

LAND USE APPLICATION SUMMARY

Property Location: 112 East Hennepin Avenue
Project Name: 116 East Hennepin Redevelopment
Prepared By: Lisa Steiner, Senior City Planner, (612) 673-3950
Applicant: Schafer Richardson
Project Contact: Katie Anthony
Request: To construct a new six-story building attached to two historic buildings.
Required Applications:

Conditional Use Permit	To increase the maximum height in the C3A District from the permitted 4 stories/56 feet to 6 stories/75 feet.
Variance	To increase the maximum floor area ratio from 3.78 to 4.51.
Variance	To reduce the required interior side yard from 15 feet to 5 feet.
Variance	To reduce the minimum window requirement below 40 percent in the PO Pedestrian Oriented Overlay District.
Variance	To reduce the nonresidential parking requirement from 8 spaces to 0 spaces.
Variance	To increase the percentage of required parking spaces that may be satisfied by providing compact spaces.
Site Plan Review	For a new six-story mixed-use building with 72 dwelling units.

SITE DATA

Existing Zoning	C3A Community Activity Center District PO Pedestrian Oriented Overlay District MR Mississippi River Critical Area Overlay District
Lot Area	20,001 square feet / 0.46 acres
Ward	3
Neighborhood	Nicollet Island – East Bank
Designated Future Land Use	Mixed Use
Land Use Features	Commercial Corridor (East Hennepin Avenue); East Hennepin Activity Center
Small Area Plan	<u>Nicollet Island - East Bank Neighborhood Small Area Plan (2014)</u>

Date Application Deemed Complete	January 29, 2016	Date Extension Letter Sent	Not applicable
End of 60-Day Decision Period	March 29, 2016	End of 120-Day Decision Period	Not applicable

BACKGROUND

SITE DESCRIPTION AND PRESENT USE. The subject property has four separate structures that are currently connected internally at the ground floor. The site houses a restaurant use with dwelling units in the upper stories of the structures. The oldest building on the site is the three-story “Harness Shop” building at 116 East Hennepin Avenue, which was constructed in 1905. The two-story building at 112 East Hennepin Avenue, at the corner of East Hennepin and Lourdes Place, was constructed in 1907. In approximately 1955, Nye’s Bar opened in the 112 East Hennepin building. In 1964, a one-story addition was constructed between the buildings to accommodate the expansion of Nye’s and the establishment of the Nye’s Polonaise dining room. Nye’s expanded into the ground floor of the “Harness Shop” building in 1967. The one-story building at 120 East Hennepin was constructed in 1960 and housed another restaurant until Nye’s expanded into the building in 1973. These four connected structures have a combined building footprint of approximately 8,000 square feet. The remainder of the approximately 20,000 square foot property is a surface parking lot.

SURROUNDING PROPERTIES AND NEIGHBORHOOD. The surrounding area contains a mix of residential, commercial, office, and institutional uses. The Our Lady of Lourdes church and rectory building are located directly southeast of the subject property. Office buildings connected by a skyway are located across Lourdes Place and East Hennepin Avenue from the property. Across 2nd Street Southeast there is currently a surface parking lot and two-story townhomes. Two high-rise multi-family residential towers are located within a few blocks of the subject property as well. There are also several restaurants and other commercial uses located to the northeast of the property on East Hennepin Avenue.

PROJECT DESCRIPTION. The applicant is proposing a mixed-use redevelopment project which would include 72 dwelling units and approximately 8,000 square feet of retail space at the first floor. The applicant is proposing to demolish the two one-story structures on the site but preserve the two-story and three-story buildings. A new six-story building would be constructed around the remaining two historic buildings on site.

A four-story infill portion of the proposed building would be constructed between the two historic buildings which would step up to six stories approximately 35 feet back from the East Hennepin Avenue façade. This part of the building would be clad in dark metal panel and would connect to a six-story portion of the new building and a three-story addition constructed over the rear 30 feet of the “Harness Shop” building. Both of these sections would be clad in brown brick. Along 2nd Street Southeast and wrapping around East Hennepin to meet the “Harness Shop” building, another portion of the new building would be clad in a darker brick with a light metal panel sixth story. The residential entrance would be located along 2nd Street Southeast. Retail entrances would be located on East Hennepin Avenue. The storefronts of the historic buildings would be restored if historic material remains or new storefronts will be installed which would incorporate the basic design features of a traditional storefront.

Thirty parking spaces (with six additional tandem spaces) would be located underground and accessed from Lourdes Place. Twenty parking spaces would be located on the ground level and accessed by a garage door and curb cut located on 2nd Street Southeast. In addition, the applicant is proposing to install a semi-automated car-stacking parking system on the ground level which would allow for fourteen additional parking spaces stacked above other parking spaces. In total, when accounting for the non-traditional tandem and stacked parking spaces, 70 parking spaces would be accommodated in the building. Excluding these non-traditional spaces, 50 parking spaces are accommodated.

RELATED APPROVALS. Because the subject property is located within the St. Anthony Falls Historic District, the project first required Heritage Preservation Commission approval. The Heritage Preservation Commission approved two Certificates of Appropriateness in December 2015. The applicant appealed several conditions of approval that had been placed by the Heritage Preservation Commission; these appeals were granted by the City Council on January 15, 2016.

Planning Case #	Application	Description	Action
BZH-28900	Certificate of Appropriateness	To allow the demolition of 120 East Hennepin and the one-story addition between 112 East Hennepin and 116 East Hennepin, noncontributing resources in the St. Anthony Falls Historic District.	Approved by the Heritage Preservation <u>on December 1, 2015.</u>
BZH-28900	Certificate of Appropriateness	To allow exterior alterations to the 112 East Hennepin building and 116 East Hennepin building, and to allow a new six story building to be constructed attached to the historic buildings.	Approved by the Heritage Preservation <u>on December 1, 2015</u> ; Appeal granted by City Council <u>on January 15, 2016.</u>

PUBLIC COMMENTS. Public comments received as of the writing of this report are included in the appendix. Any additional correspondence received prior to the public meeting will be forwarded on to the Planning Commission for consideration.

ANALYSIS

CONDITIONAL USE PERMIT

The Department of Community Planning and Economic Development has analyzed the application to increase the maximum height in the C3A District from 4 stories or 56 feet to 6 stories, 75 feet based on the following findings:

- 1. The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, comfort or general welfare.*

The maximum allowed height of a building in the C3A Community Activity Center District is 4 stories or 56 feet, whichever is less. The applicant is proposing a building that is 6 stories, or 75 feet. The two historic buildings which would remain on site are two and three stories in height and a portion of the new construction would be limited to four stories in height. The proposed 6 story/75 foot tall building will not be detrimental to or endanger the public health, safety, comfort, or general welfare provided that the development complies with all applicable building codes, life safety ordinances, and Public Works standards.

- 2. The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.*

The property is located in a developed area with a mix of building heights and multi-family residential, commercial, office and institutional uses. The height of surrounding buildings varies from two stories to towers which are nearly 30 stories. The office building located across Lourdes Place is 5-6 stories in height and the office building portion of the building across East Hennepin Avenue is

4 stories. The Our Lady of Lourdes church is approximately 67 feet in height to the peak of the roof and about 125 feet to the top of the steeple.

Because the measurement of building height in the zoning code is based on the adjacent natural grade rather than actual sea elevation, the topography of the subject property plays an important role in the impact a building's height has in conjunction with the proposed structure's bulk and height as measured by the zoning code. The building is 75 feet in height per the zoning code definition of building height. The site has a significant grade change of over 9 feet, so the height of the building at the point closest to the adjacent property to the southeast would be only 66 feet because that portion of the site is higher in elevation than the portion of the site closest to East Hennepin Avenue.

The additional two stories, or 19 feet total, of height above what is typically permitted in the C3A District is not expected to be injurious to the use and enjoyment of other property in the vicinity. It would not impede the normal and orderly development and improvement of surrounding properties.

3. *Adequate utilities, access roads, drainage, necessary facilities or other measures, have been or will be provided.*

The site would be accessed by vehicles on Lourdes Place and 2nd Street Southeast. Adequate utilities, access roads, drainage, necessary facilities or other measures, have been or will be provided.

4. *Adequate measures have been or will be taken to minimize traffic congestion in the public streets.*

The use is not expected to contribute to traffic congestion in the adjacent public streets. The applicant is providing 30 traditional parking stalls and six tandem stalls in one level of underground parking accessed from Lourdes Place. Twenty parking spaces would be located on the ground level and accessed by a garage door and curb cut located on 2nd Street Southeast. In addition, the applicant is proposing to install a semi-automated car-stacking parking system on the ground level which would allow for fourteen additional parking spaces stacked above other parking spaces. In total, when accounting for the non-traditional tandem and stacked parking spaces, 70 parking spaces would be accommodated in the building. Excluding these non-traditional spaces, 50 parking spaces are accommodated.

5. *The conditional use is consistent with the applicable policies of the comprehensive plan.*

The subject property is located on a designated Commercial Corridor and is within a designated Activity Center. The future land use identified for this property is Mixed Use. The request for increased height would be consistent with the following general land use policies of *The Minneapolis Plan for Sustainable Growth*:

Land Use Policy 1.1: Establish land use regulations to achieve the highest possible development standards, enhance the environment, protect public health, support a vital mix of land uses, and promote flexible approaches to carry out the comprehensive plan.

- 1.1.5 Ensure that land use regulations continue to promote development that is compatible with nearby properties, neighborhood character, and natural features; minimizes pedestrian and vehicular conflict; promotes street life and activity; reinforces public spaces; and visually enhances development.

Land Use Policy 1.4: Develop and maintain strong and successful commercial and mixed use areas with a wide range of character and functions to serve the needs of current and future users.

- I.4.4 Continue to encourage principles of traditional urban design including site layout that screens off-street parking and loading, buildings that reinforce the street wall, principal entrances that face the public sidewalks, and windows that provide “eyes on the street”.

Land Use Policy 1.5: Promote growth and encourage overall city vitality by directing new commercial and mixed use development to designated corridors and districts.

- I.5.3 Promote the preservation of traditional commercial storefronts wherever feasible.

Land Use Policy 1.8: Preserve the stability and diversity of the city's neighborhoods while allowing for increased density in order to attract and retain long-term residents and businesses.

- I.8.1 Promote a range of housing types and residential densities, with highest density development concentrated in and along appropriate land use features.

Land Use Policy 1.10: Support development along Commercial Corridors that enhances the street's character, fosters pedestrian movement, expands the range of goods and services available, and improves the ability to accommodate automobile traffic.

- I.10.1 Support a mix of uses – such as retail sales, office, institutional, high-density residential and clean low-impact light industrial – where compatible with the existing and desired character.
- I.10.5 Encourage the development of high-density housing on Commercial Corridors.

Land Use Policy 1.12: Support Activity Centers by preserving the mix and intensity of land uses and by enhancing the design features that give each center its unique urban character.

- I.12.1 Encourage a variety of commercial and residential uses that generate activity all day long and into the evening.
- I.12.2 Encourage mixed use buildings, with commercial uses located on the ground floor and secure entrances for residential uses.
- I.12.3 Encourage active uses on the ground floor of buildings in Activity Centers.
- I.12.6 Encourage the development of high- to very-high density housing within the boundaries of Activity Centers.

Housing Policy 3.2: Support housing density in locations that are well connected by transit, and are close to commercial, cultural and natural amenities.

- 3.2.1 Encourage and support housing development along commercial and community corridors, and in and near growth centers, activity centers, retail centers, transit station areas, and neighborhood commercial nodes.

Heritage Preservation Policy 8.1: Preserve, maintain, and designate districts, landmarks, and historic resources which serve as reminders of the city's architecture, history, and culture.

- 8.1.3 Encourage new developments to retain historic resources, including landscapes, incorporating them into new development rather than removal.

Urban Design Policy 10.5: Support the development of multi-family residential dwellings of appropriate form and scale.

- 10.5.2 Medium-scale, multi-family residential development is more appropriate along Commercial Corridors, Activity Centers, Transit Station Areas and Growth Centers outside of Downtown Minneapolis.

Additionally, the site is located within the boundaries of the *Nicollet Island-East Bank Neighborhood Small Area Plan* which was adopted by City Council in 2014. The land use and housing vision expressed in the plan is for the neighborhood to become “a vibrant pedestrian, bicycle and transit oriented neighborhood with a variety of land uses that draw people to the area at all times of the day. The addition of several new, high-density residential developments with ground floor commercial uses will substantially increase the population while enhancing the area as a thriving commercial district.” Several of the strategic actions identified in the plan are applicable to the subject proposal:

- Support increases in building heights and floor area ratios for buildings providing exceptional streetscapes and site designs. The more exceptional the streetscape and site design, the taller the building and the higher the floor area ratio NIEBNA will support.
- In all new developments, encourage first floor commercial spaces for retail, restaurant or other uses that generate pedestrian traffic and put more “eyes on the street.”

The plan calls for the Pedestrian Oriented Overlay District to cover the entire neighborhood and incorporate the following standards which are also applicable to the height of the subject proposal:

Building Massing – NIEBNA expects the East Bank to be a high density, residential, commercial and retail area. The goal is to achieve high density while preserving and reinforcing the pedestrian oriented values of the overlay district. To achieve this balance, building massing should be as follows:

- a. Building design should preserve a human scale and detail at the ground level. The apparent height of the building to a pedestrian on the sidewalk (called Tier 1 here) should be no taller than the effective width of the street. Building height above Tier 1 (called Tier 2 here) should be set back from the façade of Tier 1.

The effective width of the street is defined as the horizontal distance at ground level between the proposed building’s façade and the façade of the building across the street; if public open space is across the street (e.g., a park), then Tier 1 can be taller than if a building were across the street.

- b. The Tier 2 façade plane should be set back from the Tier 1 façade plane.
- c. No maximum height for Tier 2 is specified. In order to achieve the desired density of the neighborhood, taller slender buildings with smaller footprints are preferred to shorter maximum footprint structures.
- d. All faces of the building should be active with windows, balconies and other active elements. In particular, for Tier 2 there should be no large expanses of blank walls.

The effective width of East Hennepin Avenue noted on the survey submitted is approximately 80 feet. Along East Hennepin, the 75 foot high proposed building is consistent with this small area plan guidance that the apparent height of the building should be no taller than the effective width of the

street. Along 2nd Street Southeast and Lourdes Place, the 75 foot high building is 9 feet taller than the effective width of both streets which are each 66 feet. However, due to grade changes the building would be only approximately 66 feet in height at the highest point on 2nd Street Southeast and Lourdes Place, so this is consistent with the plan guidance as well.

Staff finds that the proposed 6 story, 75 foot height of the building is consistent with the applicable policies of the *Minneapolis Plan for Sustainable Growth* and the applicable policies of the *Nicollet Island-East Bank Neighborhood Small Area Plan*.

6. *The conditional use shall, in all other respects, conform to the applicable regulations of the district in which it is located.*

If the requested land use applications are approved, the proposal will comply with all provisions of the C3A Community Activity Center District.

Additional Standards to Increase Maximum Height

In addition to the conditional use permit standards, the Planning Commission shall consider, but not be limited to, the following factors when determining the maximum height of principal structures in commercial districts:

1. *Access to light and air of surrounding properties.*

The subject property fronts on three public streets. This provides adequate separation allowing for access to light and air for surrounding properties to the northeast, northwest, and southwest. The only adjacent property on the block is located to the southeast and is the Our Lady of Lourdes church and rectory building. The upper stories of the proposed building would be set back 5 and 10 feet from the property line. The adjacent rectory building is located 34 feet from the shared property line and the church is between 75 and 100 feet from the shared property line. Increasing the height of the proposed development should not impede access to light and air that the surrounding properties currently receive.

2. *Shadowing of residential properties, significant public spaces, or existing solar energy systems.*

The applicant has submitted a shadow study identifying shadowing impacts at various times of year. The proposed building would contribute to some shadowing of adjacent properties. Shadowing from the proposed building would impact the office building across Lourdes Place in spring and fall mornings, the northeast side of the church property in summer evenings, and the office building across East Hennepin in the winter months. None of these are residential uses. Some shadowing of the nearest residential use, the townhomes on 2nd Street Southeast, would occur during spring and fall evenings. These shadowing impacts of residential uses are anticipated to be minor. There are no known solar energy systems that would be shadowed by the proposed development.

3. *The scale and character of surrounding uses.*

The surrounding area contains a mix of residential, commercial, office, and institutional uses ranging from two to nearly 30 stories in height. The Our Lady of Lourdes church and rectory building are located directly southeast of the subject property. Office buildings connected by a skyway are located across Lourdes Place and East Hennepin Avenue from the property. Across 2nd Street Southeast there is currently a surface parking lot and two-story townhomes. Two high-rise multi-family residential towers are located within a few blocks of the subject property as well.

4. *Preservation of views of landmark buildings, significant open spaces or water bodies.*

The proposed development would not block views of significant open spaces or bodies of water. The proposed development may partially impede existing views of the Our Lady of Lourdes church and rectory building from various vantage points across East Hennepin Avenue. These views are currently only present because the rear of the subject property is undeveloped and utilized as a surface parking lot. Based on the applicant's renderings, the steeple of the church and the front façade of the church and rectory building will remain visible from Lourdes Place and East Hennepin Avenue. Views of the back of the steeple and rectory building from East Hennepin Avenue and 2nd Street Southeast will likely be impeded by the proposed development, but the rear of the church will likely remain visible from 2nd Street Southeast and East Hennepin. The impact to these views is primarily due to the proposed development of the existing surface parking lot area and is not significantly impacted by the additional two stories of height that is requested.

VARIANCE

The Department of Community Planning and Economic Development has analyzed the application for a variance of the maximum floor area ratio from 3.78 to 4.51 based on the following findings:

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

The maximum floor area ratio (FAR) before density bonuses in the C3A District is 2.7. This project qualifies for two 20% density bonuses for enclosed parking and mixed use development as outlined in Section 548.130 of the zoning code. Therefore, the maximum allowed FAR of the project is 3.78. The proposed project includes 90,249 square feet of gross floor area on a site that is 20,001 square feet in area, which equals a floor area ratio of 4.51. A variance is therefore requested to increase the maximum floor area ratio from 3.78 to 4.51.

Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The site is uniquely situated both along a Commercial Corridor and within an Activity Center. Mixed-use developments and high to very-high density housing are encouraged in Activity Centers. High density housing and mixed-use developments are also encouraged on Commercial Corridors. At 157 dwelling units/acre, the proposed mixed-use project is considered very-high density, which is appropriately located in an Activity Center and along a Commercial Corridor as designated by the *Minneapolis Plan for Sustainable Growth*. The *Nicollet Island-East Bank Neighborhood Small Area Plan* further reinforces these designations as the plan envisions several new high-density residential developments with ground floor commercial uses in the neighborhood. The policy recommendations for dense, mixed-use development in this location contribute to unique circumstances that have not been created by the applicant.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

The purpose of the maximum floor area ratio is to measure the intensity of the site being developed. The request to increase the FAR from 3.78 to 4.51 is reasonable. Although the building would exceed the floor area ratio maximum, the applicant has taken measures to reduce the building's impact by accommodating all of the off-street parking underground and within the building. Additionally, since the proposal incorporates the historic buildings into the development, there is a variety of height and massing on the site, which helps to reduce the overall appearance of bulk. The site also has unique property lines due to the angle of East Hennepin Avenue and the jogged

southeast property line which have influenced the building bulk. As stated above, both the comprehensive plan and the small area plan support mixed-use and high to very-high density development in this location. Given these factors, the proposed floor area ratio will utilize the property in a reasonable manner that is in keeping with the spirit and intent of the ordinance and the comprehensive plan.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

The proposed variance will not alter the essential character of the locality or be injurious to the use and enjoyment of other property in the vicinity. The bulk and scale of buildings in the general area varies from two to nearly 30 stories in height. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.

VARIANCE

The Department of Community Planning and Economic Development has analyzed the application for a variance of the required interior side yard from 15 feet to 5 feet based on the following findings:

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

The applicant is seeking a variance to reduce the interior side yard setback from 15 feet to 5 feet. In commercial districts, uses are not typically subject to minimum yard requirements. However, residential uses and hotels containing windows which face an interior side yard are subject to a minimum yard equal to five feet plus two feet for every story above the first. For this six-story building, the minimum yard requirement is therefore 15 feet where windows are located along the southeast property line. The second level of the building steps back above the parking garage along the side interior property line. The first level of the building, the parking garage area, would be located along the property line but is not subject to a yard requirement as it does not contain residential windows. The windows along this property line on floors 2 through 6 are set back between 5 and 10 feet. If the windows were eliminated, the building could be built up to the property line without requiring a variance.

The property is irregularly shaped due to the angle of East Hennepin Avenue as well as the jogged side interior property line which steps back 20 feet approximately halfway through the site. The location of the historic buildings limits the potential location of new construction as the historic building footprints amount to about half of the width of the site from East Hennepin Avenue to the side lot line. The applicant is proposing to repurpose the historic buildings to accommodate retail uses at the ground level and residential uses in the floors above. The irregular shape of the property makes it challenging to comply with a 15 foot interior side yard setback while retaining the two historic buildings, providing adequate access and maneuvering for off-street parking spaces, and providing new commercial uses at the ground level along East Hennepin. Practical difficulties exist in complying with the ordinance because of these circumstances unique to the property.

- The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

The applicant has proposed to set the windows back between 5 and 10 feet from the side lot line. In general, yard controls are established to provide for the orderly development and use of land and to minimize conflicts among land uses by regulating the dimension and use of yards in order to provide adequate light, air, open space and separation of uses. The urban design policies of the comprehensive plan also address this issue in calling for building placement to allow light and air into the site and surrounding properties. The building is designed in a manner that would allow for adequate access to light and air for the subject site and adjacent church property to the southeast. If the windows were eliminated, the building could be built up to the property line. The adjacent rectory building is located 34 feet from the shared property line and the church building is between 75 and 100 feet from the shared property line. The applicant proposes to use the property in a reasonable manner by incorporating residential windows and the proposal will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.

- The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

Granting the variance would not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. The applicant has demonstrated that the development, as proposed, would provide sufficient light and air for the adjacent uses while maintaining a mix of uses that is characteristic of the site and surrounding properties. The nearest proposed windows would be more than 45 feet from the adjacent church's rectory building and over 80 feet from the adjacent church. The proposed setback variance would not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.

VARIANCE

The Department of Community Planning and Economic Development has analyzed the application for a variance of the minimum window requirement below 40 percent along Lourdes Place in the PO Pedestrian Oriented Overlay District based on the following findings:

The 40 percent ground floor window requirement is only applicable to nonresidential uses in the PO Overlay District. Since the application was noticed, CPED determined that because the parking area had been redesigned so that no commercial parking was proposed to be incorporated in the project, the new construction along the Lourdes façade was not subject to the 40 percent minimum window requirement because the now fully residential parking area does not constitute a nonresidential use. Additionally, CPED has determined that the historic building is not subject to this requirement as the proposal does not increase its nonconformity. Rather, the applicant is proposing to reintroduce windows in the historic building where they have been infilled for many decades. CPED is recommending that this variance for the PO Overlay District window requirement be returned to the applicant.

VARIANCE

The Department of Community Planning and Economic Development has analyzed the application for a variance of the minimum parking requirement for the nonresidential uses based on the following findings:

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

The proposal includes four retail areas which vary in size from approximately 1,400 square feet to 2,800 square feet. Each of these areas would have a minimum parking requirement for general retail sales and services of 4 spaces, for a total of 16 spaces. Because the site is in the PO Pedestrian Oriented Overlay District, that minimum requirement is reduced by 25 percent, to 12 spaces or 3 per retailer. Utilizing the bicycle incentive, this minimum could be reduced by an additional space each to 2 spaces per retailer or 8 spaces total. In order to qualify for the bicycle incentive, the applicant would need to provide 4 spaces per retailer or a total of 16 bicycle parking spaces. The applicant is proposing 14 short-term bicycle parking spaces in the public right-of-way. Therefore, staff is recommending a condition of approval that not less than 16 bicycle spaces be utilized in order to qualify for the bicycle incentive. While the project will meet the residential parking requirement, the applicant is not proposing to provide any off-street parking spaces for the commercial uses in the building. A variance of the minimum parking requirement is required for the nonresidential uses.

This site is located within the St. Anthony Falls Historic District. The proposed redevelopment project incorporates two historic buildings on site, preserving the buildings in their original locations. This limits the developable footprint of the site and creates unique challenges in accommodating parking both underground and on the ground level at the rear of the site. The applicant contends that due to these challenges it is not possible to accommodate parking for both the residential and commercial uses in the proposed building. The applicant also notes that security and logistical concerns necessitate that the parking be solely utilized for the residential use. Staff finds that due to the unique circumstances associated with preserving the historic buildings in place, practical difficulties exist in complying with the ordinance and were not self-created.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

The site qualifies for the transit incentive which reduces the typical 72 space residential minimum parking requirement by fifty percent to 36 spaces. The total minimum parking requirement for both the commercial and residential uses in the proposed development would therefore be 44 spaces. The applicant is proposing to provide 50 traditional parking spaces that meet the standard or compact dimension requirements outlined in the zoning code. Thirty traditional residential parking spaces (with six additional tandem spaces) would be located underground and accessed from Lourdes Place. The applicant is also proposing to install a semi-automated car-stacking parking system for additional residential parking spaces at the ground level which would allow for 14 non-traditional parking spaces as well as 20 traditional parking spaces. If for any reason the car-stacking system does not get implemented, there would be 20 traditional parking spaces at the ground level. In total, 50 traditional spaces and 20 non-traditional spaces would be provided for the residential use. As discussed above, due to the preservation of the historic buildings on site, the applicant is limited in their ability to provide additional parking spaces for the commercial uses.

The C3A Community Activity Center District is intended to provide for the development of major urban activity centers with neighborhood scale retail sales and services. The PO Pedestrian Oriented Overlay District is intended to preserve and encourage the pedestrian character of commercial areas and to promote street life and activity by regulating building orientation and design and accessory parking facilities. The subject property is located along a commercial corridor in a transit-rich and walkable area. There are numerous public parking facilities in the immediate area including the River Place underground parking facility on the same block, two garages on 2nd Street

Southeast across East Hennepin, a parking garage on East Hennepin and University Avenue, as well as metered parking on the adjacent public streets.

The minimum bicycle parking requirement for the nonresidential uses is 3 spaces per retailer for a total of 12 parking spaces. In order to qualify for the bicycle incentive to reduce the vehicle parking requirement, at least 16 bicycle parking spaces are required. Staff therefore recommends a condition of approval that at least 16 bicycle parking spaces shall be provided. This will also serve as a mitigation measure for the reduction in vehicle parking for the commercial uses. Bicycle parking in the public right-of-way will require an encroachment permit from Public Works. With this condition of approval, staff finds that the applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

With the recommended condition of approval ensuring that adequate bicycle parking is provided for the nonresidential uses, the proposed variance will not alter the essential character or be injurious to the use and enjoyment of other property in the vicinity. The site is located along a commercial corridor in a transit-rich and walkable area. There are numerous public parking facilities in the immediate area as well as metered street parking. Granting the variance from the 8 required parking spaces to zero parking spaces will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.

VARIANCE

The Department of Community Planning and Economic Development has analyzed the application for a variance to increase the percentage of required parking spaces that may be satisfied by providing compact spaces based on the following findings:

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

At least 75 percent of required parking spaces must comply with the minimum dimensions for standard spaces, per Section 541.330 of the zoning code. Since 36 parking spaces are required for the residential units and 8 spaces would be required for the commercial uses, the total parking requirement would be 44 spaces. This means that 33 spaces shall comply with the minimum dimensions for standard spaces. The applicant is proposing only 22 standard parking spaces, or 50% of the required parking.

This site is located within the St. Anthony Falls Historic District. The proposed redevelopment project incorporates two historic buildings on site, preserving the buildings in their original locations. This limits the developable footprint of the site and creates unique challenges in accommodating parking both underground and on the ground level at the rear of the site. In order to accommodate as many parking spaces as possible for the residential units, the applicant has proposed a majority of spaces to be compact stalls rather than standard stalls. Accommodating 33 standard stalls would eliminate several parking stalls from the overall development. Practical difficulties existing in complying with the ordinance because of circumstances unique to the property and were not self-created.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

Thirty traditional residential parking spaces (with six additional tandem spaces) would be located underground and accessed from Lourdes Place. The applicant is also proposing to install a semi-automated car-stacking parking system for additional residential parking spaces at the ground level which would allow for 14 non-traditional parking spaces as well as 20 traditional parking spaces. In total, 50 traditional spaces and 20 non-traditional spaces would be provided for the residential use. Due to the structural elements of the car-stacking system, spaces would be limited in width and would not meet the dimensional requirements for standard stalls. However, if the car-stacking system were not to be utilized, the footprint of the proposed parking area for the system would have adequate dimensions for 14 standard size stalls. The zoning code does not address car-stacking. However, staff finds this is a reasonable solution to provide additional parking spaces, though it increases the percentage of stalls that would be compact sized. The applicant's proposal is reasonable and is in keeping with the spirit and intent of the ordinance and the comprehensive plan.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

Increasing the percentage of required parking stalls that may be satisfied with compact stalls will not alter the essential character of the area or be injurious to the use and enjoyment of other property in the vicinity. The compact parking spaces will allow the applicant to provide more parking stalls for the residential uses in the building than would typically be accommodated due to site constraints. The proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.

SITE PLAN REVIEW

The Department of Community Planning and Economic Development has analyzed the application based on the required [findings](#) and [applicable standards](#) in the site plan review chapter:

1. Conformance to all applicable standards of Chapter 530, Site Plan Review.

BUILDING PLACEMENT AND DESIGN

Building placement – Meets requirements

- The first floor of the building is located within eight feet of the front lot lines abutting East Hennepin Avenue, Lourdes Place, and 2nd Street Southeast.
- The placement of the building reinforces the street wall, maximizes natural surveillance and visibility, and facilitates pedestrian access and circulation.
- Because the building would be built to all lot lines along the street frontages, there is no space between the building and lot line to incorporate amenities. However, the applicant is proposing amenities like landscaping and bike racks in the public right-of-way along East Hennepin Avenue.
- All on-site accessory parking is located within the principal building served and below grade.

Principal entrances – Meets requirements

- The building is oriented so that the principal entrances face the front property lines. The residential entrance faces 2nd Street Southeast and there would be multiple retail entrances along East Hennepin Avenue.

- All principal entrances are clearly defined and emphasized through the use of recessed entries, canopies, and other architectural features.

Visual interest – *Requires alternative compliance*

- With the exception of the first floor building wall facing the adjacent property to the southeast, the proposed building walls provide architectural detail and contain windows in order to create visual interest.
- The proposed building emphasizes architectural elements – including recesses, projections, windows, and entries – to divide the building into smaller identifiable sections. To minimize the visual impact of the new building on the historic buildings, the design incorporates three separate “modules” of the new construction which separate the building into smaller identifiable sections.
- The proposal has blank, uninterrupted walls exceeding 25 feet in length. The first floor of the building along the southeast property line is a long blank wall. While the building jogs 20 feet approximately halfway through the site, each blank section is approximately 78 feet long. The two sections would be different colored brick to correspond to the design of that portion of the building above. This requires alternative compliance.

Exterior materials – *Meets requirements*

- The applicant is proposing brick and metal panel as the building’s primary exterior materials. Each elevation would comply with the City’s durability standards for exterior materials. Please note that exterior material changes at a later date may require review by the Planning Commission and an amendment to the site plan review, as well as Heritage Preservation Commission review.
- In addition, the application is consistent with the City’s policy of allowing no more than three exterior materials per elevation, excluding windows, doors, and foundation materials.
- Plain face concrete block is not proposed.
- The exterior materials and appearance of the rear and side walls of the building are similar to and compatible with the front of the building. To minimize the visual impact on the historic buildings, the proposed design incorporates three separate “modules” of the new construction: the lighter brown brick module, the metal panel infill portion of the building between the two historic buildings, and the darker brick with light metal panel sixth-floor and accents. Considering the historic context of this site, this meets the intent of this standard.

Windows – *Requires alternative compliance*

- For residential uses, the zoning code requires that no less than 20 percent of the walls on the first floor, and no less than ten percent of the walls on each floor above the first that face a public street shall be windows. For nonresidential uses, the zoning code requires that no less than 30 percent of the walls on the first floor are windows with clear or lightly tinted glass with a visible light transmittance ratio of six-tenths (0.6) or higher. The applicant has indicated that the proposed aluminum storefront windows will have a visible light transmittance ratio of 72 percent. As shown in Table I below, the 1st floor of the new building portion on Lourdes Place is not meeting the window requirement and therefore requires alternative compliance.
- On East Hennepin, the nonresidential portions of the project are subject to the PO Overlay District window requirement of 40% at the ground level. The historic building along Lourdes Place which will be occupied by a nonresidential use is not subject to this requirement. The project is in compliance with the minimum window requirements along East Hennepin (see Table I).
- All windows are vertical in proportion and are evenly distributed along the building walls.

Table I. Percentage of Windows per Applicable Elevation

	Code Requirement		Proposed	
Residential Uses				
1st floor – 2 nd Street Southeast	20% minimum	179 sq. ft.	42%	381 sq. ft.
1st floor – Lourdes Place	20% minimum	99 sq. ft.	10%	48 sq. ft.
2nd floor and above (East Hennepin, Lourdes Place, 2 nd Street Southeast)	10% minimum	Varies	Meets requirement	
Nonresidential Uses				
1st floor – East Hennepin	40% minimum	525 sq. ft.	73%	967 sq. ft.
1st floor – Lourdes Place	40% minimum (nonconforming – historic building)	250 sq. ft.	8%	48 sq. ft.

Ground floor active functions – Requires alternative compliance

- At least 70 percent of the first floor building frontage facing the public streets must contain active functions. The ground floor facing Lourdes Place contains 63 percent (88 feet) active functions, which requires alternative compliance. The ground floor facing 2nd Street Southeast contains only 47 percent (53 feet) active functions, which also requires alternative compliance. Along East Hennepin Avenue, 100 percent of the first floor building frontage would be active uses.

Roof line – Meets requirements

- The principal roof line of the building will be flat, which is similar to that of surrounding buildings and is compatible with the roof line of the existing historic buildings.

Parking garages – Meets requirements

- All of the proposed parking is located below ground or enclosed within the building.

ACCESS AND CIRCULATION

Pedestrian access – Meets requirements

- Building entrances would be located directly adjacent to the public sidewalk which will be clear and well-lit.

Transit access – Meets requirements

- No transit shelters are proposed as part of this development.

Vehicular access – Meets requirements

- Vehicular access and circulation has been designed to minimize conflicts with pedestrian traffic and with surrounding residential uses. The applicant is working with the Public Works department in the Preliminary Development Review process regarding sight triangles and mitigation measures for the vehicle access point on Lourdes Place.
- There are two existing curb cuts leading to the surface parking area on the site. Two curb cuts would be incorporated in the proposed development leading to parking garage areas on both Lourdes Place and 2nd Street Southeast.

- There are no public alleys adjacent to the site.
- Service vehicle access would not conflict with pedestrian traffic. There are no proposed loading spaces within the building. The applicant will work with the Public Works Department through the Preliminary Development Review process regarding loading.
- There is no maximum impervious surface requirement in the C3A District. The applicant will continue to work with Public Works regarding stormwater management in the Preliminary Development Review process.

LANDSCAPING AND SCREENING

General landscaping and screening – Meets requirements

- The building would occupy the entire footprint of the site. Therefore, there is no landscaping required for this proposal. The applicant is proposing four trees and grasses in the public right of way along East Hennepin Avenue.

Parking and loading landscaping and screening – Meets requirements

- There is no surface parking proposed for the site, so the site is not subject to the screening and landscaping requirements for parking areas per section 530.170.

ADDITIONAL STANDARDS

Concrete curbs and wheel stops – Not applicable

- There are no surface parking spaces proposed on the site.

Site context – Meets requirements

- The proposed development may partially impede existing views of the Our Lady of Lourdes church and rectory building from various points across East Hennepin Avenue. The site plan minimizes the blocking of views to the extent practical. These views are only present because the rear of the subject property is currently undeveloped and utilized as a surface parking lot. No other important elements of the city near the site would be obstructed by the proposed building.
- As discussed in the conditional use permit findings above, this building is anticipated to have only minimal shadowing effects on public spaces and adjacent properties.
- The building is not anticipated to contribute to the generation of wind currents at ground level.

Crime prevention through environmental design – Meets requirements

- The site plan employs best practices to increase natural surveillance and visibility, to control and guide movement on the site, and to distinguish between public and non-public spaces.
- The proposed site plan will promote natural observation and maximize the opportunities for people to observe adjacent spaces and public sidewalks. The retail spaces along East Hennepin Avenue will have 967 square feet of windows, or 73% of the ground floor façade, allowing for observation of the public sidewalk. Additional windows will be incorporated on both 2nd Street Southeast and Lourdes Place.
- The project provides lighting on site and at all building entrances that maintains a minimum acceptable level of security while not creating glare or excessive lighting of the site.
- The building features are located to clearly guide pedestrian movement to the site and to control and restrict people to appropriate locations.
- The entrances, exits, signs, fencing, landscaping, and lighting are located to distinguish between public and private areas, to control access, and to guide people coming to and going from the site.

Historic preservation – Meets requirements

- This site is located within the St. Anthony Falls Historic District. The proposed plan includes the rehabilitation and integration of locally designated historic structures. As noted in the background section of the report, the Heritage Preservation Commission approved the proposal for both the alteration of the historic buildings and the construction of the new building in December 2015.

2. Conformance with all applicable regulations of the zoning ordinance.

The proposed uses are permitted in the C3A Community Activity Center District.

Off-street Parking and Loading – Requires variance(s)

- The off-street vehicle parking requirement is 44 spaces. 8 nonresidential parking spaces are required if the bicycle incentive is utilized. The applicant has requested a variance of this nonresidential parking requirement. The site qualifies for the transit incentive outlined in Section 541.200 of the zoning code which reduces the typical 72 space residential minimum parking requirement by fifty percent to 36 spaces. The total minimum parking requirement is therefore 44 spaces (see Table 2).
- The applicant is providing 30 traditional parking stalls and six tandem stalls in one level of underground parking accessed from Lourdes Place. Twenty parking spaces would be located on the ground level and accessed by a garage door and curb cut located on 2nd Street Southeast. In addition, the applicant is proposing to install a semi-automated car-stacking parking system on the ground level which would allow for fourteen additional parking spaces stacked above other parking spaces. In total, when accounting for the non-traditional tandem and stacked parking spaces, 70 parking spaces would be accommodated in the building. Excluding these non-traditional spaces, 50 parking spaces are accommodated.
- The minimum bicycle parking requirement is 3 spaces for each retail use. Since there are 4 retail uses, 12 bicycle parking spaces are required. Additionally, one bicycle parking space is required for each two dwelling units, so 36 residential bicycle parking spaces are required. (See Table 3.)
- The proposed developed does not have a specific off-street loading requirement. The proposal shall provide adequate shipping and receiving facilities.

Table 2. Vehicle Parking Requirements Per Use (Chapter 541)

Use	Minimum	Reductions	Total with Reductions	Maximum Allowed	Proposed
General Retail Sales and Services	16	75% PO Reduction (4) Bike Incentive (4)	8	29	0
Residential Dwellings	72	Transit Incentive (36)	36	--	50
	--	--	44	--	50

Table 3. Bicycle Parking Requirements (Chapter 541)

Use	Minimum	Short-Term	Long-Term	Proposed
General Retail Sales and Services	3 each (12 total)	Not less than 50%	--	14
Residential Dwellings	36	--	Not less than 90%	48
	48	--	--	62

Building Bulk and Height – Requires conditional use permit and variance

- The applicant has requested a conditional use permit to increase the height of the building from 4 stories or 56 feet to six stories, 75 feet. The applicant has also requested a variance to increase the maximum floor area ratio from 3.78 to 4.51. Staff is recommending approval of both applications as evaluated above. (See Table 4.)

Table 4. Building Bulk and Height Requirements

	Code Requirement	Proposed
Lot Area	--	20,001 sq. ft. / 0.46 acres
Gross Floor Area	--	90,249 sq. ft.
Floor Area Ratio (Maximum)	3.78	4.51
Building Height (Maximum)	4 stories or 56 feet, whichever is less	6 stories/75 ft.

Lot Requirements – Not applicable

Yard Requirements – Requires variance(s)

- In commercial districts, uses are not typically subject to minimum yard requirements. However, residential uses and hotels containing windows which face an interior side yard are subject to a minimum yard equal to five feet plus two feet for every story above the first. For this six-story building, the minimum yard requirement is therefore 15 feet where windows are located along the southeast property line. The applicant is proposing to locate windows 5 feet from this property line which requires a variance.

Signs – Meets requirements

- All signs are subject to Chapter 543, On-Premise Signs. The applicant is not proposing specific signage at this time. The applicant will be required to submit a separate sign permit application for any signage that is proposed.

Screening of Mechanical Equipment – Meets requirements

- All mechanical equipment is subject to the screening requirements of Chapter 535 and district requirements, including:

535.70. Screening of mechanical equipment.

 - a) In general. All mechanical equipment installed on or adjacent to structures shall be arranged so as to minimize visual impact using one (1) of the following methods. All screening shall be kept in good repair and in a proper state of maintenance.
 - 1) Screened by another structure. Mechanical equipment installed on or adjacent to a structure may be screened by a fence, wall or similar structure. Such screening structure shall comply with the following standards:
 - a. The required screening shall be permanently attached to the structure or the ground and shall conform to all applicable building code requirements.
 - b. The required screening shall be constructed with materials that are architecturally compatible with the structure.
 - c. Off-premise advertising signs and billboards shall not be considered required screening.
 - 2) Screened by vegetation. Mechanical equipment installed adjacent to the structure served may be screened by hedges, bushes or similar vegetation.

- 3) Screened by the structure it serves. Mechanical equipment on or adjacent to a structure may be screened by a parapet or wall of sufficient height, built as an integral part of the structure.
 - 4) Designed as an integral part of the structure. If screening is impractical, mechanical equipment may be designed so that it is balanced and integrated with respect to the design of the building.
- The applicant is proposing individual HVAC units for each dwelling unit which have been designed with the intent that they appear as an integral part of the structure. No rooftop mechanical equipment is shown on the plans submitted; any additional mechanical equipment shall be required to comply with the standards above.

Refuse Screening – Meets requirements

- All refuse and recycling storage containers are subject to the screening requirements in Chapter 535. All refuse and recycling storage containers are located within the building.

Lighting – Meets requirements

- Existing and proposed lighting must comply with Chapter 535 and Chapter 541 of the zoning code, including:

535.590. Lighting.

- a) In general. No use or structure shall be operated or occupied as to create light or glare in such an amount or to such a degree or intensity as to constitute a hazardous condition, or as to unreasonably interfere with the use and enjoyment of property by any person of normal sensitivities, or otherwise as to create a public nuisance.
 - b) Specific standards. All uses shall comply with the following standards except as otherwise provided in this section:
 - 1) Lighting fixtures shall be effectively arranged so as not to directly or indirectly cause illumination or glare in excess of one-half (1/2) footcandle measured at the closest property line of any permitted or conditional residential use, and five (5) footcandles measured at the street curb line or nonresidential property line nearest the light source.
 - 2) Lighting fixtures shall not exceed two thousand (2,000) lumens (equivalent to a one hundred fifty (150) watt incandescent bulb) unless of a cutoff type that shields the light source from an observer at the closest property line of any permitted or conditional residential use.
 - 3) Lighting shall not create a sensation of brightness that is substantially greater than ambient lighting conditions as to cause annoyance, discomfort or decreased visual performance or visibility to a person of normal sensitivities when viewed from any permitted or conditional residential use.
 - 4) Lighting shall not create a hazard for vehicular or pedestrian traffic.
 - 5) Lighting of building facades or roofs shall be located, aimed and shielded so that light is directed only onto the facade or roof.
- No photometric plan has been submitted at this time. All lighting will be reviewed prior to the issuance of any permits to verify compliance with the requirements above.

Fences – Not applicable

Specific Development Standards – Not applicable

PO Pedestrian Oriented Overlay District Standards – Meets requirements

- The proposal is in compliance with the PO Pedestrian Oriented Overlay District standards. The applicable window and parking requirements have been evaluated above.

MR Mississippi River Critical Area Overlay District Standards – Meets requirements

- The proposal is in compliance with the MR Mississippi River Critical Area Overlay District standards.

3. Conformance with the applicable policies of The Minneapolis Plan for Sustainable Growth.

The Minneapolis Plan for Sustainable Growth identifies the site as Mixed Use on the future land use map. The proposed development is consistent with the following principles and policies outlined in the comprehensive plan:

Land Use Policy 1.1: Establish land use regulations to achieve the highest possible development standards, enhance the environment, protect public health, support a vital mix of land uses, and promote flexible approaches to carry out the comprehensive plan.

- 1.1.5 Ensure that land use regulations continue to promote development that is compatible with nearby properties, neighborhood character, and natural features; minimizes pedestrian and vehicular conflict; promotes street life and activity; reinforces public spaces; and visually enhances development.

Land Use Policy 1.2: Ensure appropriate transitions between uses with different size, scale, and intensity.

- 1.2.1 Promote quality design in new development, as well as building orientation, scale, massing, buffering, and setbacks that are appropriate with the context of the surrounding area.

Land Use Policy 1.4: Develop and maintain strong and successful commercial and mixed use areas with a wide range of character and functions to serve the needs of current and future users.

- 1.4.4 Continue to encourage principles of traditional urban design including site layout that screens off-street parking and loading, buildings that reinforce the street wall, principal entrances that face the public sidewalks, and windows that provide “eyes on the street”.

Land Use Policy 1.5: Promote growth and encourage overall city vitality by directing new commercial and mixed use development to designated corridors and districts.

- 1.5.3 Promote the preservation of traditional commercial storefronts wherever feasible.

Land Use Policy 1.8: Preserve the stability and diversity of the city's neighborhoods while allowing for increased density in order to attract and retain long-term residents and businesses.

- 1.8.1 Promote a range of housing types and residential densities, with highest density development concentrated in and along appropriate land use features.

Land Use Policy 1.10: Support development along Commercial Corridors that enhances the street's character, fosters pedestrian movement, expands the range of goods and services available, and improves the ability to accommodate automobile traffic.

- 1.10.1 Support a mix of uses – such as retail sales, office, institutional, high-density residential and clean low-impact light industrial – where compatible with the existing and desired character.
- 1.10.5 Encourage the development of high-density housing on Commercial Corridors.

Land Use Policy 1.12: Support Activity Centers by preserving the mix and intensity of land uses and by enhancing the design features that give each center its unique urban character.

- 1.12.1 Encourage a variety of commercial and residential uses that generate activity all day long and into the evening.
- 1.12.2 Encourage mixed use buildings, with commercial uses located on the ground floor and secure entrances for residential uses.
- 1.12.3 Encourage active uses on the ground floor of buildings in Activity Centers.
- 1.12.6 Encourage the development of high- to very-high density housing within the boundaries of Activity Centers.

Housing Policy 3.2: Support housing density in locations that are well connected by transit, and are close to commercial, cultural and natural amenities.

- 3.2.1 Encourage and support housing development along commercial and community corridors, and in and near growth centers, activity centers, retail centers, transit station areas, and neighborhood commercial nodes.

Heritage Preservation Policy 8.1: Preserve, maintain, and designate districts, landmarks, and historic resources which serve as reminders of the city's architecture, history, and culture.

- 8.1.3 Encourage new developments to retain historic resources, including landscapes, incorporating them into new development rather than removal.

Urban Design Policy 10.4: Support the development of residential dwellings that are of high quality design and compatible with surrounding development.

- 10.4.1 Maintain and strengthen the architectural character of the city's various residential neighborhoods.
- 10.4.2 Promote the development of new housing that is compatible with existing development in the area and the best of the city's existing housing stock.

Urban Design Policy 10.5: Support the development of multi-family residential dwellings of appropriate form and scale.

- 10.5.2 Medium-scale, multi-family residential development is more appropriate along Commercial Corridors, Activity Centers, Transit Station Areas and Growth Centers outside of Downtown Minneapolis.

Urban Design Policy 10.6: New multi-family development or renovation should be designed in terms of traditional urban building form with pedestrian scale design features at the street level.

- 10.6.5 Street-level building walls should include an adequate distribution of windows and architectural features in order to create visual interest at the pedestrian level.
- 10.6.6 Integrate transit facilities and bicycle parking amenities into the site design.

Urban Design Policy 10.9: Support urban design standards that emphasize traditional urban form with pedestrian scale design features at the street level in mixed-use and transit-oriented development.

- 10.9.1 Encourage both mixed-use buildings and a mix of uses in separate buildings where appropriate.
- 10.9.4 Coordinate site designs and public right-of-way improvements to provide adequate sidewalk space for pedestrian movement, street trees, landscaping, street furniture, sidewalk cafes and other elements of active pedestrian areas.

Urban Design Policy 10.10: Support urban design standards that emphasize a traditional urban form in commercial areas.

- 10.10.1 Enhance the city's commercial districts by encouraging appropriate building forms and designs, historic preservation objectives, site plans that enhance the pedestrian environment, and by maintaining high quality four season public spaces and infrastructure.
- 10.10.4 Orient new buildings to the street to foster safe and successful commercial nodes and corridors.
- 10.10.6 Require storefront window transparency to assure both natural surveillance and an inviting pedestrian experience.
- 10.10.7 Encourage the renovation of existing commercial buildings.

CPED finds that the proposed development is in conformance with the above policies of the *Minneapolis Plan for Sustainable Growth*.

4. Conformance with applicable development plans or objectives adopted by the City Council.

The site is located within the boundaries of the *Nicollet Island-East Bank Neighborhood Small Area Plan* which was adopted by City Council in 2014. The proposed height of the project was evaluated for consistency with the small area plan in the conditional use permit section above. The plan identifies Nye's Polonaise Room as an important landmark in the neighborhood as well as the adjacent Our Lady of Lourdes church. The plan also calls for the Pedestrian Oriented Overlay District standards to incorporate the following standards which are also applicable to the proposal's building design:

Pedestrian Focus –

e. Transparent ground level building façades – For commercial spaces, the ground level façades should be transparent. It should be possible to clearly see into the building from across the street as well as directly adjacent to the building.

For new construction, transparent ground floor façades should be maximized. Except for load bearing structural elements, the façade should be transparent.

For existing buildings, non-transparent façade elements should be made transparent wherever possible consistent with the overall design of the building.

f. Active façade features – To encourage walking, building designs should incorporate visually interesting and distinctive features, including public art, intriguing window displays, awnings, staggered store entryways, and other architectural details that enhance and encourage the pedestrian experience.

g. Pedestrian amenities – Streetscape amenities should engage pedestrians. Examples include benches, decorative trash receptacles, boulevard plantings, bike racks, parklets [see Chapter

Seven], animated and static information kiosks, clocks, outdoor sales areas, way-finding signage of a distinct neighborhood character, etc.

h. Vehicle parking – Wherever possible, vehicle parking should be hidden from sidewalk view. Access to parking should not be to/from main thoroughfares.

The proposed site and building design will provide transparent ground level facades where the commercial uses are located. The non-transparent façade elements of the existing historic buildings will be rehabilitated with traditional transparent storefronts. With the exception of the building walls along the parking garage area, the building has active facades. The applicant is proposing to improve the streetscape by widening the sidewalk and installing bump-outs (with Public Works approval) and incorporating street trees, grasses, and bicycle racks in the public right-of-way. All vehicle parking would be hidden from sidewalk view and would be accessed off of Lourdes and 2nd Street Southeast. The proposed building design is consistent with the applicable policies of the *Nicollet Island-East Bank Neighborhood Small Area Plan*.

5. Alternative compliance.

The Planning Commission or zoning administrator may approve alternatives to any site plan review requirement upon finding that the project meets one of three criteria required for alternative compliance. Alternative compliance is requested for the following requirements:

- **Visual interest.** The proposal has blank, uninterrupted walls exceeding 25 feet in length. The first floor of the building along the southeast property line is a long blank wall. While the building jogs about halfway through the site, each blank section is approximately 78 feet long. This feature requires alternative compliance. The two sections would be different colored brick to correspond to the design of that portion of the building above. The applicant has shown eight 62 square foot rectangular designs along these blank walls which they have indicated would have an inset brick design but this is not specifically noted on the plans. Because this wall would be built at the property line, any landscaping that could screen or soften the blank wall would need to be located on the adjacent property which will require the applicant to work with the adjacent property owner. To ensure that this blank wall is adequately mitigated, staff recommends as a condition of approval that the blank wall shall be designed with inset brick as shown in the submitted elevations in order to provide visual interest and mitigate the blank wall.
- **Windows.** Along Lourdes Place, the new portion of the building is subject to a 20 percent window requirement at the ground level (measured between 2 and 10 feet from adjacent grade). The proposal incorporates 48 square feet of windows which amounts to 10 percent of the new construction building frontage. When including the windows in the garage door and the glass in the proposed door, the amount of glass area is increased to 83 square feet, or 17 percent of the building frontage. The historic building is nonconforming and was not included in the calculation, although the applicant is proposing to install several new windows on the Lourdes Place façade where they have been infilled for decades. Staff finds that the installation of new windows in the historic building façade on Lourdes Place and the proposed transparency in the garage door and doorway mitigate any adverse effects of the proposal and therefore the design adequately meets the intent of the standard. Staff recommends granting alternative compliance. A condition of approval is recommended in order to ensure that at least 83 square feet of glass area, as shown on the submitted plans, is incorporated along this portion of the façade.
- **Ground floor active functions.** At least 70 percent of the first floor building frontage facing a public street must contain active functions. The ground floor facing Lourdes Place contains 63 percent (88 feet) active functions and the ground floor facing 2nd Street Southeast contains only 47 percent (53 feet) active functions. The inactive portions of the building façade are primarily due to the location of the parking. Considering the site constraints associated with preserving the historic

buildings in their original locations, staff finds that strict adherence to this requirement is impractical as the potential locations for parking with two-way drive aisles and adequate maneuvering space are limited. The property fronts on three public streets. The East Hennepin Avenue façade will be entirely active with additional active commercial uses in the historic building along Lourdes Place and active residential uses in the new building on 2nd Street Southeast. Staff recommends granting alternative compliance for this standard.

RECOMMENDATIONS

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt staff findings for the applications by Schafer Richardson for the property located at 112 East Hennepin Avenue:

A. Conditional Use Permit to increase height from 4 stories or 56 feet to 6 stories, 75 feet in the C3A District.

Recommended motion: **Approve** the application for an increase in maximum height, subject to the following conditions:

- I. The conditional use permit shall be recorded with Hennepin County as required by Minn. Stat. 462.3595, subd. 4 before building permits may be issued or before the use or activity requiring a conditional use permit may commence. Unless extended by the zoning administrator, the conditional use permit shall expire if it is not recorded within two years of approval.

B. Variance to increase the maximum floor area ratio from 3.78 to 4.51.

Recommended motion: **Approve** the application for a variance to increase the maximum floor area ratio.

C. Variance to reduce the required interior side yard from 15 feet to 5 feet on floors 2 through 6.

Recommended motion: **Approve** the application for a variance to reduce the required interior side yard.

D. Variance of the Pedestrian Oriented Overlay District standards.

Recommended motion: **Return** the application to reduce the minimum window requirement below 40 percent along Lourdes Place.

E. Variance of the minimum nonresidential parking requirement.

Recommended motion: **Approve** the application for a variance of the minimum nonresidential parking requirement from 8 spaces to 0 spaces, subject to the following condition:

- I. At least 16 bicycle parking spaces shall be provided for the nonresidential uses. Bicycle parking in the public right-of-way will require an encroachment permit from Public Works.

F. Variance of the percentage of required parking spaces that may be satisfied by providing compact spaces.

Recommended motion: **Approve** the application for a variance of the percentage of required parking spaces that may be satisfied by providing compact spaces.

G. Site Plan Review for a new mixed use building attached to two historic buildings.

Recommended motion: **Approve** the application for a site plan review, subject to the following conditions:

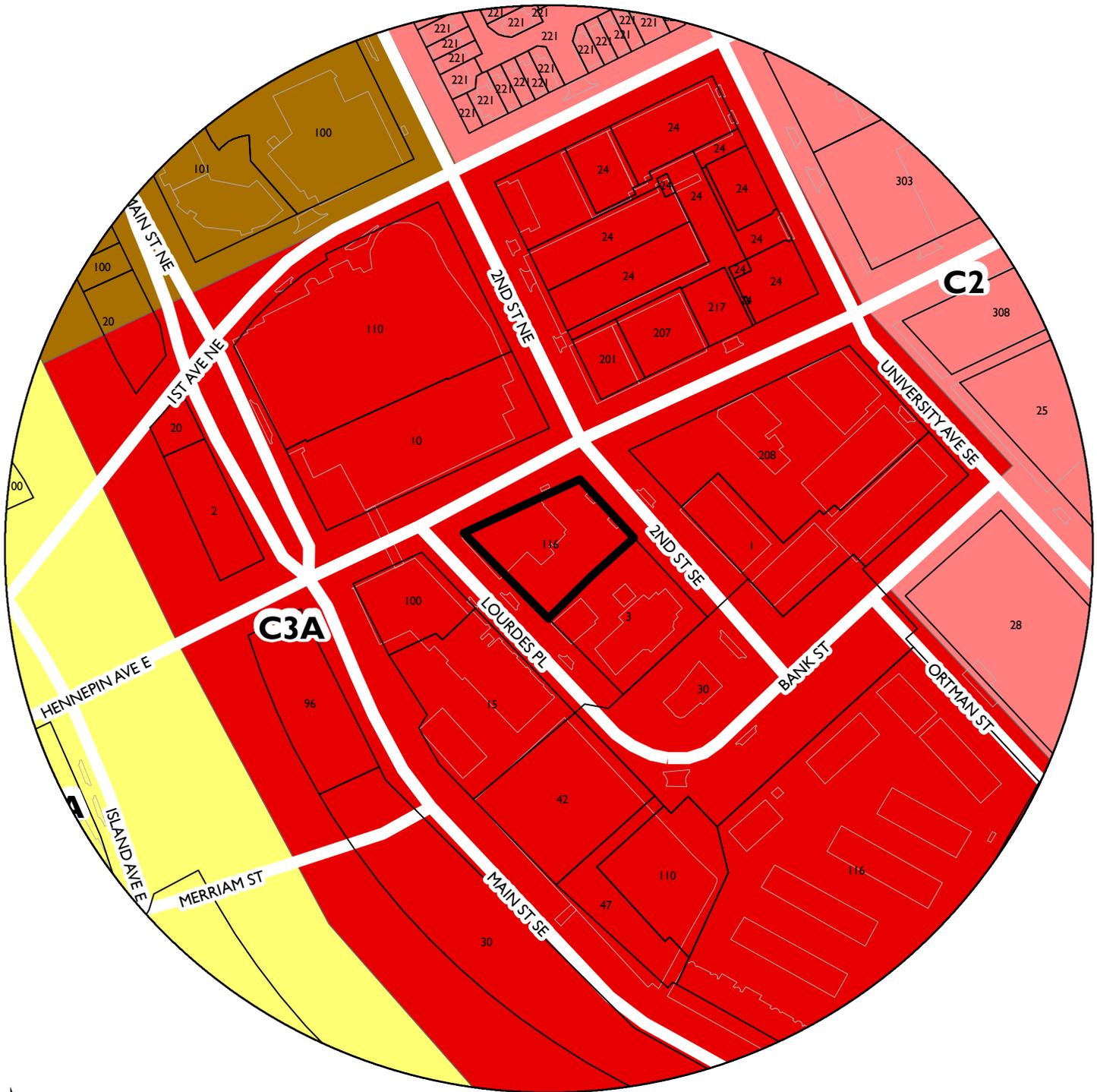
1. All site improvements shall be completed by March 14, 2018, unless extended by the Zoning Administrator, or the permit may be revoked for non-compliance.
2. CPED staff shall review and approve the final site, elevation, landscaping, and lighting plans before building permits may be issued.
3. The first floor of the southeast façade shall be designed with inset brick as shown on the submitted elevations to provide visual interest and mitigate the blank wall.
4. At least 83 square feet of glass area, as shown on the submitted plans, shall be incorporated in the new construction portion of the Lourdes Place façade. This may be accommodated in windows, garage doors, and pedestrian doors.

ATTACHMENTS

1. Zoning map
2. Oblique aerials
3. PDR report
4. Written description and findings submitted by applicant
5. Site survey
6. Plans
7. Photos
8. Renderings
9. Shadow study
10. Floor plans
11. Building elevations
12. Renderings
13. Shadow study
14. Car-stacking system information sheet
15. Correspondence

NAME OF APPLICANT

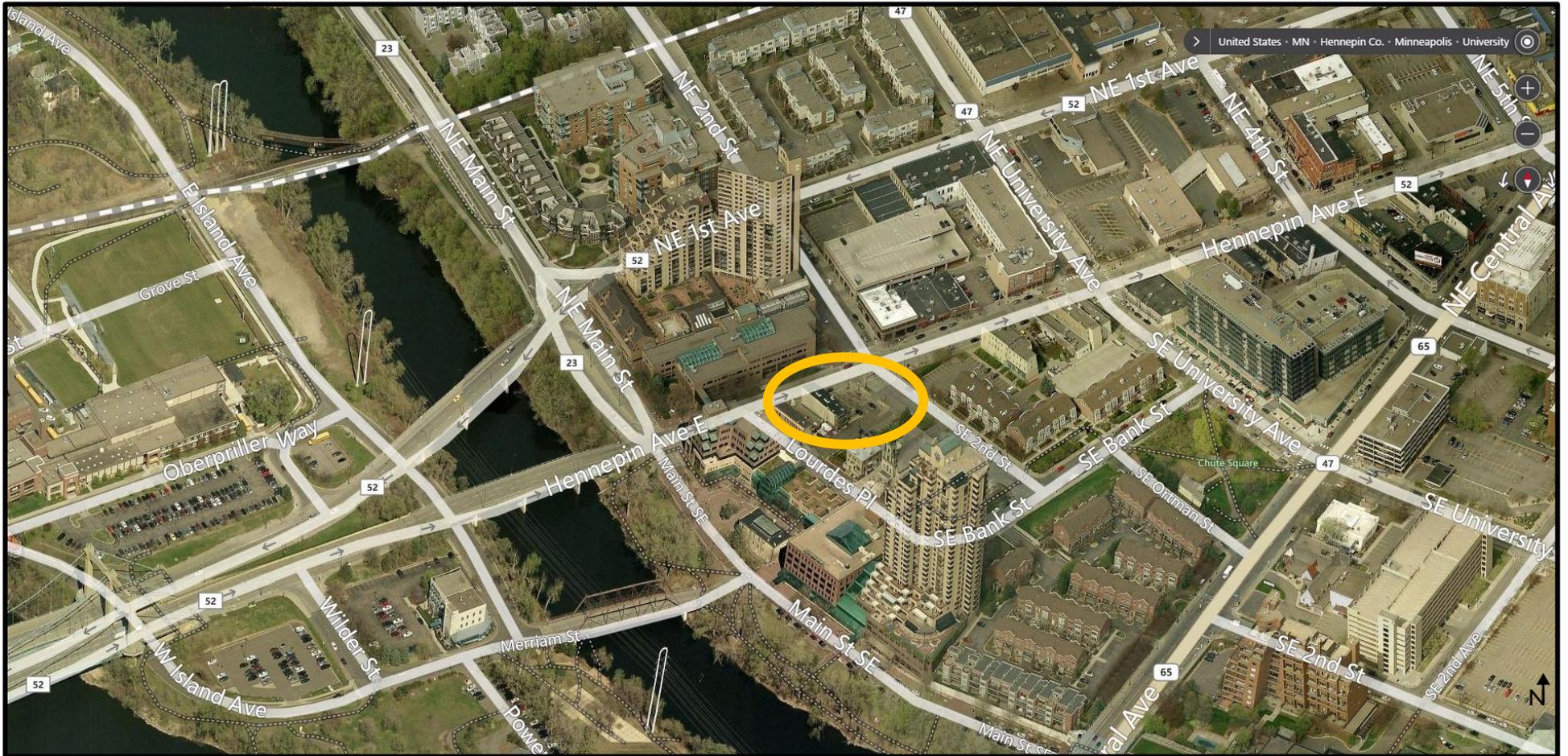
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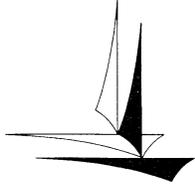


PROPERTY ADDRESS
112 East Hennepin Avenue

FILE NUMBER
BZZ-7573

BZZ-7573: 112 East Hennepin Avenue





Minneapolis Development Review
250 South 4th Street
Room 300
Minneapolis, MN 55415

Preliminary Development Review Report

Development Coordinator Assigned: DONALD ZART
(612) 673-2726
don.zart@minneapolismn.gov

Status *
RESUBMISSION REQUIRED

Tracking Number:	PDR 1001409
Applicant:	SR DEVELOPMENT LLC 900 3RD ST N MINNEAPOLIS, MN 55401
Site Address:	112 HENNEPIN AVE E
Date Submitted:	17-FEB-2016
Date Reviewed:	26-FEB-2016

Purpose

The purpose of the Preliminary Development Review (PDR) is to provide Customers with comments about their proposed development. City personnel, who specialize in various disciplines, review site plans to identify issues and provide feedback to the Customers to assist them in developing their final site plans.

The City of Minneapolis encourages the use of green building techniques. For additional information please check out our green building web page at: http://www.ci.minneapolis.mn.us/mdr/GreenBuildingOptions_home.asp.

DISCLAIMER: *The information in this review is based solely on the preliminary site plan submitted. The comments contained in this report are preliminary ONLY and are subject to modification.*

Project Scope

Proposed 4-6 story low-rise structure, connecting existing 2 story building on corner and 3 story "Harness Shop" building. There will be 76 market rate apartments with 8,000 sq ft of retail space.

Review Findings (by Discipline)

Historical Preservation Committee

- This property falls within the ST. ANTHONY FALLS HISTORIC DISTRICT and requires a preservation review.
- In addition, any structure that is wrecked requires a preservation review. This permit is to be obtained by a licensed wrecking contractor.

*Approved: You may continue to the next phase of developing your project.

*Resubmission Required: You cannot move forward or obtain permits until your plans have been resubmitted and approved.

□ Zoning - Planning

- The following land use applications have been identified for this proposal:
 - Conditional use permit to increase the maximum permitted height from 4 stories/56 feet to 6 stories/75 feet.
 - Variance to increase the maximum floor area ratio from 3.78 to 4.51.
 - Variance to reduce the required interior side yard from 15 feet to 5 feet.
 - Variance of the PO Pedestrian Oriented Overlay District standards to reduce the required window percentage along Lourdes Place from 40% to 17%.
 - Variance of the non-residential minimum parking requirement from 8 to 0.
 - Variance to increase the percentage of allowable compact parking spaces used to satisfy the parking requirement.
 - Site plan review.
- This project was originally scheduled for the 2/22 Planning Commission hearing but is being continued to the March 14 Planning Commission meeting due to plan revisions. Continue to work with staff through the Planning Commission approval process.

□ Addressing

- The proposed address for the apartment entrance will be 10 2nd St SE.
- The proposed address for the retail doors, starting at the corner of Lourdes Place and Hennepin Ave E will be 112, 114, 116, and 130 Hennepin Ave E
- Per City of Minneapolis Street Naming and Address Standard V1.22, the City of Minneapolis holds authority for assignment of all addresses, verification, change, and/or additions. Contact Development Coordinator for new address assignment. Each assigned address number uses the street that provides the best/direct access for life safety equipment and best/direct access to the occupants.
- When assigning suite sequences the following guidelines are as follows:
 - The first one to two digits of the suite sequence number will designate the floor number of the site.
 - The last two digits of the suite sequence number will designate the unique ID for the unit (condo, suite, unit, or apartment).
 - Suite sequence digit numbers will be assigned to dwelling, commercial and retail units, not common areas. For example, laundry rooms, saunas, workout rooms, etc., would not be assigned numbers.
 - Please provide each condo, suite, unit or apartment number.
- This building is also considered to have a parking ramp per MCO Chapter 108. As such, within 5 years of the date of the certificate of occupancy being issued, the parking ramp will be required to have annual inspections and apply for a Ramp Operating Certificate.

□ Street Design

- Note to the Applicant: The site plan proposes curb extensions along Hennepin Ave. that may be in conflict with roadway planning efforts currently underway between Hennepin County and the City of Minneapolis and likely will not be approved. It is recommended that the Applicant meet with the appropriate Public Works Engineering staff regarding impacts to the proposed site plan; please contact Paul Miller at (612) 673-3603 for further information.
- All driveway aprons shall be designed and constructed to City standards. All driveway aprons shall be shown graphically correct on all related plan sheets. Please refer to the following: http://www.minneapolismn.gov/publicworks/plates/public-works_road. The Minneapolis Standard Plate ROAD-2003 provided on Sheet C7.2 has been updated and shall be replaced with ROAD-2003-R1.
- All curb & gutter in the Public right-of-way shall be designed and constructed to City standards, curb & gutter to be City standard B624 Curb and Gutter. Top of Curb profiles shall be provided for any section of curb replacement in excess of 50 feet.

□ Parks - Forestry

- Contact Craig Pinkalla (cpinkalla@minneapolisparcs.org), Telephone (612)-499-9233 regarding removal or protection of trees during construction in the city right of way.
- Oak is overrepresented. Require alternate selection.
- Open planting specaes do not reflect minimum 125sq. Ft./tree.
- Increase to 6' x 20 would be approved (= 120 sq. ft./tree)
- No increase would trigger requirement for engineered root space and small tree selection only
- Overrepresented Genera -Nicollet Island - East Bank:
 - Honeylocust 74%
 - Oak 12%
- Effective January 1, 2014, the City of Minneapolis and the Minneapolis Park and Recreation Board adopted an update to the existing Parkland Dedication Ordinance.
- The adopted City of Minneapolis Parkland Dedication ordinance is located in Section 598.340 of the City's Land Subdivision ordinance:
- <http://library.municode.com/index.aspx?clientId=11490>
- As adopted, the fee in lieu of dedication for new residential units is \$1,521 per unit (affordable units excluded per ordinance) and for commercial and industrial development it is \$202.80 per development employee (as defined in ordinance). Any dedication fee (if required) must be paid at the time of building permit issuance. There is also an administration fee that is 5% of the calculated park dedication fee.
- As proposed, for the Nye's Site, the calculated dedication fee is as follows:

• Residential - 72 x \$1521	=	\$109,512
• Commercial -	=	\$ 608
• Less Credit For Existing Dwelling Units-		\$ 18,252
• Subtotal	=	\$ 91,868
• Administrative Fee - Capped at \$1,000	=	\$ 1,000
• Total	=	\$ 92,868
- This is a preliminary calculation based on your current proposal; a final calculation will be made at the time of building permit submittal.
- For further information, please contact Don Zart at (612)673-2726

□ Right of Way

- The site plan indicates a location for a proposed electric utility transformer within the Public sidewalk along 2nd St. S.E. however sidewalk ventilation grates are not identified. It is recommended that the Applicant begin discussions with Xcel Energy as soon as possible in order to identify electric utility and transformer locations on the site plan. It should not be assumed that the City will approve any proposed transformer location in the Public right-of-way.
- An encroachment permit shall be required for all streetscape elements in the Public right-of-way such as: plants & shrubs, planters, tree grates and other landscaping elements, sidewalk furniture (including bike racks and bollards), and sidewalk elements other than standard concrete walkways such as pavers, stairs, raised landings, retaining walls, access ramps, and railings (NOTE: railings may not extend into the sidewalk pedestrian area). Please contact Bob Boblett at (612) 673-2428 for further information.
- Note to the Applicant: Any elements of an earth retention system and related operations (such as construction crane boom swings) that fall within the Public right-of-way will require an encroachment permit application. If there are to be any earth retention systems which will extend outside the property line of the development then a plan must be submitted showing details of the system. All such elements shall be removed from the Public right-of-way following construction with the exception of tie-backs which may remain but must be uncoupled and de-tensioned. Please contact Bob Boblett at (612) 673-2428 for further information.
- In addition, any elements of an earth retention system and related excavations that fall within the Public right-of-way will require a "Right-of-Way Excavation Permit". This permit is typically issued to the General Contractor just prior to the start of construction. However, it is the Applicant's responsibility to insure that all required permits have been acquired by its consultants, contractors, sub-contractor's prior to the start of work.

- Per the Minneapolis Code of Ordinances, out-swinging doors are permitted to encroach a maximum of 4" into the Public right-of-way. The Applicant should consider designs that provide for in-swinging doors or recessed doorways to prevent out-swinging doors beyond the maximum allowable encroachment.
- The proposed bike rack locations within the curb extension area at the intersection of Hennepin and 2nd St. S.E. is a concern; contact Paul Cao at (612) 673-2943 for position and alignment of bike racks proposed in the Public right-of-way. If the racks are privately owned, they will require an encroachment permit.

❑ Fire Safety

- Provide required fire suppression system throughout buildings - new and existing.
- Provide a fire alarm system throughout buildings.
- Fire department connection must be located on the address side of building and within 150 feet of a fire hydrant.
- Provide and maintain fire apparatus access at all times.

❑ Sidewalk

- The Project streetscape shall be designed in accordance with the Access Minneapolis design guidelines; see <http://www.ci.minneapolis.mn.us/publicworks/transplan/index.htm> for further information. Specifically, the pedestrian zone is of concern, a minimum 6'-0" clear zone (un-obstructed) sidewalk shall be maintained for the length of any block in a straight line (the clear pedestrian zone cannot "jog" around planters and tree grates). Locations for site furniture, tree grates, planters and other proposed design elements that fall within the Public right-of-way shall be modified to provide for the required pedestrian clear zone space. For further clarification, site plans shall be fully dimensioned in relation to the property lines, Public right-of-way, sidewalks, curb extensions (including tapers), street furniture, landscaping, utilities, and other obstructions.
- ADA compliant pedestrian ramps are required at each crosswalk at the intersection of E. Hennepin Ave. and Lourdes St. (construct one (1) ADA compliant pedestrian ramp at this location); and E. Hennepin Ave. and 2nd St. S.E. (construct two (2) ADA compliant pedestrian ramps at this location). Note that any recently upgraded ADA compliant pedestrian ramps shall be protected during construction. All pedestrian ramps will need to meet current ADA regulations and be "Accessible Pedestrian Signal" (APS) ready; please contact Ryan Anderson at 612-673-3986 for more information on current APS designs.
- Note to the Applicant: Any currently defective sidewalks or other concrete infrastructure within the public right of way, or any concrete infrastructure damaged during construction, must be removed and replaced.

❑ Water

- All existing and proposed underground Public Utilities (water, sanitary sewer, and storm drain) shall be shown on the site plan with corresponding pipe sizes and types. For Public watermain infrastructure records call (612) 673-2865. Any existing connections not in use shall be noted on the plans for removal, and shall be removed per the requirements of the Utility Connections Department, call (612) 673-2451 for more information.
- Meters shall be set at the point where the service line enters the building and shall be set in a location where it is easily assessable. Domestic service lines larger than three inches require a Bypass Assembly (see attachment). Please contact Rock Rogers at (612) 673-2286, to review domestic and fire service design, connections, and sizes.

❑ Construction Code Services

- The plans as submitted meet the requirements of Construction Code Services. A full construction review is required for the building permit.
- To schedule a preliminary plan review meeting, please call (612)673-5839
- Please contact the Met Council for a SAC determination for this project. For more information please refer to this link:
http://www.ci.minneapolis.mn.us/www/groups/public/@regservices/documents/webcontent/convert_281675.pdf

□ Traffic and Parking

- The nature of the proposed development is such that traffic impacts will be an issue; please contact Allan Klugman at (612) 673-2743 to discuss the requirements of a Travel Demand Management Plan (TDMP).
- Note to the Applicant: The site plan proposes curb extensions along Hennepin Ave. that may be in conflict with roadway planning efforts currently underway between Hennepin County and the City of Minneapolis and likely will not be approved. It is recommended that the Applicant meet with the appropriate Public Works Engineering staff regarding impacts to the proposed site plan; please contact Paul Miller at (612) 673-3603 for further information.
- The Applicant shall note the location of any existing Metro Transit "bus stops" on the site plan.
- Current ordinance states that all maneuvers associated with loading, parking or sanitation pick up for a private development shall occur on private property. Please provide a narrative explaining the trash removal operations and show turning maneuvers for all truck type vehicles that will be using the parking entrance areas. Per City Ordinance the Applicant shall provide for (and identify) a solid waste collection point (SWCP) on the site plans. The location of the SWCP is subject to the review and approval of the Public Works Department.
- The vehicle access point on Lourdes St.. shall provide for an unobstructed 20' sight triangle and shall have less than a 4% slope for the first 20 feet from the property line (please indicate slope on the drawings). If these minimums cannot be attained the Applicant shall provide mitigating measures, such as pedestrian warning devices, signage, and slip prevention measures on the ramp. Generally, a vehicle should not be stopped on the sidewalk or in a moving lane of traffic waiting for a garage door to open. Please provide a narrative explaining garage door operation on Lourdes St.
- Contact Shane Morton (612)673-5517 regarding street parking
- All street lighting (existing and proposed) shall be shown clearly on the site plan. Prior to site plan approval, the Applicant shall contact Joseph Laurin at 612-673-5987 to determine street lighting requirements. Note: If decorative street lighting exists on the proposed site it must be preserved or replaced at existing levels. Per the City of Minneapolis Street Lighting Policy, street lighting will be strongly encouraged in areas immediately adjacent to existing lighting systems, in high density areas and along major pedestrian corridors and business nodes as identified in the Minneapolis Street Lighting Policy.
- Note to the Applicant: In addition to the various required construction permits, impacts to existing traffic signal and street lighting systems (including installation of new street lights) will require the Applicant and respective Contractors to enter into a separate Right-of-Way Excavation Permit (including Testing and Inspection requirements) with the Public Works Department, for further information regarding this permit please coordinate with Paul Miller at (612) 673-3603.
- Note to the Applicant: The construction of this development will likely require the use of Public right-of-way (roadway and sidewalks) for construction purposes. A request for an estimate of street use and obstruction permit fees can be made to the City's Traffic Department; please contact Scott Kramer at (612) 673-2383 for further information.
- Note to the Applicant: Please add the following notes to the site plan:
 - Street lighting installed as part of the Project shall be inspected by the City. Contractors shall arrange for inspections with the Traffic Department, please contact Dave Prehall at (612) 673-5759 for further information. Any lighting installations not meeting City specifications will be required to be reinstalled at Owner expense.
 - An obstruction permit is required anytime construction work is performed in the Public right-of-way. Please contact Scott Kramer at (612) 673-2383 regarding details of sidewalk and lane closures. Log on to <http://minneapolis.mn.roway.net/> for a permit.
 - Contact Allan Klugman at (612) 673-2743 prior to construction for the temporary removal/temporary relocation of any City of Minneapolis lighting or traffic signal system that may be in the way of construction.
 - All costs for relocation and/or repair of City Traffic facilities shall be borne by the Contractor and/or Property Owner.
 - Contact Doug Maday at (612) 673-5755 prior to construction for the removal of any City of Minneapolis right of way signs that may be in the way of construction.

❑ Business Licensing

- Please contact Don Zart (612)673-2726 regarding the process for a Health Plan Review for any portion of the project that may have a food related business occupying the space.

❑ Environmental Health

- Other projects in the area have encountered contaminated soil and addressed this through the Minnesota Pollution Control Agency Voluntary Investigation and Cleanup program. Some sites also encountered a perched water table that required dewatering during general site excavation and construction. If impacted soil is encountered during site activities work will need to stop and notification provided to the MN State Duty officer at (615) 649-5451.
- If dewatering is required during site construction see below for city permit requirements. The scope of work calls for underground parking. The highest groundwater level expected for this site should be determined and used in establishing the lowest level for underground parking. The underground parking and other subgrade structures should be designed to prevent infiltration of groundwater without the need for a permanent dewatering system being installed. If a continuously operating permanent dewatering system is needed it must be approved as part of the sanitary sewer and storm drain site plan approval prior to construction beginning.
- No construction, demolition or commercial power maintenance equipment shall be operated within the city between the hours of 6:00 p.m. and 7:00 a.m. on weekdays or during any hours on Saturdays, Sundays and state and federal holidays, except under permit. Contact Environmental Services at 612-673-3867 for permit information.
- Permits and approval are required from Environmental Services for the following activities: Temporary storage of impacted soils on site prior to disposal or reuse; Reuse of impacted soils on site; Dewatering and discharge of accumulated storm water or ground water, underground or aboveground tank installation or removal, well construction or sealing. Contact Tom Frame at 612-673-5807 for permit applications and approvals.
- A review of the project, permits issued and an inspection from Environmental Service for identification of equipment and site operations that require annual registration with the City of Minneapolis will occur for this project.

❑ Sewer Design**Surface Drainage:**

- Please identify the lowest and first floor elevations on the grading plan.
- Please provide details drainage plans for the proposed curb extensions. Based on the current spot elevations, low points will be created that are not served by catch basins.
- It appears grading is proposed on the adjacent property. Approval from that property owner will need to be obtained. Please also verify no runoff from the site is proposed to be directed to that area.

Utility Connections:

- The proposed storm sewer service connection to the City main should be core-drilled and a saddle tee fitting installed, per City of Minneapolis Standard Supplemental Specifications. A new manhole is not permitted for the connection. Please revise the plans accordingly.

Utility:

- The relocated CB at 2nd St should not be connected to the existing CB. A new CB run should be provided and the existing CB be eliminated.

Non Stormwater Discharges:

- Detail all mechanical and non-stormwater discharges. Non-stormwater discharges are not permitted unless approved by the City of Minneapolis. Non-stormwater discharges not declared and approved will not be permitted. If there currently are none and nothing is proposed declare this status on the plans.
- For comments or questions on Public Works Surface Water & Sewers Division related requirements please contact Jeremy Strehlo, (Professional Engineer) at (612) 673-3973, or jeremy.strehlo@minneapolismn.gov

END OF REPORT

116 E Hennepin Land Use Application: Proposed Use and Project Description

Schafer Richardson is proposing to redevelop the 112, 116, 120 East Hennepin site in Northeast Minneapolis. The 20,000 square foot site currently accommodates a surface parking lot and four buildings with a combined footprint area of approximately 8,000 square feet. The existing buildings are currently occupied by Nye's Bar, Polonaise Room and Chopin Room on the ground floor with residential apartments above. The two, non-historic, 1-story infill buildings, built in the 1960s, will be demolished. The existing 2-story "Nye's Bar" building, built in 1907, at the corner of Lourdes Place and East Hennepin will be preserved in its present location. The 3-story "Harness Shop" building, built in 1905, will be maintained and preserved in its present location. Renovations to both of the remaining buildings will include interior improvements that will address structural and life safety needs. The Project has received two Certificates of Appropriateness for the demolition of the two one-story additions and for the renovation and new construction of the Project, as proposed.

A new mixed-use development, not yet named, will be incorporated with the existing, rehabilitated buildings on site. A new 4-6 story low-rise structure will be constructed around and connected to the existing buildings. This new building will have three unique facades to honor the rhythm of traditional urban design.

The development collectively will contain 72 market rate apartments and approximately 8,000 square feet of retail. The development will include one level of underground parking with additional enclosed parking on the first level behind the retail/residential lobby space. There will be 50 traditional parking stalls (standard and compact), 6 tandem spaces, and a proposed CarMatrix car stacker system on the first level with 12 additional parking spaces.

The first floor will comprise approximately 4,980 square feet of commercial space in the new development along East Hennepin Avenue and at the corner of East Hennepin and 2nd Street. There will be an additional 2,721 square feet of commercial space in the existing buildings at grade. The residential pedestrian entrance will be on 2nd Street. There will be two entrances and exits to the building parking. The parking entrance on Second Street will be the grade level parking entrance. The ramp entrance on Lourdes Place will provide access to the below grade parking.

The development will feature indoor and outdoor amenity spaces at the fourth level. Building-wide amenities include fitness center, roof deck, community room(s), management offices, and balconies.

New landscape and streetscape elements will be added to the right of way surrounding the site which will be reflective of and compatible with the historic commercial use in the area. These streetscape enhancements will improve the connection between the heart of the East Hennepin commercial district and the riverfront, a connection that has been diminished in Character Area J of the St. Anthony Falls Historic District since the period of significance. Sidewalks will be widened around the site and will be between 11 and 13'6" in width. Sidewalk bump outs will be utilized on the corner of Hennepin and Lourdes and Hennepin and Second Street, if approved by City of Minneapolis Public Works.

Trees and planting beds will be added along the Hennepin Avenue side of the site and will include native plantings.

Street lights will be included on all three sidewalks surrounding the development. Lights on Lourdes Place and Second Street will be a traditional "acorn" style as is seen today. Lights on Hennepin Avenue will

mimic what is currently on the other side of Hennepin Avenue, as well as up and down the commercial corridor. These lighting strategies will support pedestrian and commercial activity, and connection to the riverfront. New bicycle racks will be added into the right of way to support various modes of transportation to the retail uses at the site.

Utilities are planned to be placed underground where possible. Transformers are proposed to be located below grade in a vault, with permission from Xcel Energy and the City of Minneapolis.

116 E Hennepin Land Use Application: Specific Application Requirements

Conditional Use Permit: The Project Team is seeking a conditional use permit to increase maximum height from 4 stories/56 feet to 6 stories/72 feet.

- (1) The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, comfort or general welfare.**

The conditional use permit applied for on the 116 E Hennepin project, to increase the maximum height from 4 stories (56 feet) to 6 stories (72 feet), will use not change the construction or operation of the project and will in no way be detrimental to or endanger the public health, safety, comfort or general welfare of the community.

- (2) The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.**

The conditional use will not be injurious to use and enjoyment of other property in the vicinity. In fact, the redevelopment of the Project Site, in particular the increased density over existing conditions, will bring a vibrancy to the neighborhood and increase community capital that can be used to support nearby commercial establishments. In this prime community corridor connecting downtown Minneapolis with Northeast, the Project will improve the streetscape and create an improved bicycle/pedestrian experience. The proposed height will not adversely impact other properties. The project site is separated from neighboring properties on three sides by public streets. The setback of the closet building on the only adjacent property is more than 35' from the shared property line, providing substantial separation and mitigating any potentially-injurious impact of the proposed 6-story building.

Furthermore, the Project height is consistent with the pattern of redevelopment in the area. The proposed 6-story, 72-foot high building is of similar scale to other recent mixed use developments in the East Hennepin area, including Cobalt and Red 20. Buildings on the blocks across E Hennepin Avenue to the Northwest and Lourdes Place to the Southwest include 4-5 story office buildings, which are comparable in height to the proposed building, and 20+ story residential towers. The Project will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.

- (3) Adequate utilities, access roads, drainage, necessary facilities or other measures, have been or will be provided.**

As an infill site that consists of existing commercial buildings and a surface parking lot, the Project will improve upon existing conditions on site including providing adequate utilities, utilizing existing access roads, but moving from three vehicular access points (curb cuts) to two; and addressing drainage.

- (4) Adequate measures have been or will be taken to minimize traffic congestion in the public streets.**

Currently there is one curb cut to access the site on Lourdes Place and two curb cuts on 2nd Street. The Project will eliminate one of the curb cuts so that vehicular access to the development is from one curb cut on Lourdes Place and one on 2nd Street, both of which are pushed away from Hennepin Avenue to minimize traffic congestion. The Project will provide enclosed, on-site vehicular parking to meet the requirements of the Zoning Code. The Project will also include ample bicycle parking for residents, as well as bike racks near the commercial and residential building entrances, that will serve to reduce vehicular traffic to the site.

(5) The conditional use is consistent with the applicable policies of the comprehensive plan.

The conditional use is consistent with applicable policies of the Minneapolis Plan for Sustainable Growth and the Nicollet Island-East Bank Small Area Plan. The Project will be located on East Hennepin Avenue, which is identified as a Commercial Corridor in the Minneapolis Plan for Sustainable Growth, on a site that is part of the East Hennepin Activity Center. The proposed building height will allow for the high-density, mixed-use development that these land use features call for. The Small Area Plan also supports new high-density residential developments with ground floor commercial uses in the neighborhood. The Small Area Plan indicates that the design of new buildings should consider and respect surrounding historic buildings. Further, the Small Area Plan calls out projects with exceptional streetscapes and site design as those where additional height would be supported by the neighborhood association. The Project achieves both these Small Area Plan goals through a design that is sensitive to the adjacent historic church property and that substantially enhances the streetscape.

(6) The conditional use shall, in all other respects, conform to the applicable regulations of the district in which it is located.

With the approval of the requested zoning applications, the Project will conform to the applicable regulations of the district in which it is located.

Increasing Maximum Height: The Project Team is seeking a conditional use permit specifically to increase maximum height from 4 stories/56 feet to 6 stories/72 feet.

(1) Access to light and air of surrounding properties.

The proposed addition of 2-stories (from four to six) will not change the access to light or air of surrounding properties. The Project is surrounded on three sides by public streets and on one side by Our Lady of Lourdes Church. The church property is to the Southeast and is at a higher grade from the Project site. As noted, buildings on that property a minimum of 35' from the property line. Furthermore, we have met with leaders from the church about the plans for the Project and they are supportive of the current design as evidenced by their letter of support submitted during the application and appeal process with the Heritage Preservation Commission for the Certificates of Appropriateness.

(2) Shadowing of residential properties, significant public spaces, or existing solar energy systems.

The proposed project will not substantially shadow any residential properties, significant public spaces, or existing solar energy systems.

(3) The scale and character of surrounding uses.

As mentioned above, the office buildings across E Hennepin Avenue and Lourdes Place are four-five stories in height, and comparable to the proposed building. There are apartment towers that are on the same block as each of these office buildings that respectively rise to 20+ stories. Moving down E Hennepin Avenue to the Northeast, there are historic commercial buildings that vary in height from 1-3-stories. Finally, the church and rectory buildings to the Southeast of the site 2-3-stories in height; however, because of the change of grade, the peak of the church building is approximately 4' higher with the steeple rising 25' taller than the sixth floor of the proposed development.

This is a neighborhood with a varied tapestry of building types and uses with the proposed development fitting in well with the scale and character of the area. The street walls of the Project will use brick and storefront as their primary materials. Brick and metal will be used on the upper floors, which reflect the traditional nature of the older buildings in the area while at the same time bringing a modern touch. Furthermore, the incorporation of the two existing historic buildings on the Project site (two and three-stories, respectively) provide variation in the scale creating a block that honors a traditional urban landscape.

(4) Preservation of views of landmark buildings, significant open spaces or water bodies.

The Our Lady of Lourdes Church is the only surrounding site applicable to this item. Because of the nature of the commercial corridor where buildings are fronted on the street, as a pedestrian on E Hennepin Avenue, the view of the church property will be the same at four stories as it will at six stories. The proposed development has been designed so that the steeple of the church rises 25' higher than the top floor of the proposed development, so it will be visible from other properties to the Northwest. The increased height of the Project does not impact the views from Lourdes Place or 2nd Street NE.

Variance for Floor Area Ratio: The Project is seeking a variance of the maximum floor area ratio from 3.78 to 4.51.

The Project is located in a C3A zone (community activity center) with a pedestrian overlay (PO). The Project is awarded density bonuses for enclosed parking (20%) and for being a mixed commercial-residential building (20%) bringing the maximum floor area ratio (FAR) to 3.78. The proposed FAR is 4.51 or a 16.2% variance.

(1) Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.

Practical difficulties exist for building within the FAR limits without a variance and complying with the City's land use guidance for the site, which calls for high-density development. Practical difficulties are also created by the restoration and preservation of the two historic buildings, which is not feasible to do as part of the site's redevelopment in a manner that would meet the FAR requirements and City design goals. These are unique circumstances not created by the developer.

- (2) The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.**

The mixed-use, pedestrian-oriented nature of the Project supports the spirit and intent of the ordinance and the comprehensive and small area plans by emphasizing principles of good urban design, including buildings that reinforce the street wall, off-street parking, windows and a roof deck that promote “eyes on the street,” commercial and residential entrances that face the public sidewalks, amenities that promote bicycling and alternative modes of transportation, and a streetscape that enhances the vibrancy, safety, and livability of the area. This Project will increase density in a location that is well connected to transportation and walkable to numerous amenities. Overall, the redevelopment of the site will bring life to a currently underutilized surface parking lot and deteriorating buildings.

- (3) The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.**

As noted above, the scale of the proposed building, and consequently the proposed FAR, is similar in character to other buildings in the vicinity, and much smaller than two nearby residential towers. The proposed variance will enable a project that enhances the character of the area and improves upon the health, safety, and welfare of the public by creating more “eyes on the street” during the day, as well as in the evenings because of the mix of uses. In addition, the increased density will bring community capital to the neighborhood, which will not only support the commercial businesses located in this project, but also businesses in the neighborhood at-large. The site design proposes several elements which lend themselves to “crime prevention through environmental design” including street and building lighting, sidewalk bump outs at the street corners for pedestrian safety, and landscape elements selected to provide good visibility along the street, as well as to the commercial entrances.

Variance for Interior Side Yard: The Project is seeking a variance of the interior side yard setback from 15 feet to 0 feet.

The Project has an interior side yard on the Northeast side of the site. The setback in this location is 15 feet (5+2x 5stories). The proposed project seeks a variance to bring this setback to 0 feet.

- (1) Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.**

The restoration and preservation of the two historic buildings, given their location on the site, make it practically difficult to develop the site in a manner that would allow for a minimum fifteen foot setback. As designed, the first aboveground level of the proposed development will not be set back from the lot line. It should be noted that the wall of the first level of the proposed

building will act as a retaining wall for the adjacent site. The façade of the upper floors (2-6) will be set back from the lot line a minimum of 5' with some sections of the building stepping back approximately 10'. We have met with representatives of the Owner of the adjacent parcel about the plans for the Project and they are supportive of the current design as evidenced by their letter of support submitted during the application and appeal process with the Heritage Preservation Commission for the Certificates of Appropriateness.

(2) The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.

Two factors that drive the need for this variance are the preservation of the existing historic buildings and the desire to have parking underground and hidden from view. Both of these factors are key for this location per the comprehensive plan. In addition, by pushing entry and exit to the parking area towards the lot line and away from East Hennepin Avenue, the Project minimizes pedestrian and vehicular conflict and promotes street life. Overall, the redevelopment of the site will bring life to a currently underutilized surface parking lot and deteriorating buildings.

More specifically, the proposed setbacks are in keeping with the spirit and purpose of the setback ordinances, which are intended to preserve access to light and air for residential uses and reinforce building code separation requirements. There will be a substantial separation between the new building and the Church's rectory building and the proposed setbacks will comply with building code requirements.

(3) The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.

The proposed Project, with the interior side yard setback variance, will not be injurious to the use or enjoyment of other property in the area, nor will it be detrimental to the health, safety, or welfare of the general public. We have met with representatives of the Owner of the adjacent parcel about the plans for the Project and they are not only supportive of the current design, but do not feel it to be injurious to the use or enjoyment of their property as evidenced by their letter of support submitted during the application and appeal process with the Heritage Preservation Commission for the Certificates of Appropriateness.

Variance for Window Area: The Project is located in the Pedestrian Oriented Overlay District (PO) and therefore is required to have at least 40% of the first floor façade of any nonresidential use that faces a public street or sidewalk with windows or doors of clear or lightly tinted glass. The Project is seeking a variance for window area on the Lourdes Place façade (Southwest façade).

- (1) Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.**

The restoration and preservation of the historic 2-story “Nye’s Bar” Building, which sits on the corner of Lourdes Place and East Hennepin Avenue, make it practically difficult to meet the 40% window area requirement on Lourdes Place. As part of the proposed renovation, brick will be removed from four historic window openings on this façade and replaced with new glass in order to increase the window area from where it is today and restore the building to its original design. In the new portion of the building fronting on Lourdes Place, there will be a garage door for entry to the parking area, as well as a door to access the residential areas. Both of these access points will be partially transparent.

- (2) The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.**

Two factors that drive the need for this variance are the preservation of the existing historic buildings and the desire to have parking underground and hidden from view. Both of these factors are key for this location per the comprehensive plan. The façade along East Hennepin, arguably the street that will be most heavily utilized by pedestrians, well exceeds the 40% requirement and therefore, the project as a whole keeps with the spirit and intent of the ordinance. Overall, the redevelopment of the site will bring life to a currently underutilized surface parking lot and deteriorating buildings and activate this block.

- (3) The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.**

The proposed Project, with the window area variance for the Lourdes Place façade, will not be injurious to the use or enjoyment of other property in the area, nor will it be detrimental to the health, safety, or welfare of the general public. By preserving the historic buildings, opening four windows on the first level of the Lourdes Place façade, and creatively infilling the site, it is our opinion that the character and enjoyment of this block will be enhanced by the redevelopment Project.

Variance for Off-Street Parking for Commercial Use: The Project is seeking a variance for minimum off-street parking requirements specific to the commercial use from 8 spaces to zero.

The Project is proposing 68 underground parking spaces in a secure garage. Due to security and logistical challenges, the Project intends to use this parking for the residential use in the building only and therefore seeks a variance from this minimum requirement.

- (1) Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.**

The restoration and preservation of the two historic buildings, given their location on the site, make it practically difficult to provide a sufficient number of parking spaces on this site to serve both the residential and commercial uses. The location of the historic buildings, in addition to the presence and elevation of bedrock, constrain the design and make it difficult to maximize parking across the entirety of the site. Likewise, there are logistical challenges with a mixed-use project like this to providing secure, enclosed parking to residents while also serving at-large patrons of the commercial tenants. Because the doors to the parking garage will be on a secure fob system, the Owner has determined that commercial parking cannot be accommodated sufficiently on site.

(2) The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.

The main factor driving the need for this variance is preservation of the existing historic buildings, thereby limiting the quantity and location of off-street parking. The mixed-use, pedestrian-oriented nature of the Project supports the spirit and intent of the ordinance and the comprehensive and small area plans by emphasizing principles of good urban design. In evaluating uses along the E Hennepin commercial corridor, it is clear that there are several thriving commercial establishments that don't have on-site dedicated parking, but are served by the existence of metered street parking and public parking garages. More specifically, there are over 45 street metered parking spaces on the block where the project is located (E Hennepin Avenue, Lourdes Place, and 2nd Street SE). During a typical week, many of these street spaces are open with peak use occurring on Sunday mornings when Our Lady of Lourdes Church holds mass.

The intent of the off-street parking requirement for commercial uses is to ensure that parking supply meets parking demand in order to adequately serve all uses in the area. Given the existence of ample street parking, public parking garages, and alternative transportation infrastructure such as bike racks, safe pedestrian routes, and transit stops within a 100 foot radius of the site, this project meets the spirit and intent of the ordinance and will bring life to a currently underutilized site.

(3) The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.

The proposed Project, with off-street parking reserved for the residential use component of the building, will not be injurious to the use or enjoyment of other property in the area, nor will it be detrimental to the health, safety, or welfare of the general public. The eight off-street parking spaces required per the zoning ordinance should easily be covered by the ample metered street parking on the block, public parking garages in the neighborhood, and alternative transportation infrastructure on and near the site. We have discussed the project with the neighborhood association and met with representatives of the Owner of the adjacent parcel about the plans for the Project and they are not only supportive of the current design and use, but do not feel it to be injurious to the use or enjoyment of their property. By preserving the historic buildings,

increasing the density and mix of uses, and redeveloping the site, it is our opinion that this project will enhance and strengthen the character and vibrancy of the neighborhood as a whole.

Steiner, Lisa

From: Katie Anthony <kanthony@sr-re.com>
Sent: Friday, February 19, 2016 1:06 PM
To: Steiner, Lisa
Subject: 116 E Hennepin Car Matrix

Lisa,
Thanks for talking through parking issues with me this afternoon. As we discussed, the driving factor in selecting the compact car matrix is the floor to ceiling height of the first floor and the related challenges of getting the new construction floors to time out with the existing historic floor heights on both the first and second levels. The parking garage structure will be constructed using traditional means; then the standalone CarMatrix system will be installed. It should be noted that if future parking demand declines and the stacked car system is no longer needed, we can remove the CarMatrix structure and revert those surface spots to Standard parking spaces. Essentially, the CarMatrix machinery and required dimensions prevent us from calling these spaces Standard.

Regards,
Katie

Katie Anthony LEED BD+C, O+M
Project Manager



Celebrating 20 Years of Excellence

900 North Third Street
Minneapolis, MN 55401
Direct: 612.305.7084
Cell: 617.519.1675
Fax: 612.359.5858
kanthony@sr-re.com
www.sr-re.com

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112, 116, 120 EAST HENNEPIN AVENUE Minneapolis, MN

METRICS

116 East Hennepin
Schafer Richardson
Parcel Size = 20,001 sq. ft.
Area Summary

Level	Use	Total GSF	Commercial GSF	Historic Buildings	Lobby Amenity	Apt GSF	RSF	Units	Parking	Efficiency
Level P1	Parking	19,694	16,554	2,469		671			30	
Level 1	Parking/Retail	19,483	9,304	4,980	2,721	1,187			20	
Level 2	Residential	18,203		3,368	853	13,982	14,465	16		79%
Level 3	Residential	17,035		1,898	288	14,849	13,760	16		81%
Level 4	Residential	16,214			705	15,509	12,100	15		75%
Level 5	Residential	14,309				14,309	12,127	14		85%
Level 6	Residential	14,309				14,309	12,127	11		85%
Total		119,247	25,858	4,980	10,456	3,187	74,816	64,579	72	50

Studio	Alcove	1 BR	2 BR	3 BR	PH	DU's	Beds
Level 2	4	6	4	2		16	24
Level 3	4	7	4	1		16	22
Level 4	4	7	4			15	19
Level 5	4	5	4		1	14	19
Level 6	2	3	3			11	17
Total	0	18	28	19	3	4	72

0% 25% 39% 26% 4% 6%

ESG Architects February 12, 2016



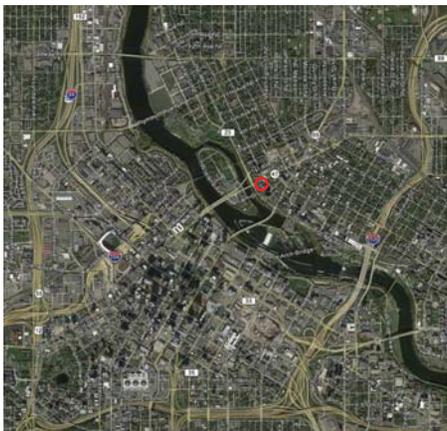
elness swenson graham architects
500 Washington Avenue South
Minneapolis Minnesota 55415
P: 612-339-5508
F: 612-339-5382
www.esgarch.com

I hereby certify that this document was prepared by me or under my direct supervision and that I am a duly licensed architect under the laws of the State of Minnesota.

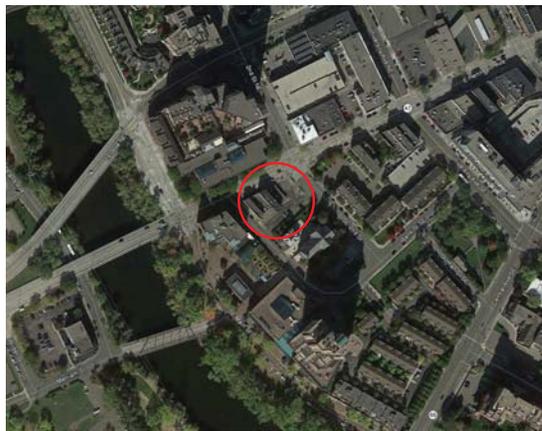
Name: _____
Type or Printed Name: _____
Date: _____

NOT FOR CONSTRUCTION

PROJECT LOCATION



Vicinity



Site Location

PROJECT TEAM

- Owner/Developer:** Schafer Richardson
901 N 2nd Street #218
Minneapolis, MN 55401
Ph: 612-371-3000
- Architect:** Elness Swenson Graham Architects Inc.
500 Washington Ave. South, Suite 1080
Minneapolis, MN 55415
Ph: 612-339-5508
Fx: 612-339-5382
- Landscape:** Damon Farber
401 N 2nd Ave, Suite 410
Minneapolis, MN 55401
Ph: 612-332-7322
- Contractor:** Friens Companies
633 Second Avenue South
Hopkins, MN 55343
Ph: 952-835-8900
Fx: 952-8358644
- Civil:** Surdo Engineering, PLLC
10820 Nesbit Avenue South
Bloomington, MN 55437
Ph: 952-881-3344
- Structural Engineer:** Meyer Borgman Johnson
510 Marquette Ave, Suite 900
Minneapolis, MN 55402
Ph: 612-338-0713
Fx: 612-337-5325
- Mechanical:** Steen Engineering, Inc.
5400 Douglas Drive North
Minneapolis, MN 55429
Ph: 769-585-6742
- Electrical:**
- Plumbing Engineers:**

SHEET INDEX

SHEET NUMBER	SHEET NAME	IPC 11-2-2015 Prelim Paving 11-18-2015 Land Use 2-12-2016
GENERAL INFORMATION		
T1.1	TITLE SHEET	• • • •
D1.5	SURVEY	• • • •
CIVIL		
C1.0	DEMOLITION AND EROSION CONTROL PLAN	• • • •
C2.0	SITE PLAN	• • • •
C3.0	GRAVITY SEWERAGE AND UTILITY PLAN	• • • •
C4.0	NOTES AND DETAILS	• • • •
C5.0	STORMWATER POLLUTION PREVENTION PLAN	• • • •
LANDSCAPE		
L1.1	LANDSCAPE PLAN	• • • •
ARCHITECTURAL		
A1.0	DEMOLITION PLAN	• • • •
A1.1	EXISTING CONTEXT SITE RANGES	• • • •
A1.2	DESIGN PERSPECTIVE VIEWS	• • • •
A1.3	CONTEXTUAL ELEVATIONS	• • • •
A1.4	ELEVATIONS IN CONTEXT	• • • •
A1.5	ELEVATIONS IN CONTEXT & BIRDSEYE PERSPECTIVE	• • • •
A1.6	SHADOW STUDIES	• • • •
A1.7	LEVEL 1 SITE PLAN	• • • •
A1.8	LEVEL 2 PLAN	• • • •
A1.9	LEVEL 3 PLAN	• • • •
A1.10	LEVEL 4 PLAN	• • • •
A1.11	LEVEL 5 PLAN	• • • •
A1.12	LEVEL 6 PLAN	• • • •
A1.13	ROOF PLAN	• • • •
A1.14	EXTERIOR ELEVATIONS	• • • •
A1.15	EXTERIOR ELEVATIONS	• • • •
A1.16	EXTERIOR ELEVATIONS	• • • •
A1.17	EXTERIOR ELEVATIONS	• • • •
A1.18	EXTERIOR DETAILS	• • • •

LAND USE APPLICATION 2/12/2016

ORIGINAL ISSUE:
REVISIONS:
No. Description Date

215533
PROJECT NUMBER
ESG PROJECT NUMBER
ESG CHECKED BY
KEY PLAN

112, 116, 120 East Hennepin Avenue

TITLE SHEET
T1.1



direct sweden graham architects
500 WASHINGTON AVENUE SOUTH
MINNEAPOLIS MINNESOTA 55415
P: 612.339.5508
F: 612.339.5382
www.esgarch.com

I hereby certify that this document was prepared by me or under my direct supervision and that I am a duly licensed architect under the laws of the State of Minnesota.
Signature _____
Type or Printed Name _____
Date: 2/12/2016

NOT FOR CONSTRUCTION

LAND USE APPLICATION		
2/12/2016		
ORIGINAL ISSUE		
REVISIONS		
No.	Description Date	
215533	PROJECT NUMBER	
ESG	ESG	
DATE	CHECKED BY	
KEY PLAN		
112, 116, 120 East Hennepin Avenue		
SURVEY		
V1.0		

ADVANCE SURVEYING & ENGINEERING CO.
5305 S. Hwy. No. 101
Minnetonka, MN 55345 Phone (952) 474-7964 WWW.ADVSR.COM

SURVEY FOR: Jacob Brothers Hennepin, LLC

ADDRESS OF PROPERTY:
116 Hennepin Ave East, Minneapolis, MN
REVISED: December 11, 2014 to show changes requested by client.
REVISED: February 25, 2015 to add as-built invert elevations.

LEGAL DESCRIPTION:
Parcel 1:
Lot 1, and that part of Lots 2, 3, 4 and 5 lying Southwesterly of a line drawn parallel to the Northerly line of said lots from a point on the Northwesterly line of Lot 2 distant 25 feet Southwesterly of the most Northerly corner of Lot 2, as measured along the Northwesterly line of Lot 2, All in Block 7, Subdivision of Grounds Between Pine, Bay, Main and Second Streets in St. Anthony Falls, (which plat was resurveyed by R. & F. Cook Dec. 1857), Hennepin County, Minnesota.

Torrans Property
Torrans Certificate No. 1158152
Parcel 2:
Lot 6, Block 7, Subdivision of Grounds between Pine, Bay, Main and Second Streets in St. Anthony Falls (which plat was resurveyed by R. & F. Cook Dec. 1857), Hennepin County, Minnesota.

Torrans Property
Torrans Certificate No. 1158152
Parcel 3:
That part of the Northerly 25.65 feet of Lot 22, measured at right angles to the Northerly line of said lot, lying Northwesterly of the Southerly 81.61 feet thereof, Block 7, Subdivision of Grounds Between Pine, Bay, Main and Second Streets in St. Anthony Falls (which plat was resurveyed by R. & F. Cook Dec. 1857), Hennepin County, Minnesota.

Torrans Property
Torrans Certificate No. 1158152
Parcel 4:
That part of Lots 2, 3, 4, 5, Block 7, Subdivision of grounds between Pine, Bay, and Second Street, as resurveyed by R. & F. Cook (December 1857) described as follows: Beginning at a point on the Southerly side of Central Avenue where said point is intersected by the line dividing Lot 2 from Lot 22, Block 7, in said Subdivision, thence Southwesterly 25 feet along line dividing Central Avenue from said Lot 2; thence Southwesterly parallel with the Westerly line of said Lot 22, in said Block to a line dividing Lot 5 from Lot 6; thence Northwesterly along said dividing line to the Southwesterly corner of said Lot 5; thence Northwesterly along the Westerly line of said Lot 22 to the place of beginning. Hennepin County, Minnesota.

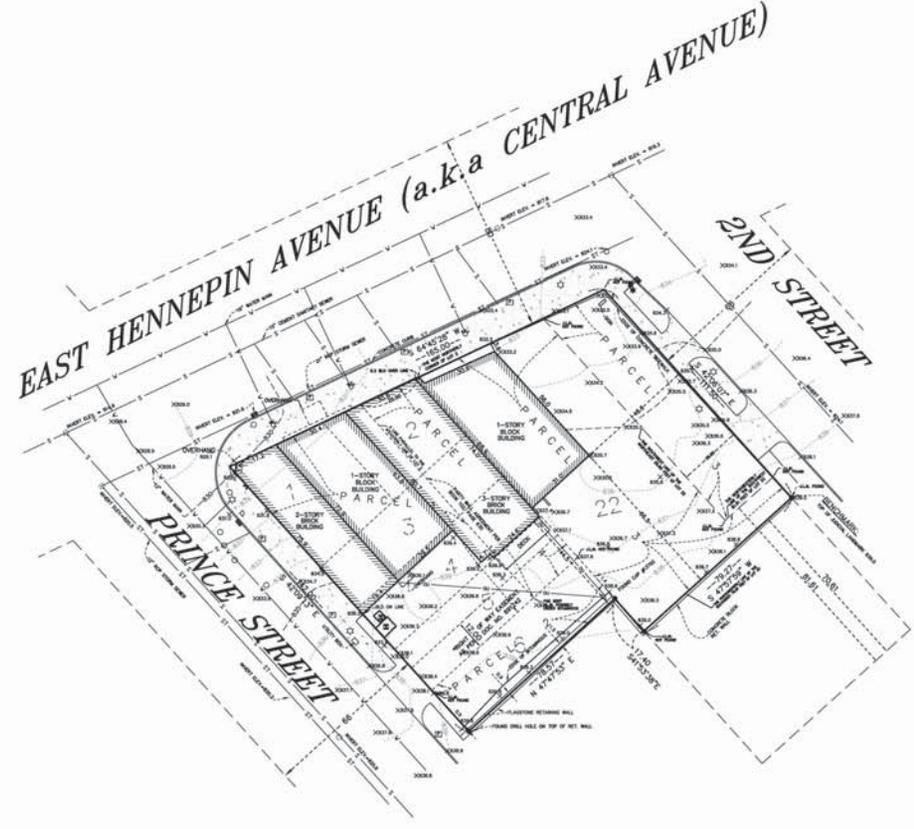
Abstract Property
NOTES & LIMITATIONS:
1. The subject property resides in Zone X of FEMA Flood Panel Number 27059C0557E, dated September 2, 2004.
2. The subject property contains 20,091 sq. ft.
3. There is no observable evidence of earth moving work, building construction or building additions, per a field inspection done on November 11, 2014.
4. There is no observable evidence that the site is being used as a solid waste dump, sump or sanitary landfill per a field inspection done on November 11, 2014.
5. There are no wetlands on site per visible inspection done on November 11, 2014. Although we are not wetland specialists, because the entire site is hard surfaces we surmise there are no wetlands on the site. If this is concern to you a wetland specialist could be hired to agree or disagree with our opinion.
6. The subject property resides in Zone CIA/Community Activity Center District, PO/Pedestrian Oriented Overlay District, and MR/Minneapolis River Critical Area Overlay District.
7. While we show utility information and invert elevations on our survey this information has NOT been field verified. This information is taken from As-Built plans supplied to us from the City of Minneapolis. Before relying on any utility information shown on this survey it must be field verified.
For the Zoning Letter dated December 9, 2014 from John Swooley at the City of Minneapolis, the current use is "bar and restaurant and second floor housing" and the proposed use is "mixed use - multi-family and commercial." "One to four dwelling units, as part of a mixed use building" is a permitted use in the CIA/Community Activity Center District, PO/Pedestrian Oriented Overlay District, and MR/Minneapolis River Critical Area Overlay District. Multiple family dwellings of three or more units are also permitted. Please note that this property lies within the St. Anthony Falls Historic District designated by the city, state, and federal governments.
Note there were no building setbacks defined in said Zoning Letter and therefore none are shown on the survey.

STANDARD SYMBOLS & CONVENTIONS:
● 1" ID pipe with plastic plug bearing State License Number 9235, set, unless otherwise noted.
CERTIFICATION:
For: Jacob Brothers Hennepin, LLC, SR Development, LLC, Schaefer Richardson, Inc. and Commercial Partners Title, LLC as lending agent for Old Republic National Title Insurance Company.
This is to certify that this map or plat and the survey on which it is based were made in accordance with the 2011 Minimum Standard Detail Requirements for ALTA/ACSM Land Title Surveys, jointly established and adopted by ALTA and NSPS, and includes items 1, 2, 3, 4, 5, 6(b), 7a, 8, 11(a), 11(b), 14, 17, 18 and 19 of Table A thereof. The field work was completed on: November 11, 2014.
December 16, 2014, *James H. P. O'Keefe*
James H. P. O'Keefe F.E. & P.S. No. 9235

LEGEND
+ SIGN
○ LIGHT POLE
○ POWER POLE
● TRAFFIC LIGHT
■ WATERMETER AND SERVICE
— SANITARY SEWER AND SERVICE
— STORM SEWER
— GAS MAIN AND SERVICE
○ MANHOLE NOT FIELD LOCATED
M WATER METER VALVE
— OVERHEAD POWER
— OVERHEAD UTILITY LINES
■ CATCH BASIN
○ PARKING METER
○ MANHOLE
○ TELEPHONE PEDESTAL
■ ELECTRIC CONTROL BOX
■ GAS METER
○ SINKHOLE WATER VALVE
○ FIRE HYDRANT
■ CONCRETE SURFACE



NOTES ON COMMITMENT NUMBER 9254, SCHEDULE B- PART II EXCEPTIONS:
Item 11 - Has been altered.
Item 13 - Easement for cable/telecommunications - This easement appears to cover the entire site.
Item 14 - Easement for Alley Purposes. This easement, shown on the survey, related only so long as the mortgage recorded in Book 3284 of Mortgages, Page 80, remained unsatisfied of record. While the mortgage has now been satisfied of record, the easement appears as a record on the Certificate of Title for Parcel 1.
Item 15 - Easement for adjoining encroachments from Lot 9 to maintain their present location. A document number or specific location has not been supplied to us and therefore not shown on the survey.
Item 17 - Boundary marked by Judicial Landmarks - These landmarks were not found.
Item 18 - Boundary marked by Judicial Landmarks - One of these monuments were found and shown on the survey.



Drw. No. 140897 TB REV 2-25-15

HENNEPIN AVENUE (a.k.a. CENTRAL AVENUE)



- MINNEAPOLIS STANDARD EROSION CONTROL NOTES:**
 City of Minneapolis Standard Erosion Control Notes
 MINNEAPOLIS STANDARD EROSION CONTROL NOTES (April 1, 2010)
- Contractor must call for a pre-construction meeting 48 hours prior to any land disturbances. Call 612-673-3887. Failure to do so may result in fines, the revocation of permits and a stop work order being issued.
 - Install perimeter erosion control at the locations shown on the plans prior to the commencement of any land disturbance or construction activities.
 - Before beginning construction, install a temporary rock construction entrance at each point where vehicles exit the construction site. Use 2 inch or greater diameter rock in a layer of at least 6 inches thick across the entire width of the entrance. Extend the rock entrance of least 50 feet into the construction zone using a geotextile fabric beneath the aggregate to prevent migration of soil into the rock from below.
 - Remove silt and sediments tracked or otherwise deposited onto public and private pavement areas. Removal shall be on a daily basis when tracking occurs and may be ordered by Minneapolis inspectors at any time if construction activity is ongoing. Sweeping shall be maintained throughout the duration of the construction and done in a manner to prevent dust being blown to adjacent properties.
 - Install inlet protection at all public and private catch basin inlets, which receive runoff from the disturbed areas. Catch basin inlets or other approved product are required in undisturbed areas that may receive runoff from the project area. Hay bales or filter fabric wrapped grates are not allowed for inlet protection.
 - Locate soil or dirt stockpiles no less than 25 feet from any public or private roadway or drainage channel. If remaining for more than seven days, stabilize the stockpiles by mulching, vegetative cover, tarps, or other means. Control erosion from oil stockpiles by placing silt barriers around the piles. Temporary stockpiles located on paved surfaces must be no less than two feet from the drainage/gutter line and shall be covered if left on site more than 24 hours.
 - Maintain all temporary erosion and sediment control devices in place until the contributing drainage area has been stabilized, inspect temporary erosion and sediment control devices on a daily basis and replace deteriorated, damaged, or tilted erosion control devices immediately.
 - Temporarily or permanently stabilize all construction areas which have undergone final grading, and all areas in which grading or site building construction operations are not actively underway against erosion due to rain, wind and running water within 7-14 days. Use seed and mulch, erosion control matting, and/or sodding and staking in green space areas. An early application of gravel base on areas to be paved recommended minimizing erosion potential.
 - Remove all temporary synthetic, structural, non-biodegradable erosion and sediment control devices after the site has undergone final stabilization with permanent rock in vegetation establishment. Final stabilization for purposes of this removal is 70% established cover over disturbed area.
 - Ready mixed concrete and concrete batch plants are prohibited within the public right of way. All concrete related production, cleaning and mixing activities shall be done in the designated concrete mixing/curb locations as shown in the erosion control plan. Under no circumstance may wastewater drain onto the public right of way or into any public or private storm water conveyance.
 - Changes to approved erosion control plan must be approved by the erosion control inspector prior to implementation. Contractor to provide installation and details for all proposed alternate type devices.

NYE'S
 Minneapolis, MN

esg
 eness swanson graham architects
 500 washington avenue south
 minneapolis minnesota 55408
 p 612.339.5108
 f 612.339.5382
 www.esgarch.com

I hereby certify that this document was prepared by me or under my direct supervision and that I am a duly licensed professional engineer under the laws of the State of Minnesota.

ABANDON AND REMOVE EXISTING SEWER AND WATER SERVICES TO THE SITE AT THE MAIN PER CITY STANDARDS.

REMOVE EXISTING ASPHALT PARKING AND PERIMETER CONCRETE CURB AND GUTTER AND EXISTING SITE SIDEWALKS.

LIMITS OF PROJECT DISTURBANCE 34,981 sq. ft.

TEMPORARY ROCK CONSTRUCTION ENTRANCE

REMOVE EXISTING SITE RETAINING WALL

SECURE TEMPORARY EXISTING CONSTRUCTION FROM ADJACENT PROPERTY OWNER FOR OFF-SITE GRADING OPERATIONS

SECURE TEMPORARY EXISTING CONSTRUCTION FROM ADJACENT PROPERTY OWNER FOR OFF-SITE GRADING OPERATIONS

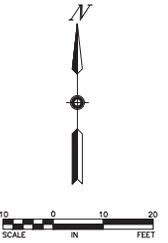
SITE CLEARING:

- Perform all clearing and grubbing work in accordance with the provisions of MNDOT Standard Specification Section 2101, and the additional requirements contained herein.
- Clearing is defined as the complete removal and disposal of all portions of natural and artificial objectionable materials, structures, trees, shrubs, bushes, windfalls, grass, sod, and other vegetation in the designated areas that exist above ground except stumps. Grubbing is defined as the excavation, removal, and disposal of all portions of natural and artificial objectionable materials, structures, trees, shrubs, bushes, windfalls, and other vegetation that exist below ground including stumps.
- Clear and grub the construction area in advance of the grading operation.
- Building Areas:** Completely remove all stumps, roots 40 mm (1.5 inches) in diameter or larger, buried logs, and all other objectionable material occurring within the lines of the new building and to horizontal distance of 4.5 m (15 feet) beyond the building walls.
- Other Areas:** Grub all stumps, roots 40 mm (1.5 inches) in diameter or larger, buried logs, and all other objectionable material occurring within the grading limits to a depth of not less than 1 m (3.28 feet) below the existing ground surface or subsurface excavation, whichever is deeper.
- Backfill and compact all depressions resulting from the clearing and grubbing operation with suitable material in order to make the surface conform to the original adjacent surface of the ground.
- After the site has been cleared and prior to any cutting or filling operations, strip all topsoil and organic soils from areas to be built upon, paved, or where grades are to be changed more than 152 mm (6 inches) above the existing topsoil to whatever depths encountered. Prevent intermingling with underlying subsurface or other objectionable material. Remove heavy growth of grass from areas before striping, where trees are to be left standing, strip topsoil striping a sufficient distance away from the trees in order to prevent damage to the root system.
- Stockpile soil to be re-used in an area clear of the new construction. Remove excess soil from the site.
- Construct stockpiles in a manner that will freely drain surface water. Maintain soil stockpiles free from debris and trash. Do not obstruct site drainage. Do not exceed a stockpile depth of 8 feet.
- Keep the soil stockpile damp in order to prevent drying and dust.

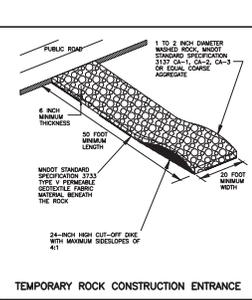
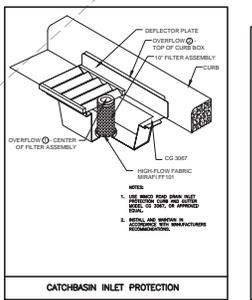
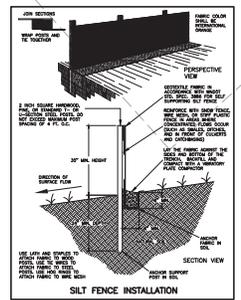
EROSION CONTROL SCHEDULE

ITEM	INSTALLATION	INSPECTION/MAINTENANCE	REMOVAL
SILENCE	PRIOR TO ANY CONSTRUCTION	INSPECT AND MAINTAIN AFTER EACH RAINY EVENT REMOVE SEDIMENTS AS REQUIRED	AFTER TRIBUTARY DRAINAGE AREA HAS BEEN RESTORED
TEMP. ROCK ENTRANCE	PRIOR TO INITIAL GRADING	INSPECT REGULARLY MAINTAIN AS REQUIRED	WHEN SITE PAVING OPERATIONS BEGIN
INLET PROTECTION	PRIOR TO ANY CONSTRUCTION OF SAME DAY STRUCTURE IS CONSTRUCTED	INSPECT AND MAINTAIN AFTER EACH RAINY EVENT REMOVE SEDIMENTS AS REQUIRED	WHEN TRIBUTARY AREA IS PAVED
SEED AND MULCH	AFTER FINAL GRADING	INSPECT AND MAINTAIN AFTER EACH RAINY EVENT RESTORE WASH-OUT AREAS IMMEDIATELY	N/A

INLET PROTECTION SHALL BE THE "SACK" TYPE AND SHALL BE INSTALLED IN SEQUENCE ON ALL EXISTING AND PROPOSED CURB MAINS EXPOSED TO CONSTRUCTION SEDIMENT.



KEYNOTES:
 ① - CATCH BASIN INLET PROTECTION (ACF SILT SACK, OR CITY OF MINNEAPOLIS APPROVED EQUIV.). SEE DETAIL.



LAND USE APPLICATION
 2/12/16

ORIGINAL ISSUE
 REVISIONS

No.	Description	Date

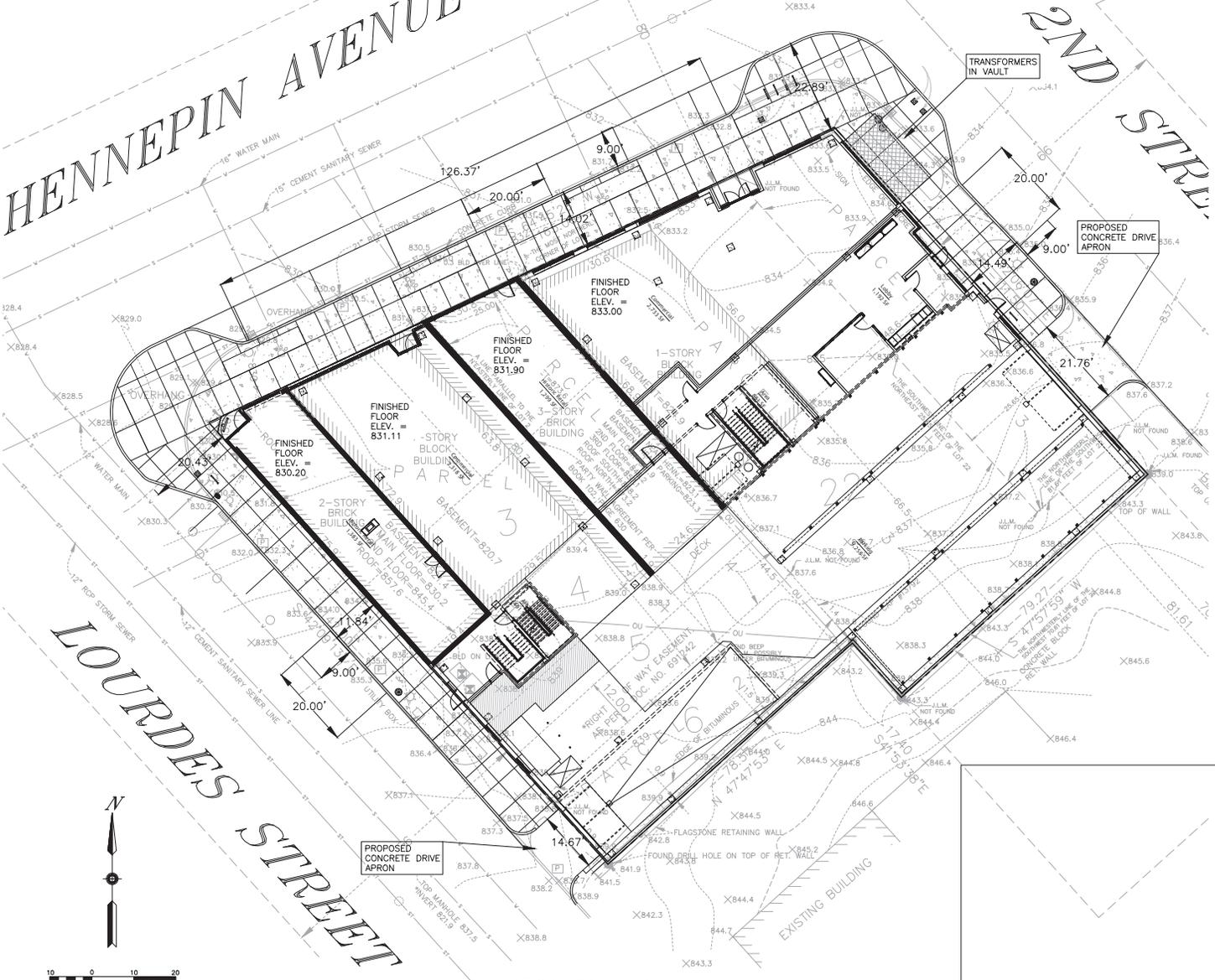
PROJECT NUMBER:
 DRAWN BY: MK
 CHECKED BY: NYE'S

DEMOLITION AND EROSION CONTROL PLAN
C1.0

HENNEPIN AVENUE (a.s.)

2ND STREET

LOURDES STREET



- SITE NOTES:**
- Existing boundary, location, topographic, and utility information shown on this plan is from a field survey by Advance Surveying.
 - Detachable warnings are required on all public and private ramps. Place the detachable warnings at the back of curbs. The ADA required truncated dome areas shall be 24 inches minimum in the direction of travel and shall extend the full width of the curb runs. Use the following specifications for "detachable warnings": All detachable warning surface installations shall be of minimum of least six (6) inches on the surrounding surfaces. Detachable warnings are to consist of truncated domes with a diameter of nominal 0.5" at a height of nominal 0.2" and a center-to-center spacing of nominal 2.5". The truncated dome areas shall extend slightly with the adjacent walking surface. Use light gray when the adjacent sidewalk is a light gray cement color. Use light gray when the adjacent sidewalk is a dark color. Install truncated domes and all related surfaces according to the manufacturer's specifications, install Ameri-Tile (www.ameritile.com) Cast in Place Truncated Dome Detachable Warning Surface Tile, or approved equal. Refer to ADA sections 4.7.7 and 4.3.2 for additional information.
 - All materials required for this work shall be new material conforming to the requirements for class, mix, grade, size, quality, and other details specified herein or as shown on the Plans. Do not use recycled or salvaged aggregate, asphaltic pavement, crushed concrete, or scrap alloys. Unless otherwise indicated, the Contractor shall furnish all material materials.
 - All dimensions are to face of curb (where applicable), edge of pavement, or exterior face of building, unless otherwise indicated.
 - All curb radii shall be three (3) feet minimum unless otherwise noted.
 - Install and make operational all brigtation before commencing with landscaping.
 - While surface markings (letters and symbols) shall be in conformance with the Standard Alphabet for Highway Signs and Pavement Markings, FHWA (M-20-20).
 - Install and maintain access routes throughout all stages of construction. Temporary access roads must be approved by the Fire Department before construction starts.
 - Fire extinguishers must be on-site and available throughout the construction site during all stages of construction.
 - Smoking is prohibited at the construction site except for approved areas designated by the Fire Department. "NO SMOKING" signs must be provided by the Contractor.
 - All cutting and welding must meet the requirements of Article 49 of the Uniform Fire Code.
 - Storage and handling of flammable liquids shall meet the requirements of Article 79 of the Uniform Fire Code.
 - Do not block access to building, fire hydrants, or other fire appliances with construction materials.
 - Provide fire lane signage as required by the Fire Department.
 - Design and maintain fire hydrant access route throughout construction to support the increased loads of the hydrants in weather driving conditions. Minimum 7-foot road design required. Pursuant to 1997 Uniform Fire Code Section 902.2.2.
 - 8x12 CONCRETE CURB AND GUTTER IS PROPOSED FOR ALL PRIVATE PROPERTY.
 - Provide traffic control devices and signage in accordance with the Minnesota Manual on Uniform Traffic Control Devices (MUTCD), retaining the same, including the Minnesota Manual on Uniform Traffic Control Devices, dated April 1995, the Minnesota Standard Signs Manual Parts 1, 2, 3 and 4 and the appropriate Manual Specifications, and MNDOT Standard Specification Section 1110. All signs must be retro-reflective.
 - Provide Advance and Construction Zone Signage including, but not limited to, signs for lane closures, slow advance, uneven lanes, and fresh of (other items as applicable); the number and location of these signs will be determined by the Contractor operations.
 - Portland cement concrete for curb and gutter shall be 3500 psi minimum 28 days compressive strength with 5.0% or entraining. Concrete aggregates shall be free of organic impurities, inert, sharp, or other deleterious substances.
 - Construct all private property concrete sidewalks in accordance with MNDOT Specification 5221. All concrete sidewalks shall be as indicated on the plans, but not less than 4" thick with 6 inch x 6 inch x 81/2" (10) wire bar woven wire mesh reinforcing.
 - Prevented expansion joints using 0.5" thickness shall be placed at each end of curb radius, at intersections, and approximately every 200 feet.
 - Contraction joints shall be spaced at 10 foot intervals in the curb and gutter.
 - For exterior concrete sites, unless otherwise indicated, provide separation joints at 30 foot intervals and at locations where the concrete surfaces or adjacent any existing fixed objects such as walls, walls, curbs, steps, drains, building foundations and other rigid structures. (2) Side exterior side into square panels of uniform size generally containing not more than 36 square feet of area.
 - Construct 2" layer at the free end of all concrete curb and gutter sections.
 - Construct all door threshold heights to within 0.05" of finished floor elevation.
 - Provide temporary steel signs and addresses during construction.

NYE'S

Minneapolis, MN

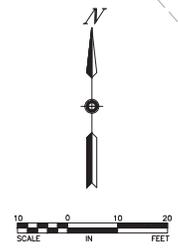
eness swenson graham architects
500 washington avenue south
minneapolis minnesota 55415
p 612.339.5108
f 612.339.5382
www.esgarchi.com

I hereby certify that this document was prepared by me or under my direct supervision and that I am a duly licensed professional engineer under the laws of the State of Minnesota.

Signature: _____
Title: Engineer
Typed or Printed Name: _____
40423
DATE: _____



CONSULTING CIVIL ENGINEERS
1000 NORTH AVENUE SOUTH
MINNEAPOLIS, MINNESOTA 55415
TEL: 612-339-5108
FAX: 612-339-5382
www.sunde.com



CONSTRUCT PUBLIC SIDEWALKS IN ACCORDANCE WITH CITY OF MPLS' SPECIFICATIONS FOR MONOLITHIC CONCRETE SIDEWALK*

No construction, demolition or commercial power maintenance equipment shall be operated within the city between the hours of 6:00pm and 7:00pm on weekdays or during any hours on Saturdays, Sundays and state and federal holidays, except under permit. Contact Environmental Services at 612-673-3867 for permit information.

- CITY OF MINNEAPOLIS - RIGHT OF WAY**
- Any elements of an earth retention system and related operations (such as construction crane boom swings) that fall within the Public right-of-way will require an encroachment permit application. If there are to be any earth retention systems which will extend outside the property line of the development, then a plan must be submitted showing details of the system. All such elements shall be removed from the Public right-of-way following construction with the exception of tie-backs which may remain but must be uncoupled and dewatered. Please contact Bob Bolet at (612) 673-2428 for further information.
 - Any elements of an earth retention system and related excavations that fall within the Public right-of-way will require a "Right-of-Way Excavation Permit".
 - Contact Dallas Hildebrand at (612) 673-5615 prior to construction for the temporary removal/relocation of any City of Minneapolis signal system that may be in the way of construction.
 - Contact Doug Mady at (612) 673-5755 prior to construction for the removal of any City of Minneapolis right-of-way signs that may be in the way of construction.
 - An obstruction permit is required anytime construction work is performed in the Public right-of-way. Contact Scott Kramer at (612) 673-2383 regarding details of sidewalk and lane closures. See <http://minneapolis.mn.gov/req/> for a permit.

LAND USE APPLICATION
2/12/16

ORIGINAL ISSUE		
REVISIONS		
No.	Description	Date

PROJECT NUMBER: _____
DRAWN BY: MK
CHECKED BY: _____
NYE'S

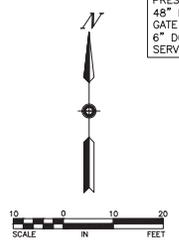
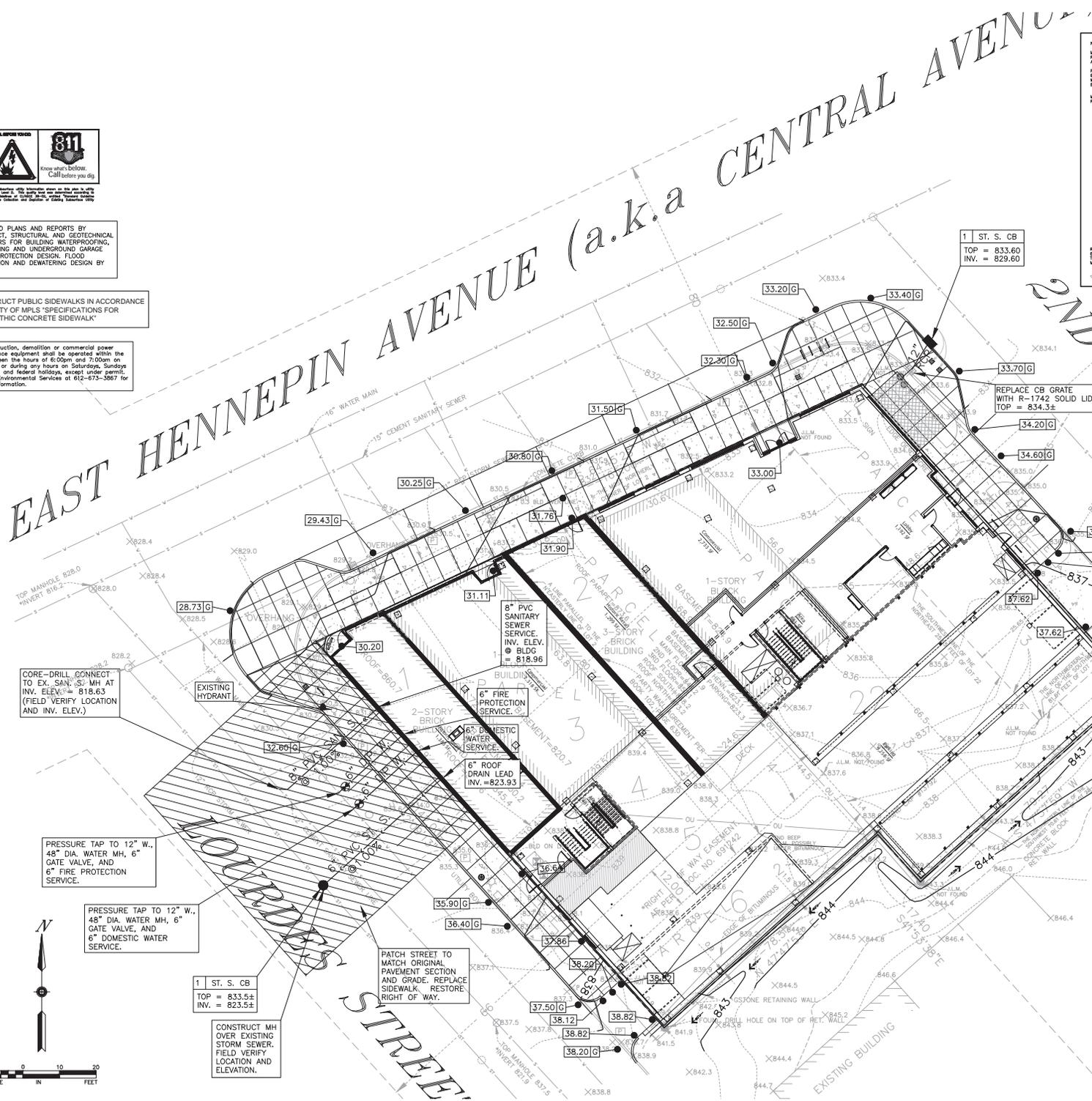
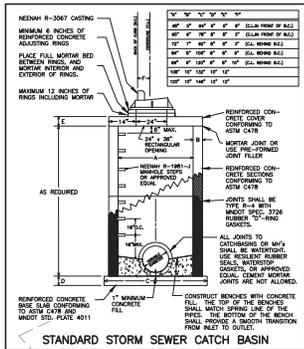
SITE PLAN
C2.0



REFER TO PLANS AND REPORTS BY ARCHITECT, STRUCTURAL AND GEOTECHNICAL ENGINEERS FOR BUILDING WATERPROOFING, DRAINAGE AND UNDERGROUND GARAGE FLOOD PROTECTION DESIGN, FLOOD PROTECTION AND DRAINAGE DESIGN BY OTHERS.

CONSTRUCT PUBLIC SIDEWALKS IN ACCORDANCE WITH CITY OF MPLS. SPECIFICATIONS FOR MONOLITHIC CONCRETE SIDEWALK.

No construction, demolition or commercial power maintenance equipment shall be operated within the city between the hours of 6:00am and 7:00pm on weekdays or during any hours on Saturdays, Sundays and state and federal holidays, except under permit. Contact Environmental Services at 612-673-3667 for permit information.



NYE'S
 Minneapolis, MN



erich swenson graham architects
 500 washington avenue south
 minneapolis minnesota 55415
 p 612.339.5508
 f 612.339.5382
 www.esgarch.com

I hereby certify that this document was prepared by me or under my direct supervision and that I am a duly licensed professional engineer under the laws of the State of Minnesota.

Signature
 Mike R. Sculler
 Typed or Printed Name
 0425
 Date



CONSULTING CIVIL ENGINEERS
 600 NORTH WABASH, SUITE 200
 MINNEAPOLIS, MINNESOTA 55401
 WWW.SUNDEENGINEERING.COM

LAND USE APPLICATION
 2/12/16

ORIGINAL ISSUE:
 REVISIONS
 No. Description Date

PROJECT NUMBER
 DRAWN BY MK
 CHECKED BY
 NYE'S

GRADING, DRAINAGE AND UTILITY PLAN
C3.0

SANITARY SEWER:

1. **Flow** - Minimum 40. ASTM D3859 Polypropylene (PP) Plastic Pipe for all installed PVC sanitary sewer joints. Joints for all sanitary sewer shall flow push-on joints with elastomeric gaskets. Use of solvent cement joints is allowed on building sewers. Solvent cement joints in PVC pipe must include use of a primer which is of contrasting color to the pipe and visible in accordance with Minnesota Rules, part 4715.0210, subpart 2. Pipe with solvent cement joints shall be 50% with PVC cement conforming to ASTM D2264. Lay all PVC pipe on a continuous granular bed. Installation must comply with ASTM D2231.
2. **Water Pressure** - Test all sanitary sewer lines in accordance with the Minnesota Rules part 4715.2020. Test of flexible sanitary sewer lines for deflection after the sewer line has been installed and backfill has been in place for at least 30 days. No pipe shall exceed a deflection of 5% if the test fails, make necessary repairs and retest.
3. **Install detectable underground marking tape** directly above all pvc, polyethylene, and other nonconductive underground utilities at a depth of 457 mm (18 inches) below finished grade, unless otherwise indicated. Bring the tape to the surface at various locations in order to provide connection points for locating underground utilities. Install the Blue Three Triflex Test Station, or approved equal, with blank caps at each location.
4. **The minimum depth of cover for sanitary sewer without insulation is 5 feet. Insulate sanitary sewer services at locations where the depth of cover is less than 5 feet. Provide a minimum insulation thickness of 2 inches. Insulation must be at least a half inch above and below the pipe. Install the insulation boards 6 inches above the top of the pipe on a non-combustible and sealed pipe bedding material. Use high density, closed cell, rigid board material equivalent to DOW Styrofoam 40-40 plastic foam insulation.**
5. **Slope pipe through manway walls.**

STORM DRAINAGE:

1. **Unless otherwise indicated, use reinforced, precast, concrete manhole boxes and catchbasins conforming to ASTM C478, furnished with wedge shape rubber gaskets and precast boxes, joints for all precast manhole box sections shall have gaskets, rubber "O" ring gaskets in accordance with ASTM C223. The inside bore diameter shall not be less than 48 inches.**
2. **All joints and connections in the storm sewer system shall be gasketed or water-tight in accordance with Minnesota Rules part 4715.0200. Approved resilient rubber joints or elastomer gaskets must be used in order to make watertight connections to manholes, catchbasins, and other structures. Cement mortar joints are not allowed.**
3. **Use mechanical joint resilient devices for joint restraint on all elastomer bands having a vertical or horizontal deflection of 25-1/2 degrees or greater, of valves, slides, water valves, hose crosses, pipe, all support valves, and all hangers in accordance with City requirements. Use "Series 1150 Mercury" manufactured by GSA Iron Inc., Eastport, Texas, or approved equal, installed in accordance with manufacturer's recommendations for restraint on ductile iron pipe.**
4. **Use mechanical joint resilient devices for joint restraint on all elastomer bands having a vertical or horizontal deflection of 25-1/2 degrees or greater, of valves, slides, water valves, hose crosses, pipe, all support valves, and all hangers in accordance with City requirements. Use "Series 1150 Mercury" manufactured by GSA Iron Inc., Eastport, Texas, or approved equal, installed in accordance with manufacturer's recommendations for restraint on ductile iron pipe.**
5. **Use a trench R-1733 form with Type "C" rigid grates, or approved equal, on CD #1.**
6. **Install detectable underground marking tape directly above all pvc, polyethylene, and other nonconductive underground utilities at a depth of 457 mm (18 inches) below finished grade, unless otherwise indicated. Bring the tape to the surface at various locations in order to provide connection points for locating underground utilities. Install the Blue Three Triflex Test Station, or approved equal, with blank caps at each location.**
7. **Slope pipe through manway walls.**

WATER DISTRIBUTION SYSTEM:

1. **Flow** - Minimum 40. ASTM D3859 Polypropylene (PP) Plastic Pipe for all installed PVC sanitary sewer joints. Joints for all sanitary sewer shall flow push-on joints with elastomeric gaskets. Use of solvent cement joints is allowed on building sewers. Solvent cement joints in PVC pipe must include use of a primer which is of contrasting color to the pipe and visible in accordance with Minnesota Rules, part 4715.0210, subpart 2. Pipe with solvent cement joints shall be 50% with PVC cement conforming to ASTM D2264. Lay all PVC pipe on a continuous granular bed. Installation must comply with ASTM D2231.
2. **Water Pressure** - Test all sanitary sewer lines in accordance with the Minnesota Rules part 4715.2020. Test of flexible sanitary sewer lines for deflection after the sewer line has been installed and backfill has been in place for at least 30 days. No pipe shall exceed a deflection of 5% if the test fails, make necessary repairs and retest.
3. **Install detectable underground marking tape** directly above all pvc, polyethylene, and other nonconductive underground utilities at a depth of 457 mm (18 inches) below finished grade, unless otherwise indicated. Bring the tape to the surface at various locations in order to provide connection points for locating underground utilities. Install the Blue Three Triflex Test Station, or approved equal, with blank caps at each location.
4. **The minimum depth of cover for sanitary sewer without insulation is 5 feet. Insulate sanitary sewer services at locations where the depth of cover is less than 5 feet. Provide a minimum insulation thickness of 2 inches. Insulation must be at least a half inch above and below the pipe. Install the insulation boards 6 inches above the top of the pipe on a non-combustible and sealed pipe bedding material. Use high density, closed cell, rigid board material equivalent to DOW Styrofoam 40-40 plastic foam insulation.**
5. **Slope pipe through manway walls.**

SITE GRADING:

1. **Visit the site.** Become familiar with the site and existing site conditions including available soil reports. Examine all local conditions of the site, including adjacent lots, streets, and other structures. Obtain all necessary permits and approvals from the City and other agencies. Obtain all necessary permits and approvals from the City and other agencies. Obtain all necessary permits and approvals from the City and other agencies.
2. **Unless otherwise noted, all proposed grades are finished grades. Finished grade at points between elevations or contours are determined by uniform slopes between the given grades. All proposed spot elevations shown on curves are to bottom of curb (gutter) unless otherwise indicated.**
3. **At locations where new work connects to existing work, field verify existing elevations and grades prior to beginning the new work. Match existing grades of construction limits.**
4. **Remove of unsuitable material (organic soils, uncontrolled fill, debris, and nature or artificial obstructions) in the zone from 1 m (3.28 feet) below the finished substrate to finished substrate in the proposed pavement areas.**
5. **Compact subgrade in all utility trenches to 98% Standard Proctor maximum dry density (ASTM D698-78 or AASHTO T-99) from the pipe zone to at least 1 m (3.28 feet) below the finished substrate, and 100% Standard Proctor maximum dry density in the final 1 m (3.28 feet). Provide 100% compaction in all utility trenches, including utility, and pavements. At least one compaction test is required for every 100 feet of trench of material placed in each zone.**
6. **Compact all fill placed in pavement areas in accordance with MDOT Standard Specification 2103.3/1 (Specified Density Method). Compact of fill placed under buildings in accordance with the recommendations of the Geotechnical Engineer.**
7. **Comply with the requirements of O.S.A. 29 CFR, Part 192, Subpart F, Excavations and Trenches." (www.osha-slc.gov)**
8. **Construct all proposed developments to meet or exceed 3:1 (horizontal to 1 vertical), unless otherwise indicated.**
9. **Provide positive drainage away from buildings at all times.**
10. **Test soil for building and pavement areas in the presence of the Geotechnical Engineer. Perform base preparation and test rolling prior to curb and gutter construction, division of gravel bases, and/or gravel sub-bases. Alternative stabilized base, or gravel mixed aggregate base on all street and pavement areas. Test roll the area between 300 mm (12 inches) inside the bulk of the curb or edge area of the paved areas. Use heavy hand-operated roller, tested by suitable laboratory equipment, in a wheel track not less than 1,200 mm (37 inches) apart (transverse wheel track), at a rate of 1800 ft/min (540 ft/min) to a pressure of 250 kPa (48 psi), and a gross mass of the roller of less than 13.5 metric tons (14.9 tons) and not more than 13.7 metric tons (15.1 tons). Test roll the above specified area in a manner such that each part of the test area is covered with one of the test cycles. Generate the heavy roller at a speed of not less than 4 m/h (2.5 mph) and not more than 8 m/h (5 mph). The subgrade shall be consolidated uniformly 7% of the time that the heavy roller passes over the subgrade, the surface shows settling or rutting of more than 10 mm (3/8 inch), measured from the original surface to the bottom of the rut. Correct any soft spots or displacements which appear during the test rolling by spreading, watering and rodding, and recompacting as required to obtain stability or by excavating to solid material and backfilling with material suitable for base construction. Remove material such as vegetation, burl, large stones, peat, and wet clay. Retest the area after correction.**
11. **Perform all construction procedures used construction in accordance with the soils report.**
12. **Coordinate inspection and approval of all subgrade within the building and pavement areas with the Geotechnical Engineer. Coordinate inspection and approval of all fill materials prior to placement within the building and pavement areas with the Geotechnical Engineer. Use only uncontrolled fill material.**
13. **Conduct all grading operations in a manner that minimizes the potential for site erosion.**
14. **Grade the site to the finished elevations shown on the plan. Import embankment material, or remove and dispose excess excavation material as required. Provide waste areas or disposal sites for excess material including, but not limited to, excavated material or broken concrete that is not desirable to be incorporated into the work involved on this project. Determination of material import and export quantities to be required by the Contractor and the cost of material import and export is incidental to the contract.**
15. **In areas where fill is placed on slopes steeper than 5:1, horizontally bench the slope in order to reduce the bond between the slope and the proposed embankment.**
16. **Structurally support exterior steps, stoops, and slides at each entry into the building on frost-free foundations bearing on footings at least 5 feet below final grade. Securely tie the foundation to the building. Foundations shall be constructed on or above the specified subsurface elevation. The completed top of finished grade shall be compacted, free from irregular surface changes, and free-graded not more than 16 mm (5/8 inch) above or below the specified finish grade.**
17. **Foundation** - The completed subgrade under slides and pavement areas shall be compacted, free from irregular surface changes, and free-graded not more than 16 mm (5/8 inch) above or below the specified subsurface elevation. The completed top of finished grade shall be compacted, free from irregular surface changes, and free-graded not more than 16 mm (5/8 inch) above or below the specified finish grade.
18. **Choose equipment and work procedures that will not disturb the subgrade soils. Route construction traffic away from foundation soils and areas of pavements and roads in order to minimize soil delamination. If the construction equipment causes rutting or soil pumping, then switch to other types of equipment or methods. The Contractor is solely responsible for the proper selection of construction equipment in order to avoid disturbing soils on the site.**
19. **It is typical to override spot elevations. Elevations shown as 86.2 or 83.1 are understood to mean 86.2 or 83.1 respectively.**

GENERAL:

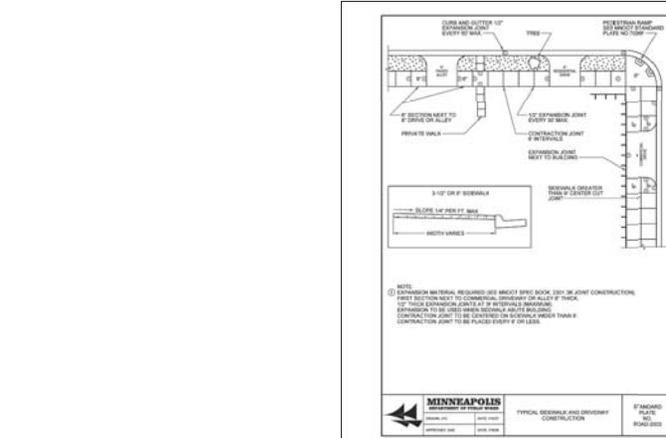
1. **Comply with the work safety procedure specified by the Occupational Safety and Health Administration (OSHA). Comply with all applicable local, state, and federal safety regulations. OSHA prohibits entry into "confined spaces," such as manholes and vaults (see 29 CFR Section 1910.146), unless undertaking certain specific procedures and precautions. Construction shall verify the responsibility of the Contractor before entering the manhole, methods, and sequencing of the construction operations.**
2. **Existing boundary, location, topographic, and utility information shown on this plan is from a field survey by Advanced Surveying.**
3. **Refer to the architectural plans for building and storm dimensions, site layout and dimensions, pavement sections and details, utilities, and other site features.**
4. **Perform all construction work in accordance with State and Local Requirements.**
5. **A licensed surveyor shall perform construction staking. The Contractor shall provide and be responsible for the staking. Verify all pipe and detail dimensions prior to construction staking. Stake the limits of excavations and cutting prior to excavation, maintenance holes, and catchbasin installation. Adjust utilities and maintenance hole locations in order to avoid conflicts with curb and gutter. Adjust catchbasin locations in order to avoid property encroachments.**
6. **Provide temporary fences, barricades, coverings, and other protections in order to preserve existing trees to remain, and to prevent injury to workers and the public.**
7. **Connect to existing sanitary sewer and storm sewer with a connecting. Use water tight gasket access in Type II or otherwise constructed. Use size of pipe that enclose water to the block. Best of City standards only. Provide watertight seal when penetrating a structure wall with a pipe. Use measurements before beginning construction to verify that service connections are correct. Use the following minimum requirements for pipe and pipe fittings:**
8. **Install and inspect all plumbing installations, including water and sewer services, must be tested in accordance with the requirements of the Minnesota Plumbing Code (Minnesota Rules Chapter 4715). Coordinate plumbing work may be covered prior to completing the required tests and inspections.**
9. **Perform back excavations for all utilities in accordance with the requirements of O.S.A. 29 CFR, Part 1926, Subpart P, Excavations and Trenches." (www.osha-slc.gov)**
10. **Coordinate building utility connection locations of 5 ft. out from the proposed building to the building's Plumbing Contractor to construction. Verify water and sewer service locations, sizes, and elevations with the Mechanical Engineer prior to construction.**
11. **Contact utility companies for locations of all public and private utilities within the work area prior to beginning construction. Contact COPEL STATE ONE (CAL) at (851) 454-0002 in the Minneapolis-St. Paul metro area, or 1-800-252-1166 elsewhere in Minnesota for location of existing utilities or at least 48 working hours (not including weekends and holidays) before beginning any construction in accordance with Minnesota Statute 216C.01. Obtain meter number and meet with representatives of the utility subject of the site. Provide the meter with the meter number information. Cooper State One Call is a free service that locates municipal and utility company lines, but does not locate private utility lines. Use an independent locator service or other means to locate or obtain locations of private utility lines including, but not limited to, underground electric cables, telephone, TV, and base application lines.**
12. **Where existing gas, electric, cable, or telephone utilities conflict with the work, coordinate the abandonment, relocation, offset, or support of the existing utilities with the appropriate local utility companies. Coordinate new gas meter and line installation, electric meter and electric service installation, cable service, and telephone service installation with the local utility companies.**
13. **Arrange for and secure adequate disposal areas off-site. Dispose of all excess soil, waste material, debris, and all materials not required for project. Remove material and waste material from, storage bins, drums, cans, and other containers, or other waste material from the construction site. Obtain the rights to any waste areas for disposal of materials or debris removed other than on or near the site. At work site, disposal of such material shall be contained in sealed containers. All disposal must conform to applicable state waste disposal permit requirements. Obtain all necessary permits at all cost to the OWNER.**
14. **All materials required for this work shall be new material conforming to the requirements for class, size, grade, size, quality, and other details specified herein or as shown on the Plans. Do not use recycled or salvaged aggregate, crushed pavement, crushed concrete, or scrap asphalt. Unless otherwise indicated, the Contractor shall furnish all materials and equipment required for this project.**
15. **Restore the public right-of-way of temporary construction entrance locations. Restore any concrete curb and gutter, stormwater treatment elements, or vegetation cover damaged by the construction activity. Restore damaged curb and gutter to original public right-of-way. The work area shown is general and not meant to be adjusted to the actual conditions of the site.**
16. **Use traffic control in accordance with local authorities and the Minnesota Manual on Uniform Traffic Control Devices (MUTCD), including the Field Manual for Temporary Traffic Control Zone Layouts, dated January 2001.**
17. **Provide and maintain temporary drainage throughout construction and the permanent drainage system and structures are in place and operational. Install temporary ditches, pipe, manholes, or other means as necessary in order to insure proper drainage of all times. Provide the points of building plots or roadways with proposed lines. Protect sub grades from damage by surface water runoff.**
18. **Full design strength is not available in stabilization pavement areas until the first fill is compacted into place. Protect pavement areas from overloading by delivery trucks, construction equipment, and other vehicles.**
19. **When sawing or drilling concrete or masonry, use saws that provide water to the block.**
20. **Adult of each station, use boxes, maintenance hole castings, catchbasin castings, cleanout covers, and similar devices to provide access to the manhole.**
21. **25 maximum slope in all directions in hand-dropped accessible parking areas.**
22. **Install all pipe with the ASTM identification numbers on the top for inspection. Commence pipe laying at the lowest point in the proposed sewer line. Use the pipe with the best end or receiving groove end at the pipe pointing upstream in the proposed line and grade below laying any pipe. Do not lay pipe in water or in the trench conditions are unsuitable for such work.**
23. **Obtain and pay for all permits, tests, inspections, etc. required by agencies that have jurisdiction over the project. Estimate and report work in accordance with all local and state codes, rules, ordinances, or regulations pertaining to the particular type of work involved.**
24. **Obtain permission from the City for work in the public right-of-way.**
25. **Construct sanitary sewer, waterlines, and storm sewer utilities in accordance with the City Engineer's Association of Minnesota Standard Specifications sections 2000, 2011, and 2061 issued 1999, or the latest revised edition.**
26. **The Contractor shall be responsible for the design and construction of the proposed retaining walls. A Minnesota Licensed Civil Engineer must design and construct the retaining walls and the Contractor shall be responsible for all costs associated with the retaining wall system design and construction, and shall include the costs of submitting detailed plans and specifications for the retaining wall system to the Owner for review.**
27. **Use City of Minneapolis standard castings on all public utilities.**

NYE'S
 Minneapolis, MN

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No. Description Date

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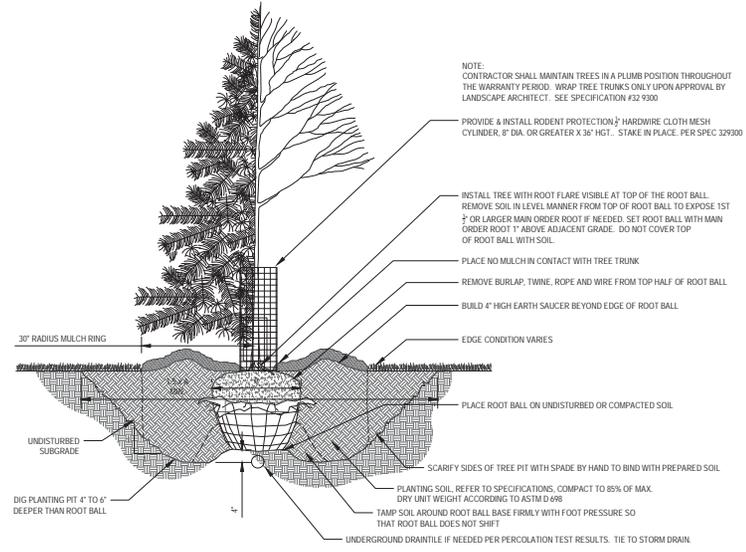
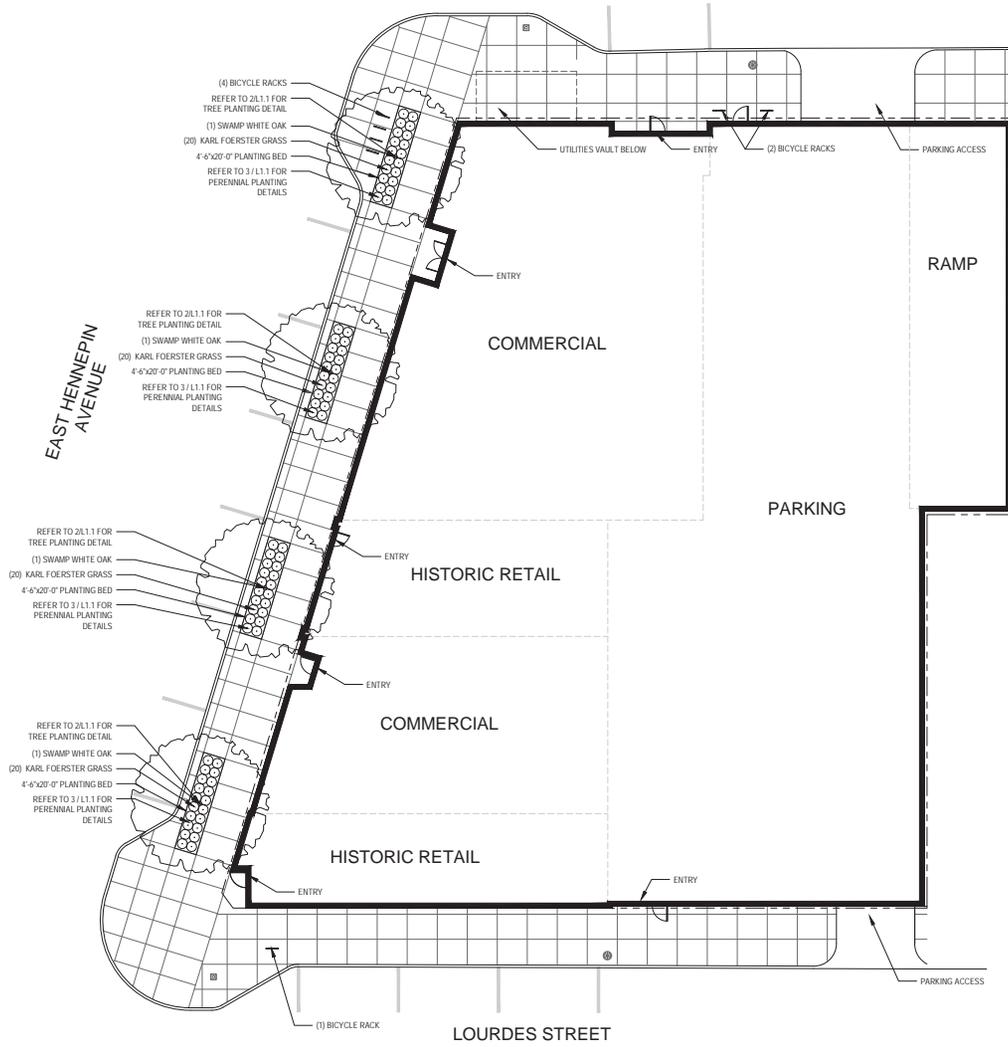
KEY PLAN

NYE'S

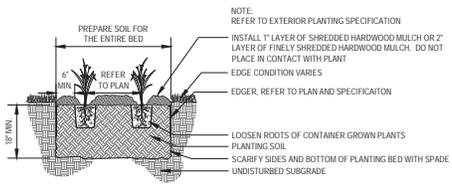
LANDSCAPE PLAN

L1.1

2ND STREET



2 TREE PLANTING DETAIL
L1.1 SCALE: 3/4" = 1'-0"



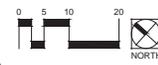
3 PERENNIAL PLANTING DETAIL
L1.1 SCALE: 3/4" = 1'-0"

1 LANDSCAPE PLAN
L1.1 SCALE: 1" = 10'-0"

PLANTING NOTES

1. LOCATE ALL UTILITIES.
2. STAKE PROPOSED PLANTING LOCATIONS PER PLAN FOR REVIEW AND APPROVAL BY LANDSCAPE ARCHITECT PRIOR TO INSTALL.
3. PLANT MATERIAL SHALL COMPLY WITH THE CURRENT EDITION OF THE AMERICAN STANDARD FOR NURSERY STOCK, AND 2011. UNLESS NOTED OTHERWISE, DECIDUOUS SHRUBS SHALL HAVE AT LEAST 4 CANES AT THE SPECIFIED HEIGHT. ORNAMENTAL TREES SHALL HAVE NO Y-CROTCHES AND SHALL BEGIN BRANCHING NO LOWER THAN 7 FEET ABOVE THE ROOT BALL. STREET AND BOULEVARD TREES SHALL BEGIN BRANCHING NO LOWER THAN 6 ABOVE PAVED SURFACE.
4. INSTALL PLANT MATERIALS ONCE FINAL GRADING AND CONSTRUCTION HAS BEEN COMPLETED IN THE IMMEDIATE AREA.
5. INSTALL PLANT MATERIALS PER PLANTING DETAILS.
6. SUBSTITUTION REQUESTS FOR PLANT MATERIAL, TYPE & SIZE SHALL BE SUBMITTED TO THE LANDSCAPE ARCHITECT FOR CONSIDERATION PRIOR TO BIDDING. ALL SUBSTITUTIONS AFTER BIDDING MUST BE APPROVED BY LANDSCAPE ARCHITECT AND ARE SUBJECT TO CONTRACT ADJUSTMENTS.
7. ADJUSTMENTS IN LOCATION OF PROPOSED PLANT MATERIALS MAY BE NEEDED IN FIELD. LANDSCAPE ARCHITECT MUST BE NOTIFIED PRIOR TO ADJUSTMENT OF PLANTS.
8. FERTILIZER PLANT MATERIAL UPON INSTALLATION WITH THE FOLLOWING INSTRUCTIONS: OTHER APPROVED FERTILIZER MIXED IN WITH THE PLANTING SOIL PER THE MANUFACTURER'S INSTRUCTIONS OR MAY BE TREATED FOR SUMMER AND FALL INSTALLATION WITH AN APPLICATION OF GRANULAR 10-0-5 OF 12 OZ. PER 2 1/2" CALIPER TREE AND 6 OZ. PER SHRUB WITH AN ADDITIONAL APPLICATION OF 10-10-10 THE FOLLOWING SPRING IN THE THREE SAUCER.

9. INSTALL 18" DEPTH OF PLANTING SOIL IN AREAS RECEIVING GROUND COVER, PERENNIALS, & ANNUALS. PLANTING SOIL SHALL CONSIST OF 80/20 3M7-C MODIFIED TO CONTAIN A MAXIMUM OF 30% SAND, A PH OF 7.1 MAX. OR AS OTHERWISE SPECIFIED IN THE PROJECT MANUAL.
10. TREE WRAPPING MATERIAL SHALL BE TWO-WALLED PLASTIC SHEETING APPLIED FROM TRUNK FLARE TO FIRST BRANCH. WRAP SMOOTH-BARKED DECIDUOUS TREES PLANTED IN THE FALL PRIOR TO DECEMBER 1 AND REMOVE WRAPPING AFTER MAY 1.
11. APPLY PRE-EMERGENT HERBICIDE (PRENOR OR APPROVED EQUAL) IN ANNUAL, PERENNIAL, AND SHRUB BEDS FOLLOWED BY SHREDDED HARDWOOD MULCH. REFER TO SPECIFICATIONS FOR ADDITIONAL INFORMATION REGARDING USE OF HERBICIDES.
12. INSTALL 2" DEEP SHREDDED HARDWOOD MULCH RINGS AT CONIFEROUS & DECIDUOUS TREES WITH NO MULCH IN DIRECT CONTACT WITH TREE TRUNK.
13. INSTALL 2" DEEP SHREDDED HARDWOOD MULCH RINGS AT SHRUB PLANTING AREAS WITH NO MULCH IN DIRECT CONTACT WITH SHRUB STEMS.
14. INSTALL 2" DEEP FINELY SHREDDED MULCH OR 1" DEEP SHREDDED HARDWOOD MULCH IN PERENNIAL PLANTING BEDS. REMOVE ALL MULCH FROM STEMS OF PERENNIALS. PLANT STEMS SHOULD NOT BE IN DIRECT CONTACT WITH MULCH.
15. WARRANTY NEW PLANT MATERIAL THROUGH ONE CALENDAR YEAR FROM THE DATE OF SUBSTANTIAL COMPLETION. NO PARTIAL ACCEPTANCE WILL BE CONSIDERED.
16. SPRING PLANT MATERIAL INSTALLATION IS FROM THE APRIL 15 TO JUNE 15.
17. FALL CONIFEROUS PLANTING IS ACCEPTABLE FROM AUGUST 21 TO SEPTEMBER 30.
18. FALL DECIDUOUS PLANTING IS ACCEPTABLE FROM AUGUST 15 UNTIL NOVEMBER 15.
19. ADJUSTMENTS TO PLANTING DATES MUST BE APPROVED IN WRITING BY THE LANDSCAPE ARCHITECT.
20. NO IRRIGATION SHALL BE PROVIDED.



KEY	QTY	BOTANICAL NAME	COMMON NAME	ROOT CONT.	SIZE
4		DECIDUOUS SHADE TREES			
4		QUERCUS BICOLOR	DM. SWAMP WHITE	BAR	22' CAL
10		PERENNIALS			
10		CAULANOPSIS & ACETROFLORA KARL FOERSTER	FEATHER REED GRASS, KARL FOERSTER	CONT.	10

PLANTING SCHEDULE - L1.1



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PEDESTRIAN VIEW LOOKING DOWN LOURDES PL



PEDESTRIAN VIEW AT E. HENNEPIN AVE AND LOURDES PL



PEDESTRIAN VIEW ALONG 2ND STREET



PEDESTRIAN VIEW ALONG E HENNEPIN AVE

LAND USE APPLICATION
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KEY PLAN

112, 116, 120 East
Hennepin Avenue

EXISTING CONTEXT SITE
IMAGES

A0.1



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PEDESTRIAN VIEW AT E. HENNEPIN AVE AND LOURDES PL



PEDESTRIAN VIEW ALONG 2ND STREET



PEDESTRIAN VIEW ALONG E HENNEPIN AVE

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KEY PLAN		
112, 116, 120 East Hennepin Avenue		
DESIGN PERSPECTIVE VIEWS		
A0.2		



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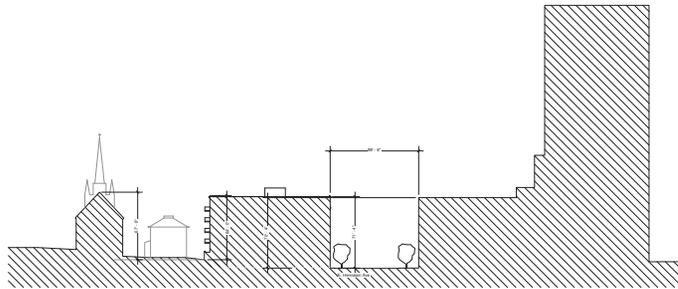
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Title Date _____



E. Hennepin Ave Site Section
Scale: 1" = 50'-0"

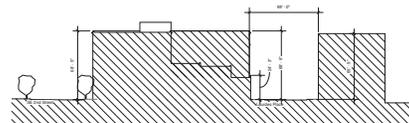


E. Hennepin Ave Site Section
Scale: 1" = 50'-0"

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Hennepin Ave Context Elevation
Scale: 1" = 50'-0"



Lourdes Place Site Section
Scale: 1" = 50'-0"

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KEY PLAN		
112, 116, 120 East Hennepin Avenue		
CONTEXTUAL ELEVATIONS		
A0.3		



ELEVATION VIEW ALONG HENNEPIN AVE



ELEVATION VIEW ALONG 2ND ST



ELEVATION VIEW ALONG LOURDES PL

112, 116, 120 East
Hennepin Avenue
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KEY PLAN

112, 116, 120 East
Hennepin Avenue

ELEVATIONS IN CONTEXT

A0.4



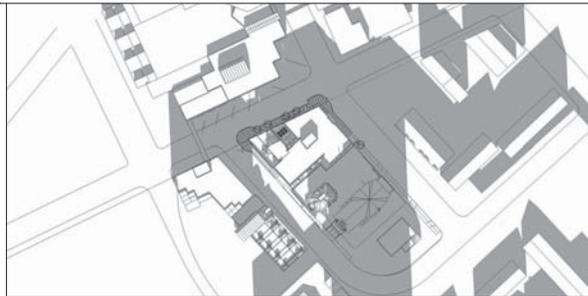
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MORNING - DECEMBER 21



NOON - DECEMBER 21



EVENING - DECEMBER 21



MORNING - MARCH/SEPTEMBER 21



NOON - MARCH/SEPTEMBER 21



EVENING - MARCH/SEPTEMBER 21



MORNING - JUNE 21



NOON - JUNE 21



EVENING - JUNE 21

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KEY PLAN

112, 116, 120 East
Hennepin Avenue

SHADOW STUDIES

A0.6

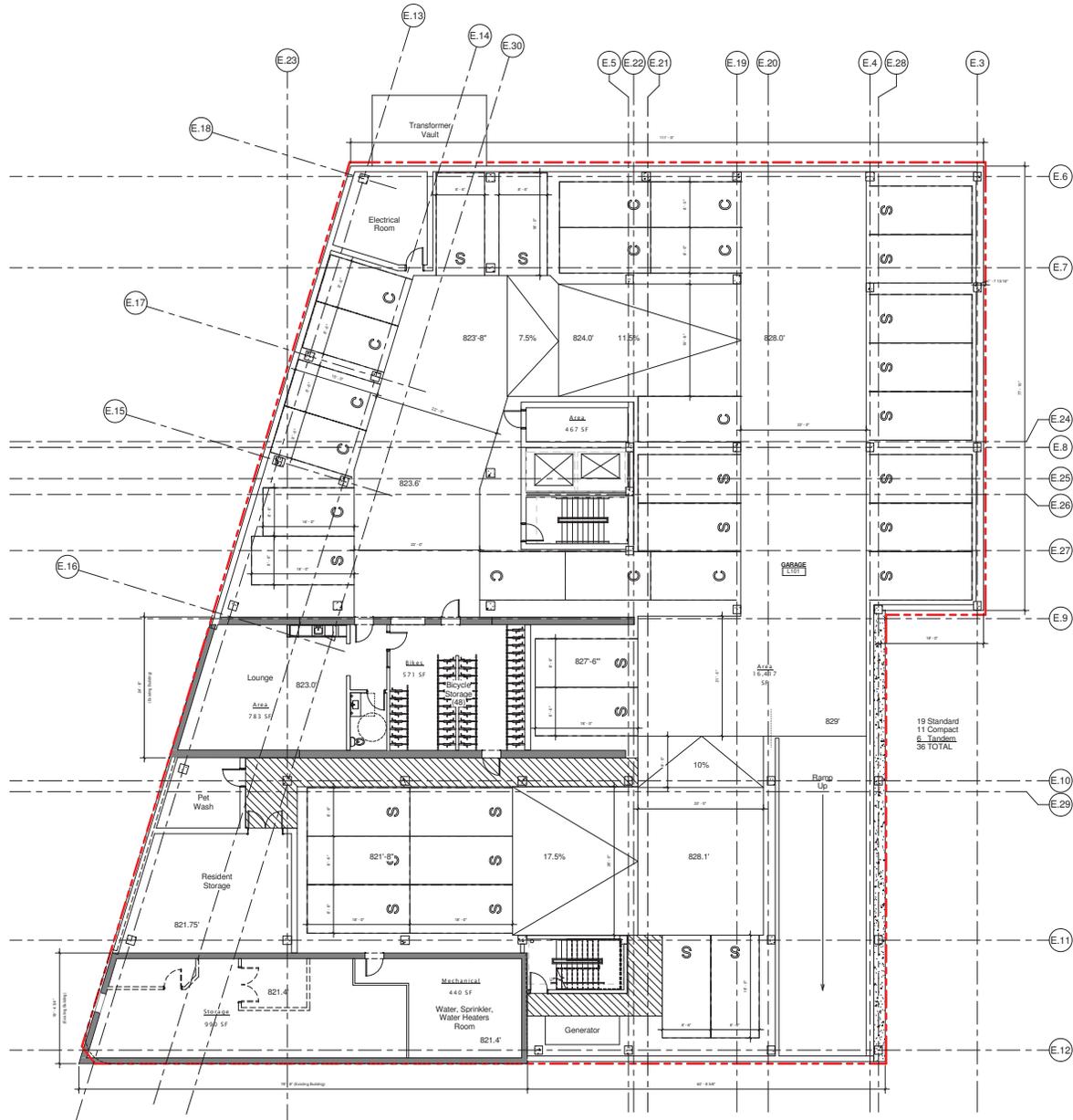


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1 LEVEL P1 PLAN
1/16" = 1'-0"



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No. Description Date

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KEY PLAN

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LEVEL P1 PLAN
A1.0

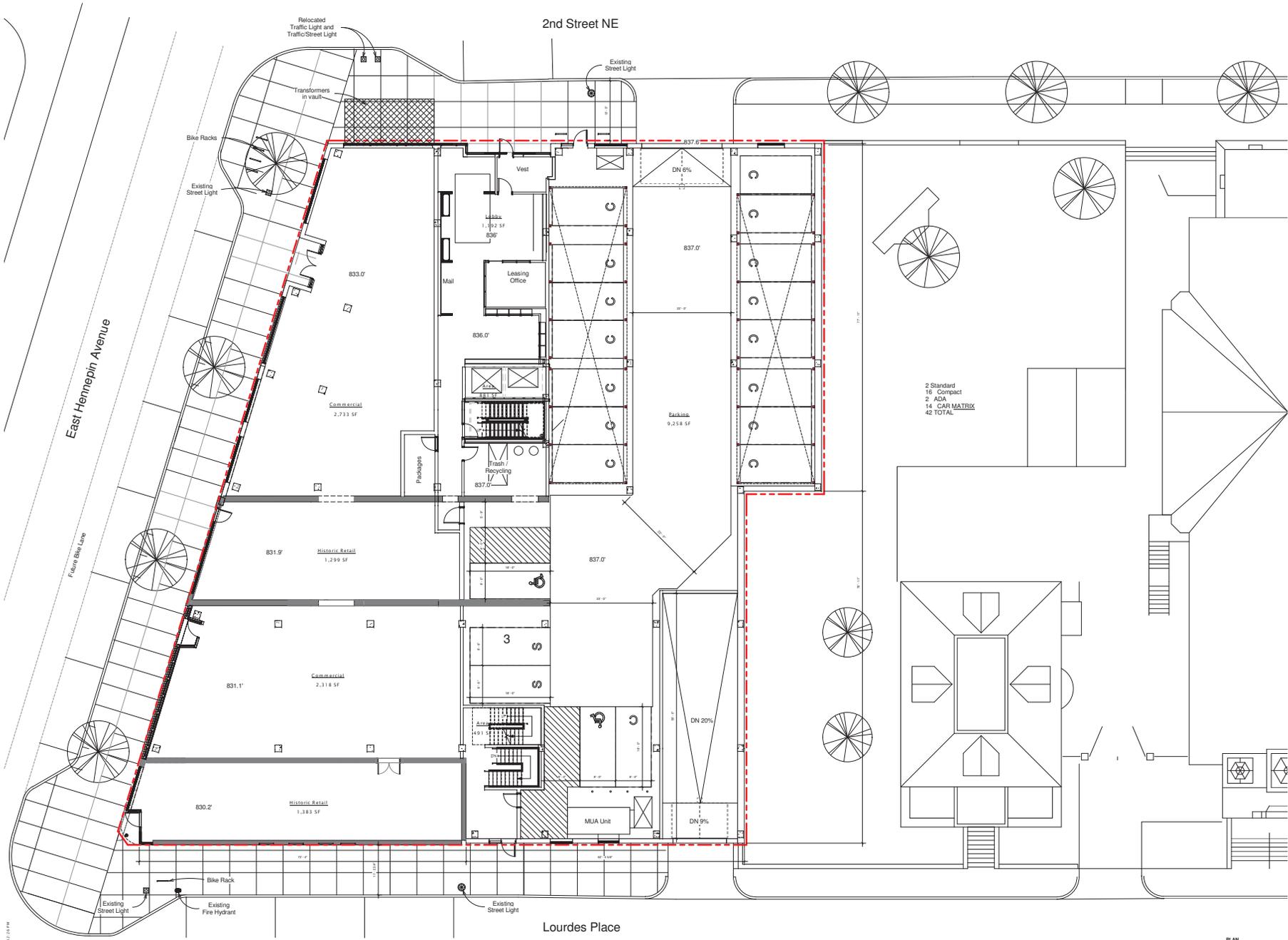


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1 SITE PLAN/LEVEL 1 PLAN
A1.1 1/8" = 1'-0"



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KEY PLAN

112, 116, 120 East Hennepin Avenue

LEVEL 1 / SITE PLAN
A1.1

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1 LEVEL 2 PLAN
A12 1/8" = 1'-0"



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KEY PLAN		
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LEVEL 2 PLAN		
A1.2		



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KEY PLAN

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Hennepin Avenue

LEVEL 3 PLAN
A1.3



1 LEVEL 3 PLAN
A1.3
1/8" = 1/4"





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1 LEVEL 4 PLAN
A1.4 1/8" = 1'-0"



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LEVEL 4 PLAN		
A1.4		



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1 LEVEL 5 PLAN
A1.5 1/8" = 1'-0"



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KEY PLAN

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LEVEL 5 PLAN
A1.5



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1 LEVEL 6 PLAN
A1.6
1/8" = 1'-0"



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KEY PLAN

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LEVEL 6 PLAN
A1.6

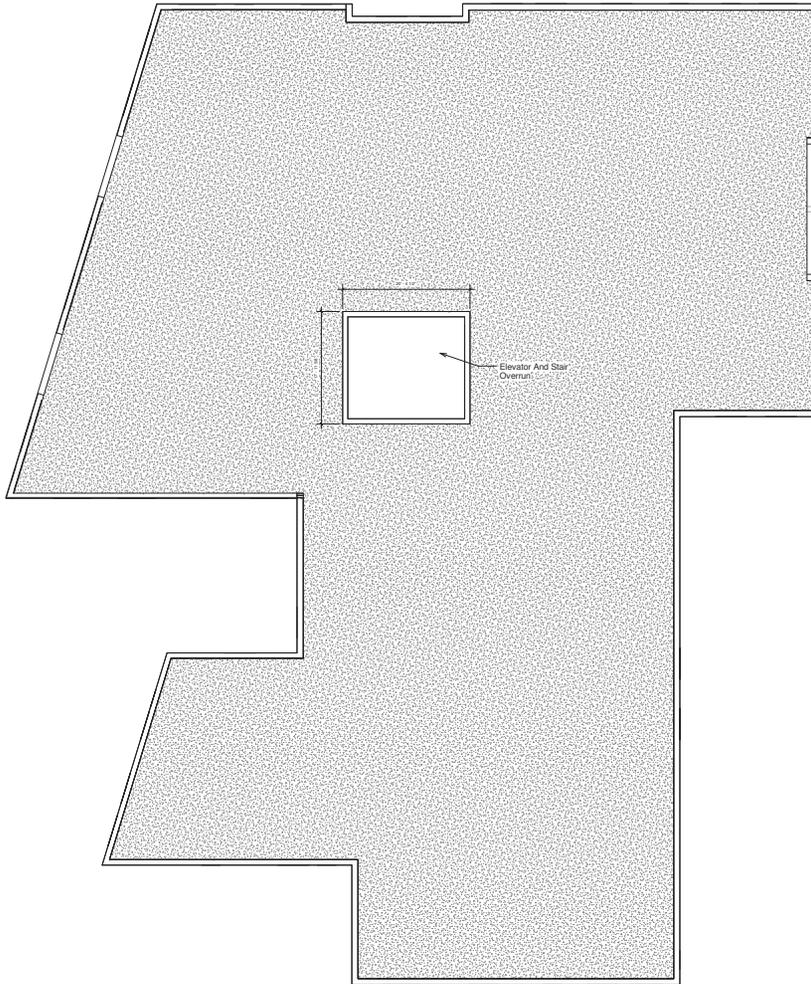


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1 BP-ROOF
A1.7 1/8" = 1'-0"

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KEY PLAN

112, 116, 120 East
Hennepin Avenue

ROOF PLAN
A1.7

EXTERIOR ELEVATION NOTES

- SIGNAGE SHOWN IS PRELIMINARY. FINAL SIGNAGE WILL COMPLY WITH CHAPTER 461 - ON PREMISES SIGNING
- SEE CUT SHEET FOR EXTERIOR LIGHT FIXTURES L1-L4
- SEE CUT SHEET FOR ANDERSEN 100 WINDOW AND DOORS
- SEE CUT SHEET FOR ANDERSEN EAGLE SERIES WINDOWS



1 NORTH ELEVATION
3/32" = 1'-0"



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2 NORTH ELEVATION
3/32" = 1'-0"

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KEY PLAN

112, 116, 120 East Hennepin Avenue

EXTERIOR ELEVATIONS

A3.1

EXTERIOR ELEVATION NOTES

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- SEE CUT SHEET FOR ANDERSEN 100 WINDOW AND DOORS.
- SEE CUT SHEET FOR ANDERSEN EAGLE SERIES WINDOWS.



1 WEST ELEVATION
3/22' = 1'-0"



2 WEST ELEVATION
3/22' = 1'-0"



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KEY PLAN

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EXTERIOR ELEVATIONS

A3.2

EXTERIOR ELEVATION NOTES

- SIGNAGE SHOWN IS PRELIMINARY. FINAL SIGNAGE WILL COMPLY WITH CHAPTER 541 - ON-PREMISES SIGNING
- SEE CUT SHEET FOR EXTERIOR LIGHT FIXTURES L1-L4
- SEE CUT SHEET FOR ANDERSEN 100 WINDOW AND DOORS
- SEE CUT SHEET FOR ANDERSEN EAGLE SERIES WINDOWS



1 EAST ELEVATION
3/52" = 1'-0"



2 EAST ELEVATION
3/52" = 1'-0"



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EXTERIOR ELEVATIONS

A3.3

EXTERIOR ELEVATION NOTES

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- SEE CUT SHEET FOR ANDERSEN 160 WINDOW AND DOORS
- SEE CUT SHEET FOR ANDERSEN EAGLE SERIES WINDOWS



1 SOUTH ELEVATION
3/22 = 1/4"



2 SOUTH ELEVATION
3/22 = 1/4"



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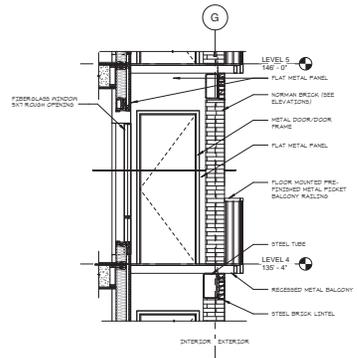
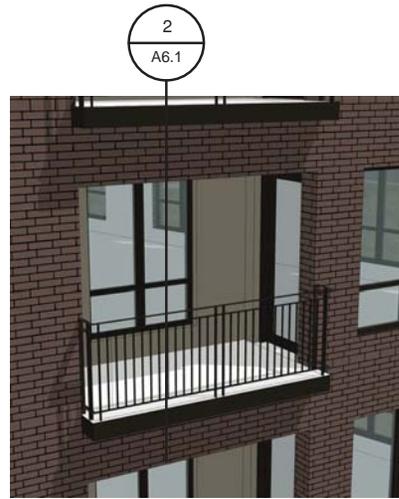
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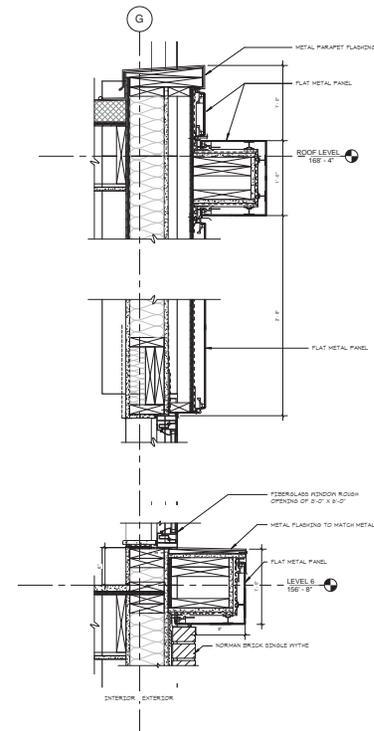
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2
A6.1
DETAIL @ RECESSED METAL BALCONY - TYPICAL
3/8" = 1'-0"



1
A6.1
DETAIL @ FIBERGLASS WINDOW AND PARAPET - TYPICAL
1/12" = 1'-0"

NOT FOR CONSTRUCTION

LAND USE APPLICATION
2/12/2016

ORIGINAL ISSUE:
REVISIONS:
No. Description Date

215533
PROJECT NUMBER
ESG ESC
DRAWN BY CHECKED BY
KEY PLAN

112, 116, 120 East Hennepin Avenue

EXTERIOR DETAILS
A6.1



Carmatrix

Soaring land prices and an appetite for more and more parking has changed the way the American real estate industry is viewing parking. Harding Steel has developed the Carmatrix puzzle system, a semi-automated mechanical parking system that is designed with the American consumer and American real estate developer in mind. These systems can be individually configured for each building, project, and parking requirement. The primary purpose of the Carmatrix is to provide high-density parking where users have the ability to park their OWN cars. Carmatrix virtually eliminates the need for attended or valet parking operators.

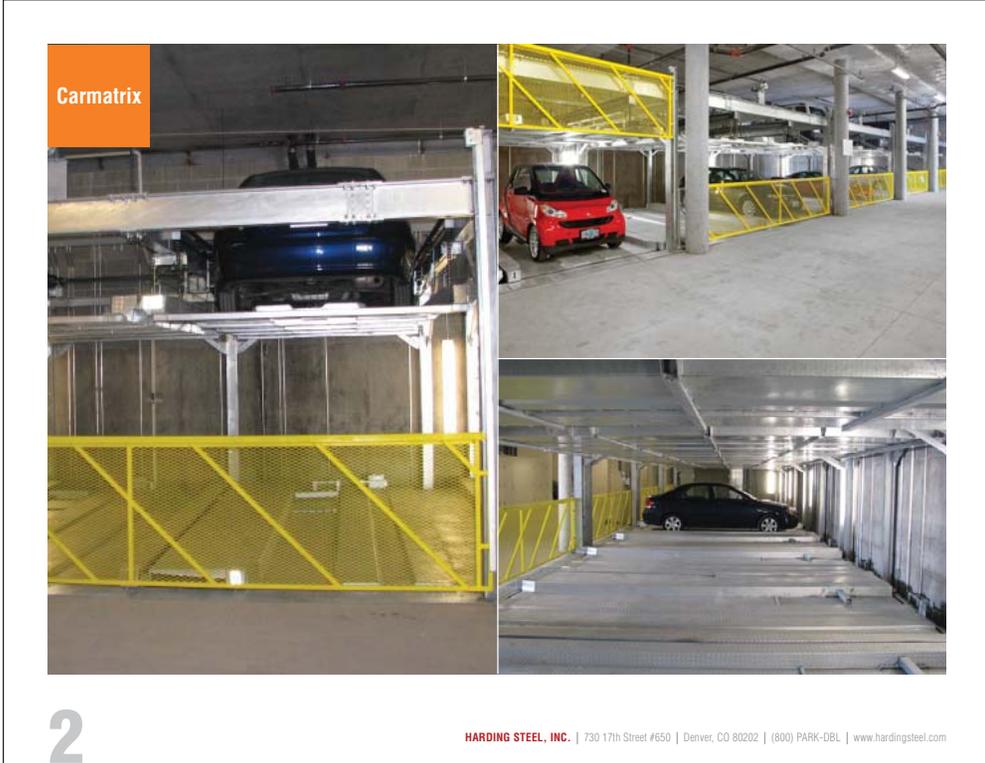
- All electric system ... no hydraulics
- Chain driven lifting and sliding mechanisms (no cables)
- Comprehensive safety features
- Manufactured in ISO 9000 certified facilities
- Self-parking capable
- Key pad control system
- Automatic operation returns each car to grade level for rapid retrieval
- 220 3-phase 60 Hz electric motors

Because no two buildings are alike, Harding Steel is able to create a Carmatrix solution specifically for each building. These systems are constructed of premium steel with 100% galvanized steel platforms for durability and a clean appearance.

Carmatrix operates on a platform-based grid system. The grade level platforms move laterally while the upper and lower platforms move vertically to allow the needed vehicle to be accessed at grade level. For each grouping of platforms there will always be a vacant space at grade for the upper and lower vehicles to be accessed.



1

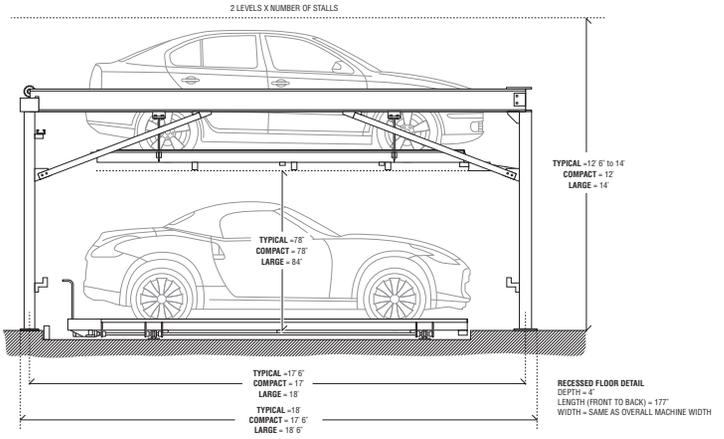


Carmatrix

2

Carmatrix

FIG. 1
2 LEVEL
CARMATRIX



3

CARMATRIX SYSTEM DOUBLE STACK

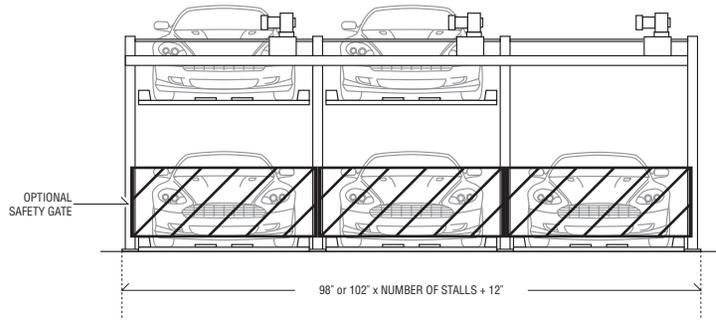


FIG. 2 2 LEVEL CARMATRIX

4

CARMATRIX SYSTEM WITH A PIT—TRIPLE STACK

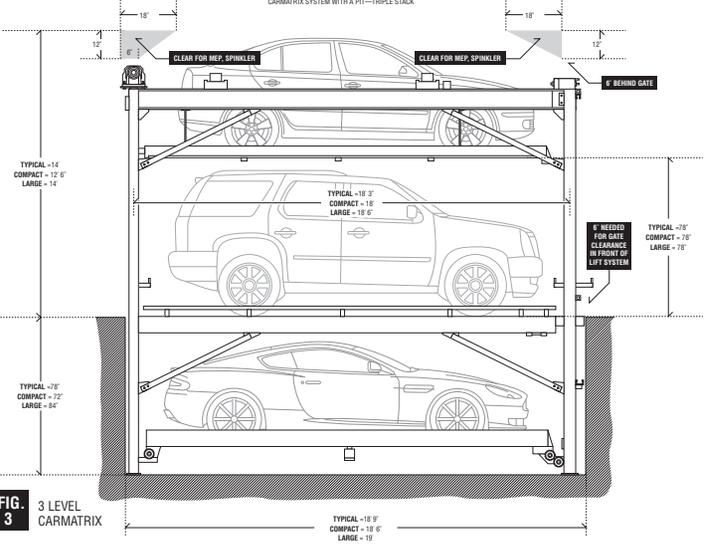


FIG. 3 3 LEVEL CARMATRIX

5

CARMATRIX SYSTEM WITH A PIT—TRIPLE STACK

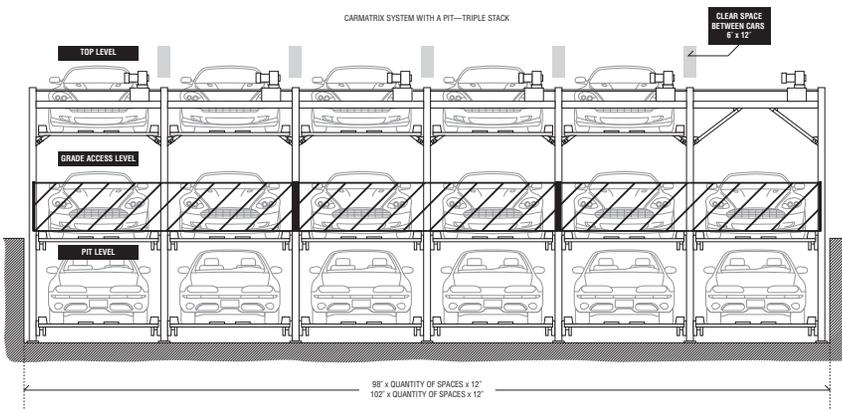


FIG. 4 3 LEVEL CARMATRIX

6

CARMATRIX SYSTEM WITHOUT PIT—TRIPLE STACK

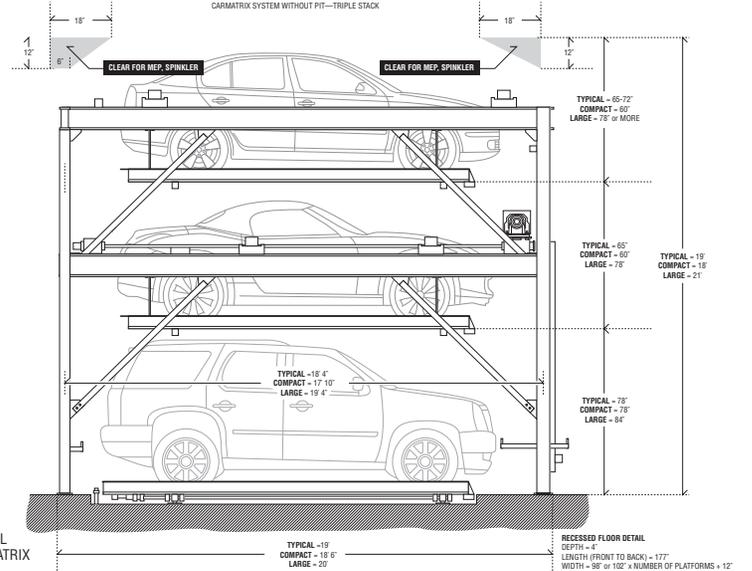


FIG. 5 3 LEVEL CARMATRIX

7

Carmatrix

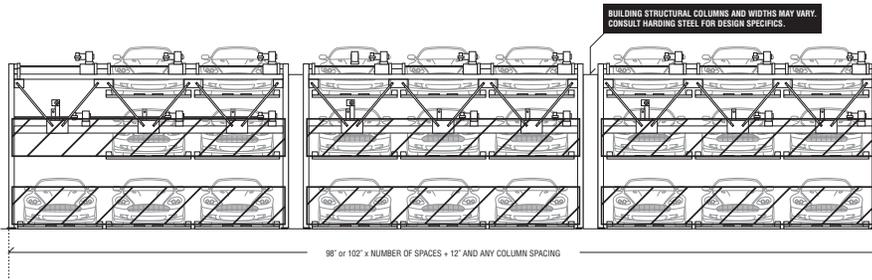


FIG. 6 3 LEVEL CARMATRIX

8

Carmatrix

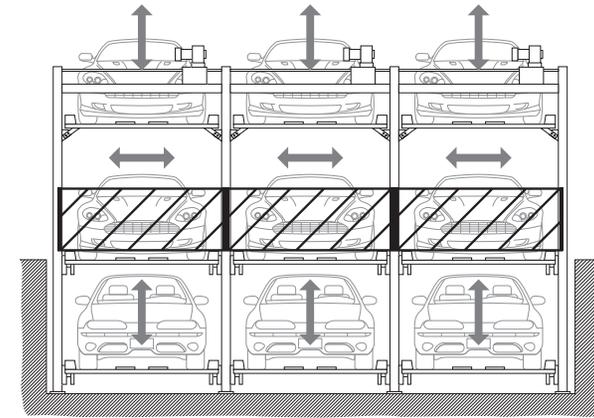


FIG. 7 DIRECTION OF TRAVEL

9

Carmatrix

FEATURES

- 100% premium steel construction
- Commercial grade design and materials
- Electric drive motors with heavy-duty chain operation (no cables)
- Suitable for indoor or outdoor installation
- 100% galvanized steel diamond-plate platforms standard
- Support vehicles weighing up to 5,000 pounds
- Electro-mechanical steel locking system for safety
- UL Approved components
- ETL Inspected controls
- Seismic and anchorages calculated for specific project and location
- Custom design for each project
- Additional options available

WARRANTY

One year electrical, two year mechanical, five year structural

SAFETY

- At-grade platforms are free of trip hazards and gaps
- Central panel controls all platforms in each section
- Can be configured for self-park (unattended) parking
- Laser sensing emergency stop for all machine movement
- Wiring block for integrating safety interlock devices
- Systems are fully configured with sensors and interlock switches
- Systems are supplied with safety gates gated for safe operation.

10



Mr. Victor Grambsch
c/o Nicollet Island-East Bank Neighborhood Association
132 Bank Street SE
Minneapolis, MN 55414-1033

February 15, 2016 (revised from January 22, 2016)

Dear Mr. Grambsch,

This week, SR Development, LLC, an affiliate of Schafer Richardson, will be submitting a Land Use Application to the Minneapolis City Planning Commission for approval of a mixed-use development located at 112-120 E Hennepin Avenue (known as 116 E Hennepin). The application is seeking approval for the redevelopment of the project site, specifically for:

- Conditional use permit to increase the maximum height
- Variance of the maximum floor area ratio (FAR)
- Variance of the interior side yard from 15 feet to 0 feet
- Variance for the window area on the Lourdes Place façade
- Variance for off-street parking requirements for commercial use
- Site plan review

The project will include preservation-in-place of the existing 2-story building at the corner of Lourdes Place and East Hennepin, known as the Nye's Bar Building, and the existing 3-story building at 116 E Hennepin, known as the "Harness Shop" building. Demolition of the two non-historic one-story buildings on site will make way for a new infill building, ranging from four-six stories. The new mixed-use development will contain approximately 72 market rate apartments and approximately 7,700-sf of ground floor retail. The development will feature indoor and outdoor amenity spaces including a fitness center, outdoor deck, community room, management office, and unit balconies. The development will include one level of underground parking with additional enclosed parking stalls located at-grade behind the first floor retail space for a total of 52 traditional parking spaces plus three tandem and six elevated spaces. In addition, there will be enclosed bicycle parking within the parking garage.

The proposed redevelopment of the site strongly aligns with the Major Strategic Goals of the Nicollet Island-East Bank Neighborhood Small Area Plan, as well as the Minneapolis Plan for Sustainable Growth, including minimizing impact on historic resources while allowing for high-quality contemporary design with an emphasis on cultivating pedestrian-oriented mixed-use corridors.

Please feel free to contact me with any questions regarding the proposed 116 E Hennepin Project at 612.359.5842 or mmichalski@sr-re.com. We look forward to continuing our relationship with you and the Nicollet Island-East Bank Neighborhood Association.

Sincerely,

A handwritten signature in blue ink, appearing to read "Maureen Michalski", written over a light blue circular scribble.

Maureen Michalski
Director of Development



Councilmember Jacob Frey
c/o City of Minneapolis
350 S. 5th Street, Room 307
Minneapolis, MN 55415

February 15, 2016 (revised from January 22, 2016)

Dear Councilmember Frey,

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Sincerely,

A handwritten signature in blue ink, appearing to read 'Maureen Michalski', written over a light blue circular scribble.

Maureen Michalski
Director of Development

Steiner, Lisa

From: Ann Fromell-Theis <fromelltheis@hotmail.com>
Sent: Wednesday, February 10, 2016 9:22 PM
To: Steiner, Lisa
Subject: 112 E Hennepin Ave

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: HPC

I received the notice regarding permits to among other issues a permit to increase the maximum height. That is NOT Acceptable.

The area is the birthplace of Minneapolis and we have an historic Church that will be obscured by a building any taller than the current proposal.

This is what we first objected to with the original proposal and it appears that Schafer Richardson now try to accomplish their very objectionable project through the "back door". I strongly object.

Ann Fromell-Theis

fromelltheis@hotmail.com

Steiner, Lisa

From: Ann Fromell-Theis <fromelltheis@hotmail.com>
Sent: Wednesday, March 02, 2016 7:30 PM
To: Steiner, Lisa
Subject: Schafer Richardson land use application

I have received another notice of a public hearing regarding the property at 112 Hennepin Ave. One of the requests that particularly concerns me is the conditional use permit to increase the maximum permitted height.

This is the second time this request has come up and I still very much oppose anything more than 6 stories. This area is of historical value and new construction needs to blend in and not disrupt. The proposed building is next to the historical land mark Our Lady of the Lourdes church and anything taller will obscure the church. The response I received from you after the last time this type of request came was that they were still only proposing 6 stories. I certainly hope that still is the case. These repeated hearings give the appearance that Schafer Richardson tries to achieve their initial proposal through the "back door".

I sincerely hope that is not the case.

Yours sincerely

Ann Fromell-Theis

fromelltheis@hotmail.com