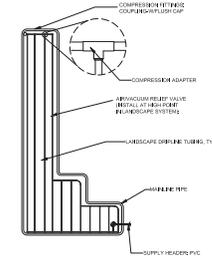
- Contractor shall establish to his/her satisfaction that soil and compaction conditions are adequate to allow for proper drainage at and around the building site.
- Contractor is responsible for ongoing maintenance of all newly installed material until time of owner acceptance. Any acts of vandalism or damage which may occur prior to owner acceptance shall be the responsibility of the contractor. Contractor shall provide the owner with a maintenance program including, but not limited to, pruning, fertilization and disease/pest control.
- The contractor shall guarantee newly planted material through one calendar year from the date of written owner acceptance. Plants that exhibit more than 10% dieback damage shall be replaced at no additional cost to the owner. The contractor shall also provide adequate tree wrap and deer/detrudent protection measures for the plantings during the warranty period.
- This layout plan constitutes our understanding of the landscape requirements listed in the ordinance. Changes and modifications may be requested by the city based on applicable information, public input, council decisions, etc.
- The landscape contractor shall be responsible for obtaining any permits and coordinating inspections as required throughout the work process.
- Plant size & species substitutions must be approved in writing prior to acceptance in the field.
- The landscape contractor shall furnish an Irrigation Layout Plan for head-to-head coverage of all tree, turf and shrub planting areas. Use commercial-grade irrigation equipment and provide product cut-sheets and (4) copies of the proposed layout plan to the landscape architect for review and approval. Coordinate irrigation connection point, controller, backflow and valving locations with the architect & general contractor. Irrigation system shall be zoned such that plants over building structure and in city right-of-way are separate. Coordinate sleeve locations and do not penetrate water-proofing without coordinating with water-proofing contractor.
- Landscape Contractor is responsible for coordination with the General Contractor, to protect the new improvements on and off-site during landscape work activities. Report any damage to the General Contractor immediately.
- Unless otherwise noted/indicated, plant beds shall receive 4" depth of natural cork, single-shed hardwood mulch, per detail. Submit mulch sample for Owner approval. Do not install weed mat under perennials, with the exception of ornamental grasses.
- Rock mulch infill under tree grate areas shall be 1"-1.5" dia. inch washed Dresser Trap rock. Install per detail. Submit mulch sample for approval.
- All planting areas shall be prepared prior to installation activities with clean, imported topsoil. Provide a firm planting bed free of stones, sticks, construction debris, etc.
- The Landscape Contractor shall furnish samples of all landscape materials for approval prior to installation.
- The Landscape Contractor shall clear and grub the underbrush from within the work limits to remove dead branches, leaves, trash, weeds and foreign materials. Remove trees where noted on the plan, including the stump to 30" below grade.
- The landscape contractor shall contact Oopher State One Call no less than 48 hours before digging for field utility locations.
- The landscape contractor shall be responsible for the removal of erosion control measures once vegetation has been established to the satisfaction of the municipal staff. This includes all curbside fencing and sediment logs placed in the landscape.
- The landscape contractor shall be responsible for visiting the site to become familiar with the conditions prior to bidding and installation. Coordinate with the general contractors on matters such as tree grading, landscaped area conditions, staging areas, irrigation connection to building, etc.
- See Site and Civil plans for additional information regarding the project.
- All sub-cut areas of the site that are designated on the site plan as open space for landscape shall have no less than 18" of imported, pulverized topsoil. Slope away from building. See detail 35 below.
- Landscape contractor must prove the open sub-grade of all planting areas after their excavation is capable of infiltrating a minimum requirement of 1/4-inch of water per hour prior to installation of plant materials, topsoil, irrigation, weed mat, and mulch. Planting areas not capable of meeting this requirement shall have 4" diameter x 48" depth holes augured every 36" on-center and filled with MNDOT Fire-Chalking Coarse Filler Aggregate. Re-test sub-grade penetration for compliance to infiltration minimum requirement.
- Fencing (if noted) per Architect's Plans and Specifications.
- Landscape contractor shall coordinate tree removal activities and ensure all trees designated for removal are gone before start of landscape installation. Grind stump to 30" below grade and backfill with topsoil.
- Landscape contractor shall include in their bid to remove any existing shrubs, sod, edger, fabric, and edger that interferes with new improvements shown in this package.
- Landscape Contractor and Excavator shall be responsible for providing 18" depth topsoil in shrub beds. CU Soil in street tree planting areas per city requirements and per Detail 14 L1.1.
- Site Furnishings: Contractor to furnish, assemble, and install the following site furnishings:
(3) Model '35' Loop Bike Parking
(2) Model '35 Pkty' Waste Receptacles with dual liner (waste/recyclables).
Contact: Mike Kortecny, Regional Sales, Landscape Forms, (800) 434-2206 ext. 1311 or mikek@landscapeforms.com
All furnishings to be surface-mounted with stainless-steel, tamper-proof hardware
Submit standard finish schedule for color selection.
- Prior to work in the city right-of-way, obtain a permit from the City of Minneapolis Forestry Department.

Irrigation Performance Requirements:

- IRRIGATION SYSTEMS DESIGNED FOR 40-45 PSI AT THE BASE OF ALL SPRINKLER HEADS. NOTIFY LANDSCAPE ARCHITECT IF EXISTING PRESSURE DIFFERS.
- LAYOUT WORK AS ACCURATELY AS POSSIBLE. THE CONTRACTOR MAY MAKE MINOR ADJUSTMENTS TO THE LOCATION AND SPACING AS NECESSARY TO ACCOMMODATE ACTUAL FIELD CONDITIONS. HEAD LOCATIONS MUST BE FLAGGED AND REVIEWED BY THE LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.
- COORDINATE SLEEVING AND IRRIGATION PIPE (HEAD) INTERNAL PLUMBING INSTALLATION WITH THE WORK OF OTHERS.
- ALL MATERIALS SHALL BE INSTALLED AS DETAILED ON DRAWINGS. (HOWEVER, IF THE CONTRACT DRAWINGS AND/OR SPECIFICATIONS DO NOT THOROUGHLY DESCRIBE THE METHOD OR TECHNIQUES TO BE USED, THE CONTRACTOR SHALL FOLLOW THE INSTALLATION METHODS ISSUED BY THE MANUFACTURER. ALL SUCH LITERATURE MUST BE SUBMITTED 48 HOURS PRIOR TO INSTALLATION FOR REVIEW AND APPROVAL BY THE LANDSCAPE ARCHITECT.)
- CHECK AND VERIFY ALL EXISTING AND PROPOSED SITE CONDITIONS, UTILITIES AND SERVICES PRIOR TO TRENCHING.
- LATERAL PIPING TO BE A MINIMUM OF 12 INCHES DEEP AND MAINLINES A MINIMUM OF 18 INCHES DEEP.
- ALL MAIN LINE PIPING AND LATERAL PIPE OF 1/2" AND LARGER SHALL BE PVC (SDR 26 CLASS 100), ALL OTHER LATERAL PIPE OF 1/4" AND SMALLER MAY BE POLYETHYLENE.
- ALL TEES AND ELBOWS ON 2-1/2" MAIN SHALL BE PVC (160 PSI).
- ALL SPRINKLERS SHALL BE AS NOTED ON DETAIL & SPECIFICATION.
- ADJUST HEADS FOR GRADE, AS NECESSARY. AFTER TURF GRASS HAS BEEN ESTABLISHED AND ALL SETTLEMENT AT HEADS HAS OCCURRED.
- ALL AUTOMATIC CONTROLLERS, PRESSURE BACKFLOW PREVENTERS AND HOSE BIBS SHALL BE SET PLUMB. SPRINKLER HEAD PRESS. QUICK COUPLER VALVES AND ALL VALVES WITH STEPS SHALL BE SET PERPENDICULAR TO FINISHED GRADE.
- CONTROL VALVE WIRES INCLUDING THE GROUND WIRE, SHALL BE #12 GAUGE ILL. UL APPROVED DIRECT BURIAL UNDERGROUND CONNECTIONS SHALL BE MADE WITH 3/4" WIRE CONNECTORS (DBY) OR APPROVED EQ.
- VALVES FOR ALL ZONES SHALL BE AS PER SPECIFICATIONS.
- PLACE ALL VALVES BY APPROVED VALVE BOXES.
- USE TEFLON TAPE ON ALL THREADED JOINTS.
- BRAND EACH VALVE BOX WITH 1" INCH LETTERING SHOWING ZONE NUMBER AND CONTROLLER LETTER (EXAMPLE "A3"). THIS STAMP IS TO MATCH THE ZONE SHOWN ON THE PLAN UNLESS OTHERWISE DIRECTED BY LANDSCAPE ARCHITECT.
- CONDUCT PERFORMANCE TEST IN THE PRESENCE OF OWNER AND LANDSCAPE ARCHITECT FOLLOWING COMPLETION OF SYSTEM INSTALLATION.
- CONDUCT AND DEMONSTRATE WINTERIZATION AND SPRING START-UP PROCESS TO OWNER IN THE FALL OF COMPLETION.
- LANDSCAPE ARCHITECT SHALL BE NOTIFIED TO VERIFY TRENCH DEPTHS BEFORE BACKFILLING.
- INSTALL FLOOD BUBBLER HEADS BY STREET TREE PLANTING AREAS.
- AFTER INSTALLATION OF DRIP IRRIGATION PIPING COMPLETE AND PRIOR TO SOODING, SEEDING OR MULCH INSTALLATION, THE CONTRACTOR SHALL CONTACT THE LANDSCAPE ARCHITECT TO REVIEW THE INSTALLATION OF THE DRIP IRRIGATION SYSTEM.
- LANDSCAPE CONTRACTOR SHALL BE RESPONSIBLE FOR SUBMITTING A COMPLETE IRRIGATION LAYOUT PLAN SET AND SUBMITTING TO THE LANDSCAPE ARCHITECT FOR REVIEW AND APPROVAL PRIOR TO COMMENCEMENT OF FIELD INSTALLATION.
- COORDINATE LOCATION OF ROOFTOP-MOUNTED ATMOSPHERIC WINDSPEED (WAW) SENSOR WITH OWNER AND ARCHITECT.
- SUBMIT LAYOUT PLAN AND PRODUCT DATA TO THE LANDSCAPE ARCHITECT FOR REVIEW AND APPROVAL PRIOR TO INSTALLATION.

5 DRIP IRRIGATION

L1.2 NOT TO SCALE



6 TYP. DRIP IRRIGATION LAYOUT PATTERN

L1.2 NOT TO SCALE



edness seaman graham architects
500 washington avenue south
minneapolis, minnesota 55415
p. 6 1 2 . 3 3 9 . 5 5 9 8
f. 6 1 2 . 3 3 9 . 5 3 8 2
www.esgarch.com

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Landscape Architect under the laws of the state of Minnesota.
Signature _____
Title or Project Name _____
Date _____



Calyx Design Group, Inc
Landscape Architecture
Sustainable Design
Master Planning

1583 Berkeley Ave. Suite A
St. Paul, MN 55105
Telephone: 651.234.5498
Internet: www.calyxdesigngroup.com

NOT FOR CONSTRUCTION

Project Status

ORIGINAL ISSUE:

REVISIONS

No. Description Date

15-131

PROJECT NUMBER

BH BH

DRAWN BY CHECKED BY

KEY PLAN

LAKE & FREMONT

LANDSCAPE DETAILS

L1.2



REFER TO SHEET L1.1 FOR LANDSCAPE LAYOUT PLAN



elness swenson gramlich architects
500 washington avenue south
minneapolis, minnesota 55415
p. 612.339.5508
f. 612.339.5382
www.esgarch.com

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly licensed architect under the laws of the State of Minnesota.

Signature _____
Type or Printed Name _____
Date _____

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LAND USE APPLICATION / PRELIMINARY DEVELOPMENT REVIEW 7/9/15

ORIGINAL ISSUE: 07/09/15

REVISIONS

No. Description Date

215524 PROJECT NUMBER

Author _____ Checker _____
DRAWN BY _____ CHECKED BY _____

KEY PLAN

LAKE & FREMONT

CONTEXT AERIAL
A0.0



eldest swenson graham architects
500 washington avenue south
minneapolis minnesota 55415
p. 612.339.5508
f. 612.339.5382
www.esgarch.com

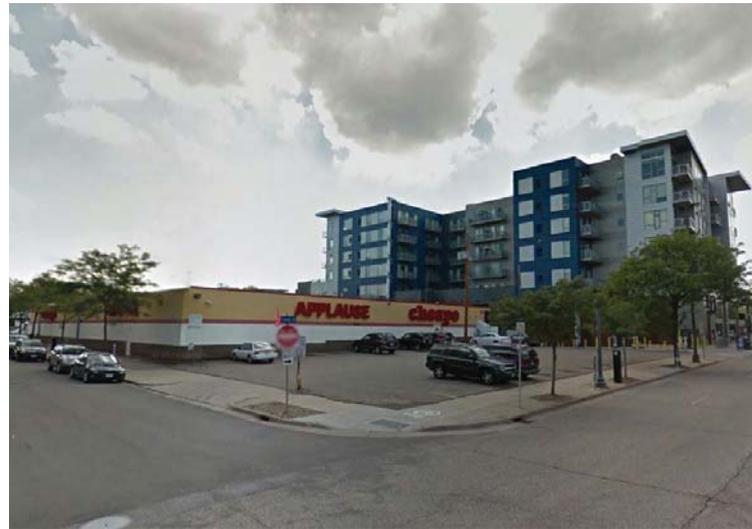
I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly licensed architect under the laws of the State of Minnesota.

Signature _____
Typed or Printed Name _____
Date _____

NOT FOR CONSTRUCTION



VIEW FROM THE NORTHWEST (MIDBLOCK AT LAGOON)



VIEW FROM THE NORTHEAST (CORNER OF FREMONT AND LAGOON)



VIEW FROM THE SOUTHWEST (MIDBLOCK AT LAKE)



VIEW FROM THE SOUTHEAST (CORNER OF FREMONT AND LAKE)

LAND USE APPLICATION / PRELIMINARY DEVELOPMENT REVIEW 7/9/15

ORIGINAL ISSUE

REVISIONS

No. Description Date

215524

PROJECT NUMBER

ESC ESC

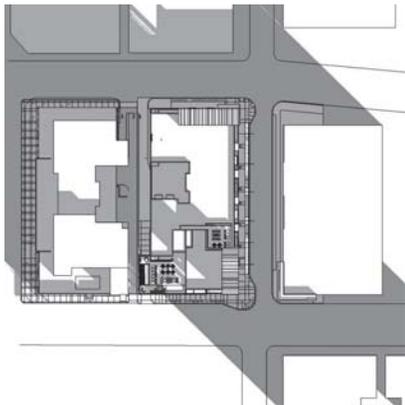
DATE BY

KEY PLAN

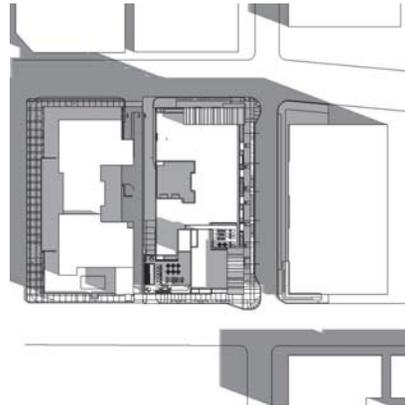
LAKE & FREMONT

EXISTING SITE PHOTOGRAPHS

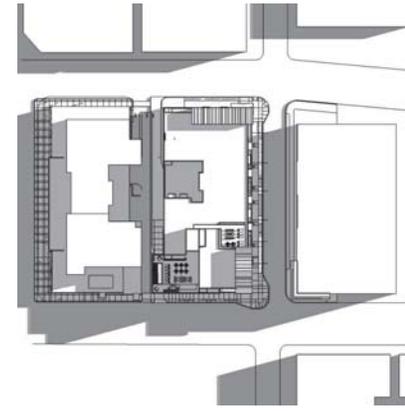
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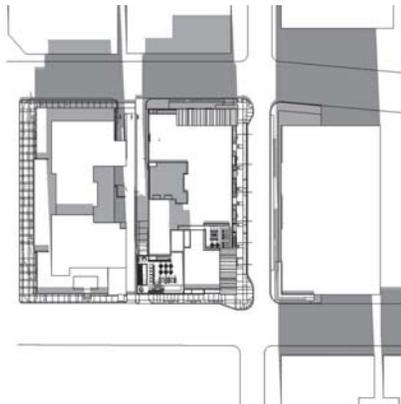
DECEMBER 21 - ONE HOUR AFTER SUNRISE



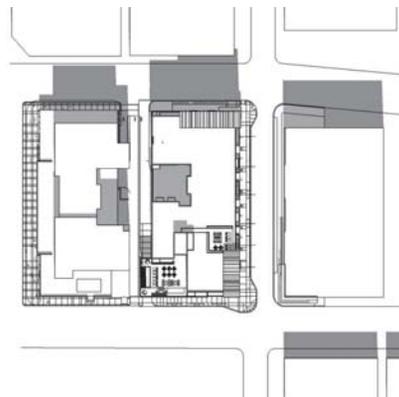
MARCH/SEPTEMBER 21 - ONE HOUR AFTER SUNRISE



JUNE 21 - ONE HOUR AFTER SUNRISE



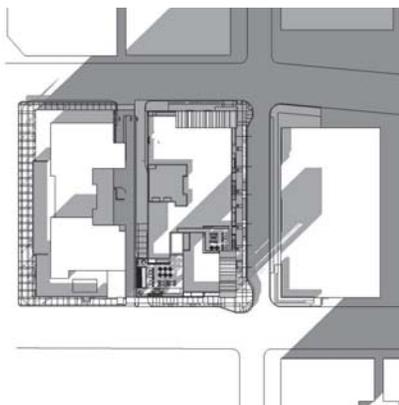
DECEMBER 21 - NOON



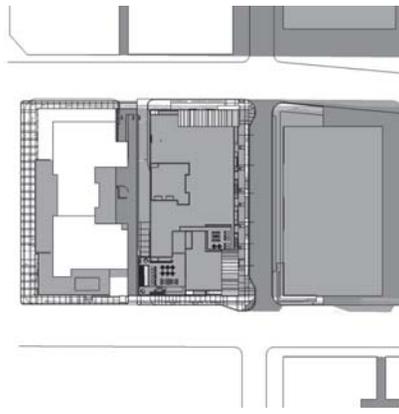
MARCH/SEPTEMBER 21 - NOON



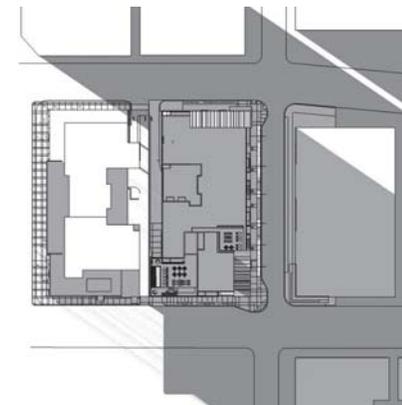
JUNE 21 - NOON



DECEMBER 21 - ONE HOUR BEFORE SUNSET



MARCH/SEPTEMBER 21 - ONE HOUR BEFORE SUNSET



JUNE 21 - ONE HOUR BEFORE SUNSET



direct sweden gram architects
 500 washington avenue south
 minneapolis minnesota 55415
 p. 612.339.5508
 f. 612.339.5382
 www.esgarch.com

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly licensed architect under the laws of the State of Minnesota.

Signature _____
 Type or Printed Name _____
 Date _____

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 CONSTRUCTION**

LAND USE
 APPLICATION
 REVISED 7/22/15

ORIGINAL ISSUE: 07/07/15

REVISIONS

No.	Description	Date

215524

PROJECT NUMBER

Author _____ Checker _____

DESIGNED BY _____ CHECKED BY _____

KEY PLAN

LAKE & FREMONT

SHADOW STUDIES

A0.10



WESTERN RED CEDAR (T&G SIDING)



METAL PANEL COLOR #1 (GRAY BLEND)



CEMENTITIOUS STUCCO COLOR #1 (WHITE)



FIBER CEMENT BOARD - COLOR #2 (GRAY)



NATURAL STONE VENEER



BRICK VENEER - COLOR #1

1300 W LAKE ST

MINNEAPOLIS, MN



elness sweden gram architects
500 washington avenue south
minneapolis minnesota 55415
p. 612.339.5508
f. 612.339.5382
www.esgarch.com

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REVISIONS		
No.	Description	Date
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Author _____	Checker _____	
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KEY PLAN		
1300 W LAKE ST		
MATERIALS		
A 0.11		

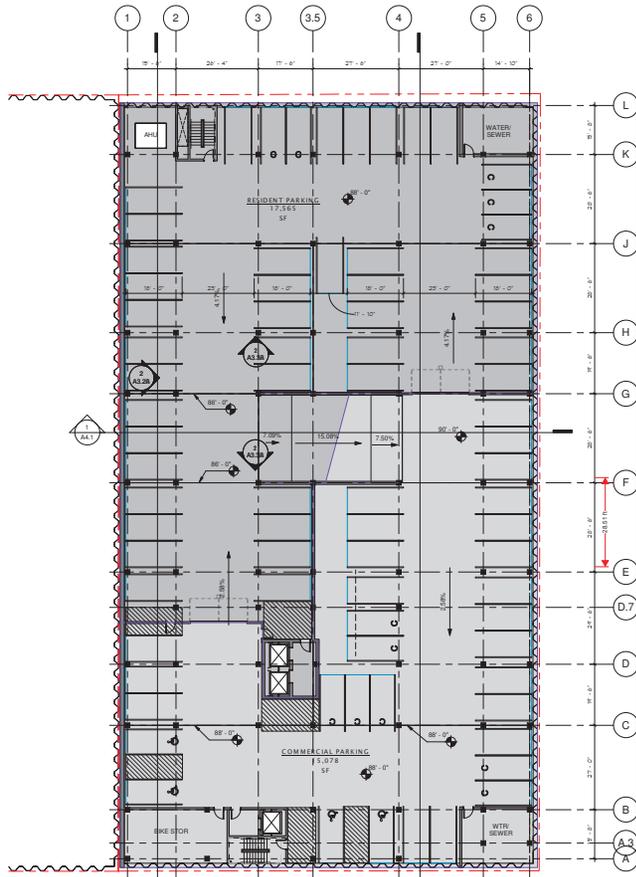


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 minneapolis, minnesota 55415
 p: 612.339.5508
 f: 612.339.5822
 www.esgarch.com

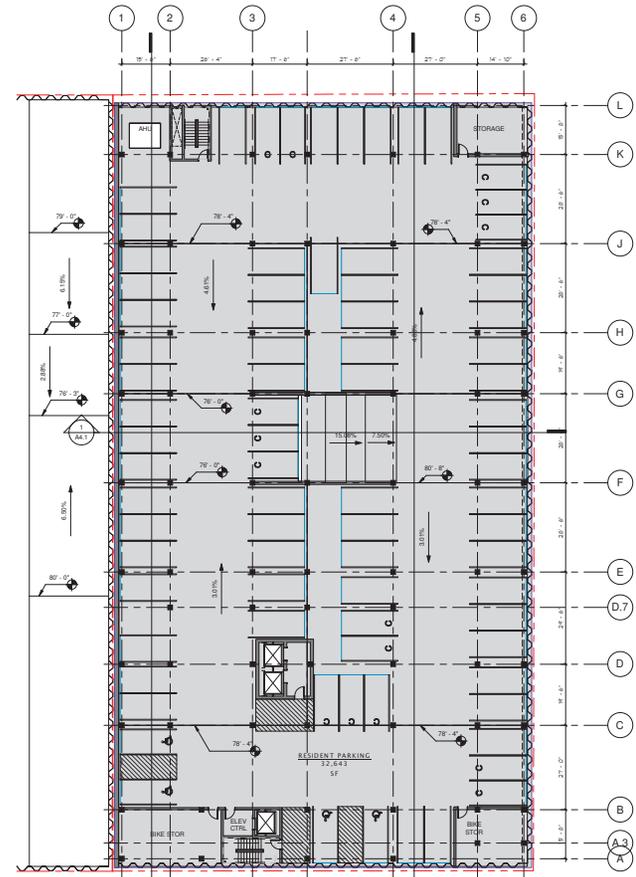
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2 MINUS 1 PLAN
 1/16" = 1'-0"



1 MINUS 2 PLAN
 1/16" = 1'-0"

LAND USE APPLICATION
 REVISED 7/22/15

ORIGINAL ISSUE:
 REVISIONS:

No.	Description	Date

215524
 PROJECT NUMBER

ESG
 DRAWN BY

ESG
 CHECKED BY

KEY PLAN

PLAN NORTH

LAKE & FREMONT

MINUS 2 & MINUS 1 PLANS
A1.1

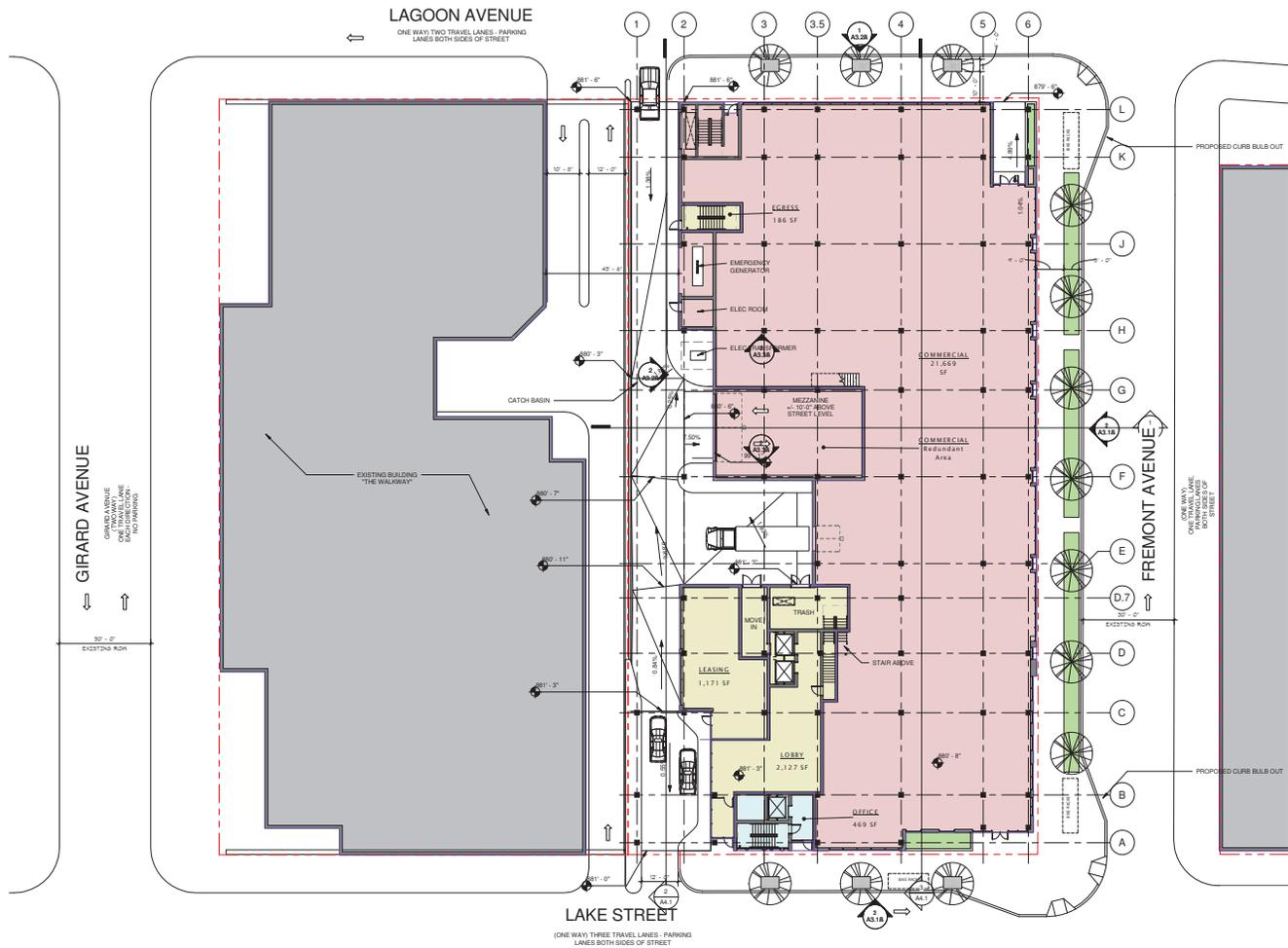


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 500 WASHINGTON AVENUE SOUTH
 MINNEAPOLIS, MINNESOTA 55415
 P. 612.339.5508
 F. 612.339.5822
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1 LEVEL 1 PLAN
 1/8" = 1'-0"

LAND USE APPLICATION
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No.	Description	Date

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KEY PLAN



LAKE & FREMONT

STREET LEVEL PLAN

A1.2

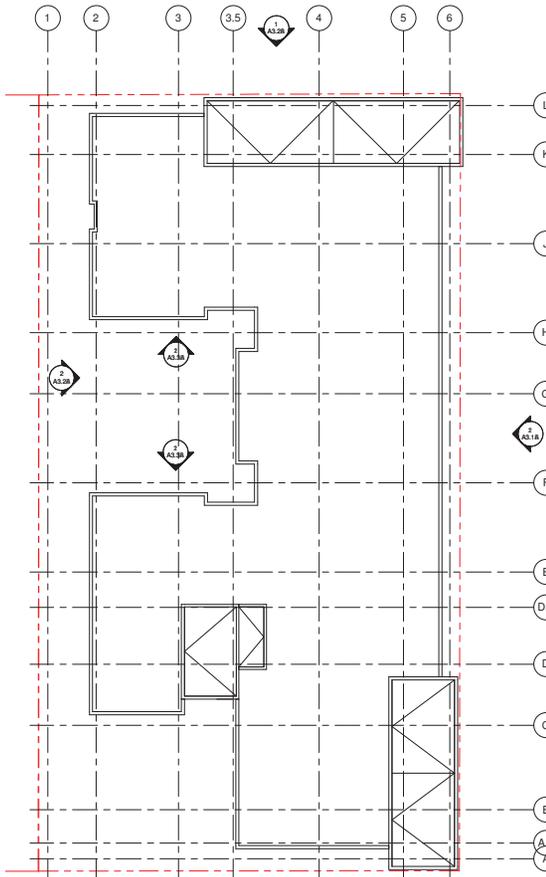


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500 washington avenue south
minneapolis minnesota 55415
p: 612-339-5508
f: 612-339-5892
www.esgarch.com

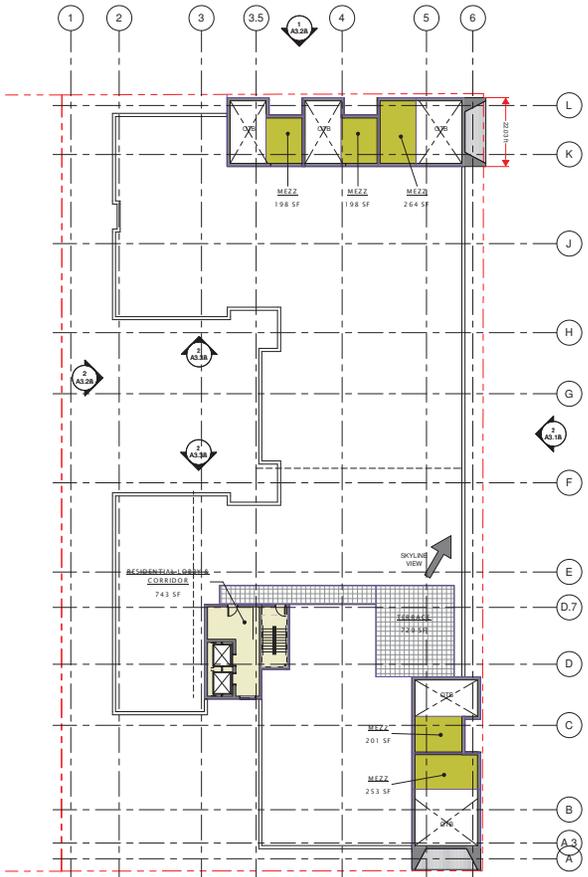
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3 UPPER ROOF PLAN
1107-1107



2 MEZZANINE PLAN
1107-1107



4 LEVEL 4 PLAN (LEVELS 5 & 6 SIM)
1107-1107

LAND USE APPLICATION
REVISED 7/22/15

ORIGINAL ISSUE: 07/02/15

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KEY PLAN

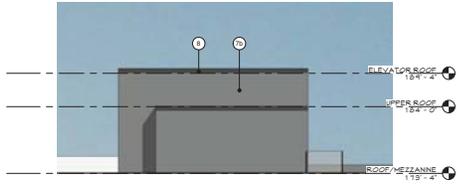


LAKE & FREMONT

LEVEL 4 - UPPER ROOF PLANS

A1.4

- EXTERIOR MATERIAL KEYNOTES**
- 1 BRICK VENEER - COLOR #1 (TERRACOTTA)
 - 2 NATURAL STONE VENEER
 - 3 WESTERN RED CEDAR TAG BOARD
 - 4 COLORED SMOOTH FACE CMU (BURNISHED AT GRADE)
 - 4b BURNISHED FACE CMU
 - 5a METAL PANEL - COLOR #1 (GRAY BLEND)
 - 5b METAL PANEL - COLOR #2 (WHITE)
 - 5c METAL PANEL - COLOR #3 (CHARCOAL)
 - 6 GEMINTOUS STUCCO - COLOR #1 (WHITE)
 - 7a FIBER CEMENT BOARD - COLOR #1 (GRAY)
 - 7b PREFINISHED METAL COPING - (MATCH WALL FINISH)
 - 11 FIN TO MECHANICAL LOUVER (MATCH WALL FINISH)
 - 12a ALUM STOREFRONT GLAZING
 - 13 FIBERGLASS WINDOW
 - 14 PREFIN ALUM BALCONY & GUARDRAIL
 - 15 OVERHEAD DOOR
 - 16 WESTER RED CEDAR SLAT SCREEN WALL
 - 17 PREFIN METAL LOUVER
 - 19 DECORATIVE METAL SCREEN



3 East Penthouse Elevation
K314 1/8" = 1'-0"



2 South Elevation-LUA
K314 1" = 10'-0"



1 East Elevation-LUA
K314 1" = 10'-0"

LAND USE APPLICATION UPDATES 8/7/15		
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Author	Checker	
DATE BY	CHECKED BY	
KEY PLAN		
1300 W LAKE ST		
EXTERIOR ELEVATIONS - COLOR		
A-3.1A		

- EXTERIOR MATERIAL KEYNOTES**
- 1 BRICK VENEER - COLOR #1 (TERRACOTTA)
 - 2 NATURAL STONE VENEER
 - 3 WESTERN RED CEDAR TAG BOARD
 - 4 COLORED SMOOTH FACE CMU (BURNISHED AT GRADE)
 - 4B BURNISHED FACE CMU
 - 5 METAL PANEL - COLOR #1 (GRAY BLEND)
 - 5B METAL PANEL - COLOR #2 (WHITE)
 - 5C METAL PANEL - COLOR #3 (CHARCOAL)
 - 6 CEMENTITIOUS STUCCO - COLOR #1 (WHITE)
 - 7B FIBER CEMENT BOARD - COLOR #2 (GRAY)
 - 8 PREFINISHED METAL COPING - (MATCH WALL FINISH)
 - 11 PLYD MECHANICAL LOUVER (MATCH WALL FINISH)
 - 12 ALUM STOREFRONT GLAZING
 - 13 FIBERGLASS WINDOW
 - 14 PREFIN ALUM BALCONY & GUARDRAIL
 - 15 OVERHEAD DOOR
 - 16 WESTERN RED CEDAR SLAT SCREEN WALL
 - 17 PREFIN METAL LOUVER
 - 19 DECORATIVE METAL SCREEN



1 South Elevation BW-LUA
A-3.1B
1" = 10'-0"



2 East Elevation BW- LUA
A-3.1B
1" = 10'-0"

1300 W LAKE ST



eldest swenson graham architects
500 WASHINGTON AVENUE SOUTH
MINNEAPOLIS MINNESOTA 55415
P. 612.339.5508
F. 612.339.5382
WWW.ESGARCH.COM

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ORIGINAL ISSUE		
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215524	PROJECT WORK	
ESG	ESG	
KEY PLAN		
1300 W LAKE ST		
EXTERIOR ELEVATIONS B&W		
A-3.1B		

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- EXTERIOR MATERIAL KEYNOTES**
- 1 BRICK VENEER - COLOR #1 (TERRACOTTA)
 - 2 NATURAL STONE VENEER
 - 3 WESTERN RED CEDAR TAG BOND
 - 4 COLLIER'S SMOOTH FACE CMU (BURNISHED AT GRADE)
 - 5 BURNISHED FACE CMU
 - 6 METAL PANEL - COLOR #1 (GRAY BLEND)
 - 7 METAL PANEL - COLOR #2 (WHITES)
 - 8 METAL PANEL - COLOR #3 (CHARCOAL)
 - 9 CONCRETE/STUCCO COLOR #1 (WHITE)
 - 10 FIBER CEMENT BOARD - COLOR #2 (GRAY)
 - 11 PREFINISHED METAL CORNER (MATCH WALL FINISH)
 - 12 PFTO MECHANICAL LOUVER (MATCH WALL FINISH)
 - 13 ALUM EXTRUDED GLAZING
 - 14 PREFIN ALUM BALCONY & GUARDRAIL
 - 15 OVERHEAD DOOR
 - 16 WESTERN RED CEDAR BLAT SCREEN WALL
 - 17 PREFIN METAL LOUVER
 - 18 DECORATIVE METAL SCREEN



LAND USE APPLICATION UPDATES 8/7/15

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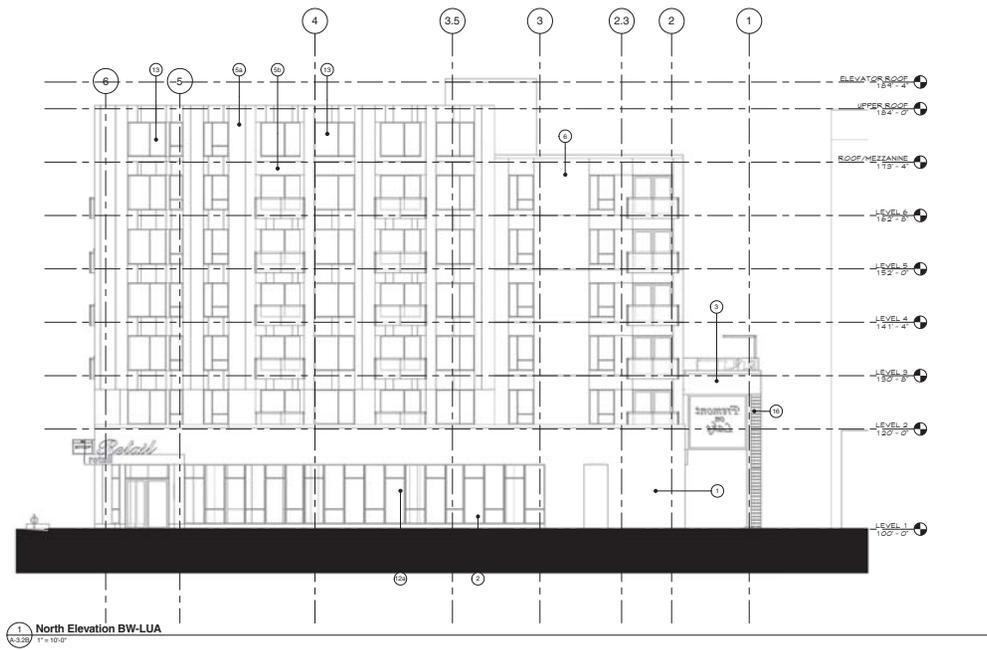
KEY PLAN

1300 W LAKE ST

EXTERIOR ELEVATIONS - COLOR

A-3.2A

- EXTERIOR MATERIAL KEYNOTES**
- 1 BRICK VENEER - COLOR #1 (TERRACOTTA)
 - 2 NATURAL STONE VENEER
 - 3 WESTERN RED CEDAR TAG SIDING
 - 4 COLORED SMOOTH FACE CMU (BUSHWASHED AT GRADE)
 - 5 BURNISHED FACE CMU
 - 6 METAL PANEL - COLOR #1 (GRAY BLEND)
 - 7 METAL PANEL - COLOR #2 (WHITE)
 - 8 METAL PANEL - COLOR #3 (CHARCOAL)
 - 9 CEMENTITIOUS STUCCO - COLOR #1 (WHITE)
 - 10 FIBER CEMENT BOARD - COLOR #2 (GRAY)
 - 11 PRESFINISHED METAL COPING - MATCH WALL FINISH
 - 12 PTD MECHANICAL LOUVER (MATCH WALL FINISH)
 - 13 ALUM STOREFRONT GLAZING
 - 14 FIREGLASS WINDOW
 - 15 PREFIN ALUM BALKONY & GUARDRAIL
 - 16 OVERHEAD DOOR
 - 17 WESTERN RED CEDAR SLAT SCREEN WALL
 - 18 PREFIN METAL LOUVER
 - 19 DECORATIVE METAL SCREEN



1300 W LAKE ST
MINNEAPOLIS, MN

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STUDENT LIVING
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500 WASHINGTON AVENUE SOUTH
MINNEAPOLIS, MINNESOTA 55415
P. 612.339.5508
F. 612.339.5382
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MINNESOTA
Type or Printed Name
DATE: 7/15/15

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KEY PLAN

1300 W LAKE ST

EXTERIOR ELEVATIONS B&W
A-3.2B

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- EXTERIOR MATERIAL KEYNOTES**
- 1 BRICK VENEER - COLOR #1 (TERRACOTTA)
 - 2 NATURAL STONE VENEER
 - 3 WESTERN RED CEDAR T&G SIDING
 - 4 COLORED SMOOTH FACE CMU (BURNISHED AT GRADE)
 - 5 BURNISHED FACE CMU
 - 6 METAL PANEL - COLOR #1 (GRAY BLEND)
 - 7 METAL PANEL - COLOR #2 (WHITE)
 - 8 METAL PANEL - COLOR #3 (CHAMPAGNE)
 - 9 CEMENTITIOUS STUCCO - COLOR #1 (WHITE)
 - 10 FIBER CEMENT BOARD - COLOR #2 (GRAY)
 - 11 PREFINISHED METAL COPING - (MATCH WALL FINISH)
 - 12 PVD MECHANICAL LOUVER (MATCH WALL FINISH)
 - 13 ALUM. STOREFRONT GLAZING
 - 14 FIBERGLASS WINDOW
 - 15 PREFIN ALUM. BALCONY GLAZING
 - 16 OVERHEAD DOOR
 - 17 WESTER RED CEDAR SLAT SCREEN WALL
 - 18 PREFIN METAL LOUVER
 - 19 DECORATIVE METAL SCREEN



2 South Court Elevation-LUA
1" = 10'-0"



1 North Court Elevation-LUA
1" = 10'-0"

LAND USE APPLICATION UPDATES 8/7/15

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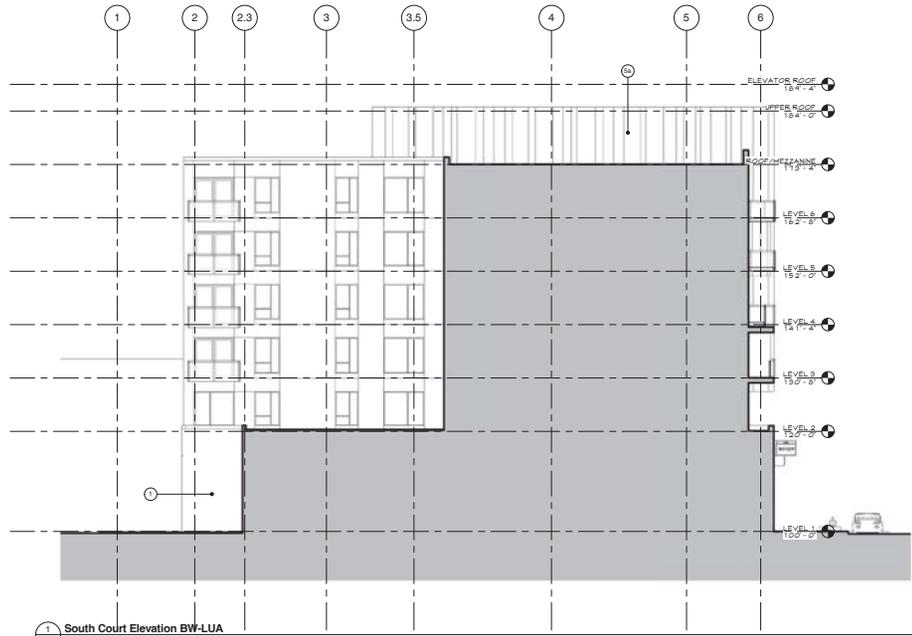
KEY PLAN

1300 W LAKE ST

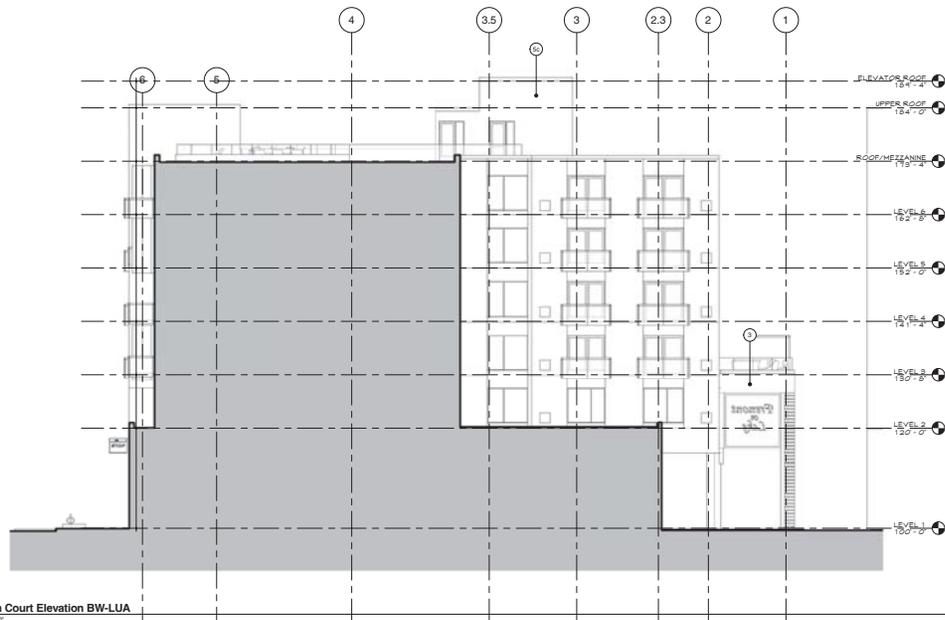
EXTERIOR ELEVATIONS
A-3.3A

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- EXTERIOR MATERIAL KEYNOTES**
- 1 BRICK VENEER - COLOR #1 (TERRACOTTA)
 - 2 NATURAL STONE VENEER
 - 3 WESTERN RED CEDAR TAG SIDING
 - 4A COLORED SMOOTH FACE CMU (BURNISHED AT GRADE)
 - 4B BURNISHED FACE CMU
 - 5A METAL PANEL - COLOR #1 (GRAY BLEND)
 - 5B METAL PANEL - COLOR #2 (WHITE)
 - 5C METAL PANEL - COLOR #3 (CHARCOAL)
 - 6 CONCRETE/STUCCO - COLOR #1 (WHITE)
 - 7B FIBER CEMENT BOARD - COLOR #2 (GRAY)
 - 8 PREFINISHED METAL CORNER - MATCH WALL FINISH
 - 11 PNTD MECHANICAL LOUVER (MATCH WALL FINISH)
 - 12A ALUM STOREFRONT GLAZING
 - 13 FIBERGLASS WINDOW
 - 14 PREFIN ALUM BALCONY & GUARDRAIL
 - 15 OVERHEAD DOOR
 - 16 WESTERN RED CEDAR SLAT WALL FINISH
 - 17 PREFIN METAL LOUVER
 - 19 DECORATIVE METAL SCREEN



1
South Court Elevation BW-LUA
1/4" = 10'-0"



2
North Court Elevation BW-LUA
1/4" = 10'-0"

LAND USE APPLICATION UPDATES 8/7/15

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KEY PLAN

1300 W LAKE ST

EXTERIOR ELEVATIONS B&W
A-3.3B



AERIAL VIEW FROM LAKE STREET



DETAIL VIEW OF LAKE STREET FACADE

1300 W LAKE ST

MINNEAPOLIS, MN



elms + swenoch gram architects
 500 WASHINGTON AVENUE SOUTH
 MINNEAPOLIS, MINNESOTA 55415
 P. 612.339.5508
 F. 612.339.5382
 WWW.ESGARICH.COM

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KEY PLAN		
1300 W LAKE ST		
3D VIEWS		
A3.4		

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VIEW FROM CORNER
OF LAKE STREET AND
FREMONT AVENUE



DETAIL VIEW OF
CORNER OF LAKE AND
FREMONT

LAND USE
APPLICATION
UPDATES 8/7/15

ORIGINAL ISSUE: 07/22/15

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215524

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KEY PLAN

1300 W LAKE ST

3D VIEWS

A3.5



VIEW OF CORNER OF
FREMONT AND
LAGOON AVENUES



DETAIL VIEW OF
FREMONT AND LAGOON
STREETScape

1300 W LAKE ST

MINNEAPOLIS, MN



eldest swenoch gram architects
500 washington avenue south
minneapolis minnesota 55415
p. 612.339.5508
f. 612.339.5822
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LAND USE
APPLICATION
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ORIGINAL ISSUE: 07/22/15

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215524

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KEY PLAN

1300 W LAKE ST

3D VIEWS

A3.6



HYDROLOGY FOR
LAKE STREET WEST & FREMONT AVENUE
MINNEAPOLIS, MINNESOTA
JULY 10, 2015

EXISTING SITE DESCRIPTION

The project is located between Lake Street West and Lagoon Avenue along Fremont Avenue in the City of Minneapolis, Minnesota. The site has a total approximate area of 0.77 acres. Currently the site includes a one story commercial building covering 23,230 SF with the remaining 10,261 SF being surface parking. There are no storm sewer management devices on site. The stormwater sheet flows to storm sewer structures located in Lagoon Avenue, Fremont Avenue, and Lake Street West.

EXISTING SITE DRAINAGE CHARACTERISTICS

Detail 1/C5.0 of the Storm Water Pollution Prevention Plan (SWPPP) shows the existing delineated site drainage areas. Table 1 briefly summarizes the ground cover characteristics and drainage routing of each numbered drainage area. Most of the site drainage to each respective drainage point is via overland sheet flow and shallow concentrated flow.

Table 1: Existing Site Drainage Descriptions (Refer to Detail 1/C5.0)		
Drainage Area No.	Description	Drainage Routes to:
1	Existing building and parking area	Sheet flows to storm sewer structures along Lagoon Avenue, Fremont Avenue, and Lake Street West.

EXISTING SITE DRAINAGE FLOW RATES

HydroCAD was used to model the existing site hydrology. The NRCS Method of Abstractions was used in the model with Type II, 24-hour rainfall events from the NOAA Atlas 14 precipitation frequency estimates (refer to Table 3). NRCS curve numbers were derived based on topographic survey information completed by Rehder and Associates, Inc.. Table 2 lists the derived curve runoff numbers for each cover type.

Table 2: NRCS Hydrologic Soil Group and Runoff Curve Numbers		
Ground Cover Type	Hydrologic Soil Group	Curve Number (CN)
Paving/Buildings	NA	98

Peak runoff computations were performed for the 2-year, 10-year, and 100-year/24-hour return period rainfall event, listed in Table 3. The weighted curve number for each drainage area is based on a weighted average of each area of ground cover type as listed in Table 2. Peak HydroCAD runoff computations are summarized in Table 4 and are included as an appendix to this report.

Table 3: NOAA Atlas 14 24-hour Duration Storm Events for Each Return Period (inches)			
2-year	10-year	100-year	
2.85"	4.28"	7.49"	

Table 4: Summary of Peak Runoff Rates: Existing Drainage Areas (Detail 1/C5.0)					
Drainage Area No.	Time of Concentration (minutes)	24-hour Storm Events Peak Runoff Rates			
		2-year (cfs)	10-year (cfs)	100-year (cfs)	
1	5	3.19	4.83	8.48	
Total		3.19	4.83	8.48	

PROPOSED DEVELOPMENT DESCRIPTION

The proposed site will have an approximate impervious area of 0.77 acres. The project consists of a 6 story multi use building, with 2 levels of underground parking. Stormwater on this site sheet flows to storm sewer structures located in Lagoon Avenue, Fremont Avenue, and Lake Street West. Stormwater from the building will be collected through roof drains and outlet into the existing storm sewer along Fremont Avenue.

PROPOSED SITE DRAINAGE ROUTING

Detail 2/C5.0 of the Storm Water Pollution Prevention Plan shows the proposed delineated site drainage areas. Table 5 gives a brief summary and routing of the proposed drainage patterns on site.

Table 5: Proposed Site Drainage Descriptions (Refer to Detail 2/C5.0)		
Drainage Area No.	Description	Drainage Routes to:
1	The project consist of a 6 story building with new pavements and sidewalks.	Sheet flows to storm sewer structures along Lagoon Avenue, Fremont Avenue, and Lake Street West.

PROPOSED SITE STORM WATER TREATMENT

HydroCAD was used to model the proposed site hydrology. The NRCS Method of Abstractions was used with Type II, 24-hour rainfall events from the NOAA Atlas 14 precipitation frequency estimates (refer to Table 3). NRCS curve numbers were derived based on a weighted average of each area of ground cover type. Refer to Table 2 for runoff curve numbers used in the proposed computations.

The project is located within the Mississippi Watershed Management Organization and the City of Minneapolis, Minnesota. Current treatment and runoff standards for Minneapolis are the following;

Water Quality and Volume Control – Treat 1.25” of runoff to 70% total suggested solids removal

Peak Stormwater Runoff Control – Meet existing runoff rates for the 2, 10, and 100 year events.

The Stormwater Management Rules for Minneapolis only apply if a project site is greater than 1 acre, according to Minneapolis Code, Chapter 52. Due to the site size being less than one acre; no new permanent stormwater management is being proposed.

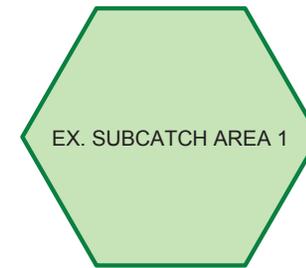
Table 6 shows a summary of the peak runoff rates for the proposed conditions. Table 7 compares the existing site runoff rates with the proposed site runoff rates.

Table 6: Summary of Peak Runoff Rates: Proposed Drainage Areas (Detail 2/C5.0)

Drainage Area No.	Time of Concentration (minutes)	24-hour Storm Events Peak Runoff Rates		
		2-year (cfs)	10-year (cfs)	100-year (cfs)
1	5	3.19	4.83	8.48
Total	5	3.19	4.83	8.48

Table 7: Comparison of Peak Runoff Rates: Existing vs. Proposed (cfs)

Condition	2-year/ 24-hr	10-year/ 24-hr	100-year/ 24-hr
Existing	3.19	4.83	8.48
Proposed	3.19	4.83	8.48



Subcat EX. SUBCATCH
AREA 1



15394 EXISTING 2015-06-29

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Page 2

Area Listing (all nodes)

Area (acres)	CN	Description (subcatchment-numbers)
0.769	98	Roofs, HSG B (EX. SUBCATCH AREA 1)
0.769	98	TOTAL AREA

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Soil Listing (all nodes)

Area (acres)	Soil Group	Subcatchment Numbers
0.000	HSG A	
0.769	HSG B	EX. SUBCATCH AREA 1
0.000	HSG C	
0.000	HSG D	
0.000	Other	
0.769		TOTAL AREA

15394 EXISTING 2015-06-29

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Ground Covers (all nodes)

HSG-A (acres)	HSG-B (acres)	HSG-C (acres)	HSG-D (acres)	Other (acres)	Total (acres)	Ground Cover	Subcatchment Numbers
0.000	0.769	0.000	0.000	0.000	0.769	Roofs	EX. SUBCATCH AREA 1
0.000	0.769	0.000	0.000	0.000	0.769	TOTAL AREA	

15394 EXISTING 2015-06-29

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Type II 24-hr 2-yr Rainfall=2.85"

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Page 5

Time span=0.01-80.00 hrs, dt=0.01 hrs, 8000 points
Runoff by SCS TR-20 method, UH=SCS, Split Pervious/Imperv.
Reach routing by Dyn-Stor-Ind method - Pond routing by Dyn-Stor-Ind method

SubcatchmentEX. SUBCATCHAREA 1: Runoff Area=0.769 ac 100.00% Impervious Runoff Depth=2.62"
Tc=5.0 min CN=0/98 Runoff=3.19 cfs 0.168 af

Total Runoff Area = 0.769 ac Runoff Volume = 0.168 af Average Runoff Depth = 2.62"
0.00% Pervious = 0.000 ac 100.00% Impervious = 0.769 ac

15394 EXISTING 2015-06-29

Type II 24-hr 2-yr Rainfall=2.85"

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Page 6

Summary for Subcatchment EX. SUBCATCH AREA 1: Subcat EX. SUBCATCH AREA 1

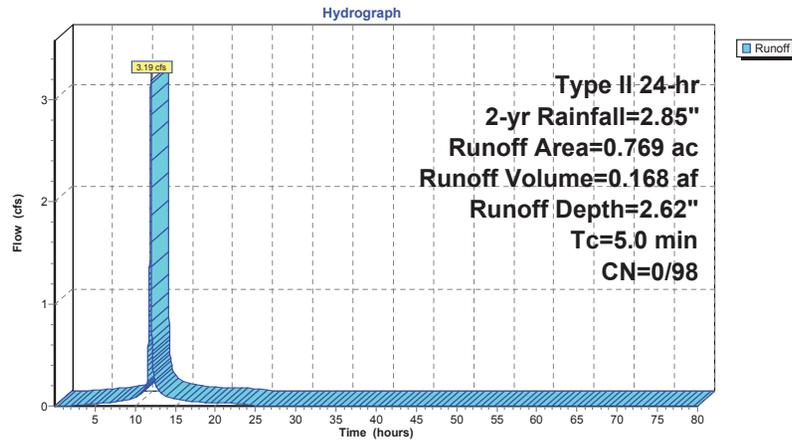
Runoff = 3.19 cfs @ 11.96 hrs, Volume= 0.168 af, Depth= 2.62"

Runoff by SCS TR-20 method, UH=SCS, Split Pervious/Imperv., Time Span= 0.01-80.00 hrs, dt= 0.01 hrs
Type II 24-hr 2-yr Rainfall=2.85"

Area (ac)	CN	Description
0.769	98	Roofs, HSG B
0.769	98	100.00% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry,

Subcatchment EX. SUBCATCH AREA 1: Subcat EX. SUBCATCH AREA 1



15394 EXISTING 2015-06-29

Type II 24-hr 10-yr Rainfall=4.28"

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Time span=0.01-80.00 hrs, dt=0.01 hrs, 8000 points
Runoff by SCS TR-20 method, UH=SCS, Split Pervious/Imperv.
Reach routing by Dyn-Stor-Ind method - Pond routing by Dyn-Stor-Ind method

Subcatchment EX. SUBCATCH AREA 1: Runoff Area=0.769 ac 100.00% Impervious Runoff Depth=4.04"
Tc=5.0 min CN=0/98 Runoff=4.83 cfs 0.259 af

Total Runoff Area = 0.769 ac Runoff Volume = 0.259 af Average Runoff Depth = 4.04"
0.00% Pervious = 0.000 ac 100.00% Impervious = 0.769 ac

15394 EXISTING 2015-06-29

Type II 24-hr 10-yr Rainfall=4.28"

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Summary for Subcatchment EX. SUBCATCH AREA 1: Subcat EX. SUBCATCH AREA 1

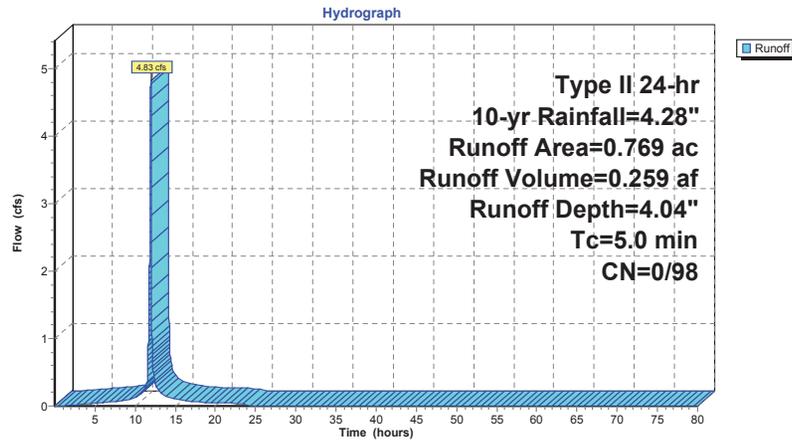
Runoff = 4.83 cfs @ 11.96 hrs, Volume= 0.259 af, Depth= 4.04"

Runoff by SCS TR-20 method, UH=SCS, Split Pervious/Imperv., Time Span= 0.01-80.00 hrs, dt= 0.01 hrs
Type II 24-hr 10-yr Rainfall=4.28"

Area (ac)	CN	Description
0.769	98	Roofs, HSG B
0.769	98	100.00% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry,

Subcatchment EX. SUBCATCH AREA 1: Subcat EX. SUBCATCH AREA 1



15394 EXISTING 2015-06-29

Type II 24-hr 100-yr Rainfall=7.49"

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Time span=0.01-80.00 hrs, dt=0.01 hrs, 8000 points
Runoff by SCS TR-20 method, UH=SCS, Split Pervious/Imperv.
Reach routing by Dyn-Stor-Ind method - Pond routing by Dyn-Stor-Ind method

Subcatchment EX. SUBCATCH AREA 1: Runoff Area=0.769 ac 100.00% Impervious Runoff Depth=7.25"
Tc=5.0 min CN=0/98 Runoff=8.48 cfs 0.465 af

Total Runoff Area = 0.769 ac Runoff Volume = 0.465 af Average Runoff Depth = 7.25"
0.00% Pervious = 0.000 ac 100.00% Impervious = 0.769 ac

15394 EXISTING 2015-06-29

Type II 24-hr 100-yr Rainfall=7.49"

Prepared by (enter your company name here)

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Summary for Subcatchment EX. SUBCATCH AREA 1: Subcat EX. SUBCATCH AREA 1

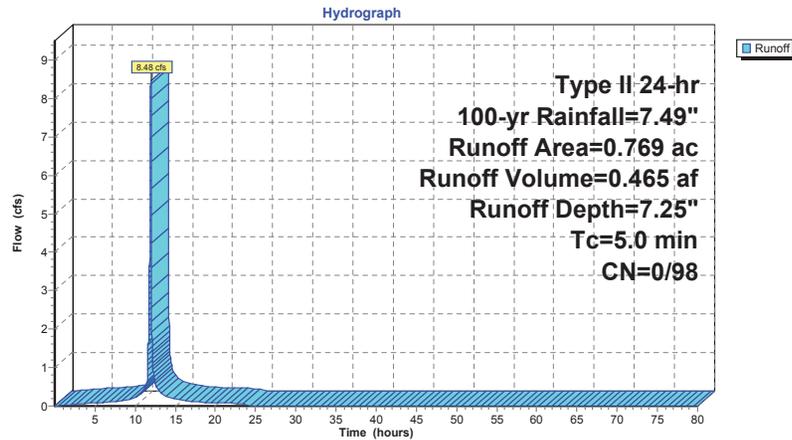
Runoff = 8.48 cfs @ 11.96 hrs, Volume= 0.465 af, Depth= 7.25"

Runoff by SCS TR-20 method, UH=SCS, Split Pervious/Imperv., Time Span= 0.01-80.00 hrs, dt= 0.01 hrs
Type II 24-hr 100-yr Rainfall=7.49"

Area (ac)	CN	Description
0.769	98	Roofs, HSG B
0.769	98	100.00% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry,

Subcatchment EX. SUBCATCH AREA 1: Subcat EX. SUBCATCH AREA 1



Travel Demand Management Plan

Lake & Fremont W Lake St & Fremont Ave S Minneapolis, MN

AUTHORIZED PROPERTY REPRESENTATIVE:

By _____ Dated: _____

Daniel Oberpriller
CPM Development LLC
2919 Knox Avenue S, Suite 200
Minneapolis, MN 55408

MINNEAPOLIS COMMUNITY PLANNING AND ECONOMIC DEVELOPMENT DEPARTMENT:

By _____ Dated: _____

Steve Poor, CPED Development Services Director

MINNEAPOLIS PUBLIC WORKS DEPARTMENT:

By _____ Dated: _____

Steve Mosing, P.E., P.T.O.E., Traffic Operations Engineer

PREPARED BY:



I hereby certify that this report was prepared by me or under my direct supervision, and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

By: Bryant J. Ficek

Bryant J. Ficek, P.E., P.T.O.E.
License No. 42802

Dated: July 13, 2015

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1. Introduction and Summary

a. Purpose of Report and Study Objectives

A mixed-use building with 123 apartment units and 21,534 square feet of ground floor retail is proposed at the current site of the Cheapo Discs at the northwest corner of the Lake Street W & Fremont Avenue S intersection in Minneapolis, MN.

The purpose of this report is to determine if the proposed development will significantly impact the adjacent transportation system. The report will satisfy the City of Minneapolis' requirements for the renovation site to have a Travel Demand Management Plan in place. Based on discussions with the City of Minneapolis Transportation Division staff, the study objectives are:

- i. Document the existing and proposed conditions with respect to the transit, parking, bicycle and pedestrian facilities.
- ii. Document the parking impacts associated with the proposed development.
- iii. Identify any traffic operations issues.
- iv. Develop and implement strategy measures to encourage non-single occupancy vehicle modes of transportation with the goal of attaining a 55% automobile, 35% transit, 10% pedestrian/bicycle mode split.

b. Executive Summary

This report reviewed the traffic impacts and transportation related design elements of the proposed development at the Lake Street/Fremont Avenue site. The findings of this study are:

- i. Adequate vehicle parking and loading zones will be provided on site per Minneapolis' Code of Ordinances.
- ii. Traffic from the proposed apartment and commercial space is not anticipated to have a significant impact on area traffic operations.
- iii. The site will need to provide convenient and safe bicycle storage for at least 67 bicycles. A minimum of three of which need to meet the needs of short-term bicycle parking and a minimum of 56 need to meet the terms for long-term bicycle parking.
- iv. No roadway or traffic control improvements are recommended as part of the renovation project.
- v. It is recommended the travel demand management measures documented in Section 5 of this report be implemented to encourage non-single occupancy vehicle modes of transportation for the site residents, visitors, and employees.

2. Proposed Development

a. Site Location

The proposed development site is located at the current site of the Cheapo Discs located on the northwest corner of the Lake Street W/Fremont Avenue S intersection in Minneapolis, MN. Figure 2.1 shows the development site.



Figure 2.1 – Location Maps

b. Land Use and Development Plan

The proposed development will consist of one six-story building that includes 123 apartment units and approximately 21,500 square feet

of commercial space on the ground level. Parking is planned to be provided in a two-story parking ramp underneath the site.

Access to the development is proposed via an entrance on Lake Street and an exit on Lagoon Avenue. A concept plan for the development is shown in Figure 2.2 and the Appendix.

The existing building on the site will be demolished as part of the development. The development is assumed to be built and fully occupied in 2017.

The parcel is currently zoned C3A (Commercial District) and is in a Pedestrian Oriented Overlay District.

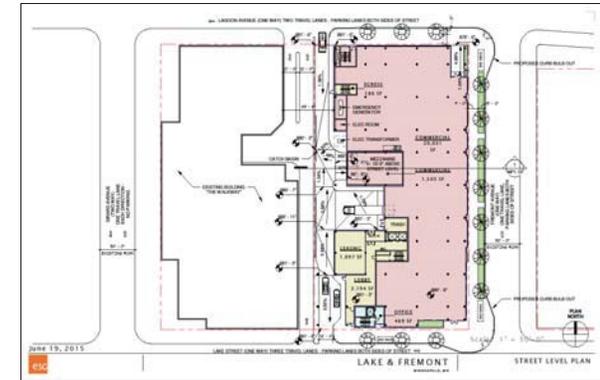


Figure 2.2 – Concept Plan

3. Analysis of Existing Conditions

a. Pedestrians & Bicycles

The area surrounding the site is heavily populated with pedestrians and bicyclists. During a 13 hour turning movement count from 6:00 a.m. to 7:00 p.m. on Tuesday, June 16, 2015 at the Lake Street/Girard Avenue intersection, over 300 bicycles and nearly 3,000 pedestrians crossed through the intersection.

The site is two blocks south of the Midtown Greenway which is a grade separated trail providing access for bicycles and pedestrians

from the western suburbs to the Mississippi River. Many other pedestrian and bicycle friendly trails link up to this trail allowing users easy access to much of the city of Minneapolis. The nearest access to the Greenway is at Girard Avenue.

Sidewalks exist adjacent to all roads surrounding the proposed site. These sidewalks provide pedestrian access to the large sidewalk network in this region of Minneapolis.

People accessing the site could also utilize the Nice Ride bicycle sharing system which has several stations within a few blocks of the site including at Girard Avenue/31st Street and Dupont Avenue/29th Street.

b. Transit

There are multiple bus stops close to the proposed site. The closest stops are on Lake Street in from of the site and on the corner of Lagoon Avenue/Fremont Avenue which serve routes 12, 21, 23 and 53.

- Route 12 provides service from Minnetonka to downtown Minneapolis. Route 12 buses run approximately every 30 minutes throughout the day.
- Route 21 provides service from the Uptown Transit Station to downtown St. Paul. Route 21 buses run approximately every 15 minutes throughout the day.
- Route 23 provides service from the Uptown Transit Center to the Highland neighborhood of St. Paul. Route 23 buses run approximately every 30 minutes throughout the day.
- Route 53 is a limited stop bus that provides service from the Uptown Transit Center to downtown St. Paul. Route 53 buses run approximately every 20 to 30 minutes during rush hour periods.

There are many other bus stops within a short walk of the site that serve many local and express routes including the nearby Uptown Transit Center at Hennepin Avenue/29th Street.

In addition to transit, there are several car and ride sharing companies operating in the City of Minneapolis.

c. Physical Characteristics of Roadway Network

Lake Street W is designated as Hennepin County State Aid Highway 3. It is a three lane, one-way eastbound road with a 30 mph speed limit near the site. There is on-street parking available on both sides of the road near the site.

Lagoon Avenue is designated as Hennepin County State Aid Highway 43. It is a three lane, one-way westbound road with parking on the north side during peak periods. During non-peak periods, on-street parking is allowed in the south through lane, resulting in Lagoon Avenue operating as a two-lane roadway. Parking restriction signs are in place on the south side prohibiting parking during peak periods. The posted speed limit is 30 mph near the site.

Fremont Avenue S is a local City of Minneapolis road. It is a one lane, one-way northbound road with a 30 mph speed limit and on-street parking on both sides near the site. North of Lagoon Avenue, Fremont Avenue becomes a two-way road.

Girard Avenue S is a local City of Minneapolis road. It is a two lane, two-way road with a 30 mph speed limit and no on-street parking near the site. Girard Avenue near the site terminates north of Lagoon Avenue into a parking ramp as well as south of Lake Street into another parking area.

4. Traffic and Parking Analyses

a. Site Traffic Forecasting & Analysis

A trip generation analysis was performed for the site based on the methods and rates published in the *ITE Trip Generation Manual, 9th Edition*. The resultant trip generation for the proposed development is shown in Table 4.1.

Table 4.1 – Vehicles Generated by Development

ITE Land Use Code	Description	Daily			AM Peak Hour			PM Peak Hour		
		In	Out	Pass by	In	Out	Pass by	In	Out	Pass by
220	Apartment (123 units)	196	196	0	6	25	0	23	12	0
826	Specialty Retail (21,534 square feet)	395	395	407	12	8	10	32	34	34
Total		591	591	407	18	33	10	55	46	34

A detailed *Traffic Impact Study*, which includes a more detailed trip generation, was completed for the development and can be seen in the Appendix. This trip generation includes reductions in vehicle traffic to reflect the multi-modal nature of the area the site is in. The *Traffic Impact Study* found that the addition of traffic from the

proposed development will not cause any additional issues on the surrounding transportation system through the year 2017.

b. Vehicle Parking Forecasting & Analysis

According to the Minneapolis Code of Ordinances *Table 541-1 Specific Off-Street Parking Requirements*, for a 21,500 square foot general retail area, a minimum of 35 parking spaces are needed with a maximum of 108 spaces. According to that same table, for a 123 unit apartment building a minimum of 123 parking spaces are needed with no maximum.

The Minneapolis Code of Ordinances *Section 551.140 Accessory Parking* for the Pedestrian Oriented Overlay District says the minimum and maximum off-street parking limits for nonresidential uses should be 75% of that listed in *Table 541-1*. That puts the required number of parking stalls for the commercial space at a minimum of 26 and a maximum of 81.

Overall, the City of Minneapolis requirements put the site at needing at least 149 parking spaces and at most 204 parking spaces.

The residential parking requirement can be reduced by 10% (or 12 spaces) since the site is within 300 feet of a transit stop with midday service headways of 30 minutes or less in each direction. If this reduction is approved by the zoning administrator, the minimum number of parking spaces required would be 137.

The development is proposed to have 158 parking spaces located in its two story underground parking ramp. With these 158 spaces being provided, the development will meet the City of Minneapolis requirements for off-street vehicle parking.

The Institute of Transportation Engineers has put together a document that compiled parking demand data from different land uses. This document, *ITE Parking Generation, 4th Edition*, lists that for a high-rise apartment (five or more levels) in a central city (not downtown) during a weekday the average peak period parking demand is 1.37 vehicles per dwelling unit. For the proposed 123 apartment units the average peak period parking demand for the residential portion of the site is 169 parking spaces.

For a shopping center, the *ITE Parking Generation, 4th Edition*, lists an average peak period parking demand per 1,000 square feet of 2.55 vehicles on a non-Friday weekday not in December. For the

21,500 square feet of commercial space this puts the average peak period parking demand at 55 spaces on a weekday.

Totaling up the *ITE Parking Generation, 4th Edition* residential and commercial land uses, the average peak period parking demand on a weekday for both land uses would be a total development site average peak period parking demand of 224 vehicle spaces.

Although ITE provides a good number for comparison, the number of calculated vehicle spaces is higher than the likely demand at the proposed site. The ITE numbers do not account for overlap of parking, a resident parking and then use the retail. In addition, the ITE rates include suburban and rural sites that do not have the transit and bicycle/pedestrian options available.

It should also be noted there are two parking ramps within one block of the site that could be utilized by visitors if the sites parking ramp is full.

c. Bicycle Parking Forecasting & Analysis

According to the Minneapolis Code of Ordinances *Table 541-3 Bicycle Parking Requirements*, a minimum of five bicycle parking spaces are required for the commercial space (one parking spot for every 5,000 square feet) with at least three of them meeting the criteria for short-term bicycle parking (within 50 feet of a principle entrance, permit the locking of the frame and one wheel to the rack and support the bicycle in a stable position). For the apartment, a minimum of one space per two dwelling units is needed; or 62 bicycle parking spots with at least 56 of them meeting requirements for long-term bicycle parking (in an enclosed and secured or supervised area with protection from theft and weather).

d. Loading Zones

Per the requirements of Minneapolis Code of Ordinances Section 541.480, one small loading space is required for apartment complexes with between 100 and 250 units and one small space is required for a retail area with between 20,000 and 50,000 square feet. A small loading space is defined to be at least 10 feet wide by at least 25 feet long. According to the site plan shown in Figure 2.2 there is an off-street area near the ramp for the parking that can be utilized as the loading zone area.

5. Travel Demand Management (TDM) Plan

The developer, building owner, property manager or their successors of the site will develop and implement measures to encourage non-single occupancy vehicle modes of transportation with the goal of having at least 45% of its transportation trips being made by transit, bicycling, or walking. At a minimum, the following strategies will be implemented to minimize the site's impact on the area transportation system.

a. Designate a Transportation Coordinator

The developer will designate an employee or contractor to act as the Transportation Coordinator. That employee will maintain and monitor TDM activities as well as serve as liaison to Metro Commuter Services and Metro Transit. The Transportation Coordinator will serve as the conduit for providing up-to-date information on alternative commute programs and incentives to all building occupants. At a minimum, the Transportation Coordinator will:

- **Make available information on various bus incentive programs (e.g. Metro Transit Go-To Cards, U-Pass and Commuter Challenge program) as well as vanpool incentives (e.g. Metro Vanpool program).**
- Set up and maintain a display of commuter information at the entrance of the building or in an employee break room. This information, which will be supplied by Metro Transit, will include transit schedules, rideshare applications, bike information, Guaranteed Ride Home Program brochures, etc. To maintain an awareness of alternative modes of transportation, information may be distributed through e-mail, flyers, posters in frequented locations, etc.
- Provide a move-in packet for new residents containing information on alternative transportation modes available at the time. Packets as well as other resources are available from the Downtown Minneapolis Transportation Management Organization (<http://www.mplstmo.org/>).
- Distribute information on Mn/DOT's real-time traveler information program: 5-1-1 or www.511mn.org.

b. Promote Walking

Well lit sidewalks will be provided on each side of the site. The site will meet Minneapolis' requirements for on-site lighting to provide a pedestrian friendly environment. If any existing sidewalks are impacted by construction, the sidewalks shall be rebuilt with ADA-compliant tactile dome curb ramps to assist the visually impaired.

c. Promote Bicycling

The building will provide convenient and safe bicycle storage for at least 67 bicycles. Three of which need to meet the needs of short-term bicycle parking and 56 need to meet the terms for long-term bicycle parking. This will meet requirements in Minneapolis' Code of Ordinances *Table 541-3 Minimum Bicycle Parking Requirements*.

d. Minimize the Impact of Trucks

To the extent possible, the developer or their successors will encourage truck and service vehicles (not including small package deliveries) to access the site outside of the weekday peak hours (from 7-9 a.m. and 4-6 p.m.). Loading/unloading of deliveries as well as garbage trucks can be accommodated on-site within the loading zone area.

6. Conclusions & Recommendations

Traffic from the proposed apartment and commercial space is not anticipated to have a significant impact on area traffic operations. No roadway or traffic control improvements are recommended as part of the development project. Adequate loading zones and vehicle parking will be provided on site per Minneapolis' Code of Ordinances. The site will need to provide convenient and safe bicycle storage for at least 67 bicycles. Three of which need to meet the needs of short-term bicycle parking and 56 need to meet the terms for long-term bicycle parking.

It is recommended the travel demand management measures documented in Section 5 of this report be implemented to encourage non-single occupancy vehicle modes of transportation for the site's residents, employees and visitors.

7. Appendix

a. Site Plan

b. Traffic Impact Study

Technical Memorandum

To: Allan Klugman, P.E., P.T.O.E., City of Minneapolis
From: Bryant Ficek, P.E., P.T.O.E.
Date: July 13, 2015
Re: Traffic Impact Study for Lake & Fremont Apartments

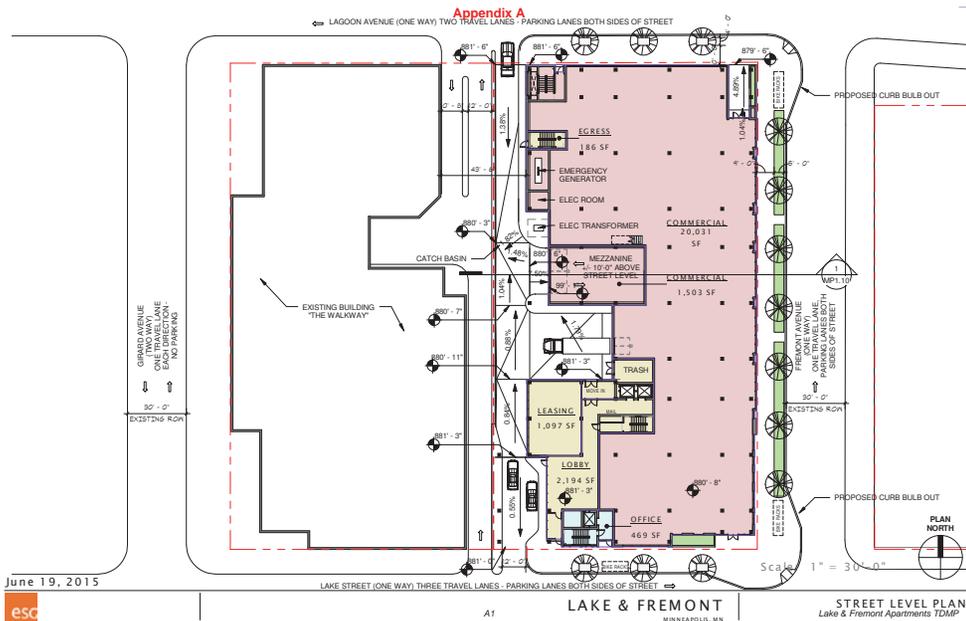
Purpose of Report and Study Objectives

A mixed-use building with 123 apartment units and 21,534 square feet of ground floor retail is proposed at the current site of the Cheapo Discs at the northwest corner of the Lake Street W & Fremont Avenue S intersection in Minneapolis, MN. The purpose of this report is to determine if the proposed development will significantly impact the adjacent transportation system and to recommend mitigation measures if necessary. Based on discussions with City of Minneapolis staff, the study objectives are:

- i. Document how the Lake Street W/Fremont Avenue S, Lake Street W/Girard Avenue S, Lagoon Avenue/Girard Avenue S and Lagoon Avenue/Fremont Avenue S intersections currently operate.
- ii. Forecast the amount of traffic expected to be generated by the proposed development accounting for the expected high use of transit/walking/bicycling.
- iii. Analyze how the Lake Street W/Fremont Avenue S, Lake Street W/Girard Avenue S, Lagoon Avenue/Girard Avenue S and Lagoon Avenue/Fremont Avenue S intersections and site driveways will operate with full development of the site in the year 2017.
- iv. Examine one-way versus two-way operations on Fremont Avenue S between Lake Street W and Lagoon Avenue.
- v. Recommend improvements if needed.

Conclusions and Recommendations

Based on the following analysis, the proposed development is forecast to generate approximately 1,180 new vehicle trips per day with 50 new vehicle trips generated in the a.m. peak hour and 100 new vehicle trips in the p.m. peak hour. The existing study intersections currently operate acceptably at overall Level of Service B or better in the weekday a.m. and p.m. peak hours and are forecast to operate acceptably at Level of Service B or better in the future with the addition of the development's traffic. The site driveways are also forecast to operate acceptably in the 2017 Build scenarios. No modifications to the existing public transportation system or the proposed development design will be needed to accommodate forecasted traffic levels from this development. A quick review of one-way versus two-way operation on Fremont Avenue, between Lake Street and Lagoon Avenue, revealed little value in changing the current one-way design.



June 19, 2015



Appendix B

Proposed Development Plan

The development site is located at the site of the existing Cheapo Discs at the northwest corner of the Lake Street/Fremont Avenue intersection in Minneapolis, MN (see Figure 1 for site location and Figure 2 for a concept plan). It will consist of one six-story building with 123 apartment units and approximately 21,500 square feet of commercial space on the ground level.

Parking is planned to be provided in a two-story underground ramp beneath the site. There is proposed to be vehicle access to the development via an entrance on Lake Street and an exit on Lagoon Avenue.

The existing building on the site will be demolished as part of the development. The development is assumed to be built and fully occupied in 2017 for the purposes of this study.

Area Transportation Network Characteristics

Lake Street W is designated as Hennepin County State Aid Highway 3. It is a three lane, one-way eastbound road with a 30 mph speed limit near the site. There is on-street parking available on both sides of the road near the site.

Lagoon Avenue is designated as Hennepin County State Aid Highway 43. It is a three lane, one-way westbound road with parking on the north side during peak periods. During non-peak periods, on-street parking is allowed in the south through lane, resulting in Lagoon Avenue operating as a two-lane roadway. Parking restriction signs are in place on the south side prohibiting parking during peak periods. The posted speed limit is 30 mph near the site.

Fremont Avenue S is a local City of Minneapolis road. It is a one lane, one-way northbound road with a 30 mph speed limit and on-street parking on both sides near the site. North of Lagoon Avenue, Fremont Avenue becomes a two-way road.

Girard Avenue S is a local City of Minneapolis road. It is a two lane, two-way road with a 30 mph speed limit and no on-street parking near the site. Girard Avenue near the site terminates north of Lagoon Avenue into a parking ramp as well as south of Lake Street into another parking area.

The two study intersections on Fremont Avenue are stop controlled with stop signs on Fremont Avenue while Lake Street and Lagoon Avenue are free flowing. The two study intersections on Girard Avenue are signalized. The existing roadway configuration is shown in Figure 3.

Existing Traffic Volumes

Video of the study intersections was collected during normal weekday conditions in June 2015. Turning movement volumes were counted at the study intersections from 6:00 a.m. to 7:00 p.m. and during the peak periods at the two existing site driveways. The a.m. and p.m. peak hour times for each intersection were found from these counts. The peak hour turning movement volumes at the study intersections are shown in the Attached capacity analysis section for each study scenario. The turning movement count data is contained in fifteen minute intervals is also Attached. The data also includes bicycle and pedestrian counts. There are a significant amount of bicycles and pedestrians in this area with over 300 bicycles and nearly 3,000 pedestrians crossing just at the Lake Street/Girard Avenue intersection during the 13 hour count.

The existing site has a driveway access on Fremont Avenue as well as on Lagoon Avenue. From the traffic counts it can be seen that volumes are low at each of these driveways. Because of that, the

Appendix B

existing counts at the driveways are not being used in the analysis and will not be factored out of the network for the future scenarios.

Another note that was made watching the videos for the turning movement counts was that during the p.m. peak hour, the traffic headed westbound on Lagoon Avenue would occasionally queue up at the Hennepin Avenue intersection and back up to the Girard Avenue intersection. This would cause vehicles to not be able to move freely through the intersection.

Existing Level of Service Capacity Analyses

An intersection capacity analysis was conducted for the existing intersections per the *Highway Capacity Manual*. Intersections are assigned a "Level of Service" letter grade for the peak hour of traffic based on the number of lanes at the intersection, traffic volumes, and traffic control. Level of Service A (LOS A) represents light traffic flow (free flow conditions) while Level of Service F (LOS F) represents heavy traffic flow (over capacity conditions). LOS D at intersections is typically considered acceptable in the Twin Cities region. Individual movements are also assigned LOS grades. One or more individual movements typically operate at LOS F when the overall intersection is operating acceptably at LOS D.

The LOS results for the existing study hours are shown in Table 1. These are based on the existing lane configurations and lane usages. The existing turning movement volumes were used in the LOS calculations. The LOS calculations were done per the 2010 *Highway Capacity Manual* using VISTRO™ software. Signal timings were provided from the city. The complete LOS calculations, which include grades for individual movements, are Attached.

Table 1 – Existing Peak Hour Level of Service (LOS)¹

Intersection	A.M. Peak Hour	P.M. Peak Hour
Lake St/Fremont Ave	A (c)	A (f)
Lake St/Girard Ave	A (d)	B (d)
Lagoon Ave/Girard Ave	A (e)	A (e)
Lagoon Ave/Fremont Ave	A (c)	A (f)

¹The first letter is the Level of Service for the intersection. The second letter (in parentheses) is the Level of Service for the worst operating movement.

The study intersections currently operate acceptably at LOS B or better during both the a.m. and p.m. peak hours. In the p.m. peak hour the northbound through movements at the Lake Street/Fremont Avenue intersection are at LOS F and the northbound approach at the Lagoon Avenue/Fremont Avenue intersection is also at LOS F. The average delays for both of these LOS F spots is a bit over one minute long and 95th percentile queues are approximately two vehicles at Lake Street/Fremont Avenue and approximately five vehicles at Lagoon Avenue/Fremont Avenue. These types of results are common during the peak periods in the Twin Cities.

The Girard Avenue movements at the intersection with Lagoon currently operate at LOS E during the peak hours. This result is due to the low volumes on the side street and the signal timing, which puts the majority of green time to the main road. Lagoon Avenue has over 95% of the traffic volume and should have the majority of green time.

Although a result less than LOS D is not desired, the cost outweigh any benefits of mitigation in this case and no changes are recommended for the existing roadway network.

Appendix B

Site Traffic Forecasting

A trip generation analysis was performed for the site based on the methods and rates published in the *ITE Trip Generation Manual, 9th Edition*. The resultant trip generation is shown in Table 2. This trip generation utilizes internal trip percentages from the methods published in the *ITE Trip Generation Handbook, 2nd Edition*.

Due to the location of the proposed development, it is expected there will be a significant amount of people travelling to and from the site via transit, bicycling and walking. To account for this, a reduction was made from the vehicle trip generation. A goal of the Travel Demand Management Plan for the site, of which this study is a part of, is to have at least 45% of the trips to/from the site be made by transit, bicycling or walking. To see if this reduction is feasible, vehicle trip generation data at the nearby Buzza Historic Lofts building located three blocks east of this site was compared to data from the *ITE Trip Generation Manual, 9th Edition* used in a previous TDMP done by Spack Consulting for the Buzza building. Four days of weekday trip generation data were collected at the Buzza building in the fall of 2014. Averaging the data from those days showed that the number of vehicles visiting the site only equaled approximately 45% of the estimated trips using ITE data; a reduction of 55%. Therefore, using the desired 45% reduction in vehicle trips is reasonable for this site. This data as well as a detailed trip generation is Attached.

Table 2 – Trip Generation

ITE Land Use Code	Description	Daily			AM Peak Hour			PM Peak Hour		
		In	Out	Pass by	In	Out	Pass by	In	Out	Pass by
220	Apartment (123 units)	196	196	0	6	25	0	23	12	0
826	Specialty Retail (21,534 sq ft)	395	395	407	12	8	10	32	34	34
Total		591	591	407	18	33	10	55	46	34

There are three different types of trips that will visit a development at a location with multiple land uses such as that of the proposed site; new trips, pass-by trips and internal trips. New trips are drivers that visit the site specifically to go to one land use within the proposed development and then return from where they came. Pass-by trips are drivers that are already on the roads passing by the site, but will include a stop at the proposed development in the future. When these vehicles leave the site they then continue in the direction they were originally heading. Internal trips are those involving stops at two or more land uses within the proposed site. For this development, internal trips would consist of people going to both the residential and retail portions of the site. Internal trips reduce the overall number of generated trips and the number of vehicles through the study intersections.

The estimated directional orientation of the generated traffic is shown in Figure 4 with a detailed distribution shown in Figure 5. This orientation was determined by using the collected turning movement counts as well as taking into account site access and access to the regional transportation system. The traffic generated by the development was assigned to the area roadways per the distribution pattern and then added to the study roadways

Traffic forecasts were developed for the year 2017 No-Build scenarios by applying a 1% compounded annual growth rate to the existing traffic volume data. This accounts for potential redevelopments within the region. This growth rate is a standard growth rate used for the City of Minneapolis.

Traffic forecasts were developed for each study period in the 2017 Build scenarios by adding the traffic generated by the proposed development to the 2017 No-Build volumes. The peak hour turning

Appendix B

movement volumes at the study intersections are shown in the Attached capacity analysis section for each study scenario.

Forecasted Level of Service Capacity Analyses

The LOS results for the 2017 peak hour scenarios are shown in Table 3. These are based on the existing traffic control, lane usages, and lane configurations at the study intersection. No improvements are programmed at the existing study intersection.

The forecast turning movement volumes for the No-Build and Build scenario peak hours were used in the LOS calculations. The LOS calculations were done in accordance with the *2010 Highway Capacity Manual* using VISTRO™ software. The signal timings from the existing scenarios were used. The complete LOS calculations, which include grades for individual movements, are Attached.

Table 3 – 2017 Peak Hour Level of Service (LOS)¹

Intersection	AM Peak Hour		PM Peak Hour	
	No-Build	Build	No-Build	Build
Lake St/Fremont Ave	A (c)	A (c)	A (f)	A (f)
Lake St/Girard Ave	A (d)	A (d)	B (d)	B (d)
Lagoon Ave/Girard Ave	A (e)	A (e)	A (e)	A (e)
Lagoon Ave/Fremont Ave	A (c)	A (c)	B (f)	B (f)
Lake St/Site Access	n/a	A (a)	n/a	A (a)
Lagoon Ave/Site Access	n/a	A (c)	n/a	A (e)

¹The first letter is the Level of Service for the intersection. The second letter (in parentheses) is the Level of Service for the worst operating movement.

The results in Table 3 show there is no change in LOS at the study intersections between existing and the 2017 scenarios, meaning the proposed development will have little to no impact on the study intersections. All intersections are forecast to operate overall at LOS B or better in the 2017 scenarios.

There are some movements that are forecast to operate at LOS F. At the Lake Street/Fremont Avenue intersection, the northbound through movement is forecast to be at LOS F in both 2017 scenarios. 95th percentile queue lengths are forecast to be less than three vehicles in both scenarios and delays topping out at about one and a half minutes in the Build scenario. At the Lagoon Avenue/Fremont Avenue intersection, the northbound approach is forecast to be at LOS F in both 2017 scenarios. 95th percentile queue lengths are forecast to be approximately six vehicles in the No-Build and approximately seven vehicles in the Build scenario with delays going from about 90 to 110 seconds between the No-Build and Build. While some of these movements do operate with poor LOS grades, the queue lengths and delay times are not unreasonable for intersections in a busy, urban commercial area such as this.

The Girard Avenue movements at the intersection with Lagoon are expected to continue to operate at LOS E during the peak hours. As mentioned, this result is due to the low volumes on the side street and the signal timing, which puts the majority of green time to Lagoon Avenue. These types of results are also common during the peak periods in the Twin Cities.

No modifications to the existing roadway network are needed to accommodate this development.

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One-Way vs Two-Way Operations on Fremont Avenue

Currently Fremont Avenue operates as a one-way northbound road between Lake Street and Lagoon Avenue next to the site. Fremont Avenue is a one-way northbound road south of Lake Street and is a two-way road north of Lagoon Avenue. The section of Fremont Avenue between Lake Street and Lagoon Avenue has undergone changes in the past from being a two-way road.

For this site there are no accesses on Fremont Avenue, so changing it from a one-way to a two-way road would have no effect on the proposed development's vehicle traffic.

The City of Minneapolis has requested that when the site is redeveloped that bulb outs be made on the sidewalks on Fremont Avenue at both Lake Street and Lagoon Avenue. See the Attached site plan in for an image of this. These bulb outs effectively guarantee on-street parking will be present on the west side of Fremont Avenue, adjacent to the proposed development. If Fremont Avenue is converted to a two-way road, the east lane of parking could be easily restricted to allow for the additional through lane of traffic.

Converting Fremont Avenue to a two-way road would reduce the amount of on-street parking on that one block. The proposed development is expected to have sufficient off-street parking and would not be impacted by the removal of parking. However, the City should consider the area-wide ramifications of removing public parking spaces, which is beyond the scope of this study.

Two-way operation would also increase the complexity of the Lake Street/Fremont Avenue and Lagoon Avenue/Fremont Avenue intersections by introducing an additional southbound movement. This increase in conflicting movements would result in minor delay increases at these intersections. However, vehicles attempting to go south on Fremont Avenue to access Lake Street would no longer need to route around Girard Avenue via Lagoon Avenue. This improvement in accessibility in the area is tempered by the movements not being very high in volume. Combined with having the one way pair Emerson Avenue one block east of Fremont Avenue and accessible via 28th or 29th Street just north of Lagoon Avenue, there appears to be little value in converting Fremont Avenue back to a two-way road.

Attachments

- Figures 1 through 5
- Trip Generation Tables
- Traffic Counts
- Capacity Analysis Backup

Appendix B



Figure 1
Location Maps

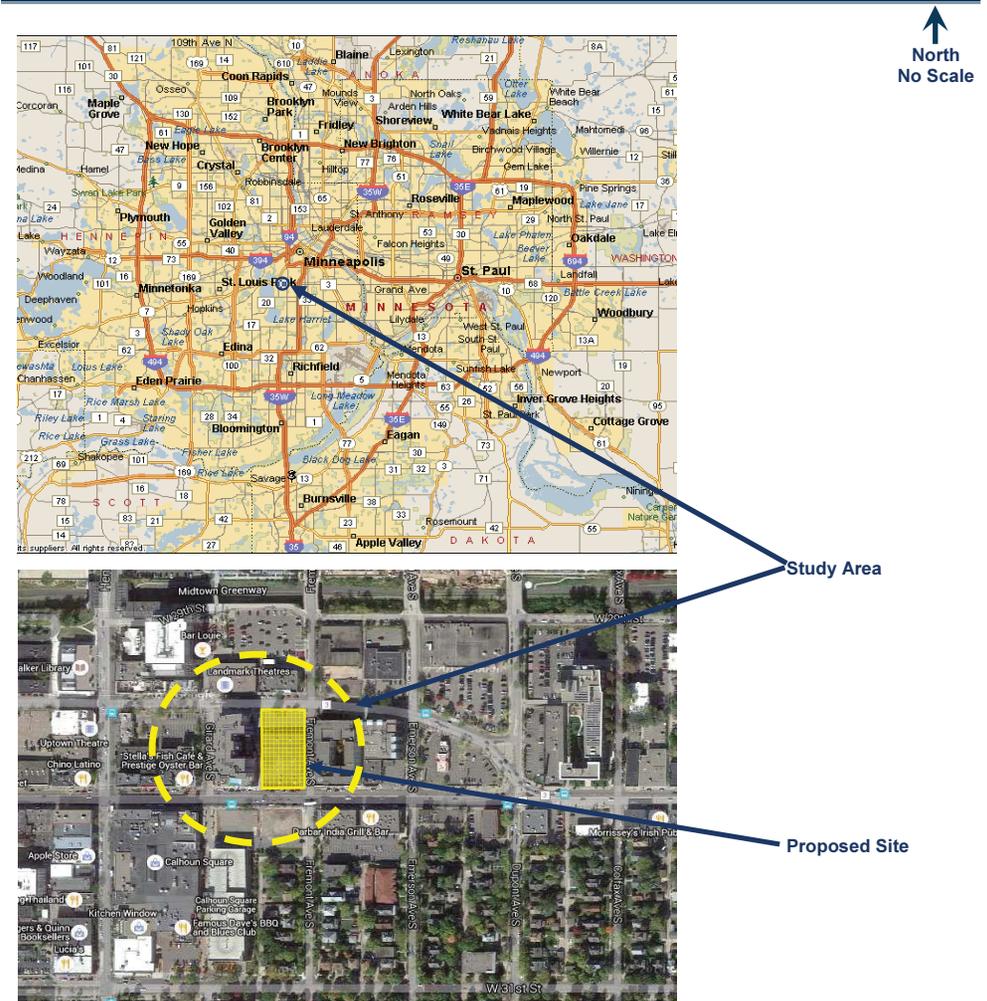


Figure 2
Concept Plan

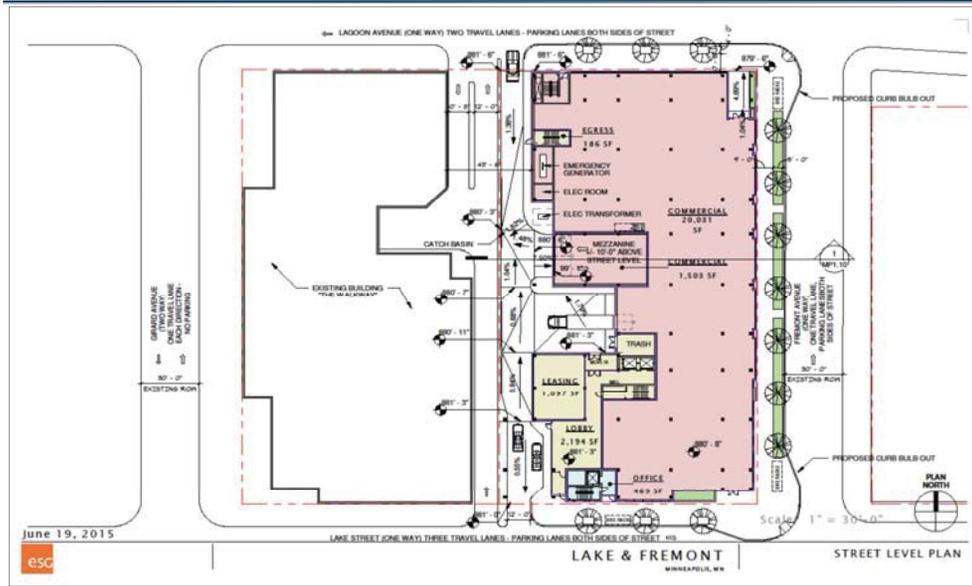
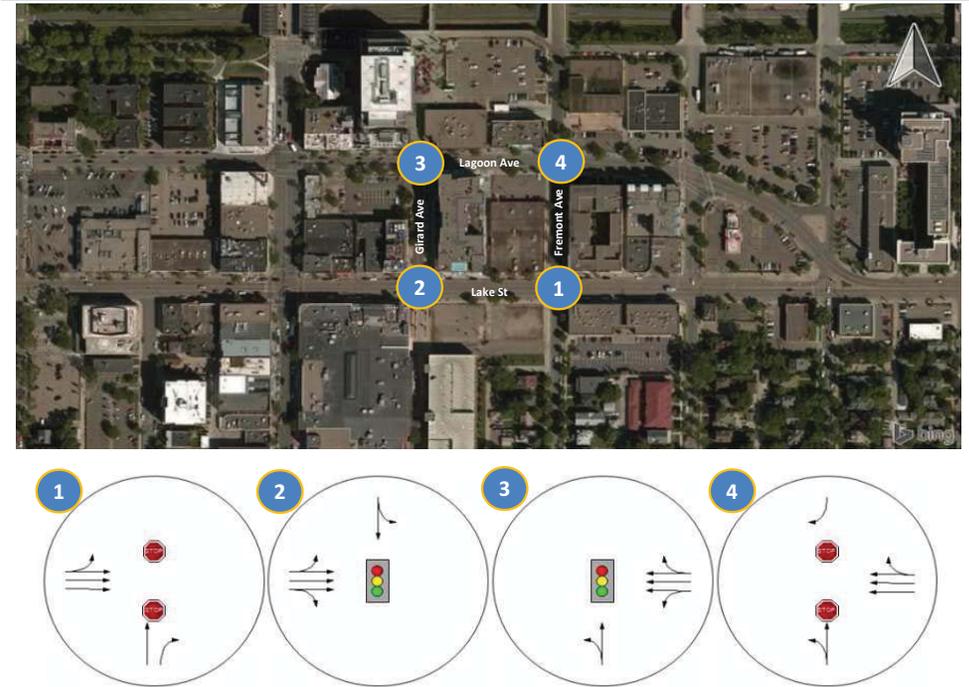


Figure 3
Existing Roadway Configuration



Appendix B



Appendix B



Appendix B



Table B1
Forecast Trip Generation

Daily Volumes													
LAND USE	ITE CODE #	DEVELOPMENT UNITS	QUANTITY	DAILY RATE	ENTER PERCENT	EXIT PERCENT	PED/BIKE/TRANSIT REDUCTION	INTERNAL PERCENT	INTERNAL TRIPS	PASSBY PERCENT	PASSBY TRIPS	NEW TRIPS	
												ENTER	EXIT
Apartment	220	Dwelling Unit	123.0	6.65	50%	50%	45%	13%	58	0%	0	196	196
Specialty Commercial/Retail	820	1,000 GFA	21.5	116.24	50%	50%	45%	13%	179	34%	407	385	385
			Total						237		407	591	591

AM Peak Hour													
LAND USE	ITE CODE #	DEVELOPMENT UNITS	QUANTITY	DAILY RATE	ENTER PERCENT	EXIT PERCENT	PED/BIKE/TRANSIT REDUCTION	INTERNAL PERCENT	INTERNAL TRIPS	PASSBY PERCENT	PASSBY TRIPS	NEW TRIPS	
												ENTER	EXIT
Apartment	220	Dwelling Unit	123.0	0.51	20%	80%	45%	10%	3	0%	0	6	25
Specialty Commercial/Retail	820	1,000 GFA	21.5	2.84	62%	38%	45%	10%	3	34%	10	12	8
			Total						7		10	18	33

PM Peak Hour													
LAND USE	ITE CODE #	DEVELOPMENT UNITS	QUANTITY	DAILY RATE	ENTER PERCENT	EXIT PERCENT	PED/BIKE/TRANSIT REDUCTION	INTERNAL PERCENT	INTERNAL TRIPS	PASSBY PERCENT	PASSBY TRIPS	NEW TRIPS	
												ENTER	EXIT
Apartment	220	Dwelling Unit	123.0	0.62	55%	35%	45%	15%	6	0%	0	23	12
Specialty Commercial/Retail	820	1,000 GFA	21.5	9.94	48%	52%	45%	15%	18	34%	34	32	34
			Total						24		34	55	46

- NOTES:
- CFA = Gross Floor Area
 - All trip generation rates based on "Trip Generation", Institute of Transportation Engineers, 9th Edition unless otherwise noted.
 - Reduction for internal trips (Internal Percent) is based on "Trip Generation Handbook", Institute of Transportation Engineers, 2nd Edition.
 - Reduction for pass-by trips (Passby Percent) is based on "Trip Generation Handbook", Institute of Transportation Engineers, 2nd Edition.
 - A.M. Trip Generation is for the peak hour of adjacent street traffic (one hour between 7 and 9 a.m.).
 - P.M. Trip Generation is for the peak hour of adjacent street traffic (one hour between 4 and 6 p.m.).
 - Rates for ITE 820 based on Fitted Curve Equation for each time period.

Appendix B



Table B2
Forecast Trip Generation Reduction Calculation

Collected Trip Generation Data at Buzaa Building (Total Trips)					
	Tuesday 8/26/2014	Wednesday 8/27/2014	Wednesday 9/10/2014	Thursday 9/10/2014	Average Trips
Daily	351	305	402	362	355
AM Peak Hour - Adjacent Street (7-9am)	31	29	32	33	31
PM Peak Hour - Adjacent Street (4-6pm)	22	24	42	35	31

ITE Trip Generation Data for Buzaa Building (Total Trips)			
	Rate of trips per unit	Dwelling Units	Total Trips
Daily	6.65	136	904
AM Peak Hour - Adjacent Street (7-9am)	0.51	136	69
PM Peak Hour - Adjacent Street (4-6pm)	0.62	136	84

Collected Data vs ITE Estimates (Total Trips)			
	Collected Data	ITE Estimate	% of ITE
Daily	355	904	39%
AM Peak Hour - Adjacent Street (7-9am)	31	69	45%
PM Peak Hour - Adjacent Street (4-6pm)	31	84	36%

6/18/15 COW Submittal

EXTERIOR MATERIAL KEYNOTES

- 1 BRICK VENEER - COLOR #1 (DARK GRAY)
- 2 NATURAL STONE VENEER
- 3 WESTERN RED CEDAR T&G SIDING
- 4 COLORED SMOOTH FACE CMU (BURNISHED AT GRADE)
- 5a METAL PANEL - COLOR #1 (GRAY BLEND)
- 5b METAL PANEL - COLOR #2 (WHITE)
- 5c METAL PANEL - COLOR #3 (CHARCOAL)
- 5d COMPOSITE METAL PANEL - COLOR #4 (OCHRE)
- 5e COMPOSITE METAL PANEL - COLOR #5 (PURPLE)
- 5f COMPOSITE METAL PANEL - COLOR #6 (SILVER)
- 5g COMPOSITE METAL PANEL - COLOR #7 (GREEN BLEND)
- 6 CEMENTITIOUS STUCCO - COLOR #1 (WHITE)
- 7 FIBER CEMENT BOARD - COLOR #1 (WHITE)
- 8 PREFINISHED METAL COPING - (MATCH WALL FINISH)
- 11 PNTD MECHANICAL LOUVER (MATCH WALL FINISH)
- 12a ALUM STOREFRONT GLAZING
- 12b ALUM STOREFRONT GLAZING - SPANDREL
- 13 FIBERGLASS WINDOW
- 14 PREFIN ALUM BALCONY & GUARDRAIL
- 15 OVERHEAD DOOR
- 16 DECORATIVE METAL SCREEN
- 17 PREFIN METAL LOUVER
- 18 SIGNAGE



Scale: 1/16" = 1'-0"

June 9, 2015



LAKE & FREMONT - 6/18/15 CPC CoW Meeting
MINNEAPOLIS, MN

SOUTH ELEVATION
(LAKE ST)



Scale: 1/16" = 1'-0"

June 9, 2015



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EAST ELEVATION
(FREMONT AVE)



6/18/15 COW Submittal

EXTERIOR MATERIAL KEYNOTES

- 1 BRICK VENEER - COLOR #1 (DARK GRAY)
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- 18 SIGNAGE



June 9, 2015



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NORTH ELEVATION
(LAGOON AVE)

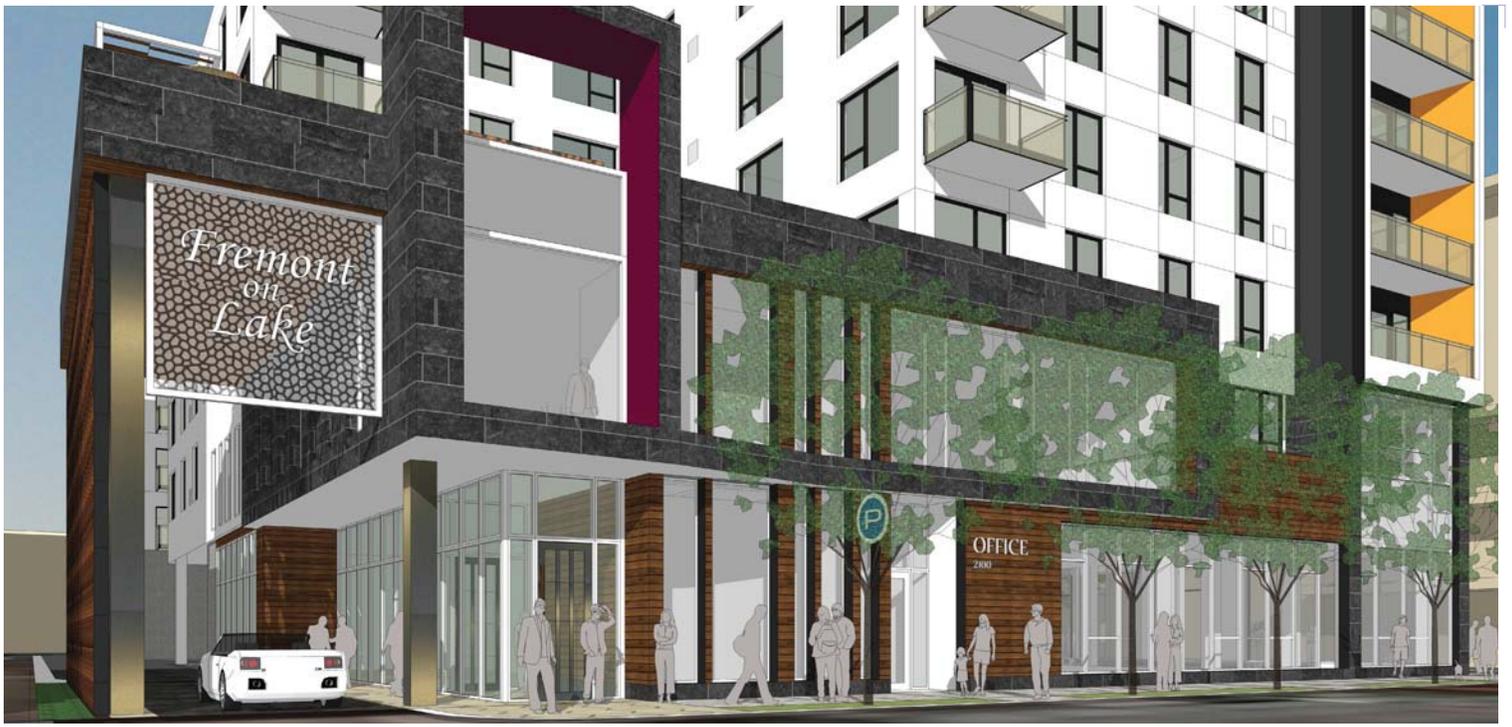


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MINNEAPOLIS, MN

WEST ELEVATION



6/18/15 COW Submittal

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esc

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LAKE STREET FACADE



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AERIAL VIEW FROM LAKE ST

6/18/15 COW Submittal



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LAKE & FREMONT - 6/18/15 CPC CoW Meeting
MINNEAPOLIS, MN

VIEW FROM CORNER OF
LAGOON & FREMONT



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LAKE & FREMONT - 6/18/15 CPC CoW Meeting
MINNEAPOLIS, MN

VIEW FROM LAGOON AVE

6/18/15 COW Submittal



June 9, 2015



LAKE & FREMONT - 6/18/15 CPC CoW Meeting
MINNEAPOLIS, MN

VIEW FROM LAKE & FREMONT