

LAND USE APPLICATION SUMMARY

Property Location: 2200-2218 Snelling Ave
Project Name: The Bessemer at Seward
Prepared By: Janelle Widmeier, Senior City Planner, (612) 673-3156
Applicant: Seward Redesign Inc.
Project Contact: Renee Spillum
Request: To allow a new multiple-family dwelling with 121 units.
Required Applications:

Conditional Use Permit	To allow a multiple-family dwelling with 121 units.
Conditional Use Permit	To increase the maximum allowed height of a building from 4 stories to 5 stories and from 56 feet to 62 feet.
Variance	Of the PO Overlay District standards to allow the first floor of the building to be located more than 8 feet from a lot line adjacent to a street (Cedar Avenue, 22 nd Street, and Snelling Avenue).
Variance	To reduce the minimum interior side yard requirement adjacent to the west lot line from 13 feet to 8 feet to allow the building.
Variance	To reduce the minimum interior side yard requirement adjacent to the south lot line from 5 feet to 0 feet to allow a transformer and a generator.
Variance	To allow signage on a non-primary building wall, increase the maximum allowed sign area on a building wall from 0 square feet to 48 square feet, and increase the maximum allowed height from 28 feet to 36 feet to allow a projecting sign on the wall facing the Hiawatha Bike Trail.
Variance	To increase the maximum allowed size of a projecting sign from 20 square feet to 48 square feet to allow a projecting sign facing Snelling Avenue.
Site Plan Review	For a new 5-story residential building.

Date Application Deemed Complete	April 29, 2016	Date Extension Letter Sent	Not applicable
End of 60-Day Decision Period	June 28, 2016	End of 120-Day Decision Period	Not applicable

SITE DATA

Existing Zoning	I2 Medium Industrial District IL Industrial Living Overlay District PO Pedestrian Oriented Overlay District
Lot Area	54,060 square feet / 1.24 acres
Ward(s)	6
Neighborhood(s)	Seward
Designated Future Land Use	Mixed Use and Transitional Industrial
Land Use Features	Commercial Corridor (Cedar Avenue) Transit Station Area (Franklin Avenue)
Small Area Plan(s)	<u>Franklin-Cedar/Riverside Transit-Oriented Development Master Plan (2001)</u>

BACKGROUND

SITE DESCRIPTION AND PRESENT USE. The proposed development site includes 3 one-story industrial buildings located on the properties of 2200-2218 Snelling Avenue South. The site has frontage on three streets: Snelling Avenue, 22nd Street East, and Cedar Avenue. The Hiawatha Bike Trail and LRT Blue Line are adjacent to the west side of the site. The north end of the subject site is a vacated portion of 22nd Street East, which was not needed for public right-of-way after the street was reconfigured to connect to Cedar Avenue. The grade along Cedar Avenue slopes down significantly from the site an average of 14 feet. The grade also slopes down towards 22nd Street.

SURROUNDING PROPERTIES AND NEIGHBORHOOD. There is a mix of uses in the surrounding area. The immediate area was historically industrial. The subject block has been transitioning from industrial to residential in the last three years with the construction of two 4-story residential developments. The properties on the east side of Snelling Avenue remain industrial.

PROJECT DESCRIPTION. Seward Redesign Inc. is proposing to construct a new 5-story, multiple-family dwelling with 121 units. Enclosed parking with 95 spaces would be accommodated in two levels with access from Snelling Avenue and 22nd Street. The main entrance would face Snelling Avenue. An entrance would also be provided on the bike/pedestrian trail side of the building. A public trail connection is also proposed on the northwest side of the building.

The applicant is requesting the following land use applications:

- *Conditional use permit to allow 121 dwelling units:* In the IL Industrial Living Overlay District, multiple-family dwellings are allowed as conditional uses.
- *Conditional use permit to increase the maximum height of a building:* In the Industrial Districts, the maximum allowed height of a building is 4 stories, 56 feet. The proposed building height is 5 stories, 62 feet (measured to the tallest part of the parapet, which would be 4.5 feet in height). A conditional use permit is required to increase the maximum height. Upon approval of the conditional use permits, the actions must be recorded with Hennepin County as required by state law.

- *Variances of the PO Overlay District standards:* In the PO Overlay District, ground floor building walls are required to be located within 8 feet of a lot line adjacent to a public street. The first floor building wall would be set back 10 to 31 feet from Snelling Avenue, more than 20 feet from 22nd Street, and over 10 feet from Cedar Avenue. A variance is required to allow the proposed setbacks.
- *Variance of the southwest interior side yard to allow the building:* Where residential uses containing windows face an interior side yard, a side yard of at least five feet plus two feet for each story above the first floor is required, but is not to exceed 15 feet if no other yard requirements apply. Part of the southwest building wall would only be setback 8 feet from the interior side lot line and would contain windows. A variance is required to allow the proposed setback.
- *Variance of the southeast interior side yard to allow a transformer:* The southeast lot line is adjacent to a residential use. An interior side yard is required where a side lot line abuts a side lot line of a structure used for permitted or conditional residential purposes. Unless a permitted obstruction, a setback of at least 5 feet from the interior side lot line is required for obstructions other than the building. A transformer and a generator are not permitted obstructions. A variance is required to allow the proposed transformer and generator location.
- *Variance of signage requirements:* A 48 square foot projecting sign is proposed to be located on the wall facing the Hiawatha Bike Trail. This wall is not a primary building wall. Signage is only allowed on primary building walls. Variances are needed to allow the sign on the wall and to allow the proposed sign area since no signage is allowed on that wall. Building signs are also limited in height to 28 feet above the adjacent grade. The proposed sign would be located 36 feet above the adjacent grade. A variance is needed to increase the maximum height. A 48 square foot projecting sign is proposed on the wall facing Snelling Avenue as well. The maximum allowed size of a projecting sign in the industrial districts is 20 square feet. A variance is required to increase the maximum size allowed.
- *Site plan review.* Site plan review is required for any new building containing 5 or more dwelling units.

Since the project was last reviewed at the February 11, 2016, Planning Commission Committee of the Whole meeting, the project team made some changes to the proposal. The fiber cement panels and siding was eliminated to reduce the total number of primary exterior materials to three. Additional architectural elements were added around the main entrance. Some changes were made to the patio door and window types and modifications were made to the proposed stormwater management system. More detailed descriptions of the changes are attached to this report for reference.

PUBLIC COMMENTS. As of the writing of this report, correspondence from the neighborhood group has not been received. Any correspondence received prior to the public meeting will be forwarded on to the Planning Commission for consideration.

ANALYSIS

CONDITIONAL USE PERMIT

The Department of Community Planning and Economic Development has analyzed the application to allow a multiple-family dwelling with 121 units based on the following findings:

1. *The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, comfort or general welfare.*

Construction of a multiple-family dwelling with 121 units on the site would not prove detrimental to public health, safety, comfort or general welfare provided the development complies with all applicable building codes and life safety ordinances as well as Public Works Department standards.

2. *The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.*

Uses in the immediate area are primarily industrial. North of 24th Street, the long range vision in the comprehensive plan for this area is transit oriented development. The subject site shares a block face with a 60-unit senior housing development and a mixed use development that includes 40 dwelling units. Provided residents of the development understand that they are living in an industrial area that involves truck traffic and other reasonable industrial activity, surrounding uses should continue to operate (and may continue to operate in a legal fashion) without incident. Existing and future development of uses allowed in the industrial districts or IL Overlay District should not be negatively affected by the use.

3. *Adequate utilities, access roads, drainage, necessary facilities or other measures, have been or will be provided.*

The site would be served by existing infrastructure. Vehicle access would be from Snelling Avenue and 22nd Street East. The Public Works Department has reviewed the project for appropriate drainage and stormwater management as well as to ensure the safety of the position and design of improvements in or over the public right of way.

4. *Adequate measures have been or will be taken to minimize traffic congestion in the public streets.*

Adequate measures would be provided to minimize traffic congestion in the public streets. The minimum vehicle parking requirement is 61 spaces. A total of 95 parking spaces are proposed. The proposed amount of bicycle parking (117 spaces total) would greatly exceed the minimum requirement (60 spaces). The short-term bike spaces would be located adjacent to the main entrance and the entrance on the bike trail side of the building. The site is located within two blocks of two bus transit stops and the Franklin Avenue LRT station. There is direct access to the Hiawatha Bike Trail adjacent to the southwest side of the site. The applicant has also completed a Travel Demand Management Plan (TDMP), which is attached to this report for reference. According to the TDMP, additional measures that would be taken to address transportation issues include distributing information about available alternate modes of transportation, encouraging loading activities in off-peak hours, and offering pre-loaded Metro Transit Go-To Cards to new tenants. With these measures and the close proximity to alternative modes of transportation, the development should have little effect on congestion in the streets.

5. *The conditional use is consistent with the applicable policies of the comprehensive plan.*

The proposed use would be consistent with the applicable policies of the comprehensive plan. The future land use designation of the site is transitional industrial in *The Minneapolis Plan for Sustainable Growth*. Cedar Avenue is a commercial corridor. The site is located two blocks from the Franklin Avenue LRT station. For reference, the southern boundary for the Franklin Avenue LRT Station activity center is located at the north end of the block and the northern boundary for the Seward/Hiawatha industrial employment district is located on 24th Street south of the subject site. The following principals and policies apply to this proposal:

Land Use Policy 1.10: Support development along Commercial Corridors that enhances the street's character, fosters pedestrian movement, expands the range of goods and services available, and improves the ability to accommodate automobile traffic.

- 1.10.5 Encourage the development of high-density housing on Commercial Corridors.

Land Use Policy 1.13: Support high density development near transit stations in ways that encourage transit use and contribute to interesting and vibrant places.

Housing Policy 3.1: Grow by increasing the supply of housing.

- 3.1.1 Support the development of new medium- and high-density housing in appropriate locations throughout the city.

Housing Policy 3.2: Support housing density in locations that are well connected by transit, and are close to commercial, cultural and natural amenities.

- 3.2.1 Encourage and support housing development along commercial and community corridors, and in and near growth centers, activity centers, retail centers, transit station areas, and neighborhood commercial nodes.

The site is also located within the boundaries of the *Franklin-Cedar/Riverside Area Master Plan* that was adopted by the City Council in 2001. The plan recommends that a transit-oriented use, including higher density housing, be developed on this site.

6. *The conditional use shall, in all other respects, conform to the applicable regulations of the district in which it is located.*

The proposed use would conform to the applicable regulations upon the approval of the conditional use permits, variances, and site plan review.

CONDITIONAL USE PERMIT

The Department of Community Planning and Economic Development has analyzed the application to increase the maximum allowed height of a building from 4 stories to 5 stories and from 56 feet to 62 feet based on the following findings:

1. *The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, comfort or general welfare.*

The construction of a 5 story, 62 foot building would not prove detrimental to public health, safety, comfort or general welfare provided the development complies with all applicable building codes and life safety ordinances as well as Public Works Department standards.

2. *The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.*

The proposed height is only 6 feet higher than is allowed by the zoning. Allowing the proposed increase in height would have little effect on surrounding properties. The subject site is located to the north of a 4-story multiple-family residence. It is separated from all other surrounding properties by public streets. The Hiawatha Bike Trail is located to the west of the site. The proposed building would be L-shaped with only a relatively small portion of the building in close proximity to the public path. Adopted City policies encourage higher density development in this location. A taller building would allow more density to be established and provide more open space adjacent to a public path.

3. *Adequate utilities, access roads, drainage, necessary facilities or other measures, have been or will be provided.*

The site would be served by existing infrastructure. Vehicle access would be from Snelling Avenue and 22nd Street East. The Public Works Department has reviewed the project for appropriate drainage and stormwater management as well as to ensure the safety of the position and design of improvements in or over the public right of way.

4. *Adequate measures have been or will be taken to minimize traffic congestion in the public streets.*

Adequate measures would be provided to minimize traffic congestion in the public streets. The minimum vehicle parking requirement is 61 spaces. A total of 95 parking spaces are proposed. The proposed amount of bicycle parking (117 spaces total) would greatly exceed the minimum requirement (60 spaces). The short-term bike spaces would be located adjacent to the main entrance and the entrance on the bike trail side of the building. The site is located within two blocks of two bus transit stops and the Franklin Avenue LRT station. There is direct access to the Hiawatha Bike Trail adjacent to the southwest side of the site. The applicant has also completed a Travel Demand Management Plan (TDMP), which is attached to this report for reference. According to the TDMP, additional measures that would be taken to address transportation issues include distributing information about available alternate modes of transportation, encouraging loading activities in off-peak hours, and offering pre-loaded Metro Transit Go-To Cards to new tenants. With these measures and the close proximity to alternative modes of transportation, the development should have little effect on congestion in the streets.

5. *The conditional use is consistent with the applicable policies of the comprehensive plan.*

The proposed height would be consistent with the comprehensive plan to allow density appropriate to this location. In addition to the principles and policies in the above conditional use permit section of this staff report, the following apply to this proposal:

Land Use Policy 1.2: Ensure appropriate transitions between uses with different size, scale, and intensity.

- 1.2.1 Promote quality design in new development, as well as building orientation, scale, massing, buffering, and setbacks that are appropriate with the context of the surrounding area.

Land Use Policy 1.10: Support development along Commercial Corridors that enhances the street's character, fosters pedestrian movement, expands the range of goods and services available, and improves the ability to accommodate automobile traffic.

- I.10.4 Encourage a height of at least two stories for new buildings along Commercial Corridors, in keeping with neighborhood character.

Urban Design Policy 10.5: Support the development of multi-family residential dwellings of appropriate form and scale.

- 10.5.2 Medium-scale, multi-family residential development is more appropriate along Commercial Corridors, Activity Centers, Transit Station Areas and Growth Centers outside of Downtown Minneapolis.

6. *The conditional use shall, in all other respects, conform to the applicable regulations of the district in which it is located.*

With the approval of the conditional use permits, variances and site plan review, this development would be in conformance with the applicable regulations of the zoning code..

Additional Standards to Increase Maximum Height

In addition to the conditional use permit standards, the Planning Commission shall consider, but not be limited to, the following factors when determining the maximum height of principal structures in commercial districts:

1. *Access to light and air of surrounding properties.*

The increase in height would have little effect on the access to light and air of surrounding properties. The subject site is located to the north of a 4-story multiple-family residence. It is separated from all other surrounding properties by public streets. The Hiawatha Bike Trail is located to the west of the site. The proposed building would be L-shaped with only a relatively small portion of the building in close proximity to the public path. Adopted City policies encourage higher density development in this location. A taller building would allow more density to be established and provide more open space adjacent to a public path.

2. *Shadowing of residential properties, significant public spaces, or existing solar energy systems.*

Shadowing impacts would be minimal. The proposed building would be located north of the closest residential property. The Hiawatha Bike Trail is located to the west of the site. The proposed building would be L-shaped with only a relatively small portion of the building in close proximity to the public path. With the proposed setbacks, the building would not significantly shadow the adjacent streets or public path. A solar energy system is located on the roof of the adjacent residential building where it would not be impacted by the proposed development. Staff is not aware of any other solar energy systems in close proximity to the subject site.

3. *The scale and character of surrounding uses.*

The scale and character of the buildings in the surrounding area varies from one- to 4story buildings. Historically, the area was industrial. The recent trend of development is larger in scale and residential. The scale of the proposed building would be compatible with the surrounding uses.

4. *Preservation of views of landmark buildings, significant open spaces or water bodies.*

There are no landmark buildings, significant open spaces or water bodies near the development site that would be affected by the height of the proposed building.

VARIANCE

The Department of Community Planning and Economic Development has analyzed the application for a variance of the PO Overlay District standards to allow the first floor of the building to be located more than 8 feet from a lot line adjacent to a street (Cedar Avenue, 22nd Street, and Snelling Avenue) based on the following findings:

- 1. Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

In a PO Overlay District, all ground floor building walls are required to be located not more than eight feet from a lot line adjacent to a street. The first floor building wall would be set back 10 to 31 feet from Snelling Avenue, more than 20 feet from 22nd Street, and over 10 feet from Cedar Avenue. Practical difficulties exist in complying with the ordinance due to circumstances unique to the property. The subject site has frontage on three streets and is odd-shaped. There are also significant grade changes adjacent to Cedar Avenue and 22nd Street. The Cedar Avenue interior boulevard is also over 60 feet wide. The front of the proposed building and the main entrance would be oriented to Snelling Avenue, which is consistent with how other the other residential buildings are oriented on the block.

- 2. The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

The PO Pedestrian Oriented Overlay District is established to preserve and encourage the pedestrian character of commercial areas and to promote street life and activity by regulating building orientation and design and accessory parking facilities, and by prohibiting certain high impact and automobile-oriented uses. The standard pertaining to the placement of buildings is intended to reinforce the street wall, maximize natural surveillance and visibility, and facilitate pedestrian access and circulation. The subject site is located in an area that has been historically industrial and does not exhibit traditional commercial storefront character. The building would reinforce the street wall along Snelling Avenue by following the curvature of the street. The main entrance would be oriented to Snelling Avenue, which is consistent with how other the other residential buildings are oriented on the block. The columns of the covered walkway and entrance landing would be 3.5 feet from the Snelling Avenue lot line. The main entrance would be located on the area of the site that is flattest to facilitate pedestrian access and circulation to the building. Although an entrance would not face Cedar Avenue or 22nd Street, a public trail connection is proposed between the building and Cedar Avenue to facilitate pedestrian access. Also, an abundant amount of windows would be provided on each building wall facing a street to maximize natural surveillance and visibility. The request is reasonable and in keeping with the spirit and intent of the ordinance and the comprehensive plan.

- 3. The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

Granting of the variance would have little effect on surrounding properties and would not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties. The subject site is located in an area that has been historically industrial and does not exhibit traditional commercial storefront character. The setbacks of surrounding properties on each adjacent street vary. This block face has also transitioned to residential. An

abundant amount of windows would be provided on each building wall elevation facing a street to maximize natural surveillance and visibility.

VARIANCE

The Department of Community Planning and Economic Development has analyzed the application for a variance to reduce the minimum interior side yard requirement adjacent to the west lot line from 13 feet to 8 feet to allow the building based on the following findings:

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

Where residential uses containing windows face an interior side lot line, a side yard of at least five feet plus two feet for each story above the first floor, is required. The wall facing the southwest interior side lot line would contain windows. For a 5-story building, the minimum side yard requirement is 13 feet. Of the over 200 foot wide southwest facing wall, only 45.5 feet of the wall would be located less than 13 feet from the side lot line. Practical difficulties exist in complying with the ordinance due to circumstances unique to the property that were not created by the applicant. The odd shape of the site and significant grade changes adjacent to the north side of the site impact the placement of the building. Further, Chapter 530 Site Plan Review standards require emphasizing architectural elements, including recesses and projections, to divide larger buildings into smaller identifiable sections. The Hiawatha Bike Trail corridor abuts the southwest side of the property. Chapter 530 standards also require windows on walls facing a public pathway to contain windows.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

In general, yard controls are established to provide for the orderly development and use of land and to minimize conflicts among land uses by regulating the dimension and use of yards in order to provide adequate light, air, open space and separation of uses. Of the over 200 foot wide southwest facing wall, only 45.5 feet of the wall would be located less than 13 feet from the side lot line. The yard requirement applies only to walls with windows for residential uses to ensure adequate light and air for a residence. The Hiawatha Bike Trail corridor abuts the southwest side of the property. The bike path is located 10 feet from the southwest lot line. The separation between the building and path would be sufficient to prevent any adverse impacts as a result of reducing the yard requirement. Further, an abundant amount of windows would be provided on the southwest side of the building to promote natural surveillance and visibility. The request is reasonable and in keeping with the spirit and intent of the ordinance and the comprehensive plan.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

Granting the variance would not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity and would not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties. Of the over 200 foot wide southwest facing wall, only 45.5 feet of the wall would be located less than 13 feet from the side lot line. The yard requirement applies only to walls with windows for residential uses to ensure adequate light and air for a residence. The Hiawatha Bike Trail corridor abuts the southwest side of the property. The bike path is located 10 feet from the southwest lot line. The separation between the building and path would be sufficient to prevent any adverse

impacts as a result of reducing the yard requirement. Further, an abundant amount of windows would be provided on the southwest side of the building to promote natural surveillance and visibility.

VARIANCE

The Department of Community Planning and Economic Development has analyzed the application for a variance to reduce the minimum interior side yard requirement adjacent to the south lot line from 5 feet to 0 feet to allow a transformer and a generator based on the following findings:

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

The southeast lot line is adjacent to a residential use. An interior side yard is required where a side lot line abuts a side lot line of a structure used for permitted or conditional residential purposes. Unless a permitted obstruction, a setback of at least 5 feet from the interior side lot line is required for obstructions other than the building. A transformer and a generator are not permitted obstructions. Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The site has frontage on three streets. Locating the transformer and generator between the building and a street where it would be highly visible is not desirable. A transformer location also needs to be accessible for maintenance. The proposed location next to the service drive off of Snelling Avenue would also be in close proximity to the basement electrical room.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

In general, yard controls are established to provide for the orderly development and use of land and to minimize conflicts among land uses by regulating the dimension and use of yards in order to provide adequate light, air, open space and separation of uses. The proposed transformer and generator would be directly adjacent to a service area and the transformer located on the adjacent property. Shrubs that would grow to a height greater than 6 feet would provide screening on the east and west sides, but no screening is shown along the south lot line. There are residential units that face the shared lot line on the adjacent property. Additional screening needs to be provided to sufficiently screen the mechanical equipment, which is included as a condition in the site plan review recommendation. With the additional screening, the request is reasonable and in keeping with the spirit and intent of the ordinance and the comprehensive plan.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

With more screening along the south lot line, granting the variance would not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity and would not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties. The proposed transformer and generator would be directly adjacent to a service area and the transformer located on the adjacent property. Shrubs that would grow to a height greater than 6 feet would provide screening on the east and west sides, but no screening is shown along the south lot line. There are residential units that face the shared lot line on the adjacent property. Additional screening needs to be provided to sufficiently screen the mechanical equipment, which is included as a condition in the site plan review recommendation.

VARIANCE

The Department of Community Planning and Economic Development has analyzed the application for a variance to allow signage on a non-primary building wall, increase the maximum allowed sign area on a building wall from 0 square feet to 48 square feet, and increase the maximum allowed height from 28 feet to 36 feet to allow a projecting sign on the wall facing the Hiawatha Bike Trail based on the following findings:

- 1. Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

To create identification visibility for the property as well as provide wayfinding for commuters passing by, a 48 square foot projecting sign is proposed to be located on the wall facing the Hiawatha Bike Trail. This wall is not a primary building wall. Signage is only allowed on primary building walls, which are walls facing public streets or on-site parking lots. Building signs are also limited in height to 28 feet above the adjacent grade. The proposed sign would be located 36 feet above the adjacent grade. Practical difficulties exist in complying with the ordinance due to circumstances unique to the property for the proposed sign location, but not for the size or height proposed. The building has frontage on three streets. The main entrance would face Snelling Avenue, which is appropriate in this location. However, this block of Snelling Avenue has a very low traffic level and limited visibility. The southwest side of the building has exposure to high levels of traffic with its adjacency to the Hiawatha Bike Trail and the LRT Blue Line as well as visibility from Highway 55. Because the bike path is not a public street, signage is not allowed on the southwest side of the building. Although additional signage could be located on the wall facing Cedar Avenue, this signage would have limited visibility from the trail or LRT line. In the industrial districts, the maximum allowed size of projecting signs is 20 square feet. A 48 square foot projecting sign and a height limit of greater than 28 feet is first allowed in the C3A Community Activity Center district. A 20 square foot projecting sign at 28 feet above grade would provide sufficient visibility for users of the bike trail and LRT.

- 2. The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

Regulations governing on-premise signs are established to allow effective signage appropriate to the planned character of each zoning district, to promote an attractive environment by minimizing visual clutter and confusion, to minimize adverse effects on nearby property, and to protect the public health, safety and welfare. The I1 Industrial Living Overlay District is established to encourage the rehabilitation and reuse of existing industrial structures and to provide for limited residential and retail uses in the I1 and I2 Industrial Districts where such uses are compatible with other uses in the area.

The proposed sign is intended to create more identification visibility for the property as well as provide wayfinding for commuters passing by. In the industrial districts, the maximum allowed size of projecting signs is 20 square feet and the maximum allowed height of signs above the adjacent grade is 28 feet. A 48 square foot projecting sign and a height limit of greater than 28 feet is first allowed in the C3A Community Activity Center district. Even downtown, most signs are subject to a 28 foot height limitation. A 48 square foot projecting sign located 36 feet above grade is not characteristic of signs for residential development outside of downtown or C3A districts. Allowing a 20 square foot projecting sign located 28 feet above the adjacent grade on the wall adjacent to the Hiawatha Bike Trail would be reasonable and in keeping with the spirit and intent of the ordinance and the comprehensive plan.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

Provided the proposed sign would not exceed 20 square feet in area or be located more than 28 feet above the adjacent grade, granting the variances to allow a sign on a non-primary building wall facing the Hiawatha Bike Trail would not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity and would not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties. The surrounding properties include the Touchstone Supportive Housing and Wellness Center (a mixed-use building to the south that was recently constructed to allow for 40 assisted living units, a medical clinic and offices) as well as the Cooperage (a 60 unit senior housing development). Additionally there are industrial and commercial uses in the vicinity. The proposed sign location is intended to create more identification visibility for the property as well as provide wayfinding for commuters passing by. It would be located over 80 feet from the adjacent residential building. The scale of the sign would also not be disproportionate to the size of the building. However, the proposed size and height are not characteristic of signs for residential developments outside of downtown or C3A districts. A projecting sign that would comply with the size and height requirements of a sign located on a primary building wall would provide sufficient visibility to users of the bike trail and LRT.

Additional Standards for Sign Adjustments

In addition, the Planning Commission shall consider, but not be limited to, the following factors when considering an adjustment to the number, type, height, area, or location of allowed signs on property located in an OR2 or OR3 District or a commercial, downtown, or industrial district:

1. *The sign adjustment will not significantly increase or lead to sign clutter in the area or result in a sign that is inconsistent with the purpose of the zoning district in which the property is located.*

The purpose of the IL Industrial Living Overlay District is to encourage the rehabilitation and reuse of existing industrial structures and to provide for limited residential and retail uses in the I1 and I2 Industrial Districts where such uses are compatible with other uses in the area. A total of three signs are proposed for the development. Two would be located adjacent to the main entrance facing Snelling Avenue. The third sign would face the bike trail and would also be visible from the LRT line. Although allowing a sign to face the bike trail would not lead to sign clutter, allowing the proposed size and height would not be consistent with the intent of the IL overlay district as they are not characteristic of signs for residential developments outside of downtown or C3A districts.

2. *The sign adjustment will allow a sign that relates in size, shape, materials, color, illumination and character to the function and architectural character of the building or property on which the sign will be located.*

The primary exterior materials of the 5-story, residential building would be brick, stucco and burnished concrete masonry units. The scale of the proposed 48 square foot sign would not be disproportionate to the size of the building. However, the proposed size and height are not characteristic of signs for residential developments outside of downtown or C3A districts. The sign cabinet would be metal with cut-out letters that would be internally illuminated. Provided the proposed sign would not exceed 20 square feet in area or be located more than 28 feet above the adjacent grade, the sign adjustment would allow for a sign that relates in size, shape, material, color, illumination and character of the building and the use.

VARIANCE

The Department of Community Planning and Economic Development has analyzed the application for a variance to increase the maximum allowed size of a projecting sign from 20 square feet to 48 square feet to allow a projecting sign facing Snelling Avenue based on the following findings:

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

A 48 square foot projecting sign is proposed on the wall facing Snelling Avenue near the main entrance to create more identification visibility for the property. The maximum allowed size for a projecting sign in the industrial districts is 20 square feet. Practical difficulties do not exist in complying with the ordinance because of circumstances unique to the property. The site has frontage on three streets. Because no freestanding sign is proposed, 1.5 square feet of signage is allowed for every one linear foot of building wall facing a street. On the wall facing Snelling Avenue, approximately 420 square feet of signage is allowed. Individual wall signs can be up to 180 square feet in area. Although this block of Snelling Avenue has a very low traffic level and limited visibility, the zoning code allows for a sufficient amount of signage without the need to increase the maximum allowed size of a projecting sign.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

Regulations governing on-premise signs are established to allow effective signage appropriate to the planned character of each zoning district, to promote an attractive environment by minimizing visual clutter and confusion, to minimize adverse effects on nearby property, and to protect the public health, safety and welfare. The IL Industrial Living Overlay District is established to encourage the rehabilitation and reuse of existing industrial structures and to provide for limited residential and retail uses in the I1 and I2 Industrial Districts where such uses are compatible with other uses in the area.

The proposed sign is intended to create more identification visibility for the property. A total of three signs are proposed on the building, so the request would not result in sign clutter. However, a 48 square foot projecting sign is not characteristic of signs for residential development outside of downtown or C3A districts. In the industrial and other commercial districts, the maximum allowed size of projecting signs ranges from 16 to 24 square feet. A 48 square foot projecting sign is only allowed in the C3A Community Activity Center district and downtown districts. Further, additional wall signage on the street-facing building walls is allowed by the zoning code to address identification needs. For these reasons, the request is not reasonable nor in keeping with the spirit and intent of the ordinance and the comprehensive plan.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

Granting the variance would alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. The surrounding properties include residential and nonresidential uses. The proposed sign is intended to create more identification visibility for the property. It would be located over 60 feet from the adjacent residential building. The scale of the sign would also not be disproportionate to the size of the building. However, the proposed size is not characteristic of signs for residential developments outside of downtown or C3A districts. Larger signs are also not typical for the surrounding nonresidential properties. The sign would not

be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties provided it is installed by a licensed and registered sign contractor.

Additional Standards for Sign Adjustments

In addition, the Planning Commission shall consider, but not be limited to, the following factors when considering an adjustment to the number, type, height, area, or location of allowed signs on property located in an OR2 or OR3 District or a commercial, downtown, or industrial district:

1. *The sign adjustment will not significantly increase or lead to sign clutter in the area or result in a sign that is inconsistent with the purpose of the zoning district in which the property is located.*

The purpose of the IL Industrial Living Overlay District is to encourage the rehabilitation and reuse of existing industrial structures and to provide for limited residential and retail uses in the I1 and I2 Industrial Districts where such uses are compatible with other uses in the area. Two signs are proposed to be located adjacent to the main entrance facing Snelling Avenue. The third sign would face the bike trail. With a total of three signs proposed for the development, the request would not result in sign clutter. However, allowing the proposed size would not be consistent with the intent of the IL overlay district as it is not characteristic of signs for residential developments outside of downtown or C3A districts.

2. *The sign adjustment will allow a sign that relates in size, shape, materials, color, illumination and character to the function and architectural character of the building or property on which the sign will be located.*

The primary exterior materials of the 5-story, residential building would be brick, stucco and burnished concrete masonry units. The scale of the proposed 48 square foot sign would not be disproportionate to the size of the building. However, the proposed size is not characteristic of signs for residential developments outside of downtown or C3A districts. The sign cabinet would be metal with cut-out letters that would be internally illuminated. Provided the proposed sign would not exceed 20 square feet in area, the sign would relate in size, shape, material, color, illumination and character of the building and the use.

SITE PLAN REVIEW

The Department of Community Planning and Economic Development has analyzed the application based on the required findings and applicable standards in the site plan review chapter:

I. Conformance to all applicable standards of Chapter 530, Site Plan Review.

BUILDING PLACEMENT AND DESIGN

Building Placement – *Requires alternative compliance*

- The building would reinforce the street wall along Snelling Avenue, but not along Cedar Avenue and 22nd Street. Alternative compliance is requested. Although alternative compliance is requested for the placement of the building, windows greatly exceeding the minimum requirements would be provided on each side of the building to maximize natural surveillance and visibility. The main entrance would face Snelling Avenue and would be connected directly to the public sidewalk by a walkway. An entrance would also be provided on the west side of the building to provide residents convenient access to and from the Hiawatha Bike Trail.
- The first floor of the building would face three streets. The first floor building wall would be set back 10 to 31 feet from Snelling Avenue, more than 20 feet from 22nd Street, and over 10 feet from Cedar Avenue. Alternative compliance is requested to allow the first floor of the building to be set

back more than 8 feet from each street. A variance of the PO Pedestrian Oriented Overlay District standards is also required to allow the proposed setbacks.

- Landscaping would be located between the building and the adjacent streets. A connection to the Hiawatha Bike Trail is also proposed between the building and Cedar Avenue.
- All on-site accessory parking would be located within the principal building served.

Principal entrances – Meets requirements

- The main entrance would face Snelling Avenue. An entrance would also be provided on the west side of the building to provide residents convenient access to and from the Hiawatha Bike Trail.
- The principal entrance would be emphasized with sidelights and would be sheltered. To accentuate the entrance further, the applicant is proposing enhanced lighting and galvanized metal panels as an accent material on the columns of the entrance landing and on the columns adjacent to the accessible ramp leading to the entrance.

Visual interest – Meets requirements

- Building walls would provide architectural details and contain windows to create visual interest and increase security of adjacent outdoor spaces.
- Architectural elements, including recesses, projections, and windows, would be emphasized to divide the building into smaller identifiable sections.
- The building would not have any blank, uninterrupted walls that do not include architectural elements exceeding 25 feet in length.

Exterior materials – Meets requirements with Conditions of Approval

- Brick, stucco, and burnished concrete masonry units are proposed as the building’s primary exterior materials. It appears that each elevation would comply with the City’s durability standards for exterior materials (see Table I). Not more than three primary exterior building materials would be used. One brick color, one concrete block color, and three stucco colors are proposed. Although more than one stucco color is proposed, the changes in color would be used to divide the building into smaller identifiable sections, e.g. the light colored stucco would only be used on the fifth level above brick sections with a taller parapet wall. The dark colored stucco would be used sparingly as well. Updated percentages of the exterior materials for each elevation have not been provided since the project was reviewed at Committee of the Whole. Each building elevation will need to comply with the City’s standards for the maximum percentage of stucco and burnished concrete masonry units allowed. Please note that exterior material changes at a later date may require review by the Planning Commission and an amendment to the site plan review.
- The building design is consistent with the City’s policy of allowing no more than three exterior materials per elevation, excluding windows, doors, and foundation materials.
- Plain face concrete block is not proposed as an exterior material.
- The materials and appearance of each side of the building are similar and compatible to each other.

Table I. Percentage of Exterior Materials per Elevation

Material	Allowed Max	North	South	East	West
Brick (face)	100%	Percentages not specified			
Glass	100%				
Stucco	75%				
Burnished CMU	50%				

Windows – Requires alternative compliance

- The project would comply with the minimum window requirements for a residential use, except on the first floor of the north elevation (see Table 3). Alternative compliance is requested.
- All windows used to the minimum requirements would be vertical in proportion and would be more or less evenly distributed along the building walls.

Table 2. Percentage of Windows Required for Elevations Facing a Public Street, Sidewalk, Pathway, or On-Site Parking

		Code Requirement		Proposed	
Residential Uses					
Snelling Ave (northeast and east)	1 st Floor	20% minimum	461 sq. ft.	32.3%	745 sq. ft.
	2 nd -5 th Floors	10% minimum	302 sq. ft.	29.7%	897 sq. ft.
22 nd St (north)	1 st Floor	20% minimum	99 sq. ft.	4.4%	22 sq. ft.
	2 nd -5 th Floors	10% minimum	65 sq. ft.	26.7%	174 sq. ft.
Cedar Ave (west)	1 st Floor	20% minimum	198 sq. ft.	34%	337 sq. ft.
	2 nd -5 th Floors	10% minimum	130 sq. ft.	29.5%	384 sq. ft.
Public pathway (southwest)	1 st Floor	20% minimum	306 sq. ft.	27.1%	414 sq. ft.
	2 nd -5 th Floors	10% minimum	200 sq. ft.	30.8%	618 sq. ft.

Ground floor active functions – Requires alternative compliance

- More than 70 percent of the first floor building frontage facing Snelling Avenue and Cedar Avenue would contain active functions. The ground floor facing 22nd Street would contain 0 percent active functions. The ground floor facing the Hiawatha Bike Trail would contain 67.5 percent (129 feet) active functions. Alternative compliance is requested.

Roof line – Meets requirements

- A flat roof is proposed, which is similar to surrounding buildings.

Parking garages – Not applicable

- See above findings.

ACCESS AND CIRCULATION

Pedestrian access – Meets requirements

- A clear and well-lit walkway exceeding 4 feet in width would connect the main building entrance to the adjacent public sidewalk.

Transit access – Meets requirements

- No transit shelters are proposed as part of this development.

Vehicular access – Choose an item.

- Vehicular access and circulation is designed to minimize conflicts with pedestrians and surrounding residential uses. Two points of vehicle access are proposed. A service drive on Snelling Avenue is proposed that would provide access to the first floor parking level where refuse storage is also located and to the on-site loading space. A bullnose is proposed between the on-site service drive

and the adjacent service drive to provide a refuge for pedestrians. A driveway on 22nd Street is proposed to provide access to the lower parking level. The applicant has worked with Public Works and Planning staff to minimize the width of the curb cuts and the impact on the pedestrian realm.

- There is no public alley adjacent to the site.
- To the extent practical, the site plan would minimize the use of impervious surfaces. Existing conditions are almost entirely impervious (94 percent). Over 22,000 square feet of the site would be landscaped, which is approximately 74 percent of the net lot area. No surface parking is proposed.

LANDSCAPING AND SCREENING

General Landscaping and Screening – Meets requirements

- The overall composition and location of landscaped areas complement the scale of development and its surroundings.
- At least 20 percent of the site not occupied by the building is landscaped. The applicant is proposing approximately 22,475 square feet of landscaping on site, or approximately 74 percent of the site not occupied by buildings (see Table 3).
- The applicant is proposing at least one canopy tree per 500 square feet of the required landscaped area, including all required landscaped yards. The tree requirement for the site is 13 and the applicant is proposing a total of 44 trees.
- The applicant is proposing at least one shrub per 100 square feet of the required landscaped area, including all required landscaped yards. The shrub requirement for the site is 61 and the applicant is proposing 327 shrubs.
- The remainder of the required landscaped area and all other areas not occupied by buildings, loading facilities, or driveways would be covered with turf grass, native grasses, perennial flowering plants, annuals, shrubs and other trees.
- The landscape plan indicates that the landscape materials would comply with the installation and maintenance requirements.

Table 3. Landscaping and Screening Requirements

	Code Requirement	Proposed
Lot Area	--	54,060 sq. ft.
Building Footprint	--	23,576 sq. ft.
Remaining Lot Area	--	30,484 sq. ft.
Landscaping Required	6,097 sq. ft.	22,475 sq. ft.
Canopy Trees (1:500 sq. ft.)	13 trees	44 trees
Shrubs (1:100 sq. ft.)	61 shrubs	327 shrubs

Parking and loading landscaping and screening – Meets requirements

- There is no surface parking proposed for the site, so the site is not subject to the screening and landscaping requirements for parking areas per section 530.170.
- A loading area, adjacent to a residential use, would contain an on-site landscaped yard of at least 7 feet in width.
- The applicant is proposing a six-foot screen that is at least 95 percent opaque consisting of arborvitae shrubs between the loading space and the adjacent residential use.

ADDITIONAL STANDARDS

Concrete curbs and wheel stops – Meets requirements

- No surface parking is proposed on the site. The driveways would be defined by continuous concrete curbing. Paved areas for vehicular purposes would cover a very small percentage of the site. On-site retention of stormwater is not practical due to grade changes and proximity to adjacent buildings.

Site context – Meets requirements

- The proposed building would not impede views of important elements of the city, and would be located and arranged to minimize shadowing on public spaces and adjacent properties and to minimize the generation of wind currents at the ground level. Plantings around the perimeter of the building would also help to prevent generation of wind currents.

Crime prevention through environmental design – Meets requirements with Conditions of Approval

- The development includes environmental design elements to prevent crime. An abundant amount of windows that will promote natural observation and allow views into and out of the building at eye level would be provided on all sides of the building. The applicant has indicated that lighting would be used to accentuate the main entrance. However, a complete lighting plan has not been submitted as part of the application. A photometric plan will also need to be provided as part of the final plans. Landscaping, walkways and fencing would be used to distinguish between public and private spaces and to guide pedestrian movement through the site.

Historic preservation – Meets requirements

- The existing structures on the site will be demolished in order to construct the new building. The buildings are not locally designated historic structures or potential historic resources.

2. Conformance with all applicable regulations of the zoning ordinance.

The proposed use is *conditional* in the IL Overlay District.

Off-street Parking and Loading – Meets requirements

- The minimum vehicle parking requirement for a multiple-family dwelling is one space per dwelling unit. Within one-half mile of the Franklin Avenue LRT Station, multiple family dwellings with more than 50 units are required to provide spaces equal to only 50 percent of this requirement. Therefore, 61 spaces must be provided. A total of 95 spaces would be provided. Of those, 77 would be standard, 14 would be compact, and 4 would be accessible. The minimum requirements for standard and accessible spaces would be exceeded. See Table 4.
- The minimum bicycle parking requirement is 0.5 spaces per dwelling unit (see Table 5).
- One small loading space is required for multiple family dwellings with 100 to 250 units (see Table 6).

Table 4. Vehicle Parking Requirements Per Use (Chapter 54I)

Use	Minimum Vehicle Parking	Applicable Reductions	Total Minimum Requirement	Maximum Allowed	Proposed
Multiple-family dwelling	121	60	61	N/A for enclosed parking	95 enclosed

Table 5. Bicycle Parking Requirements (Chapter 54I)

Use	Minimum	Short-Term	Long-Term	Proposed
Multiple-family dwelling	60	--	Not less than 90%	6 short-term and 111 long-term

Table 6. Loading Requirements (Chapter 54I)

Use	Loading Requirement	Minimum Requirement	Proposed
Multiple-family dwelling	Determined by number of dwelling units	One small space	One small space

Building Bulk and Height – Requires conditional use permit

- In the I2 district, the maximum allowed FAR is 2.7. In the IL Overlay District, the maximum FAR of multiple-family dwellings may be increased by 30 percent if all required parking is provided within the building, entirely below grade, or in a parking garage of at least two levels.
- A conditional use permit is required to increase the maximum height of the building.

Table 7. Building Bulk and Height Requirements

	Code Requirement	Proposed
Lot Area	--	54,060 sq. ft.
Gross Floor Area (GFA)	--	111,450 sq. ft.
Maximum Floor Area Ratio (GFA/Lot Area)	3.51	2.06
Maximum Building Height	4 stories or 56 feet, whichever is less	5 stories, 62 ft.

Lot Requirements – Meets requirements

- The minimum lot area requirement for a multiple-family dwelling located in the IL Overlay District is 5,000 square feet.

Table 8. Lot Requirements Summary

	Code Requirement	Proposed
Dwelling Units (DU)	--	121 DUs
Density (DU/acre)	--	97.5 DU/acre
Lot Area (Minimum)	5,000 sq. ft.	54,060 sq. ft.
Impervious Surface Area (Maximum)	N/A	N/A
Lot Coverage (Maximum)	N/A	N/A
Lot Width (Maximum)	N/A	N/A

Yard Requirements – Requires variance(s)

- Where a side lot line abuts a side lot line of a structure used for permitted or conditional residential purposes, a yard of not less than five plus two feet for every story above the first shall be provided along such side lot line. Other obstructions are generally required to be setback at least 5 feet from the interior side lot line, unless they are permitted obstructions as outlined in [Chapter 535](#) or [Chapter 541](#). A driveway providing access to properly located parking area is a permitted obstruction. A transformer is not a permitted obstruction. A variance is requested to allow the proposed transformer location.
- Where residential uses containing windows face an interior side yard, a side yard of at least five feet plus two feet for each story above the first floor is required, but is not to exceed 15 feet if no other yard requirements apply. Part of the southwest building wall would only be setback 8 feet from the interior side lot line. A variance is requested.

Table 9. Minimum Yard Requirements for the Building

Setback	Zoning District	Overriding Regulations	Total Requirement	Proposed
Interior Side (SE)	13 ft. for the building	--	13 ft.	25 ft.
Interior Side (SW)	13 ft. for the building	--	13 ft.	8 ft.

Signs – Requires variance(s)

- Signs are subject to [Chapter 543](#) of the Zoning Code. All new signs are required to meet the requirements in [Chapter 543](#).
- Two projecting signs are proposed, one adjacent to Snelling Avenue and the other adjacent to the Hiawatha Bike Trail. Each projecting sign would be 48 square feet in area and internally illuminated. The Snelling Avenue sign would be located 28 feet above the adjacent grade while the bike trail sign would be 36 feet above grade. The plans indicate that both signs would project 4.5 feet from the building wall. However, the applicant has stated that their intent is to comply with the maximum allowed projection of 4 feet and not request a variance. The Snelling Avenue building wall is a primary building wall. Since no freestanding signs are proposed, up to 1.5 square feet of sign area is allowed for each one foot of linear building wall length. Notwithstanding the total amount of sign area allowed for the building wall, a projecting sign is not allowed to exceed 20 square feet in area. A variance is requested to increase the maximum allowed size of the proposed Snelling Avenue projecting sign. The bike trail building wall is not a primary building wall and no sign area is allowed. Variances are requested to allow the sign on the wall and to allow the proposed sign area since no signage is allowed on that wall. Lastly, a variance is requested to increase the maximum sign height from 28 feet to 36 feet.
- An address sign is also proposed above the main entrance on the awning. It would be approximately 3 square feet in area and is therefore subject to the sign regulations.

Table 10. Signage Summary

	Number Allowed/Lot	Proposed Number	Maximum Size Allocation	Maximum Area Per Sign	Proposed Area	Maximum Allowed Height	Proposed Height
Projecting (Snelling)	No limit	1	420 sq. ft.	20 sq. ft.	48 sq. ft.	28 ft.	28 ft.
Projecting (bike trail)		1	0 sq. ft.		48 sq. ft.		36 ft.
Attached (Snelling)		1	420 sq. ft.	180 sq. ft.	3 sq. ft.		10 ft.

Refuse Screening – Meets requirements

- Refuse storage containers are required to be enclosed on all four sides by screening compatible with the principal structure not less than two feet higher than the refuse container or shall be otherwise effectively screened from the street, adjacent residential uses located in a residence or office residence district and adjacent permitted or conditional residential uses. Refuse storage containers will be stored in the building.

Screening of Mechanical Equipment – Meets requirements with Conditions of Approval

- All mechanical equipment is required to be arranged so as to minimize visual impact by using screening and must comply with Chapter 535 and district requirements. The transformer and generator would be screened by shrubs that would grow to a height greater than 6 feet on the east and west sides. Screening is not proposed along the south lot line where the equipment would be visible from residential units on the adjacent property. Additional screening is required. Rooftop mechanical equipment is also proposed, but no screening around the units is indicated. The height of the units has also not been identified. The parapet walls, 1.3 and 4.5 feet tall, would provide some screening. Additional screening will need to be provided if the screening from the parapet walls is not sufficient. Magic pak units are proposed for each dwelling unit. The applicant has indicated that the ventilation louvers for the HVAC units would be painted to match the color of the adjacent exterior material to lessen their aesthetic impact.

Lighting – Lighting plan required

- Lighting must comply with Chapter 535 and Chapter 541 of the zoning code. A lighting plan was not provided as part of the application. One will need to be provided with the final plans.

Fences – Choose an item.

- Fences must comply with the requirements in Chapter 535. The applicant is proposing fencing around the dog run and the outdoor terrace area. A 4 foot tall, vinyl coated chain link fence is proposed around the dog run. A 6 foot tall aluminum decorative fence is proposed around the terrace area. Both fences would comply with the applicable fence requirements.

Specific Development Standards – Not applicable

PO Pedestrian Oriented Overlay District Standards – Requires variance(s)

- Drive-through facilities, automobile services uses, transportation uses, self-service storage, commercial parking lots (including the expansion of any existing commercial parking lot), and the conversion of any accessory parking lot to a commercial parking lot are prohibited. These uses are not proposed.

- The first floor of the building must be located within eight feet of a lot line adjacent to a street unless a greater yard is required. The building would be located more than 8 feet from the lot line adjacent to Cedar Avenue, 22nd Street, and Snelling Avenue. A variance of this overlay district standard is requested.
- Amenities are required between the building and lot lines adjacent to a street. The areas between the building and the adjacent streets would be landscaped. A connection from Cedar Avenue to the bike trail would also be provided.
- At least one principal entrance must face the street. A principal entrance would face Snelling Avenue.
- Awnings and canopies are encouraged, but backlit awnings and canopies are prohibited. No awnings or canopies are proposed. However, the main entrance would be sheltered under the projection of the upper floors above the entrance landing the accessible ramp leading to the entrance.
- Pole signs, backlit awning and canopy signs, and backlit insertable panel projecting signs are prohibited. These types of signs are not proposed.
- Accessory parking is required to be located at the rear or interior of the site, within the building, or entirely below grade. All parking would be enclosed in the building.
- Parking lots are limited to 60 feet of street frontage. A parking lot is not proposed.
- A minimum floor area ratio of 1.0 is required for new development. The proposed development would have an FAR of 2.06.

3. Conformance with the applicable policies of *The Minneapolis Plan for Sustainable Growth*.

The Minneapolis Plan for Sustainable Growth identifies the site as mixed use on the future land use map. The proposed development would be consistent with the comprehensive plan. In addition to the principals and policies identified in the conditional use permit sections of this staff report, the following apply:

Land Use Policy 1.3: Ensure that development plans incorporate appropriate transportation access and facilities, particularly for bicycle, pedestrian, and transit.

- 1.3.1 Require safe, convenient, and direct pedestrian connections between principal building entrances and the public right-of-way in all new development and, where practical, in conjunction with renovation and expansion of existing buildings.
- 1.3.2 Ensure the provision of high quality transit, bicycle, and pedestrian access to and within designated land use features.

Transportation Policy 2.3: Encourage walking throughout the city by ensuring that routes are safe, comfortable, pleasant, and accessible.

- 2.3.2 Identify and encourage the development of pedestrian routes within Activity Centers, Growth Centers, and other commercial areas that have superior pedestrian facilities.

Urban Design Policy 10.6: New multi-family development or renovation should be designed in terms of traditional urban building form with pedestrian scale design features at the street level.

- 10.6.1 Design buildings to fulfill light, privacy, and view requirements for the subject building as well as for adjacent properties by building within required setbacks.
- 10.6.4 Orient buildings and building entrances to the street with pedestrian amenities like wider sidewalks and green spaces.

- 10.6.5 Street-level building walls should include an adequate distribution of windows and architectural features in order to create visual interest at the pedestrian level.
- 10.6.6 Integrate transit facilities and bicycle parking amenities into the site design.

Urban Design Policy 10.9: Support urban design standards that emphasize traditional urban form with pedestrian scale design features at the street level in mixed-use and transit-oriented development.

- 10.9.1 Encourage both mixed-use buildings and a mix of uses in separate buildings where appropriate.
- 10.9.2 Promote building and site design that delineates between public and private spaces.
- 10.9.3 Provide safe, accessible, convenient, and lighted access and way finding to transit stops and transit stations along the Primary Transit Network bus and rail corridors.
- 10.9.4 Coordinate site designs and public right-of-way improvements to provide adequate sidewalk space for pedestrian movement, street trees, landscaping, street furniture, sidewalk cafes and other elements of active pedestrian areas.

Urban Design Policy 10.19: Landscaping is encouraged in order to complement the scale of the site and its surroundings, enhance the built environment, create and define public and private spaces, buffer and screen, incorporate crime prevention principles, and provide shade, aesthetic appeal, and environmental benefits.

4. Conformance with applicable development plans or objectives adopted by the City Council.

The proposed development would be in conformance with the *Franklin-Cedar/Riverside Area Master Plan*. The plan recommends that a transit-oriented use, including higher density housing, be developed on this site. The plan also calls for an urban form of development near station areas that provides an attractive pedestrian environment, facilitates bus, vehicle, and bicycle traffic, and promotes community values.

5. Alternative compliance.

The Planning Commission or zoning administrator may approve alternatives to any site plan review requirement upon finding that the project meets one of three criteria required for alternative compliance. Alternative compliance is requested for the following requirements:

- **Building Placement.** The building would face three streets. The first floor building wall would be set back 10 to 31 feet from Snelling Avenue, more than 20 feet from 22nd Street, and over 10 feet from Cedar Avenue. Alternative compliance is requested to allow the first floor of the building to be set back more than 8 feet from each street. A variance of the PO Pedestrian Oriented Overlay District standards is also required to allow the proposed setbacks. The subject site is odd-shaped. There are also significant grade changes adjacent to Cedar Avenue and 22nd Street. The Cedar Avenue interior boulevard is also over 60 feet wide. The building would reinforce the street wall along Snelling Avenue by following the curvature of the street. The front of the proposed building and the main entrance would be oriented to Snelling Avenue, which is consistent with how other the other residential buildings are oriented on the block. Further, the columns of the covered walkway and entrance landing would be 3.5 feet from the Snelling Avenue lot line. The subject site is located in an area that has been historically industrial and does not exhibit traditional commercial character. An abundant amount of windows would be provided on each building wall facing a street to maximize natural surveillance and visibility. For these reasons, staff is recommending that the Planning Commission grant alternative compliance. Staff is also recommending approval of the PO overlay district standard variance.

- **Windows.** Windows exceeding the minimum requirements would be provided on each floor of the building facing an adjacent street or public pathway, with the exception of the first floor level facing 22nd Street. Only 22 square feet of the first floor windows, or 4.4 percent of the wall area, would be located between 2 and 10 feet above the adjacent grade. The grade is lowest on the north side of the building. Most of the wall area measured between 2 and 10 feet above grade is adjacent to the lower parking level. A few small windows are proposed on the parking garage level. Providing larger windows on that level would not result in more natural surveillance opportunities. As an alternative, the applicant is proposing to provide an abundant amount of windows (26.7 percent of the wall area of each floor) in the dwelling units located on floors above the parking garage level. For these reasons, staff is recommending that the Planning Commission grant alternative compliance.

- **Ground floor active functions.** More than 70 percent of the first floor building frontage facing Snelling Avenue and Cedar Avenue would contain active functions. The ground floor facing 22nd Street would contain 0 percent active functions due to the grade change on the north end of the site that exposes the lower level walls of the parking garage. The ground floor facing the Hiawatha Bike Trail would contain 67.5 percent (129 feet) active functions. The first floor parking level would occupy the remainder of the frontage. The building wall would be setback 8 feet from the lot line adjacent to the bike trail. In between the building and lot line, shrubs would be planted at the base of the building to screen the parking garage walls. Likewise, the north wall would be setback more than 20 feet from 22nd street with landscaping screening the base of wall. The walls above the parking garage walls would be adjacent to dwelling units with an abundant amount of windows. Further, an excessive amount of parking is not proposed. For these reasons, staff is recommending that the Planning Commission grant alternative compliance.

RECOMMENDATIONS

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt staff findings for the applications by Seward Redesign Inc. for the properties located at 2200-2218 Snelling Ave:

A. Conditional Use Permit to allow a multiple-family dwelling.

Recommended motion: **Approve** the application for a conditional use permit to allow a multiple-family dwelling with 121 units, subject to the following conditions:

1. The conditional use permit shall be recorded with Hennepin County as required by Minn. Stat. 462.3595, subd. 4 before building permits may be issued or before the use or activity requiring a conditional use permit may commence. Unless extended by the zoning administrator, the conditional use permit shall expire if it is not recorded within two years of approval.
2. At least 4 short-term bicycle parking spaces shall be provided adjacent to the Snelling Avenue main entrance.

B. Conditional Use Permit to increase maximum height.

Recommended motion: **Approve** the application for a conditional use permit to increase the maximum allowed height of a building from 4 stories to 5 stories and from 56 feet to 62 feet, subject to the following conditions:

1. The conditional use permit shall be recorded with Hennepin County as required by Minn. Stat. 462.3595, subd. 4 before building permits may be issued or before the use or activity

requiring a conditional use permit may commence. Unless extended by the zoning administrator, the conditional use permit shall expire if it is not recorded within two years of approval.

C. Variance of overlay district standards.

Recommended motion: **Approve** the application for a variance of the PO Overlay District standards to allow the first floor of the building to be located more than 8 feet from a lot line adjacent to a street (Cedar Avenue, 22nd Street, and Snelling Avenue).

D. Variance of the southwest minimum side yard requirement.

Recommended motion: **Approve** the application for a variance to reduce the minimum interior side yard requirement adjacent to the west lot line from 13 feet to 8 feet to allow the building.

E. Variance of the southeast minimum side yard requirement.

Recommended motion: **Approve** the application for a variance to reduce the minimum interior side yard requirement adjacent to the south lot line from 5 feet to 0 feet to allow a transformer and a generator.

F. Variance of the sign requirements on a non-primary building wall.

Recommended motion: **Approve** the application for a variance to allow signage on a non-primary building wall and increase the maximum allowed sign area on that building wall from 0 square feet to 20 square feet, and to **deny** the variance to increase the maximum allowed height from 28 feet to 36 feet for a projecting sign on the wall facing the Hiawatha Bike Trail, subject to the following conditions:

1. Approval of the sign permit by the Department of Community Planning and Economic Development.

G. Variance of the projecting sign size requirement.

Recommended motion: **Deny** the application for a variance to increase the maximum allowed size of a projecting sign from 20 square feet to 48 square feet to allow a projecting sign facing Snelling Avenue.

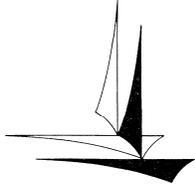
H. Site Plan Review for a new 5-story residential building.

Recommended motion: **Approve** the application for a site plan review application to allow a 5-story building, subject to the following conditions:

1. Each building elevation shall comply with the City's standards for the maximum percentage of stucco and burnished concrete masonry units allowed.
2. All mechanical equipment shall be screened as required by section 535.70 of the zoning code. Ventilation louvers for HVAC units shall also be painted to match the adjacent exterior material.
3. Department of Community Planning and Economic Development staff review and approval of the final building elevations, floor, site, lighting and landscape plans.
4. Site improvements required by Chapter 530 or by the City Planning Commission shall be completed by May 23, 2018, unless extended by the Zoning Administrator, or the permit may be revoked for non-compliance.

ATTACHMENTS

1. PDR report
2. Zoning map
3. Written description and findings submitted by applicant
4. Aerials
5. Site survey
6. Site plan
7. Civil drawings
8. Landscape plan
9. Floor plans
10. Building elevations
11. Renderings
12. Materials
13. Photos
14. Travel demand management plan approved copy



Minneapolis Development Review
 250 South 4th Street
 Room 300
 Minneapolis, MN 55415

Preliminary Development Review Report

Development Coordinator Assigned: **DONALD ZART**
(612) 673-2726
don.zart@minneapolismn.gov

Status *
RESUBMISSION REQUIRED

Tracking Number:	PDR 1001418
Applicant:	SEWARD REDESIGN INC 2619 FRANKLIN AVE MINNEAPOLIS, MN 55406
Site Address:	2206 SNELLING AVE 2200 SNELLING AVE 2210 SNELLING AVE 2214 SNELLING AVE 2218 SNELLING AVE
Date Submitted:	23-MAR-2016
Date Reviewed:	01-APR-2016

Purpose

The purpose of the Preliminary Development Review (PDR) is to provide Customers with comments about their proposed development. City personnel, who specialize in various disciplines, review site plans to identify issues and provide feedback to the Customers to assist them in developing their final site plans.

The City of Minneapolis encourages the use of green building techniques. For additional information please check out our green building web page at: http://www.ci.minneapolis.mn.us/mdr/GreenBuildingOptions_home.asp.

DISCLAIMER: *The information in this review is based solely on the preliminary site plan submitted. The comments contained in this report are preliminary ONLY and are subject to modification.*

Project Scope

121 market rate rental apartments and a public connection to the LRT bike / ped trail.

Review Findings (by Discipline)

Zoning - Planning

- Please continue to work with Janelle on the Land Use application process.

*Approved: You may continue to the next phase of developing your project.
 *Resubmission Required: You cannot move forward or obtain permits until your plans have been resubmitted and approved.

□ Addressing

- The proposed address will be 2214 Snelling Ave
- Per City of Minneapolis Street Naming and Address Standard V1.22, the City of Minneapolis holds authority for assignment of all addresses, verification, change, and/or additions. Contact Development Coordinator for new address assignment. Each assigned address number uses the street that provides the best/direct access for life safety equipment and best/direct access to the occupants.
- When assigning suite sequences the following guidelines are as follows:
 - The first one to two digits of the suite sequence number will designate the floor number of the site.
 - The last two digits of the suite sequence number will designate the unique ID for the unit (condo, suite, unit, or apartment).
 - Suite sequence digit numbers will be assigned to dwelling, commercial and retail units, not common areas. For example, laundry rooms, saunas, workout rooms, etc., would not be assigned numbers.
 - Please provide each condo, suite, unit or apartment number.
- This building is also considered to have a parking ramp per MCO Chapter 108. As such, within 5 years of the date of the certificate of occupancy being issued, the parking ramp will be required to have annual inspections and apply for a Ramp Operating Certificate.

□ Parks - Forestry

- Contact Craig Pinkalla (cpinkalla@minneapolisparcs.org), Telephone (612)-499-9233 regarding removal or protection of trees during construction in the city right of way.
- Require diversity component in ROW. minimum (2) genera
- include alternate for Maple in ROW - overrepresented
- Include planting detail - Detail Plate available in city standards
- Overrepresented Genera - avoid planting - Seward
 - Hackberry 11%
 - Linden 18%
 - Maple 21%
- Effective January 1, 2014, the City of Minneapolis and the Minneapolis Park and Recreation Board adopted an update to the existing Parkland Dedication Ordinance.
- The adopted City of Minneapolis Parkland Dedication ordinance is located in Section 598.340 of the City's Land Subdivision ordinance:
- <http://library.municode.com/index.aspx?clientId=11490>
- As adopted, the fee in lieu of dedication for new residential units is \$1,521 per unit (affordable units excluded per ordinance) and for commercial and industrial development it is \$202.80 per development employee (as defined in ordinance). Any dedication fee (if required) must be paid at the time of building permit issuance. There is also an administration fee that is 5% of the calculated park dedication fee.
- As proposed, for your project, The Bessemer at Seward, the calculated dedication fee is as follows:

Residential - 121 x \$1521	=	\$184,041
Administrative Fee - 5% of dedication fee, capped at \$1,000	=	\$ 1,000
Total	=	\$185,041
- This is a preliminary calculation based on your current proposal; a final calculation will be made at the time of building permit submittal.
- The contact with the Park Board to discuss the dedication of land in lieu of the fee is Adam Arvidson, (612)230-6470.
- For further information, please contact Don Zart at (612)673-2726

❑ Business Licensing

- There is not a requirement for Business License review at this time.

❑ Right of Way

- An encroachment permit shall be required for all streetscape elements in the Public right-of-way such as: plants & shrubs, planters, tree grates and other landscaping elements, sidewalk furniture (including bike racks and bollards), and sidewalk elements other than standard concrete walkways such as pavers, stairs, raised landings, retaining walls, access ramps, and railings (NOTE: railings may not extend into the sidewalk pedestrian area). Please contact Bob Boblett at (612) 673-2428 for further information.
- Note to the Applicant: Any elements of an earth retention system and related operations (such as construction crane boom swings) that fall within the Public right-of-way will require an encroachment permit application. If there are to be any earth retention systems which will extend outside the property line of the development then a plan must be submitted showing details of the system. All such elements shall be removed from the Public right-of-way following construction with the exception of tie-backs which may remain but must be uncoupled and de-tensioned. Please contact Bob Boblett at (612) 673-2428 for further information.
- In addition, any elements of an earth retention system and related excavations that fall within the Public right-of-way will require a "Right-of-Way Excavation Permit". This permit is typically issued to the General Contractor just prior to the start of construction. However, it is the Applicant's responsibility to insure that all required permits have been acquired by its consultants, contractors, sub-contractor's prior to the start of work.
- Property lines and related area boundaries including the part to be vacated should be better identified on all plan sheets.

❑ Street Design

- All driveway aprons shall be designed and constructed to City standards. All driveway aprons shall be shown graphically correct on all related plan sheets. Please refer to the following: http://www.minneapolismn.gov/publicworks/plates/public-works_road. The Minneapolis Standard Plate ROAD-2003 provided on Sheet C9.02 has been updated and shall be replaced with ROAD-2003-R1.
- Note to the Applicant: The roadway alignment, sidewalk, and curbs depicted in the site plan shall be coordinated with Hennepin County as much as possible; please contact Josh Potter - Hennepin County at (612) 596-0820.

❑ Sidewalk

- The plan as submitted meets the requirements of the Public Works Sidewalk Inspections Division.
- Note to the Applicant: Any concrete infrastructure in the public right of way that is currently defective, or that is damaged during construction, must be removed and replaced during the time of site development work.

❑ Water

- All existing and proposed underground Public Utilities (water, sanitary sewer, and storm drain) shall be shown on the site plan with corresponding pipe sizes and types. For Public watermain infrastructure records call (612) 673-2865. Any existing water service connections to the site shall be noted on the plans for removal, and shall be removed per the requirements of the Utility Connections Department before any new service lines can be installed, call (612) 673-2451 for more information.
- Meters shall be set at the point where the service line enters the building and shall be set in a location where it is easily assessable. Domestic service lines larger than three inches require a Bypass Assembly (see attachment). Please contact Rock Rogers at (612) 673-2286, to review domestic and fire service design, connections, and sizes.
- The site plan includes notes related to abandonment of existing watermains; these notes would appear to conflict with current as-built information related to the site. Please contact Rock Rogers at (612) 673-2286, to review as-built information.

□ Historical Preservation Committee

- There is no preservation flag on this property. However there is a preservation review required as part of the process for the wrecking permit.

□ Traffic and Parking

- The nature of the proposed development is such that traffic impacts will be an issue; please contact Allan Klugman at (612) 673-2743 to discuss the requirements of a Travel Demand Management Plan (TDMP).
- The Applicant shall note the location of any existing Metro Transit "bus stops" on the site plan.
- Current ordinance states that all maneuvers associated with loading, parking or sanitation pick up for a private development shall occur on private property. Please provide a narrative explaining the trash removal operations and show turning maneuvers for all truck type vehicles that will be using the loading dock/parking entrance areas. Per City Ordinance the Applicant shall provide for (and identify) a solid waste collection point (SWCP) on the site plans. The location of the SWCP is subject to the review and approval of the Public Works Department.
- All street lighting (existing and proposed) shall be shown clearly on the site plan. Prior to site plan approval, the Applicant shall contact Joseph Laurin at 612-673-5987 to determine street lighting requirements. Note: If decorative street lighting exists on the proposed site it must be preserved or replaced at existing levels.
- Note to the Applicant: In addition to the various required construction permits, impacts to existing traffic signal and street lighting systems (including installation of new street lights) will require the Applicant and respective Contractors to enter into a separate Right-of-Way Excavation Permit (including Testing and Inspection requirements) with the Public Works Department, for further information regarding this permit please coordinate with Paul Miller at (612) 673-3603.
- Ramp slopes for all vehicle access points shall be less than a 4 percent slope for the first 20' from the property line; show ramp slope on the site plans.
- Public Works would recommend a different design for the trail ending point where it connects to the Public sidewalk at E. 22nd St. The current design puts one trail end next to the driveway that will not leave enough site distance for cars exiting the parking ramp. The Applicant shall consider having only one connection at the corner of E. 22nd St. and Cedar Ave. S.
- Note to the Applicant: The construction of this development will likely require the use of Public right-of-way (roadway and sidewalks) for construction purposes. A request for an estimate of street use and obstruction permit fees can be made to the City's Traffic Department; please contact Scott Kramer at (612) 673-2383 for further information.
- Note to the Applicant: Please add the following notes to the site plan:
 - Street lighting installed as part of the Project shall be inspected by the City. Contractors shall arrange for inspections with the Traffic Department, please contact Dave Prehall at (612) 673-5759 for further information. Any lighting installations not meeting City specifications will be required to be reinstalled at Owner expense.
 - An obstruction permit is required anytime construction work is performed in the Public right-of-way. Please contact Scott Kramer at (612) 673-2383 regarding details of sidewalk and lane closures. Log on to <http://minneapolis.mn.roway.net/> for a permit.
 - Contact Allan Klugman at (612) 673-2743 prior to construction for the temporary removal/temporary relocation of any City of Minneapolis lighting or traffic signal system that may be in the way of construction.
 - All costs for relocation and/or repair of City Traffic facilities shall be borne by the Contractor and/or Property Owner.
 - Contact Doug Maday at (612) 673-5755 prior to construction for the removal of any City of Minneapolis right of way signs that may be in the way of construction.

□ Fire Safety

- Provide required fire suppression system throughout building.

- Fire department connection must be located on the address side of building and within 150 feet of a fire hydrant.
- Provide required fire alarm system throughout building.
- Provide and maintain fire apparatus access at all times.

□ Sewer Design

Groundwater:

- Please identify if pumping of groundwater is anticipated in order to complete the below grade portions of the building dry after completion of construction. If not, please clearly indicate this on the plans.

Stormwater Management:

- Please include the stormwater management report on the electronic versions of the submittal materials.
- The Stormwater Design memo references bioretention basins. It does not appear bioretention basins are proposed in the plans, please verify.
- The P8 model uses a Precipitation Scale Factor of 1.25, but the model simulation dates are for a 2-year period. A single type-2 storm event should be used for a 1.25" rain event.
- Please add the following note to the appropriate plan sheet: The Contractor, property owner or responsible party shall contact Minneapolis Surface Waters and Sewers 48 hours prior to any excavation or construction related to or in the location of the proposed Stormwater Management BMP (Contact Paul Chellsen, 673-2406 or paul.chellsen@minneapolismn.gov).
- Please add the following note to the appropriate plan sheet: Upon the project's completion the General Contractor, Property Owner or Responsible Party shall provide to the Department of Public Works a Final Stormwater Management Report including record drawings. This report will serve as a means of verification that the intent of the approved stormwater management design has been met. This final report shall substantiate that all aspects of the original design have been adequately provided for by the construction of the project.
- An operations and maintenance plan is required for the stormwater treatment devices. The O&M plan shall define the maintenance regimen, including type and interval of maintenance and party to conduct such maintenance. Please provide a copy of the O&M Plan. The plan should be developed with the expectation that the future property manager will be able to reference it for all necessary inspection and maintenance activities of the stormwater management system.

Surface Drainage:

- Please clearly identify the proposed property lines on the plans. The current property lines, as drawn, indicate portions of the building and other site improvements are outside the property boundaries.

Utility Connections:

- It is preferred that the proposed sanitary sewer connection be made at the existing manhole just downstream of the current proposed connection location. Please evaluate if this is feasible, if not please contact Jeremy Strehlo, 612-673-3973, for further requirements for the proposed connection directly to the main. If connecting to the manhole, the invert of the service should be above the spring line of the outgoing sewer and below 2' above the crown of the outgoing sewer.

Utility:

- The linetypes and symbols used in the plans do not match those on the legend on Sheet G0.01. This appears to be the case for some utilities, survey lines and grading information.
- There is an existing sanitary sewer manhole within the boulevard of Snelling Avenue, near the 22nd St E intersection, that does not appear to be shown on the plans.

Non Stormwater Discharges:

- Detail all mechanical and non-stormwater discharges. Non-stormwater discharges are not permitted unless approved by the City of Minneapolis. Non-stormwater discharges not declared and approved will not be permitted. If there currently are none and nothing is proposed declare this status on the plans.

For comments or questions on Public Works Surface Water & Sewers Division related requirements please contact Jeremy Strehlo, (Professional Engineer) at (612) 673-3973, or jeremy.strehlo@minneapolismn.gov

□ Environmental Health

- Several of the parcels on this block have been in the Minnesota Pollution Control Voluntary Investigation and Cleanup Program. This project was not included in a prior review it should enter the VIC program. The Bystrom Brothers at 2200 Snelling Avenue has 3 aboveground storage tanks that will need to be removed if this portion is to be redeveloped for residential use. If impacted soil is encountered during site activities that was not included in a site assessment and approved work plan work will need to stop and notification provided to the MN State Duty officer at (615) 649-5451. Approval for removal, disposal and/or reuse of impacted soils must be must occur from the MCPA and the City of Minneapolis prior to continuing excavation activities.
- If dewatering is required during site construction see below for city permit requirements. Subgrade structures should be designed to prevent infiltration of groundwater without the need for a permanent dewatering system being installed. If a continuously operating permanent dewatering system is needed it must be approved as part of the sanitary sewer and storm drain site plan approval prior to construction beginning.
- No construction, demolition or commercial power maintenance equipment shall be operated within the city between the hours of 6:00 p.m. and 7:00 a.m. on weekdays or during any hours on Saturdays, Sundays and state and federal holidays, except under permit. Contact Environmental Services at 612-673-3867 for permit information.
- Permits and approval are required from Environmental Services for the following activities: Temporary storage of impacted soils on site prior to disposal or reuse; Reuse of impacted soils on site; Dewatering and discharge of accumulated storm water or ground water, underground or aboveground tank installation or removal, well construction or sealing. Contact Tom Frame at 612-673-5807 for permit applications and approvals.
- A review of the project, permits issued and an inspection from Environmental Service for identification of equipment and site operations that require annual registration with the City of Minneapolis will occur for this project.

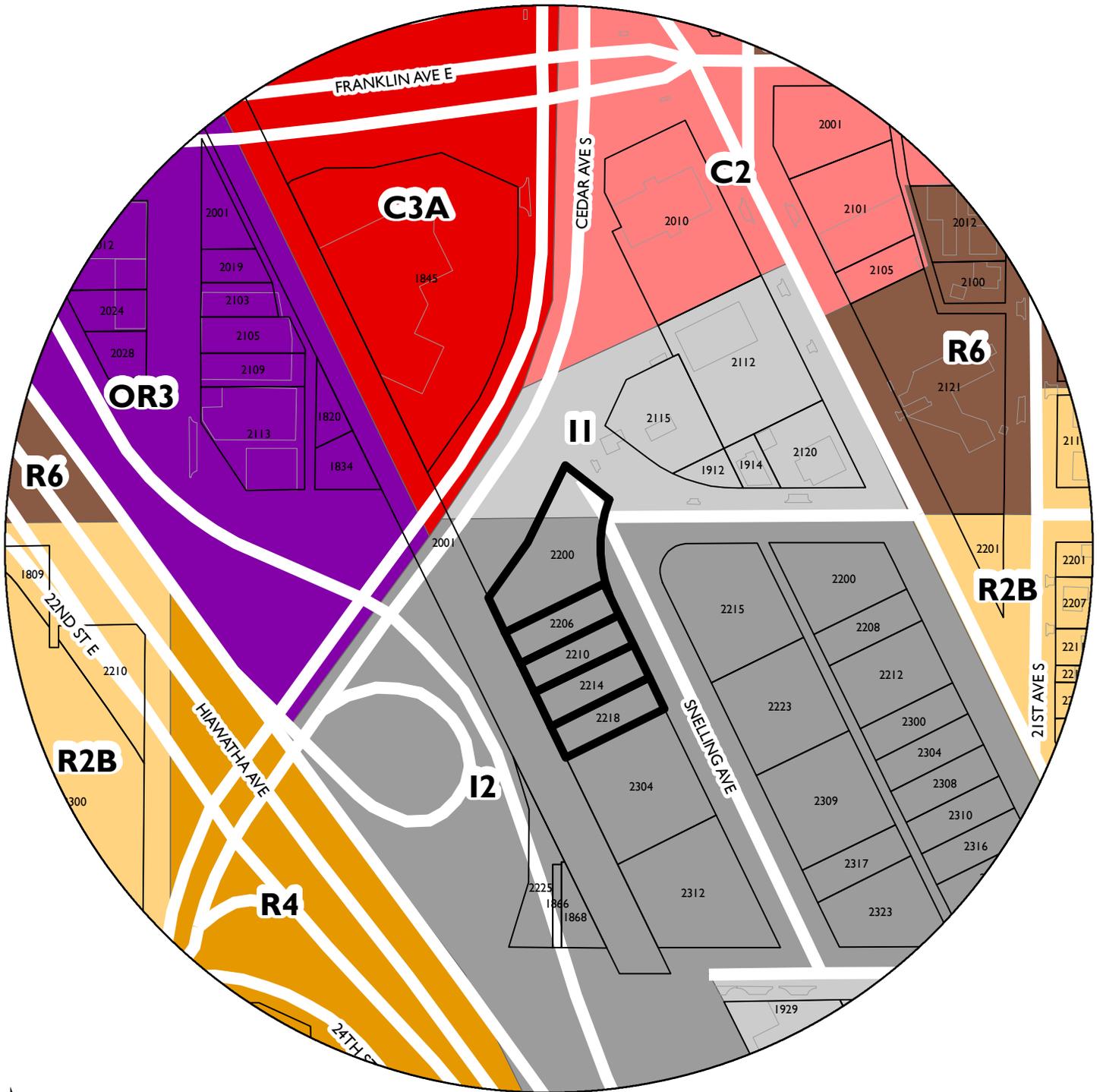
□ Construction Code Services

- Building Code Plan Review comments for PDR. This is not intended as a complete code review, but to confirm basic design elements for exterior building code items.
- No building code issues are observed in these plans.
- The following are typical code comments;
 - Exterior Accessible routes shall consist of walking surfaces or slopes not steeper than 1:20. IBC 1104.1
 - Accessible parking spaces shall be located per MN 1341.1106.6
 - Accessible parking spaces and access aisles to be 96" (8') width 502.4.2
 - Access aisles are to be marked "no parking" per 502.4.4 ANSI A117.1 2003
 - Accessible curb ramps as per MN 1341.0406
 - Maximum slope 1:48 for access aisle and parking space per 502.5
 - Accessible signage to comply with MN 1341.1110

END OF REPORT

NAME OF APPLICANT

WARD



PROPERTY ADDRESS
2200-2218 Snelling Avenue

FILE NUMBER
BZZ-7677

The Bessemer at Seward

Seward Commons Phase III

Project Description

Seward Commons is a master-planned phased redevelopment of the 4-acre former Bystrom Brothers Machining site in the Franklin LRT Station Area. Seward Redesign acquired the site in 2009 with financing provided by BMO Harris Bank, City of Minneapolis, Hennepin County, Minnesota Housing and Metropolitan Council. The purpose of the redevelopment is to transform the Franklin Station from a blighted, industrial area to a higher-density, transit-oriented housing development. Phase I consists of 40 units of supportive housing for the chronically mental ill, and was completed in partnership with Touchstone Mental Health and Project for Pride in Living. Phase II of is a 60-unit apartment building for low-income seniors, and was completed in partnership with CommonBond Communities.

The Bessemer (Phase III of Seward Commons) will be the first new construction multifamily development in the neighborhood in over 35 years to have no income limits. This will bring some balance to the mix of incomes at Seward Commons and to the census tract, which is overwhelmingly low income and includes a high proportion of rental units subject to income limits. The Bessemer will be both transit and bike oriented, with high level bicycle amenities, and direct access to the Light-Rail Transit pedestrian/bike path. The project consists of 121 apartments in a 5 story structure with underground parking.

The site currently has 3 one-story industrial buildings. Two were built in the 1940s and 1950s, and the center building was added as warehouse in 1994. They are basic block structures with outdated systems. Currently all buildings are occupied by interim tenants at below market rates, including warehouse, machine shop and vehicle storage. Tenants will be relocated to accommodate development. The buildings are functionally and physically obsolete as well as being an aesthetic detraction for transit users accessing the station via the Bike/Ped path.

Type and size of buildings (# of floors, units, etc.)

One building with underground parking with 5 stories of housing for a total of 119 units.

Type of construction and materials

The building will be four stick built stories on top of a concrete ground floor and underground parking garage.

Size of units (sq. ft.) and number of bedrooms

26 Studios 476 sf

64 1-Bedrooms 672 sf

31 2- Bedrooms 952 sf

Description of building/unit amenities

The units will have 9 foot ceilings and include in-unit washers and dryers, dishwashers, oversize windows, balconies where appropriate, walk in closets and high-quality cabinetry. The building will offer underground parking, bicycle storage room with repair station, fitness room, community room, and wireless internet in common areas. Units on the ground floor on the trail side will separate entrances with dedicated outdoor space.

CONDITIONAL USE PERMITS – answers apply to both

1: Residential Use in an ILOD and

2: Increasing Maximum Height

(1) The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, comfort or general welfare.

This project is an ideal fit for the developing dense residential character of Seward Commons, which takes advantage of the LRT amenity and aims to increase ridership. The project will increase “eyes on the street” at the LRT path approaching the Franklin station, particularly after dark, and will improve public safety.

(2) The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.

The project is providing adequate underground parking with access designed to keep traffic off of Snelling Avenue as much as possible, which is used for occasional truck access to remaining industrial businesses. Overall the nearby sites are gradually moving toward more TOD style uses, including creative office, urban farming, small artisan manufacturing, and health and wellness. This development will complement those uses.

(3) Adequate utilities, access roads, drainage, necessary facilities or other measures, have been or will be provided.

The site will be regraded and paved where needed and utilities will be upgraded to meet the new development’s needs.

(4) Adequate measures have been or will be taken to minimize traffic congestion in the public streets. Access to most of the parking will be from 22nd Street, which will keep traffic off the narrower Snelling Avenue which must continue to accommodate truck traffic.

(5) The conditional use is consistent with the applicable policies of the comprehensive plan.

This project fulfills the longstanding desire of the neighborhood, reiterated in several small area plans and the comprehensive plan, to see increased density, income diversity and TOD style development near the Franklin Station.

(6) The conditional use shall, in all other respects, conform to the applicable regulations of the district in which it is located.

It shall.

2 - Increasing Maximum Height - Additional Height Questions

(1) Access to light and air of surrounding properties.

This development will slightly shade the single story industrial building to the east. Redesign, the developer, owns that building and plans to redevelop it into more multifamily housing in the near future.

(2) Shadowing of residential properties, significant public spaces, or existing solar energy systems.
None.

(3) The scale and character of surrounding uses.

This development is appropriate related to the 4 story buildings to the south as the peak height for the overall Seward Commons redevelopment, which will be developing the adjacent parcels as well. This height maximizes desirable views of downtown and sends a highly visible signal to the community about density in LRT station areas that we hope will encourage density along the line moving forward. The proposed height ranges from approximately 58' on the south edge to 68' on the north edge due to the substantial grade change across the parcel.

(4) Preservation of views of landmark buildings, significant open spaces or water bodies.
None.

VARIANCES – answers apply to both

3 – Increase Setback along Snelling Avenue, Cedar and 22nd

4 - Setback from LRT Trail on west side to less than 13 feet

(1) Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.

The parcel where the building will be built is uniquely shaped in that it has three frontages on public streets as well as frontage along the LRT path. It also has a curved street frontage along Snelling. Our building comes as close to complying as is feasible, with an emphasis on the Snelling frontage with a façade that has a bend in it to mirror the street curvature, and a close approach to 22nd Street.

(2) The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.

A compromise between the three setbacks is achieved with our design, which intentionally aligns with the Phase II project south on Snelling to create a cohesive streetscape that works with the site's grade change and balances between treating the trail as a secondary frontage (2), maximizing density, providing green space and public access to the LRT path, and compromising between the three setback requirements on the north end of the site.

(3) The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.

This variance allows us to continue the transformation of this area to fulfill neighborhood planning that was begun by Phase I and Phase II, and will greatly enhance the connection of the Seward neighborhood to the LRT station. This building's setbacks intentionally reflect the Phase I and Phase II projects to the south, also developed by Seward Redesign.

5 – Sideyard reduction for Transformer

(1) Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.

In order to provide a new transformer without impeding pedestrian traffic and to be within closest proximity to the basement electrical room, the most practical location is to place the transformer between the two service drives serving Phase II and this project. The space the transformer fits in this arrangement requires reduction of the sideyard requirement for several feet.

(2) The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.

Our goal is to keep the curb cuts to a minimum and as close together as possible in order to create certainty and visibility for pedestrians on Snelling and to limit the total width of the area where auto access will occur.

(3) The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.

This variance keeps the driveway areas to a minimum and will not inhibit views from units in either building, whose windows are above the height of the transformer. This location is not intended for any use other than the fenced dog run, which is accommodated in this design adjacent to the transformer. The transformer will be screen with trees, while still maintaining the necessary clearances.

SIGN VARIANCE: *In addition, the following findings must be addressed if applying for a sign variance:*

(1) The sign adjustment will not significantly increase or lead to sign clutter in the area or result in a sign that is inconsistent with the purpose of the zoning district in which the property is located.

Sign #2 is planned for the west elevation of the building facing the LRT trail. It is intended for wayfinding for users of transit and the path, and there is minimal signage anywhere nearby to assist bicyclists and LRT riders in identifying their location as this is considered by the city to be the "back" of the building. The Cooperage has a blade sign to the south of this project, but it is much lower on the building. Ours will be similar in size to this one but will better serve the wayfinding function.

(2) The sign adjustment will allow a sign that relates in size, shape, materials, color, illumination and character to the function and architectural character of the building or property on which the sign will be located.

It will. We plan on metal signs with cut out letters illuminated from the interior, similar to those on the Cooperage to the south that harken to the industrial history of the Seward Commons site along with other architectural, branding and design elements of the building. We hope to accompany the monument sign with some kind of metal public art piece that is to be designed, but has been conceived as an interpretation of our name "The Bessemer" which is informing our design choices.

STANDARD FINDINGS FOR SIGN VARIANCE

(1) Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.

The purpose of a sign is to create visibility for the property as well as to provide wayfinding signals for multi modal commuters passing by. We learned from the Cooperage that the sign requirements do not adequately allow either purpose to be served, given the LRT's speed as it passes and the need for cyclists to have visibility from further away. There is no worry about sign clutter in this area due to it being considered the "back" of most adjacent property, which increases the need for wayfinding. There is no economic consideration leading to this request.

(2) The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.

We believe that providing signage that intentionally treats the LRT and bicycle path as a "frontage" of the building meets the spirit of the regulations and will benefit multi modal commuters.

(3) The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.

For the reasons stated above, we believe that this will improve wayfinding for cyclists and LRT riders, making it easier for people to use transit to get to our building, which is one of the purposes of this transit oriented development project. As previously mentioned, there is no sign clutter in the area, in fact there is really a lack of helpful signage near the station area to orient transit and bicycle path users.

The Bessemer at Seward

Seward Commons Phase III

Changes to the Project Following Committee of the Whole

Schafer Richardson, Seward Redesign, and LHB presented The Bessemer at Seward to the Committee of the Whole on February 11, 2016. We modified a few design elements that responded to the Staff and Committee comments. Below is a summary of the changes:

- **Eliminated all the fiber cement panel and fiber cement siding.** Per staff comments, we reduced the number of primary materials from four to three. The building will now consist of brick, stucco, and burnished CMU. We also added 72 lineal feet of brick on the southwest elevation, spanning from the second to the fifth floor.
- **Changed the unit patio doors from an outswing door with accompanying window to a sliding glass door.** There will be better weatherproofing with one integrated patio glass system compared to the separate door and window system.
- **Switched from awning window with fixed panel to single-hung window with fixed panel.** This has minimal impact on the exterior.
- **Consolidated management of stormwater management system into one underground infiltration chamber.** We reduced the number of stormwater management areas from three areas (one underground storage chamber, one rain garden, and porous concrete) to one underground storage chamber on the west side of the building. This allowed us to have only one connection to the street, and provided improved access for future maintenance.

ADDITIONAL INFORMATION on SIGN VARIANCE 5.16.2016

With the understanding that the Industrial zoning only allows for a 20 square foot projecting sign rather than 48 square feet as originally believed, the following answers apply:

The images below show the neighboring Cooperage, which has a 20 sf projecting sign. Even from midway down the Phase III site on the bike trail, the sign disappears and is not functional for wayfinding even for a cyclist until you are right next to the building, not to mention LRT riders. It is not large enough to be visible from the Franklin LRT station for pedestrians wanting to be sure they're walking the right direction. Phase III will be slightly closer to the station area, but not so close that 20 sf is sufficient for wayfinding purposes.



View from Southwest



View from Northwest

(1) The sign adjustment will not significantly increase or lead to sign clutter in the area or result in a sign that is inconsistent with the purpose of the zoning district in which the property is located.

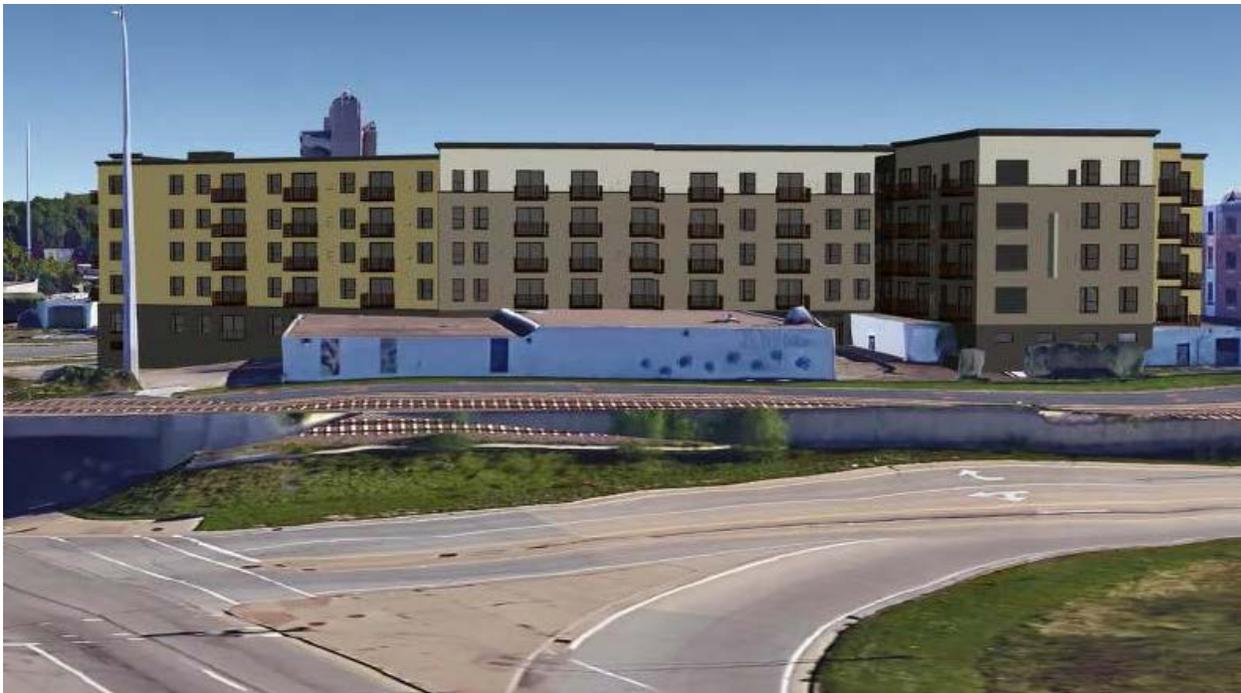
In the first two images, you can see that no other nearby signage is available to orient a cyclist, pedestrian or LRT rider. To the right is image of the Snelling avenue streetscape. There is no concern about sign clutter on this street and the mass of the buildings is large enough to support a more substantial sign size. The larger projecting sign size will also serve to attract attention to the main entrance of the building on Snelling, something we've been asked to improve.



an

(2) The sign adjustment will allow a sign that relates in size, shape, materials, color, illumination and character to the function and architectural character of the building or property on which the sign will be located.

We believe our proposed signage is appropriately sized for the scale, location, and function of the building, which is more in line with residential districts (and becoming moreso as Seward Commons develops) than with the underlying Industrial Zoning restrictions. The size and height of the building are depicted below, and the sign seems quite modestly sized with an appropriate height given the tall building, with little wayfinding in either direction.



STANDARD FINDINGS

(1) Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.

No new narrative.

(2) The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.

The restrictions of industrial zoning on allowable signage are not removed in an ILOD even though the use is allowable with a CUP. The intent of keeping signage to a minimum in industrial zoned areas seems counter to the goal of establishing a transit oriented redevelopment using the tool of the overlay district, and a variance would allow signage appropriate to the future identity of the area, and appropriate to the adjacent transit amenity.

(3) The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.

No new narrative.

Seward Commons is a master planned, phased development one block south of the Franklin LRT Station along the bicycle pedestrian path. Phase I and Phase II provide housing for 40 low income individuals with persistent mental health issues and 60 low income seniors. Phase III includes 121 rental apartments served by underground parking that will be made available without limits on residents' incomes, the first housing of its kind in this low income census tract in more than three decades.

The project design is heavily influenced by the grade change across the site and by the intent to be physically oriented toward the transit amenity, which we consider a second façade. The building consists of four and five stories of apartments, extensive bike parking facilities, community space, lobby, a fitness center, and underground parking.

The building will require two slight setback variances related to the dual façade – slightly larger on the street side and narrower on the trail side. There will be a third variance required to eliminate a landscaped area on the east edge of the south side of the property between the driveway for this property and the service driveway for the adjacent Cooperage.

Two conditional use permits will also be needed, one for a residential-only use in the Industrial Living Overlay District and one for a 5 story building, both of which are in the spirit of the planning with the city and the neighborhood for this site and the station area.

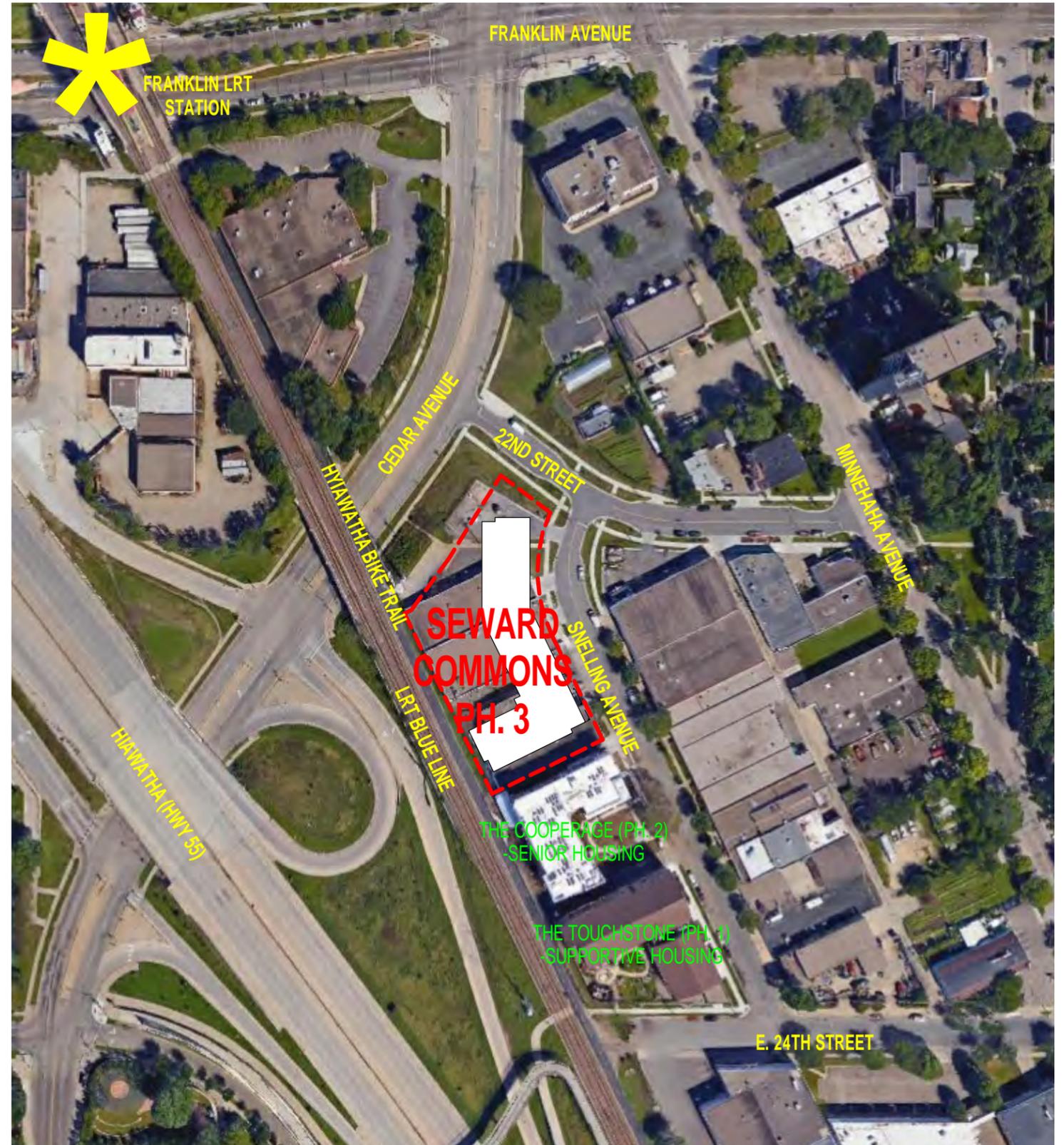
This project will provide a new housing opportunity for those who work along the Blue Line that is unique in the neighborhood. It will emphasize Seward values of sustainability and bicycle friendliness, and will set a high standard for design in the Seward Community.

FLOOR	PARKING STALL TYPE			TOTAL STALLS
	COMPACT	STANDARD	HANDICAPPED	
1ST		16	2	
BASEMENT	10	65	2	
TOTAL	10	81	4	95

FLOOR	BUILDING AREA	
	GROSS SF	
5TH	23910	
4TH	23910	
3RD	23910	
2ND	23910	
1ST	23559	
BASEMENT	23559	
TOTAL	142758	

FLOOR	DWELLING UNIT INFORMATION			TOTAL UNITS
	UNIT TYPE			
	EFFICIENCY	1BR	2BR	
5TH	6	14	7	
4TH	6	14	7	
3RD	6	14	7	
2ND	6	14	7	
1ST	2	8	3	
TOTAL	26	64	31	121

BUILDING HEIGHT	
Elevation	
T.O. HIGH PARAPET	158'-1 3/8"
T.O. LOW PARAPET	154'-5 3/8"
5TH FLOOR	142'-9 5/8"
4TH FLOOR	132'-3 3/4"
3RD FLOOR	121'-9 7/8"
2ND FLOOR	111'-4"
1ST FLOOR	100'-0"
BASEMENT	89'-10 3/4"





ABOVE: View of the southern portion of the site, 2214 and 2218 Snelling.

BELOW: View from the same location, looking toward the northern portion of the site, 2200 and 2210 Snelling

LEFT: All 3 existing warehouses will be demolished to make room for the new development





ABOVE: View of the northern portion of the site looking south along Cedar Avenue

BELOW: View looking north along the Hiawatha Bike Trail with the adjacent LRT line to the left

LEFT: All 3 existing warehouses will be demolished to make room for the new development



SEWARD COMMONS, LLC

2619 E. FRANKLIN AVE.
MINNEAPOLIS, MN 55406

MISCELLANEOUS NOTES

- MN 1** Legal description and easements, if any, per title commitment from Commonwealth Land Title Insurance Company, Commitment No. 237875, dated October 21, 2015. Note: Survey revised to show adjacent vacated right of way and this survey is subject to updated title commitment. Vacated right of way description from document number T05216920.
- MN 2** Property Address: 2200, 2206, 2210, 2214 and 2218 Snelling Ave., Minneapolis, MN
- MN 3** Area of Parcel = 54060 sq.ft (1.24 acres)
- MN 4** Benchmark: Top nut of hydrant located at East 22nd Street. Elevation = 840.90 feet
- MN 5** Property is in Flood Zone "X" (area of minimal flooding) per FEMA panel map number 27053CO3378E, dated September 2, 2004.
- MN 6** Above ground utility locations have been field located as shown. All underground utility services which serve the property have attempted to be field located through the services of Gopher State One Call per ticket number 115360196. However some of the utility companies failed to field locate underground utility locations. In those cases, utilities as shown are from available survey maps. Further information contact Gopher State One Call (651-454-0002 or 811)

Current Zoning Information

Source of Information: City of Minneapolis Zoning web site			
Address: 250 S. 4th St., Room 300 Minneapolis, MN 55415			
Phone: (612) 673-3000 or 311			
Zoning District(s): I-2			
Zoning Definition: Medium Industrial District with Industrial and Pedestrian Living Overlay			
Building Setback Requirements I-2			
	Observed	Required	Notes
Front Yard Setback	see survey	not listed	
Side Yard Setback	see survey	not listed	N/A
Rear Yard Setback	see survey	not listed	
Parking Tabulation			
	Observed	Required	Notes
Regular Spaces	not marked	see note	Parking requirements dependent on use of property and floor area
Handicapped Spaces	not marked	see note	
Total Parking Spaces	not marked	see note	
Height Restrictions			
	Observed	Max. Allowed	Notes
Maximum Height	not listed	not listed	
Bulk Restrictions			
	Observed	Required	Notes
Lot Area	xxxxx Sq. Ft.	not listed	
Coverage Ratio	0%	0%	
Other:	N/A	N/A	

NOTE: Because there may be a need for further interpretation of the applicable zoning codes, we refer you to the above referenced municipality and the applicable zoning codes.

Legal Description

Parcel 7: (2200 Snelling Avenue; PIN 36-029-24-22-0045)
All of Lot 2 and that part of Lot 1, Block 7, Westfall's Addition to Minneapolis, which lies Southeastly of a line drawn from a point on the Northernly line of said Lot 1, 85 feet Westerly from the Northeastly corner thereof to a point on the Southwestly line for said Lot 1 lying 10 feet Northwestly from the most Southernly corner thereof; together with vacated alley lying adjacent thereto measured at right angles thereto.

Parcel 8: (2206 Snelling Avenue; PIN 36-029-24-22-0046)
Lot 4, Block 7, Westfall's Addition to Minneapolis, including that part of the adjoining vacated alley lying between the Southwestly extensions across it of the Northwestly line of Lot 3 and the Westerly extension across it of the South line of Lot 3.

Parcel 9: (part of 2206 Snelling Avenue; PIN 36-029-24-22-0047)
Lots 5 and 6, Block 7, Westfall's Addition to Minneapolis, including that part of the adjoining vacated alley lying between the Southwestly extensions across it of the Northwestly line of Lot 4 and the Westerly extension across it of the South line of Lot 4.

Parcel 10: (2210 and 2214 Snelling Avenue; PIN 36-029-24-22-0048 and 36-029-24-22-0049)
Lots 5 and 6, Block 7, Westfall's Addition to Minneapolis, including that part of the adjoining vacated alley lying between the Southwestly extensions across it of the Northwestly line of Lot 5 and the Westerly extension across it of the South line of Lot 6.

and

That part of Snelling Avenue and 22nd Street East, as dedicated in the plat of WESTFALL'S ADDITION TO MINNEAPOLIS described as beginning at the northeast corner of Lot 1, Block 7, WESTFALL'S ADDITION TO MINNEAPOLIS, according to the recorded plat thereof; thence southeasterly along the northeasterly line of said Lot 7 a distance of 112.32 feet to a point of curve; thence northerly 165.21 feet along a curve concave to the east having a radius of 190.00 feet and a central angle of 49 degrees 49 minutes 17 seconds to the intersection with a line run parallel with and distant 80.00 feet southwestly of the following described "Line A"; thence northwestly along said parallel line distance of 80.00 feet; thence southwestly to a point on the north line of said Lot 1 distant 85.00 feet westerly of the northeast corner thereof; thence easterly along the north line of said Lot 1 a distance of 85.00 feet to the point of beginning. Said "Line A" is described as beginning at a point on the south line of Block 2, WESTFALL'S ADDITION TO MINNEAPOLIS, according to the recorded plat thereof, distant 182.30 feet easterly of the southwest corner thereof; thence westerly along the said south line a distance of 10.00 feet; thence northwestly 137.62 feet along a tangential curve concave to the northeast having a radius of 212.00 feet and a central angle of 37 degrees, 11 minutes, 40 seconds; thence northwestly, tangent to said curve, 132.00 feet to the southwestly line of said Block 2 and there terminating.

Note Corresponding to Schedule B, Part II

- ① Items 1 thru 11 - Are not a survey issue or not to our knowledge.
- ② Item 12 - Portion of Lot 1 condemned for Highway per doc. no. 2820147 as shown. Also remainder of Lot 1 no right of access to Highway.
- ③ Item 13 - Portion of Lot 1 taken for Highway per doc. no. 4886827 as shown. Also remainder of Lot 1 no right of access to Highway.
- ④ Item 14 - Is not a survey issue.
- ⑤ Item 15 - Property subject to redevelopment agreement per doc. no. 4651025 & 9373494.
- ⑥ Item 16 thru 22 - Are not survey issues.
- ⑦ Item 23 - Vacated street and easement area as described on City Resolution in documents 10140070 and 5216920 as shown hereon. Ownership of this area not clarified.
- ⑧ Item 24 - Temporary construction easement area as described in doc. no. 9662124 appears to have expired December 31, 2013.

ALTA/ACSM Land Title Survey

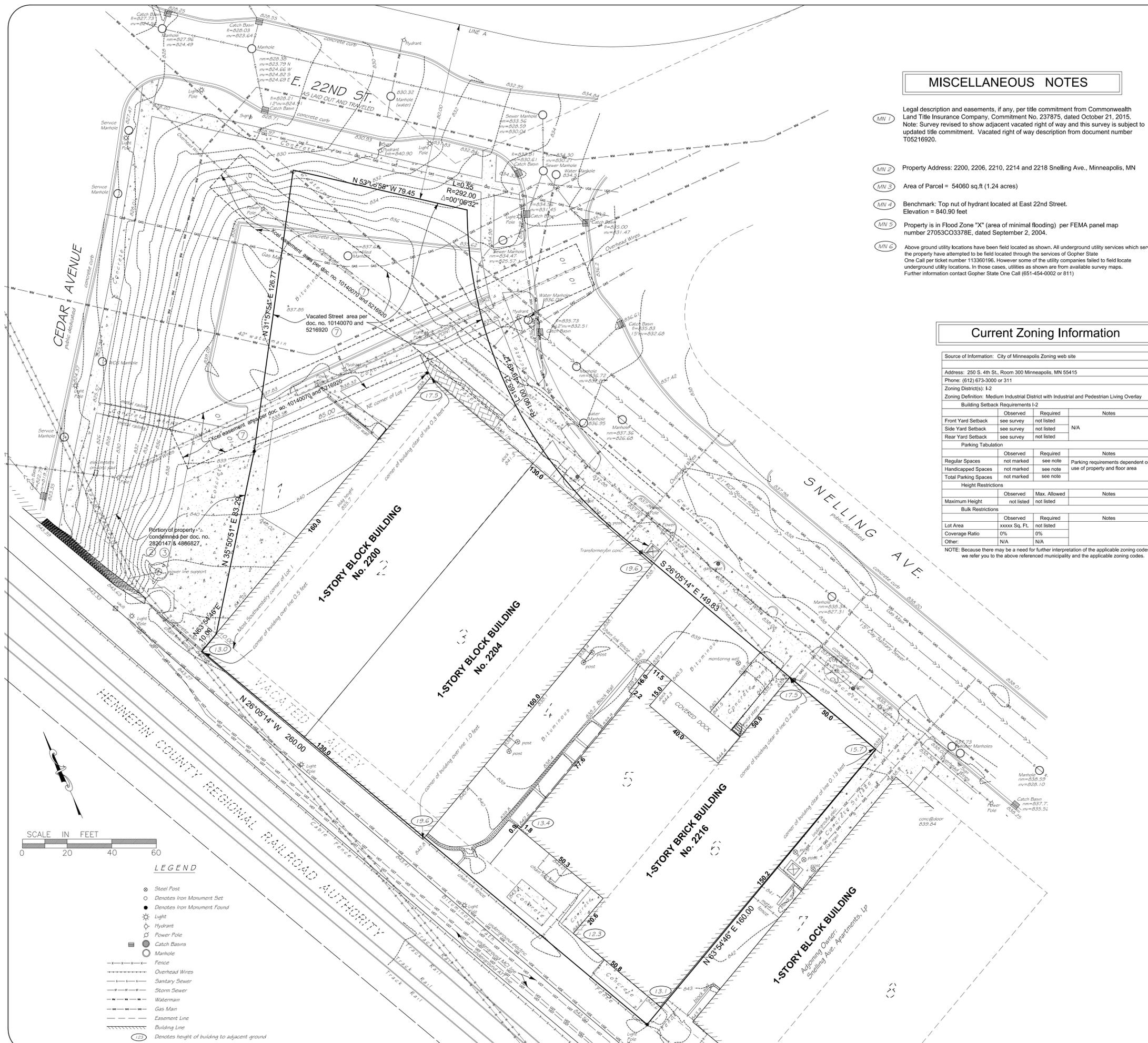
The undersigned, being a registered surveyor of the State of Minnesota, certifies that Seward Commons LLC, a Minnesota limited liability company and Commonwealth Land Title Insurance Company as follows:

This is to certify that this map or plat and the survey on which it is based were made in accordance with the 2011 Minimum Standard Detail Requirements for ALTA/ACSM Land Title Surveys, jointly established and adopted by ALTA, and NSPS in 2011, and includes Items 1, 2, 3, 4, 5, 6(b), 7(a), 8, 9 and 11(a) of Table A thereof.

Dated this 29th day of October, 2015.
4-13-2016 contours & manhole location
5-3-2016 Addition of vac row

Prepared by:
The Gregory Group, Inc. d.b.a.
Lot Surveys Company
7601 73rd Avenue N.
Brooklyn Park, MN 55428
phone 763-560-3093
fax 763-560-3522

Signed: 
Gregory R. Prash Registration No. 24992



LEGEND

- Steel Post
- Denotes Iron Monument Set
- Denotes Iron Monument Found
- ⊙ Light
- ⊙ Hydrant
- ⊙ Power Pole
- ⊙ Catch Basins
- ⊙ Manhole
- Fence
- Overhead Wires
- Sanitary Sewer
- Storm Sewer
- Watermain
- Gas Main
- Easement Line
- Building Line
- ⊙ Denotes height of building to adjacent ground



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St. Paul, MN 55113
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CLIENT:
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900 N. 3rd St.
Minneapolis, MN 55401
REDESIGN
2619 East Franklin Avenue
Minneapolis, MN 55406

THIS SQUARE APPEARS 1/2"X1/2" ON
FULL SIZE SHEETS

3/22/16	DD SET
3/21/16	PDR REVIEW
NO	DATE ISSUED FOR

NO	DATE	REVISION
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I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
PRINT NAME: DAVID
SIGNATURE: [Signature]
DATE: 3/21/16
**PRELIMINARY
NOT FOR CONSTRUCTION**
3/21/16

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PROJECT NAME:
THE BESSEMER AT SEWARD

2214 Snelling Avenue
Minneapolis, MN 55404

DRAWING TITLE:
REMOVAL PLAN

FILE:
DRAWN BY: DAA
CHECKED BY: GDK
PROJ. NO: 193803451
DRAWING NO:

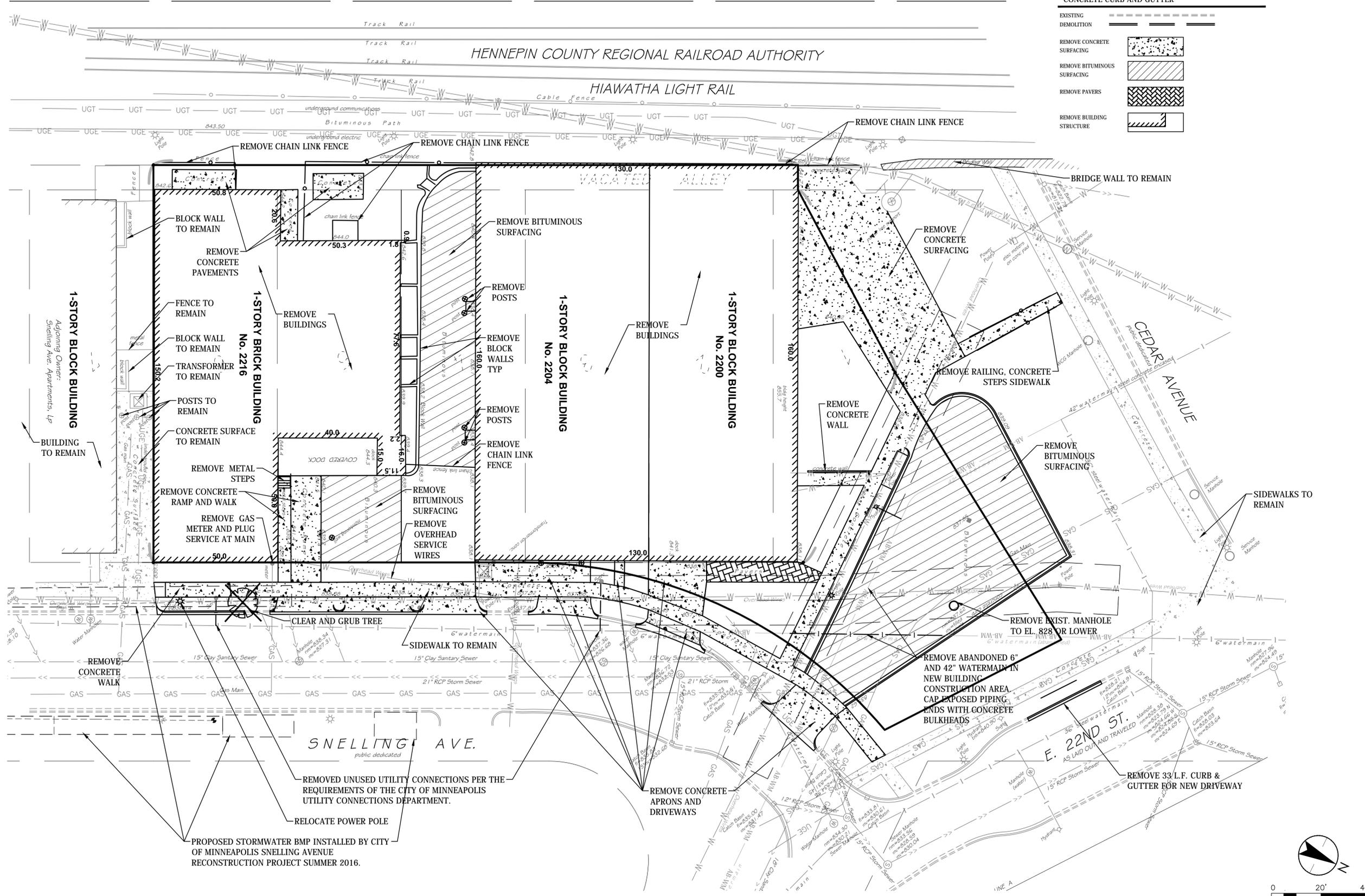
C0.01

UTILITY LINES

	EXISTING	DEMOLITION
FORCE MAIN	FM FM	FM FM
SANITARY SEWER	S S	S S
STORM SEWER	SS SS	SS SS
WATER MAIN	WM WM	WM WM
UNDERGROUND ELECTRIC	UGE UGE	E E
FIBER OPTIC	FO FO	FO FO

CONCRETE CURB AND GUTTER

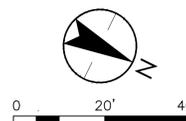
EXISTING	DEMOLITION
REMOVE CONCRETE SURFACING	[Pattern]
REMOVE BITUMINOUS SURFACING	[Pattern]
REMOVE PAVERS	[Pattern]
REMOVE BUILDING STRUCTURE	[Pattern]



REMOVED UNUSED UTILITY CONNECTIONS PER THE REQUIREMENTS OF THE CITY OF MINNEAPOLIS UTILITY CONNECTIONS DEPARTMENT.

RELOCATE POWER POLE

PROPOSED STORMWATER BMP INSTALLED BY CITY OF MINNEAPOLIS SNELLING AVENUE RECONSTRUCTION PROJECT SUMMER 2016.



Plot Date: 04/13/2016 - 8:53am
Drawing name: V:\193803451\CAD\DWG\193803451\C001.dwg
User: 193803451\BDDK 193803451\KSVI

NOTES:

1. THERE ARE NO STRUCTURE ENCROACHMENTS INTO THE RIGHT OF WAY.
2. THERE ARE NO IMPACTS TO EX. TRAFFIC SIGNAL SYSTEMS.
3. THERE ARE NO IMPACTS TO EX. STREET LIGHTS.
4. CONTRACTOR SHALL REFER TO THE ARCHITECTURAL PLANS FOR EXACT LOCATIONS AND DIMENSIONS OF STOOPS, EXITS AND BUILDING DIMENSIONS AND BUILDING UTILITY ENTRANCE LOCATIONS.
5. ALL DIMENSIONS AND RADII ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
6. CURB DEPTH (FACE CURB TO BACK CURB) IS 8" (0.67').
7. SITE BOUNDARY, TOPOGRAPHY, UTILITY AND ROAD INFORMATION TAKEN FROM A SURVEY BY A LAND SURVEYOR.
8. REFER TO LANDSCAPE PLANS FOR SITE FURNISHINGS AND PERMANENT GROUND COVER.
9. SURFACE MOUNT BIKE RACKS ACCORDING TO MANUFACTURER'S INSTRUCTIONS.
10. STREET LIGHTING INSTALLED AS PART OF THE PROJECT SHALL BE INSPECTED BY THE CITY. CONTRACTORS SHALL ARRANGE FOR INSPECTIONS WITH THE TRAFFIC DEPARTMENT. PLEASE CONTACT DAVE PREHALL (612) 673-5759 FOR FURTHER INFORMATION. ANY LIGHTING INSTALLATIONS NOT MEETING THE CITY SPECIFICATIONS WILL BE REQUIRED TO BE REINSTALLED AT OWNER'S EXPENSE.
11. AN OBSTRUCTION PERMIT IS REQUIRED ANYTIME CONSTRUCTION WORK IS PERFORMED IN THE PUBLIC RIGHT-OF-WAY. PLEASE CONTACT SCOTT KRAMER (612) 673-2383 REGARDING DETAILS OF SIDEWALK AND LANE CLOSURES. LOG ON TO [HTTP://MINNEAPOLIS.MN.ROWAY.NET/](http://MINNEAPOLIS.MN.ROWAY.NET/) FOR A PERMIT.
12. CONTACT ALLAN KLUGMAN AT (612) 673-2743 PRIOR TO CONSTRUCTION FOR THE TEMPORARY REMOVAL/TEMPORARY RELOCATION OF ANY CITY OF MINNEAPOLIS LIGHTING OR SIGNAL SYSTEM THAT MAY BE IN THE WAY OF CONSTRUCTION.
13. ALL COSTS FOR RELOCATION AND/OR REPAIR OF CITY TRAFFIC FACILITIES SHALL BE BORNE BY THE CONTRACTOR AND/OR PROPERTY OWNER.
14. CONTACT DOUG MADAY AT (612) 673-5755 PRIOR TO CONSTRUCTION FOR THE REMOVAL OF ANY CITY OF MINNEAPOLIS RIGHT OF WAY SIGNS THAT MAY BE IN THE WAY OF CONSTRUCTION.
15. THERE ARE NO TRANSIT STOPS LOCATED ON SNELLING AVENUE. THERE ARE BUS STOPS AT LOCATED ALONG MINNEHAHA AVENUE AT 24TH STREET AND 22ND STREET.
16. REFER TO LANDSCAPE PLAN FOR PROPOSED STREET LIGHTING.

SITE DATA
 ZONING CLASSIFICATION: I-2 (MEDIUM INDUSTRIAL)
 ZONING OVERLAY: INDUSTRIAL LIVING (ILOD)
 PEDESTRIAN (POD)

TOTAL SITE AREA = 44,621 S.F. (1.02 AC.)

BUILDING SETBACKS	REQUIRED	MEASURED
SNELLING AVENUE* SIDE	8' MAX.	10.5' (5.5' TO BALCONIES)
E. 22ND STREET REAR	0'	22.5' (20.5' TO BALCONIES)
	0'	8.6'

*POD OVERLAY LIMITS BUILDING SETBACK TO 8' MAXIMUM ALONG SNELLING AVENUE

PARKING DATA	REQUIRED	PROPOSED
STANDARD SPACES		81
COMPACT SPACES		14
ADA SPACES		4
TOTAL STALLS (ALL INDOOR)		95
BICYCLE		115

*X BICYCLE STALLS WITHIN THE BIKE ROOM, PARKING GARAGE, AND 4 BICYCLE RACKS OUTSIDE

BUILDING DATA	EXISTING	PROPOSED
BASEMENT PARKING LEVEL		23,559 SF
1ST FLOOR (INCL. PARKING)		23,559 SF
2ND FLOOR		23,910 SF
3RD FLOOR		23,910 SF
4TH FLOOR		23,910 SF
5TH FLOOR		23,910 SF
TOTAL BUILDING AREA		142,758 SF

GROUND COVER DATA	EXISTING	PROPOSED
IMPERVIOUS AREA (SF)	50,703	31,585
PERVIOUS AREA (SF)	3,357	22,475
TOTAL (SF)	54,060	54,060
PERVIOUS PERCENT	6.2%	41.6%



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 St. Paul, MN 55113
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CLIENT:
SCHAFFER RICHARDSON
 900 N. 3rd St.
 Minneapolis, MN 55401

REDESIGN
 2619 East Franklin Avenue
 Minneapolis, MN 55406

THIS SQUARE APPEARS 1/2"x1/2" ON FULL SIZE SHEETS

NO	DATE	ISSUED FOR
5/9/16		GMP SET
3/22/16		DD SET
3/21/16		PDR REVIEW
NO	DATE	ISSUED FOR
NO	DATE	REVISION

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: DAVID
 SIGNATURE: [Signature]
 DATE: 5/9/16
PRELIMINARY NOT FOR CONSTRUCTION

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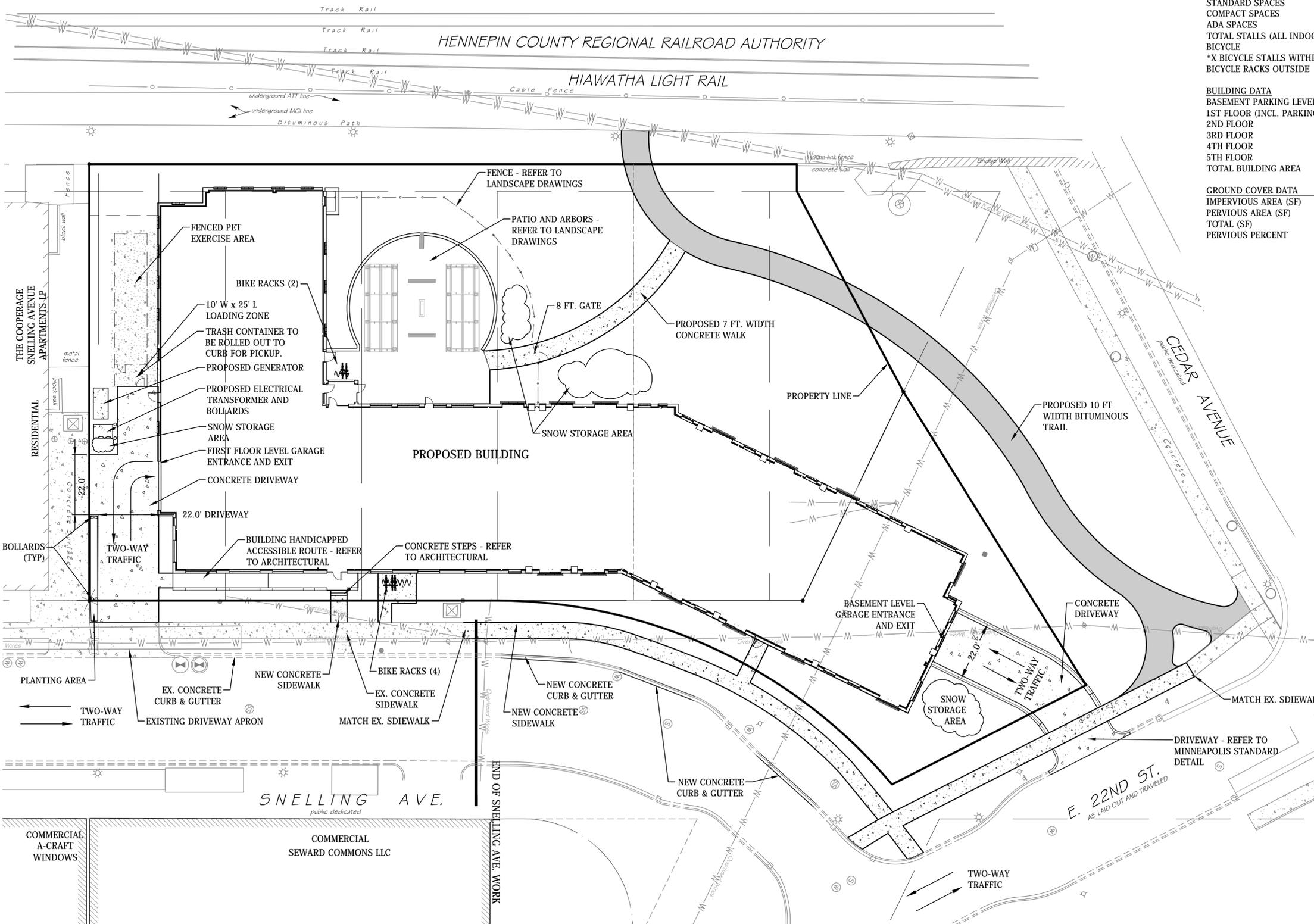
PROJECT NAME:
THE BESSEMER AT SEWARD

2214 Snelling Avenue
 Minneapolis, MN 55404

DRAWING TITLE:
SITE PLAN

FILE:
 DRAWN BY: DAA
 CHECKED BY: GDK
 PROJ. NO: 193803451
 DRAWING NO:

C1.01



Plot Date: 05/09/2016 - 11:44am
 Drawing name: V:\1938\active\193803451\C1.01.dwg
 Xref: 15006.LDSE, 193803451.XSN, 193803451.XST, 193803451.XSHD



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3/22/16		DD SET
3/21/16		PDR REVIEW

NO	DATE	REVISION

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WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION
AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER
UNDER THE LAWS OF THE STATE OF MINNESOTA.
PRINT NAME: J.E.
SIGNATURE: [Signature]
DATE: 5/9/16 P.C. NO.

**PRELIMINARY
NOT FOR CONSTRUCTION**

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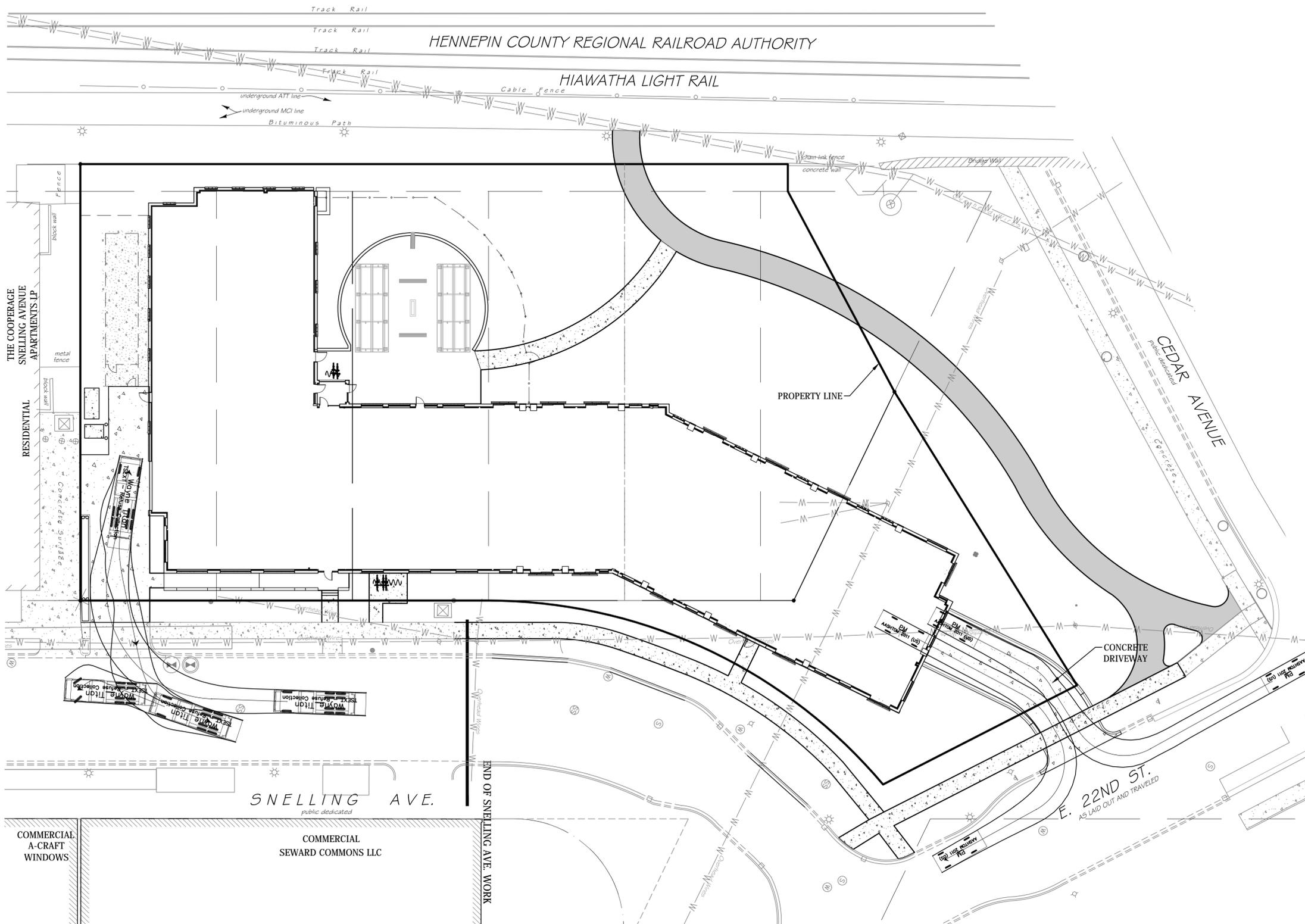
PROJECT NAME:
THE BESSEMER AT SEWARD

2214 Snelling Avenue
Minneapolis, MN 55404

DRAWING TITLE:
**SITE PLAN - TURNING
MODELING**

FILE:
DRAWN BY:
CHECKED BY: GDK
PROJ. NO: 193803451
DRAWING NO:

C1.01AT



Plot Date: 05/09/2016 - 11:48am
Drawing Name: V:\1938\active\193803451\CAD\Drawg\193803451C1.dwg
Xref: 150161.DWG, 193803451.XREF, 193803451.XREF, 193803451.XREF

GENERAL EROSION AND SEDIMENT CONTROL NOTES:

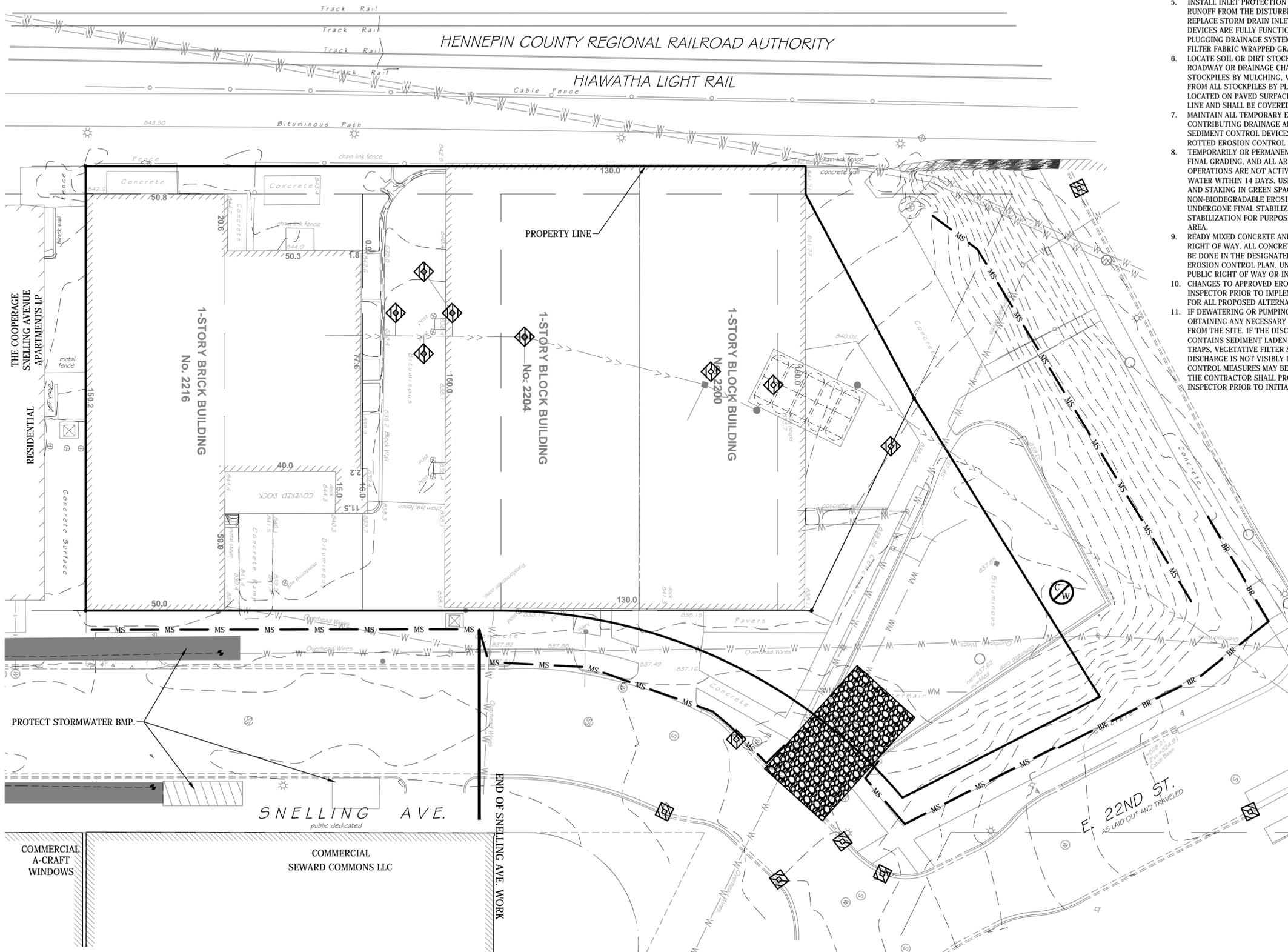
1. ALL PERMITTEES, CONTRACTORS, AND SUBCONTRACTORS INVOLVED WITH STORM WATER POLLUTION PREVENTION SHALL OBTAIN A COPY OF THE STORM WATER POLLUTION PLAN AND THE STATE OF MINNESOTA NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES PHASE II PERMIT) AND BECOME FAMILIAR WITH THEIR CONTENTS AND IS RESPONSIBLE TO COMPLY WITH ALL REQUIREMENTS STATED WITHIN.
2. THE BMP'S SHOWN ON THE PLANS ARE THE MINIMUM REQUIREMENTS FOR THE ANTICIPATED SITE CONDITIONS. AS CONSTRUCTION PROGRESSES, THE PERMITTEE/CONTRACTOR SHALL ANTICIPATE THAT ADDITIONAL BMP'S MAY BE REQUIRED AS SITE CONDITIONS CHANGE AND SHALL PROVIDE ADDITIONAL BMP'S TO MEET APPLICABLE REQUIREMENTS.
3. ALL WORK AND MATERIALS SHALL BE CONSTRUCTED ACCORDING TO THE APPROVED PLANS AND SWPPP. ANY DEVIATION FROM THE APPROVED PLANS SHALL REQUIRE WRITTEN APPROVAL FROM THE OWNER.
4. REFER TO LANDSCAPE PLANS FOR PERMANENT EROSION CONTROL MEASURES.
5. EXCESS SITE MATERIAL SHALL BE REMOVED FROM SITE OR PROTECTED IN ACCORDANCE WITH THE STANDARD EROSION CONTROL NOTES.

CONSTRUCTION SCHEDULE:

1. INSTALL PERIMETER EROSION CONTROL DEVICES AND INLET PROTECTION DEVICES.
2. CLEAR AND GRUB TREES AND PERFORM REMOVALS NECESSARY FOR INSTALLATION OF CONSTRUCTION ENTRANCE.
3. INSTALL CONSTRUCTION ENTRANCE AND TEMPORARY STABILIZATION MEASURES OVER AREAS OUTSIDE OF PERIMETER CONTROL.
4. PERFORM REMOVALS.
5. ROUGH GRADE SITE.
6. CONSTRUCT BUILDING, PARKING LOT, DRIVEWAYS AND UTILITIES.
7. FINAL GRADE SITE. INSTALL LANDSCAPING AND STABILIZATION MEASURES.
8. IMPLEMENTATION AND MAINTENANCE OF FINAL EROSION CONTROL STRUCTURES.
9. REMOVE TEMPORARY EROSION CONTROL DEVICES.

CITY OF MINNEAPOLIS STANDARD EROSION AND SEDIMENT CONTROL NOTES FOR PLANS

1. CONTRACTOR MUST CALL A CONSTRUCTION START 48 HOURS PRIOR TO ANY LAND DISTURBANCES 612-673-3867. FAILURE TO DO SO MAY RESULT IN FINES. THE REVOCATION OF PERMIT AND A STOP WORK ORDER BEING ISSUED.
2. INSTALL PERIMETER EROSION CONTROL AT THE LOCATIONS SHOWN ON THE PLANS PRIOR TO THE COMMENCEMENT OF ANY LAND DISTURBANCE OR CONSTRUCTION ACTIVITIES.
3. BEFORE BEGINNING CONSTRUCTION, INSTALL A TEMPORARY ROCK CONSTRUCTION ENTRANCE AT EACH POINT WHERE VEHICLES EXIT THE CONSTRUCTION SITE. USE 2 INCH OR GREATER DIAMETER ROCK IN A LAYER AT LEAST 6 INCHES THICK ACROSS THE ENTIRE WIDTH OF THE ENTRANCE. EXTEND THE ROCK ENTRANCE AT LEAST 50 FEET INTO THE CONSTRUCTION ZONE USING A GEO-TEXTILE FABRIC BENEATH THE AGGREGATE TO PREVENT MIGRATION OF SOIL INTO THE ROCK FROM BELOW.
4. REMOVE ALL SOILS AND SEDIMENTS TRACKED OR OTHERWISE DEPOSITED ONTO PUBLIC AND PRIVATE PAVEMENT AREAS. REMOVAL SHALL BE ON A DAILY BASIS WHEN TRACKING OCCURS AND MAY BE ORDERED BY MINNEAPOLIS INSPECTORS AT ANY TIME IF CONDITIONS WARRANT. SWEEPING SHALL BE MAINTAINED THROUGHOUT THE DURATION OF THE CONSTRUCTION AND DONE IN A MANNER TO PREVENT DUST BEING BLOWN TO ADJACENT PROPERTIES.
5. INSTALL INLET PROTECTION AT ALL PUBLIC AND PRIVATE CATCH BASIN INLETS, WHICH RECEIVE RUNOFF FROM THE DISTURBED AREAS. CONTRACTOR SHALL CLEAN, REMOVE SEDIMENT OR REPLACE STORM DRAIN INLET PROTECTION DEVICES ON A ROUTINE BASIS SUCH THAT THE DEVICES ARE FULLY FUNCTIONAL FOR THE NEXT RAIN EVENT. SEDIMENT DEPOSITED IN AND/OR PLUGGING DRAINAGE SYSTEMS IS THE RESPONSIBILITY OF THE CONTRACTOR. HAY BALES OR FILTER FABRIC WRAPPED GRATES ARE NOT ALLOWED FOR INLET PROTECTION.
6. LOCATE SOIL OR DIRT STOCKPILES NO LESS THAN 25 FEET FROM ANY PUBLIC OR PRIVATE ROADWAY OR DRAINAGE CHANNEL. IF REMAINING FOR MORE THAN SEVEN DAYS, STABILIZE THE STOCKPILES BY MULCHING, VEGETATIVE COVER, TARP, OR OTHER MEANS. CONTROL EROSION FROM ALL STOCKPILES BY PLACING SILT BARRIERS AROUND THE PILES. TEMPORARY STOCKPILES LOCATED ON PAVED SURFACES MUST BE NO LESS THAN TWO FEET FROM THE DRAINAGE/GUTTER LINE AND SHALL BE COVERED IF LEFT MORE THAN 24 HOURS.
7. MAINTAIN ALL TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES IN PLACE UNTIL THE CONTRIBUTING DRAINAGE AREA HAS BEEN STABILIZED. INSPECT TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES ON A DAILY BASIS AND REPLACE DETERIORATED, DAMAGED, OR ROTTED EROSION CONTROL DEVICES IMMEDIATELY.
8. TEMPORARILY OR PERMANENTLY STABILIZE ALL CONSTRUCTION AREAS WHICH HAVE UNDERGONE FINAL GRADING, AND ALL AREAS IN WHICH GRADING OR SITE BUILDING CONSTRUCTION OPERATIONS ARE NOT ACTIVELY UNDERWAY AGAINST EROSION DUE TO RAIN. WIND AND RUNNING WATER WITHIN 14 DAYS. USE SEED AND MULCH, EROSION CONTROL MATTING, AND/OR SODDING AND STAKING IN GREEN SPACE AREAS. REMOVE ALL TEMPORARY SYNTHETIC, STRUCTURAL, NON-BIODEGRADABLE EROSION AND SEDIMENT CONTROL DEVICES AFTER THE SITE HAS UNDERGONE FINAL STABILIZATION WITH PERMANENT VEGETATION ESTABLISHMENT. FINAL STABILIZATION FOR PURPOSES OF THIS REMOVAL IS 70% ESTABLISHED COVER OVER DENUDEED AREA.
9. READY MIXED CONCRETE AND CONCRETE BATCH/MIX PLANTS ARE PROHIBITED WITHIN THE PUBLIC RIGHT OF WAY. ALL CONCRETE RELATED PRODUCTION, CLEANING AND MIXING ACTIVITIES SHALL BE DONE IN THE DESIGNATED CONCRETE MIXING/WASHOUT LOCATIONS AS SHOWN IN THE EROSION CONTROL PLAN. UNDER NO CIRCUMSTANCE MAY WASHOUT WATER DRAIN ONTO THE PUBLIC RIGHT OF WAY OR INTO ANY PUBLIC OR PRIVATE STORM DRAIN CONVEYANCE.
10. CHANGES TO APPROVED EROSION CONTROL PLAN MUST BE APPROVED BY THE EROSION CONTROL INSPECTOR PRIOR TO IMPLEMENTATION. CONTRACTOR TO PROVIDE INSTALLATION AND DETAILS FOR ALL PROPOSED ALTERNATE TYPE DEVICES.
11. IF DEWATERING OR PUMPING OF WATER IS NECESSARY, THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ANY NECESSARY PERMITS AND/OR APPROVALS PRIOR TO DISCHARGE OF ANY WATER FROM THE SITE. IF THE DISCHARGE FROM THE DEWATERING OR PUMPING PROCESS IS TURBID OR CONTAINS SEDIMENT LADEN WATER, IT MUST BE TREATED THROUGH THE USE OF SEDIMENT TRAPS, VEGETATIVE FILTER STRIPS, OR OTHER SEDIMENT REDUCING MEASURES SUCH THAT THE DISCHARGE IS NOT VISIBLY DIFFERENT FROM THE RECEIVING WATER. ADDITIONAL EROSION CONTROL MEASURES MAY BE REQUIRED AT THE DISCHARGE POINT TO PREVENT SCOUR EROSION. THE CONTRACTOR SHALL PROVIDE A DEWATERING/PUMPING PLAN TO THE EROSION CONTROL INSPECTOR PRIOR TO INITIATING DEWATERING ACTIVITIES.



ACREAGE SUMMARY

EXISTING IMPERVIOUS AREA	1.24 ACRES
PROPOSED IMPERVIOUS AREA (NET)	0.78 ACRES
PROPOSED TURF AREA	0.87 ACRES
TOTAL DISTURBED AREA	1.65 ACRES

EROSION CONTROL LEGEND

	BIOROLL
	MACHINE SLICED SILT FENCE
	INLET PROTECTION
	CONCRETE WASHOUT
	CONSTRUCTION ENTRANCE / EXIT

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 www.stantec.com

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 900 N. 3rd St.
 Minneapolis, MN 55401

REDESIGN
 2619 East Franklin Avenue
 Minneapolis, MN 55406

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NO	DATE	ISSUED FOR
	5/9/16	GMP SET
	3/22/16	DD SET
	3/21/16	PDR REVIEW
NO	DATE	REVISION

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: DAVID
 SIGNATURE: [Signature]
 DATE: 5/9/16
 LIC. NO. 193803451

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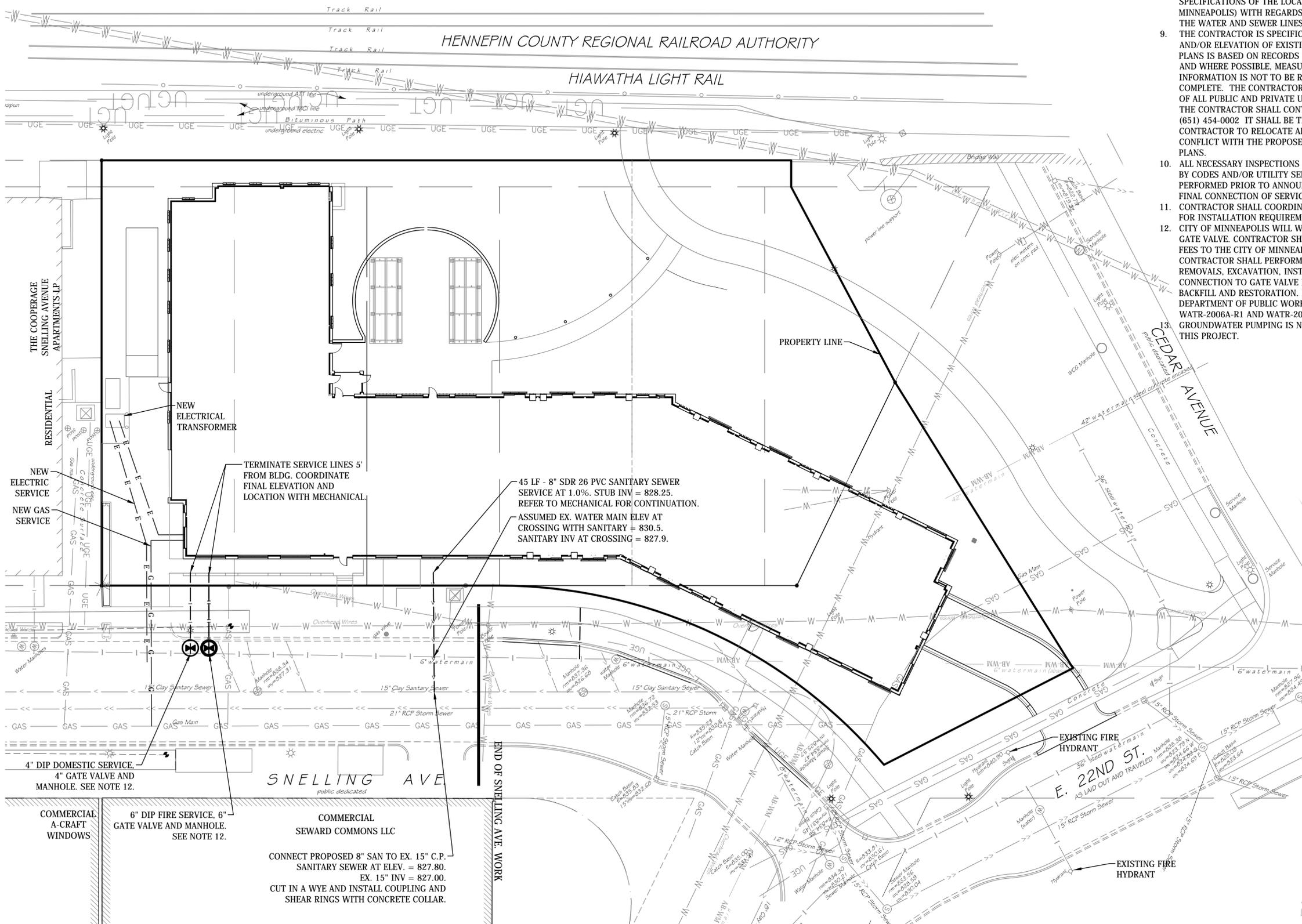
PROJECT NAME:
THE BESSEMER AT SEWARD

2214 Snelling Avenue
 Minneapolis, MN 55404

DRAWING TITLE:
EROSION AND SEDIMENT CONTROL PLAN

FILE:
 DRAWN BY: DAA
 CHECKED BY: GDK
 PROJ. NO: 193803451
 DRAWING NO: **C2.01**

Plot Date: 05/09/2016 - 11:48am
 Drawing Name: V:\1938\active\193803451\CAD\Drawg\193803451\C201.dwg
 Xref: 15036 LBASE, 193803451 XSNVA, 193803451 BRDR, 193803451 XSNL, 193803451 XSNZ, 193803451 XSNW



- SITE UTILITY NOTES:**
1. ALL FILL MATERIAL IS TO BE IN PLACE, AND COMPACTED BEFORE INSTALLATION OF PROPOSED UTILITIES.
 2. CONTRACTOR SHALL NOTIFY THE UTILITY AUTHORITIES INSPECTORS 72 HOURS BEFORE CONNECTING TO ANY EXISTING LINE.
 3. MINIMUM TRENCH WIDTH SHALL BE 2 FEET.
 4. CONTRACTOR SHALL MAINTAIN A MINIMUM OF 8'-0" COVER ON ALL WATERLINES.
 5. DRAWINGS DO NOT PURPORT TO SHOW ALL EXISTING UTILITIES.
 6. EXISTING UTILITIES SHALL BE VERIFIED IN FIELD PRIOR TO INSTALLATION OF ANY NEW LINES.
 7. REFER TO INTERIOR PLUMBING DRAWINGS FOR TIE IN OF ALL UTILITIES.
 8. CONTRACTOR IS RESPONSIBLE FOR COMPLYING TO THE SPECIFICATIONS OF THE LOCAL AUTHORITIES (CITY OF MINNEAPOLIS) WITH REGARDS TO MATERIALS AND INSTALLATION OF THE WATER AND SEWER LINES.
 9. THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES, AND WHERE POSSIBLE, MEASUREMENTS TAKEN IN FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR SHALL VERIFY THE FIELD LOCATION OF ALL PUBLIC AND PRIVATE UTILITIES PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL CONTACT GOPHER STATE ONE CALL AT (651) 454-0002 IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES WHICH CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS.
 10. ALL NECESSARY INSPECTIONS AND/OR CERTIFICATIONS REQUIRED BY CODES AND/OR UTILITY SERVICE COMPANIES SHALL BE PERFORMED PRIOR TO ANNOUNCED BUILDING POSSESSION AND THE FINAL CONNECTION OF SERVICE.
 11. CONTRACTOR SHALL COORDINATE WITH ALL UTILITY COMPANIES FOR INSTALLATION REQUIREMENTS AND SPECIFICATIONS.
 12. CONTRACTOR SHALL WILL WET TAP WATER MAIN AND INSTALL GATE VALVE. CONTRACTOR SHALL BE RESPONSIBLE FOR ALL PERMIT FEES TO THE CITY OF MINNEAPOLIS FOR THE WATER SERVICES. CONTRACTOR SHALL PERFORM ALL OTHER WORK, INCLUDING REMOVALS, EXCAVATION, INSTALLATION OF VALVE MANHOLE, CONNECTION TO GATE VALVE INSTALLED BY CITY OF MINNEAPOLIS DEPARTMENT OF PUBLIC WORKS STANDARD PLATE NO. WATR-2006A-R1 AND WATR-2000A-R2. GROUNDWATER PUMPING IS NOT ANTICIPATED TO BE REQUIRED ON THIS PROJECT.

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NO	DATE	ISSUED FOR
5/9/16		GMP SET
3/22/16		DD SET
3/21/16		PDR REVIEW
NO	DATE	ISSUED FOR
NO	DATE	REVISION

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: DAVID
 SIGNATURE: [Signature]
 DATE: 5/9/16
 LIC. NO. [Number]

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PROJECT NAME:
THE BESSEMER AT SEWARD

2214 Snelling Avenue
 Minneapolis, MN 55404

DRAWING TITLE:
UTILITY PLAN

FILE:
 DRAWN BY: DAA
 CHECKED BY: GDK
 PROJ. NO: 193803451
 DRAWING NO:

C4.01

Plot Date: 05/09/2016 - 11:45am
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 Xref: 15006 LBASE, 193803451RDR, 193803451RDR, 193803451RDR



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	3/22/16	DD SET
	3/21/16	PDR REVIEW
NO	DATE	REVISION

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AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER
UNDER THE LAWS OF THE STATE OF MINNESOTA.
PRINT NAME: DAVID J. ...
SIGNATURE: ...
DATE: 5/9/16

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PROJECT NAME:
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Minneapolis, MN 55404

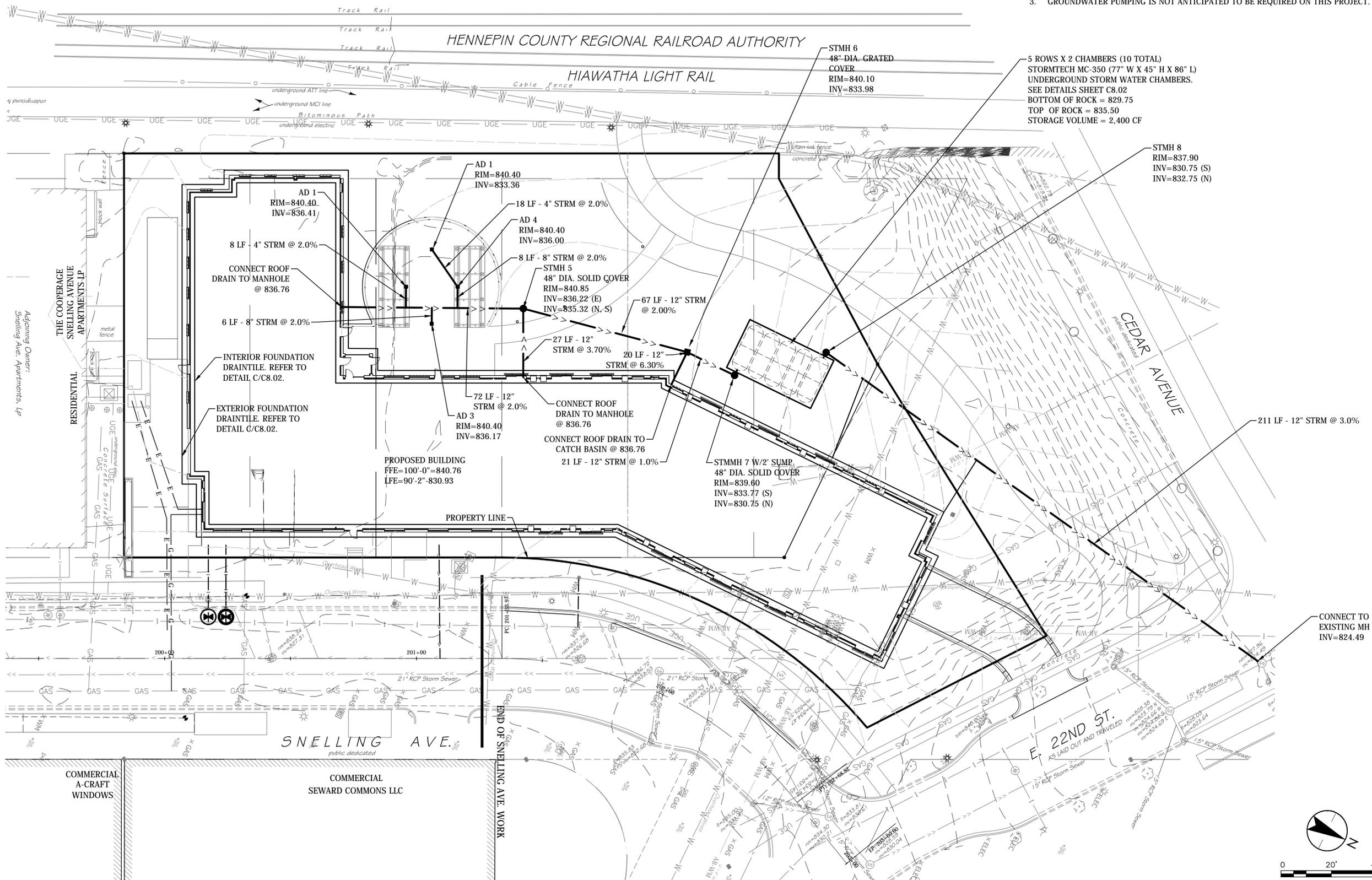
DRAWING TITLE:
STORM SEWER PLAN

FILE:
DRAWN BY: DAA
CHECKED BY: GDK
PROJ. NO: 193803451
DRAWING NO:

C5.01

NOTES:

1. THE CONTRACTOR, PROPERTY OWNER OR RESPONSIBLE PARTY SHALL CONTACT MINNEAPOLIS SURFACE WATERS AND SEWERS 48 HOURS PRIOR TO ANY EXCAVATION OR CONSTRUCTION RELATED TO OR IN THE LOCATION OF THE PROPOSED STORMWATER MANAGEMENT BMP (CONTACT PAUL CHELSEN, 612-673-2406 OR PAUL.CHELSEN@MINNEAPOLISMN.GOV).
2. UPON THE PROJECT'S COMPLETION THE GENERAL CONTRACTOR, PROPERTY OWNER OR RESPONSIBLE PARTY SHALL PROVIDE TO THE DEPARTMENT OF PUBLIC WORKS A FINAL STORMWATER MANAGEMENT REPORT INCLUDING RECORD DRAWINGS. THIS REPORT WILL SERVE AS A MEANS OF VERIFICATION THAT THE INTENT OF THE APPROVED STORMWATER MANAGEMENT DESIGN HAS BEEN MET. THIS FINAL REPORT SHALL SUBSTANTIATE THAT ALL ASPECTS OF THE ORIGINAL DESIGN HAVE BEEN ADEQUATELY PROVIDED FOR THE BY THE CONSTRUCTION OF THE PROJECT.
3. GROUNDWATER PUMPING IS NOT ANTICIPATED TO BE REQUIRED ON THIS PROJECT.



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Xref: 150161.LISE, 193803451.XSVA, 193803451.BDR, 193803451.XSMT, 193803451.XSNO, 193803451.XSNU, 193803451.XSNZ



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705 raymond avenue : suite 200
suite 200 : st. paul : mn : 55114
651.248.6155 or 651.341.3611

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1	5.9.2016	GMP SET
NO	DATE	ISSUED FOR

NO	DATE	REVISION
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PRELIMINARY
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 TYPE OR PRINTED: 03/21/2016
 DATE: 9/13/2016 REG. NO.: 42162

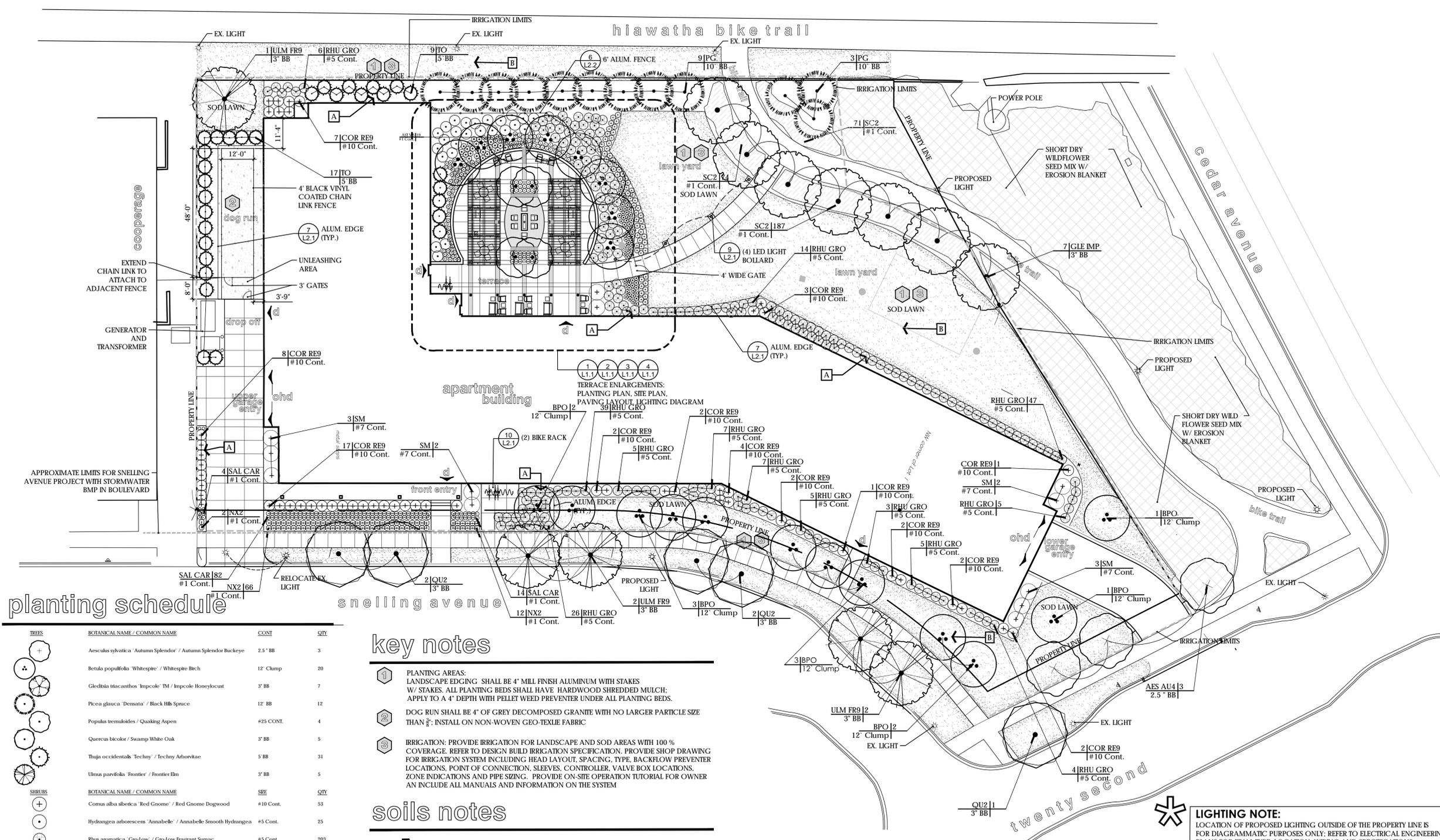
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PROJECT NAME:
**THE BESSEMER
at SEWARD**

2200 SNELLING AVENUE
MINNEAPOLIS, MN

DRAWING TITLE:
LANDSCAPE PLAN

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 DRAWN BY: jpa
 CHECKED BY: jpa
 PROJ. NO: asonia
 DRAWING NO:
L1.0



planting schedule

TREES	BOTANICAL NAME / COMMON NAME	CONT	QTY	
	Aesculus sylvatica 'Autumn Splendor' / Autumn Splendor Buckeye	2.5" BB	3	
	Betula populifolia 'Whitespire' / Whitespire Birch	12' Clump	20	
	Gleditsia triacanthos 'Impcoke' / Impcoke Honeylocust	3' BB	7	
	Picea glauca 'Densata' / Black Hills Spruce	12' BB	12	
	Populus tremuloides / Quaking Aspen	#25 CONT.	4	
	Quercus bicolor / Swamp White Oak	3' BB	5	
	Thuja occidentalis 'Techny' / Techny Arborvitae	5' BB	31	
	Ulmus parvifolia 'Frontier' / Frontier Elm	3' BB	5	
SHRUBS	BOTANICAL NAME / COMMON NAME	SIZE	QTY	
	Comus alba sibirica 'Red Gnome' / Red Gnome Dogwood	#10 Cont.	53	
	Hydrangea arborescens 'Annabelle' / Annabelle Smooth Hydrangea	#5 Cont.	25	
	Rhus aromatica 'Gro-Low' / Gro-Low Fragrant Sumac	#5 Cont.	203	
	Syringa meyeri 'Palibin' / Dwarf Korean Lilac	#7 Cont.	12	
ANNUALS/PERENNIALS	BOTANICAL NAME / COMMON NAME	SIZE	QTY	
	Astilbe chinensis 'Vikings' / Vikings Astilbe	#1 Cont.	82	
	Hemerocallis 'Lavender Starburst' / Lavender Starburst Daylily	#1 Cont.	59	
	Nepeta x faassenii 'Walkers Low' / Walkers Low Catmint	#1 Cont.	141	
	Sabia nemorosa 'Cardamom' / Cardamom Sage	#1 Cont.	169	
	Sedum x Autumn Fire' / Autumn Fire Sedum	#1 Cont.	25	
SHRUB AREAS	BOTANICAL NAME / COMMON NAME	CONT	SPACING	QTY
	Schizachyrium scoparium 'Minnblue A' / Blue Heaven Little Bluestem	#1 Cont.	24" o.c.	332
GROUND COVERS	BOTANICAL NAME / COMMON NAME	CONT	SPACING	QTY
	Poa pratensis / Kentucky Bluegrass	sod		18,295 sf
	SHORT DRY WILDFLOWER Seed mix / w erosion blanket	seed		9,027 sf

key notes

1. PLANTING AREAS: LANDSCAPE EDGING SHALL BE 4" MILL FINISH ALUMINUM WITH STAKES W/ STAKES. ALL PLANTING BEDS SHALL HAVE HARDWOOD SHREDDED MULCH; APPLY TO A 4" DEPTH WITH PELLET WEED PREVENTER UNDER ALL PLANTING BEDS.
2. DOG RUN SHALL BE 4" OF GREY DECOMPOSED GRANITE WITH NO LARGER PARTICLE SIZE THAN 3/8"; INSTALL ON NON-WOVEN GEO-TEXTILE FABRIC
3. IRRIGATION: PROVIDE IRRIGATION FOR LANDSCAPE AND SOD AREAS WITH 100% COVERAGE. REFER TO DESIGN BUILD IRRIGATION SPECIFICATION. PROVIDE SHOP DRAWING FOR IRRIGATION SYSTEM INCLUDING HEAD LAYOUT, SPACING, TYPE, BACKFLOW PREVENTER LOCATIONS, POINT OF CONNECTION, SLEEVES, CONTROLLER, VALVE BOX LOCATIONS, ZONE INDICATIONS AND PIPE SIZING. PROVIDE ON-SITE OPERATION TUTORIAL FOR OWNER AN INCLUDE ALL MANUALS AND INFORMATION ON THE SYSTEM

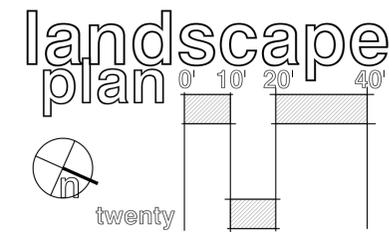
soils notes

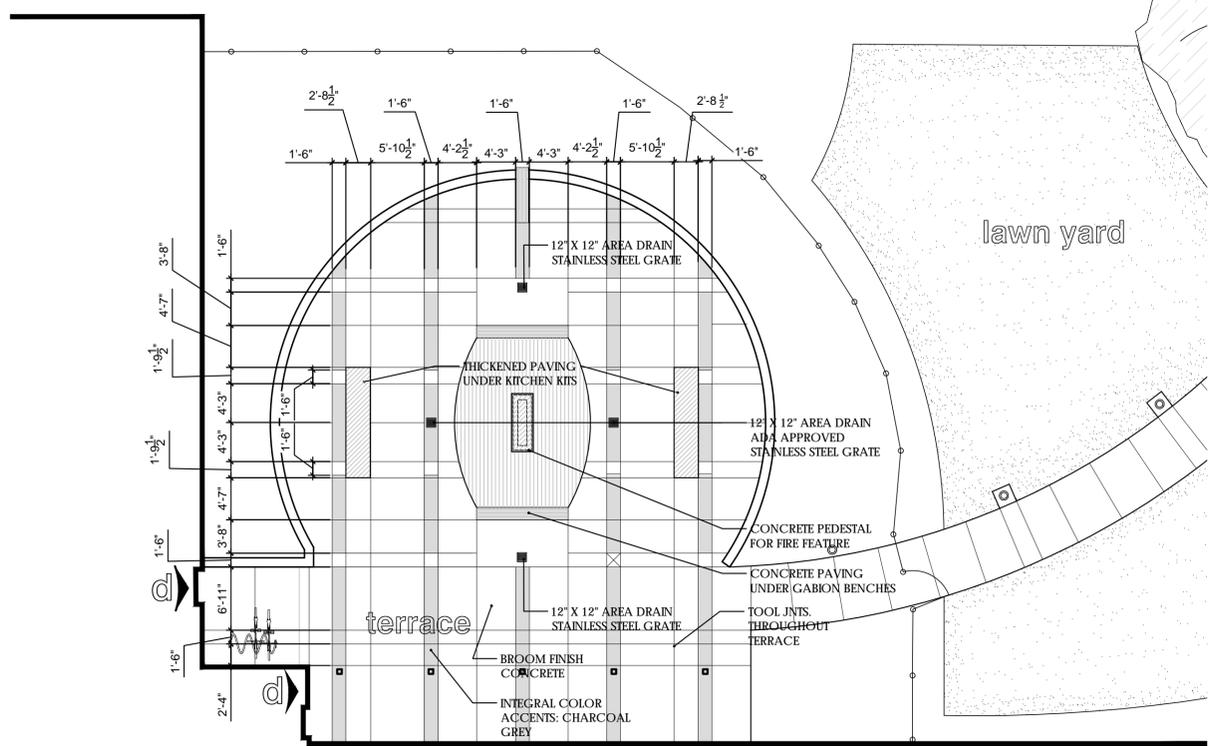
- A. ALL PLANTING BEDS 18" PULVERIZED LANDSCAPING SOIL ENHANCE WITH AGED AND SCREEN COMPOST
- B. 6" MIN. TOPSOIL

planting notes

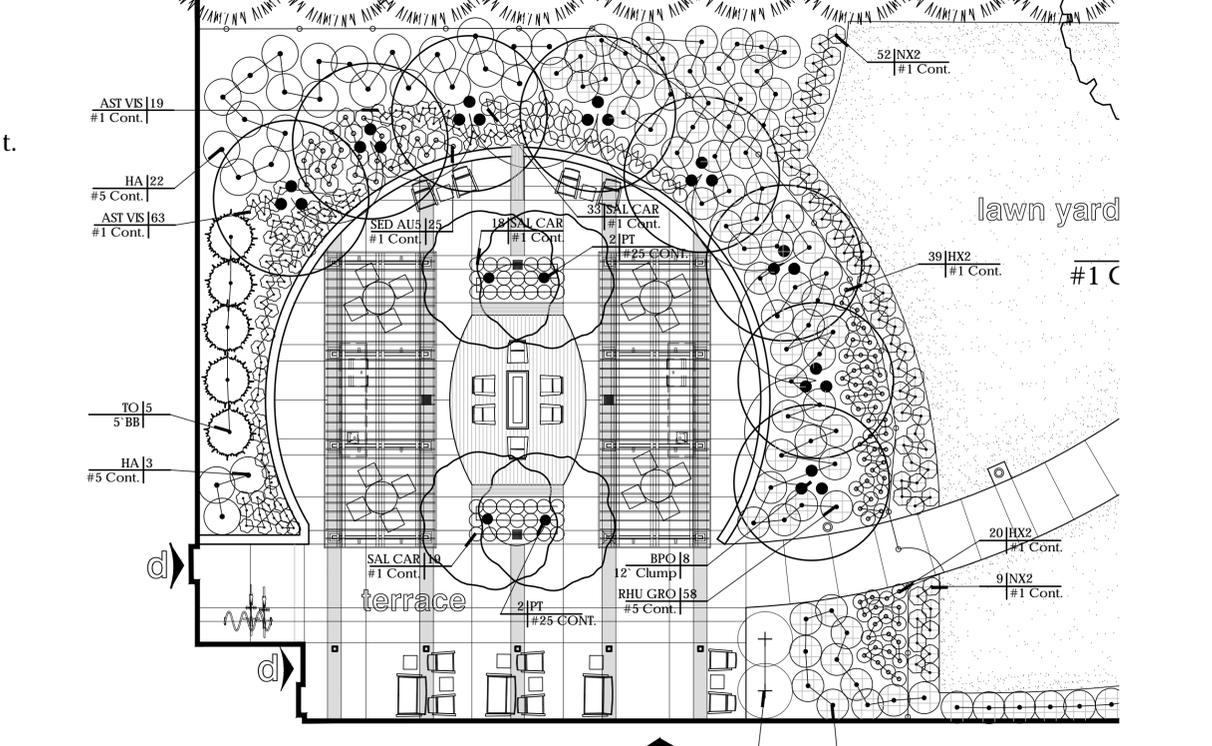
1. CONFIRM ALL QUANTITIES, SHAPES AND LOCATIONS OF BEDS, AND ADJUST AS REQUIRED TO CONFORM TO THE SITE CONDITIONS. CONFIRM ANY ADJUSTMENTS WITH THE LANDSCAPE ARCHITECT.
2. LOCATE ALL UTILITIES. NOTIFY THE LANDSCAPE ARCHITECT OF ANY CONFLICTS WITH NEW CONSTRUCTION.
3. THE CONTRACTOR SHALL REMOVE FROM THE SITE ALL SOD/TURF WHICH HAS BEEN REMOVED FOR NEW PLANT BEDS. ANY PLANT STOCK NOT PLANTED ON DAY OF DELIVERY SHALL BE HEeled IN AND WATERED UNTIL INSTALLATION. PLANTS NOT MAINTAINED IN THIS MANNER WILL BE REJECTED. THE PLAN TAKES PRECEDENCE OVER THE PLANT SCHEDULE IF DISCREPANCIES EXIST. ADVISE LANDSCAPE ARCHITECT OF ANY DISCREPANCIES.
4. THE CONTRACTOR SHALL AVOID DAMAGING EXISTING TREES. DO NOT STORE OR DRIVE HEAVY MATERIALS OVER TREE ROOTS. DO NOT DAMAGE TREE BARK OR BRANCHES.
5. THE CONTRACTOR SHALL KEEP PAVEMENTS, FIXTURES AND BUILDINGS CLEAN AND UNSTAINED. ANY DAMAGE TO EXISTING FACILITIES SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. THE PROJECT SITE SHALL BE KEPT CLEAR OF CONSTRUCTION WASTES AND DEBRIS.
6. THE LANDSCAPE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING SOIL QUANTITIES TO COMPLETE THE WORK SHOWN ON THE PLAN. MULCH, PLANTING SOIL AND OTHER MISCELLANEOUS PLANTING COMPONENTS SHALL BE CONSIDERED INCIDENTAL TO THE RELATED PLAN. VERIFY ALL QUANTITIES. CONTRACTOR IS RESPONSIBLE FOR WATERING AND ALL PLANT CARE UNTIL FINAL ACCEPTANCE BY THE OWNER.
7. PLANT MATERIAL SHALL BE PROTECTED AND MAINTAINED UNTIL THE INSTALLATION OF PLANTING IS COMPLETE. INSPECTION HAS BEEN MADE AND PLANTING IS ACCEPTED EXCLUSIVE OF THE GUARANTEE.
8. MAINTENANCE SHALL INCLUDE WATERING, WEEDING, MULCHING.
9. REMOVAL OF DEAD MATERIAL PRIOR TO GROWING SEASON. RE-SETTING PLANTS AND PROPER GRADE, AND KEEPING PLANTS IN A PLUMB POSITION. AFTER ACCEPTANCE, THE OWNER SHALL ASSUME MAINTENANCE RESPONSIBILITIES. HOWEVER, THE CONTRACTOR SHALL CONTINUE TO BE RESPONSIBLE FOR KEEPING THE TREES PLUMB THROUGHOUT THE GUARANTEE PERIOD.
10. WATERING: MAINTAIN A WATERING SCHEDULE WHICH WILL THOROUGHLY WATER ALL PLANTS ONCE A WEEK AND SOD EVERY OTHER DAY UNTIL ACCEPTANCE BY THE OWNER. IN EXTREMELY HOT, DRY WEATHER, WATER MORE OFTEN AS REQUIRED BY INDICATIONS OF HEAT STRESS SUCH AS WILTING LEAVES. CHECK MOISTURE UNDER MULCH PRIOR TO WATERING TO DETERMINE NEED. CONTRACTOR SHALL MAKE THE NECESSARY ARRANGEMENTS FOR WATER.

LIGHTING NOTE:
 LOCATION OF PROPOSED LIGHTING OUTSIDE OF THE PROPERTY LINE IS FOR DIAGRAMMATIC PURPOSES ONLY; REFER TO ELECTRICAL ENGINEERING PLANS FOR FINAL TYPE, LOCATION, WIRING AND SPECIFICATIONS.

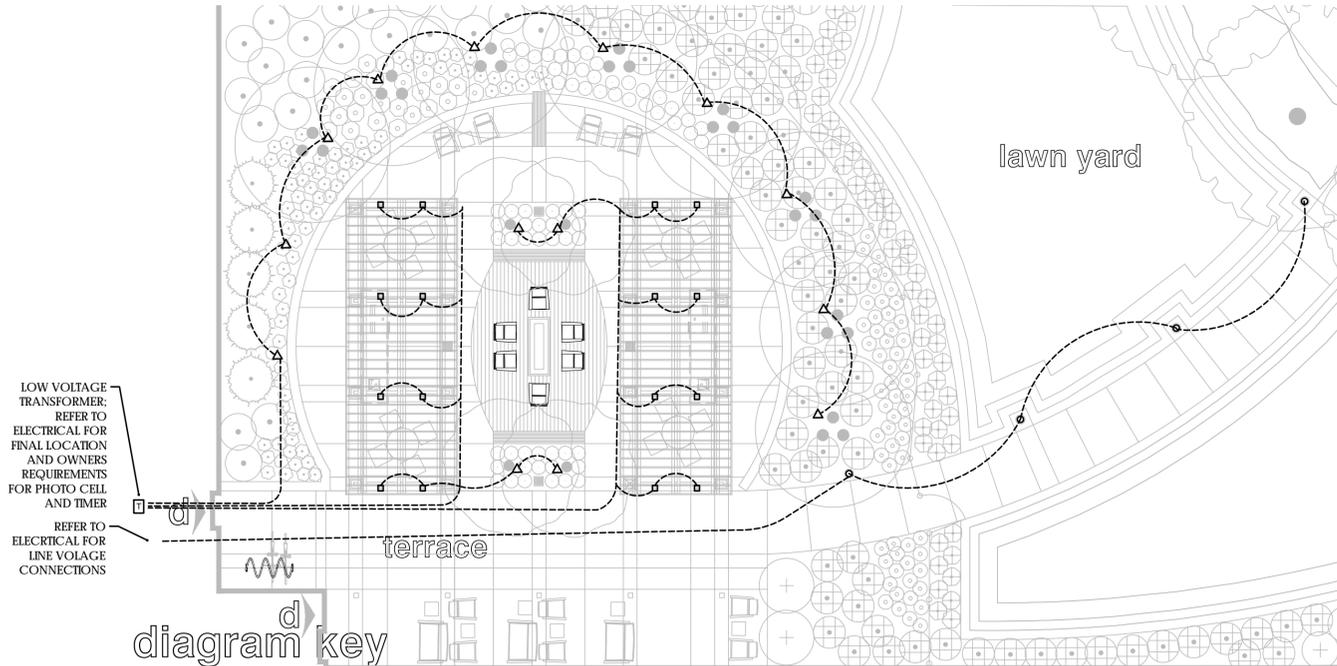




3 terrace enlargement: planting
scale: 1" = 10'-0"

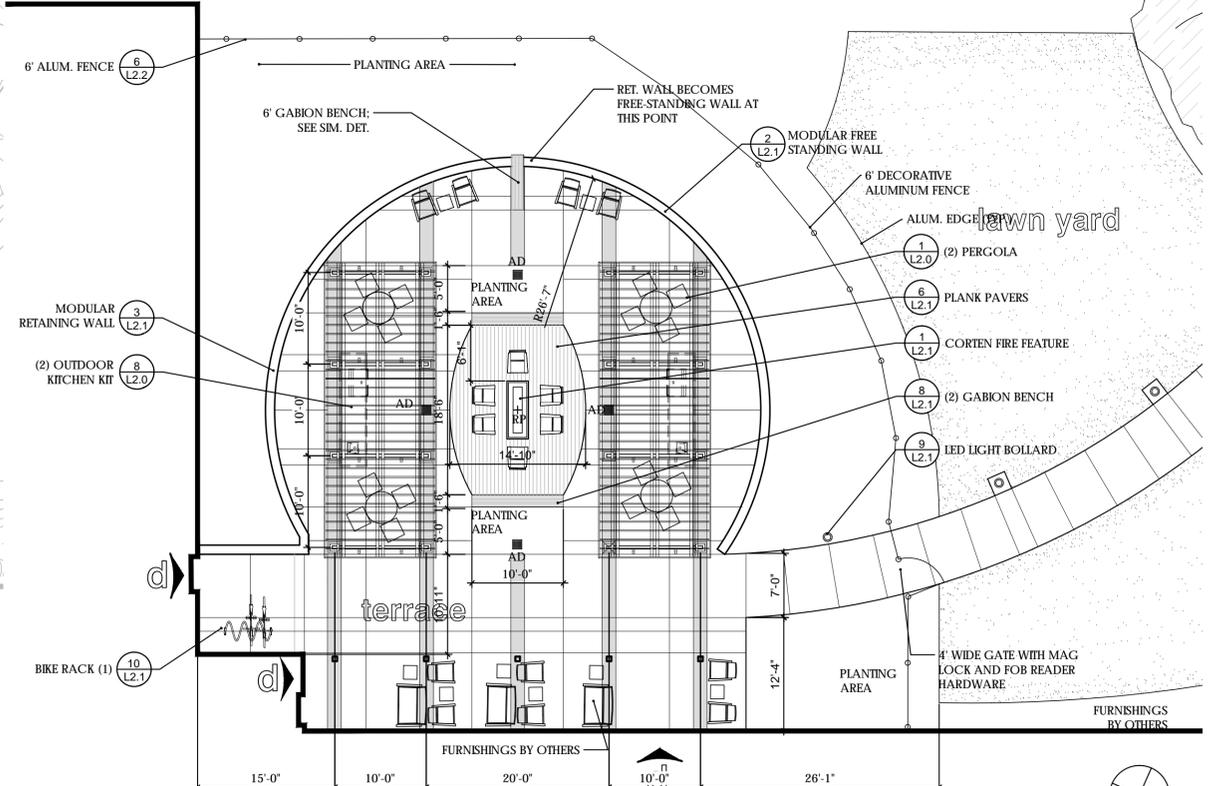


1 terrace enlargement: planting
scale: 1" = 10'-0"



QTY	SYMBOL	LIGHT TYPE	FIXTURE
14	△	12V UP LIGHT LED	HK ZXLI6 6W LED ; w/ LOUVER 16, GSA 16 GLARE SHIELD AND MHVL 12" STAKE-6" LEGS; ALL COMPONENTS FINISHED ANNOZIZED CLEAR; 2700 K COLOR TEMP.
16	□	12V PERGOLA DOWN LIGHT LED	HK ZXL 20f-WM-SQ LED; ALL COMPONENTS FINISHED ANODIZED CLEAR; 2700 K COLOR TEMP.
1	⊞	TRANSFORMER	DESIGN BUILD BY ELECTRICAL
4	○	120V LED BOLLARD	LIGMAN LIGHTWAVE BOLLARD HP LED ULW 10872 W30 SILVER METALLIC

4 terrace enlargement: lighting diagram
scale: 1" = 10'-0"



2 terrace enlargement: site plan
scale: 1" = 10'-0"

CLIENT:

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NO	DATE	ISSUED FOR
1	5.9.2016	GMP SET

NO	DATE	REVISION
----	------	----------

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PRELIMINARY
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DATE: 9.13.2016 REG. NO.: 42162

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PROJECT NAME:
THE BESSEMER at SEWARD

2200 SNELLING AVENUE
MINNEAPOLIS, MN

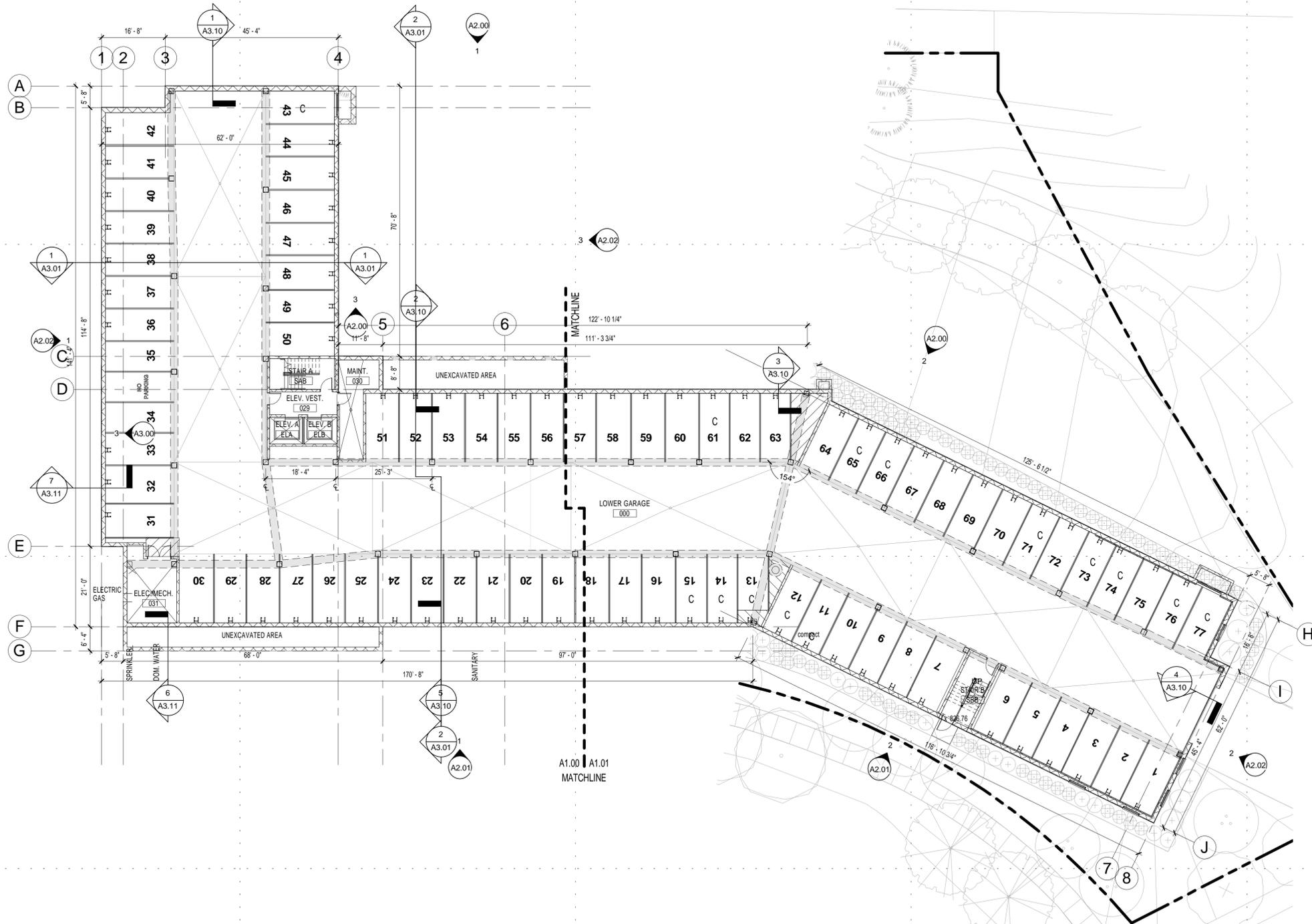
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DRAWN BY: jpo
CHECKED BY: jpo
PROJ. NO: asonia
DRAWING NO:

OVERALL BASEMENT PLAN

1/16" = 1'-0"

COOPERAGE



SEE SHEETS A1.00 & A1.01 FOR ENLARGED BASEMENT PLANS

GENERAL BUILDING PLAN NOTES:

- COORDINATE THIS PLAN WITH STRUCTURAL, PLUMBING, HVAC, ELECTRICAL AND SPRINKLER PLANS. NOTIFY ARCHITECT OF ANY DISCREPANCIES. NOTE THAT SYMBOLS USED ON THOSE PLANS MAY DIFFER FROM ARCHITECTURAL SYMBOLS.
- REFER TO CODE ANALYSIS (SHEET G0.01) TO VERIFY TYPES AND LOCATIONS OF REQUIRED FIRE RATINGS. FIRE RATINGS NOTED ON WALL TYPES (SHEET A5.00 AND FOLLOWING) ARE MAXIMUM RATINGS AND MAY EXCEED THE RATINGS THAT ARE ACTUALLY REQUIRED FOR THIS PROJECT.
- COORDINATE ALL SHOP DRAWINGS AND PRODUCT AND EQUIPMENT SIZES AND SPECIFICATIONS WITH THIS PLAN. NOTIFY ARCHITECT OF ANY DISCREPANCIES OR REQUIREMENTS.
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- ROOM AND UNIT NUMBERS ON UPPER LEVELS TO HAVE THE SAME LAST TWO DIGITS AS SECOND FLOOR WITH CORRESPONDING PREFIX (201, 301, 401).

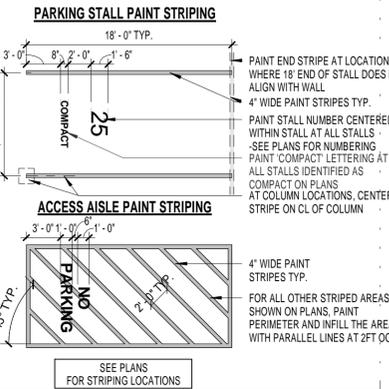
GENERAL GARAGE PLAN NOTES:

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- PAINT PARKING STALL NUMBERS ON FLOOR CENTERED WITHIN THE STALL.
- PARKING SPACE STRIPING SHALL BE 4" WIDE.
- PAINT COLOR SHALL BE WHITE FOR STRIPES AND NUMBERS IN THE GARAGE.
- NOTIFY ARCHITECT IMMEDIATELY OF ANY DISCREPANCIES.
- C = INDICATES COMPACT STALL WIDTH AND/OR DEPTH. DO NOT PAINT.
- x 90'-2" = SLAB SPOT ELEVATION IDENTIFICATION.

PARKING

BASEMENT PARKING	
2	ACCESSIBLE PARKING STALLS
15	COMPACT PARKING STALLS
60	REGULAR PARKING STALLS
FIRST FLOOR PARKING	
2	ACCESSIBLE PARKING STALLS
1	COMPACT PARKING STALLS
15	REGULAR PARKING STALLS
TOTAL	
4	ACCESSIBLE PARKING STALLS
16	COMPACT PARKING STALLS
75	REGULAR PARKING STALLS
GRAND TOTAL	
95	TOTAL STALLS
BIKE STORAGE	
20	WALL MOUNTED RACKS (IN BIKE ROOM)
91	WALL MOUNTED RACKS (IN FRONT OF EACH COMPACT/REGULAR STALL)
GRAND TOTAL	
111	TOTAL RACKS

*CITY OF MINNEAPOLIS ZONING ORDINANCE REQUIRES AT LEAST 75% OF THE REQUIRED PARKING SPACES COMPLY WITH THE MINIMUM DIMENSIONS FOR STANDARD SPACES.



PERFORMANCE DRIVEN DESIGN.
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CLIENT:
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Minneapolis, MN 55401
REDESIGN
2619 East Franklin Avenue
Minneapolis, MN 55406

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	5/9/16	GMP SET
NO	DATE	ISSUED FOR

		REVISION
NO	DATE	

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5/9/16

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PROJECT NAME:
THE BESSEMER AT SEWARD

2200 Snelling Avenue
Minneapolis, MN 55404

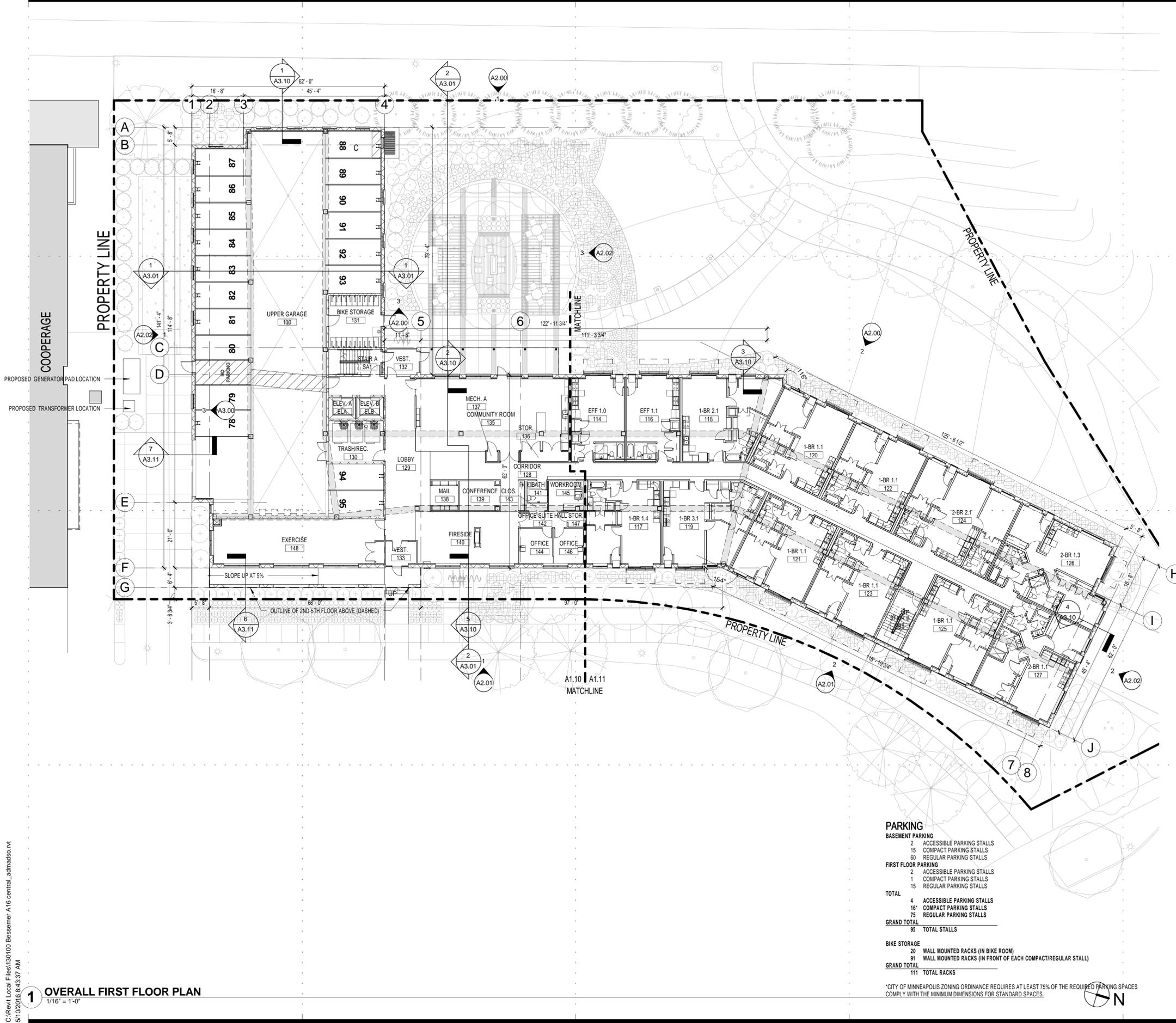
DRAWING TITLE:
Overall Basement Plan

FILE: XXX
DRAWN BY: Author
CHECKED BY: Checker
PROJ. NO: 130100
DRAWING NO:

A0.10

C:\Revit\Local Files\130100 Bessemer A16 central_admin\adms01.rvt
5/10/2016 8:43:37 AM

1 OVERALL FIRST FLOOR PLAN
1/16" = 1'-0"



PARKING

BASEMENT PARKING	
2	ACCESSIBLE PARKING STALLS
15	COMPACT PARKING STALLS
60	REGULAR PARKING STALLS
FIRST FLOOR PARKING	
2	ACCESSIBLE PARKING STALLS
1	COMPACT PARKING STALLS
15	REGULAR PARKING STALLS
TOTAL	
4	ACCESSIBLE PARKING STALLS
16	COMPACT PARKING STALLS
75	REGULAR PARKING STALLS
GRAND TOTAL	95 TOTAL STALLS
BIKE STORAGE	
20	WALL MOUNTED RACKS (IN BIKE ROOM)
91	WALL MOUNTED RACKS (IN FRONT OF EACH COMPACT/REGULAR STALL)
GRAND TOTAL	111 TOTAL RACKS

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SEE SHEETS A1.10 & A1.11 FOR ENLARGED FIRST FLOOR PLANS



701 Washington Ave. N. Ste 200 | Minneapolis, MN 55401 | 612.338.2029

CLIENT:
SCHAFFER RICHARDSON
900 N. 3rd St.
Minneapolis, MN 55401
REDESIGN
2619 East Franklin Avenue
Minneapolis, MN 55406

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NO	DATE	ISSUED FOR
	5/9/16	GMP SET
NO	DATE	REVISION

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NOT FOR CONSTRUCTION
5/9/16

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PROJECT NAME:
THE BESSEMER AT SEWARD

2200 Snelling Avenue
Minneapolis, MN 55404

DRAWING TITLE:
Overall First Floor Plan

FILE: XXX
DRAWN BY: Author
CHECKED BY: Checker
PROJ. NO: 130100
DRAWING NO:
A0.11

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DRIVEN DESIGN.**
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CLIENT:
SCHAFER RICHARDSON
900 N. 3rd St.
Minneapolis, MN 55401
REDESIGN
2619 East Franklin Avenue
Minneapolis, MN 55406

THIS SQUARE APPEARS 1/2"x1/2" ON FULL SIZE SHEETS

NO	DATE	GMP SET	ISSUED FOR

NO	DATE	REVISION

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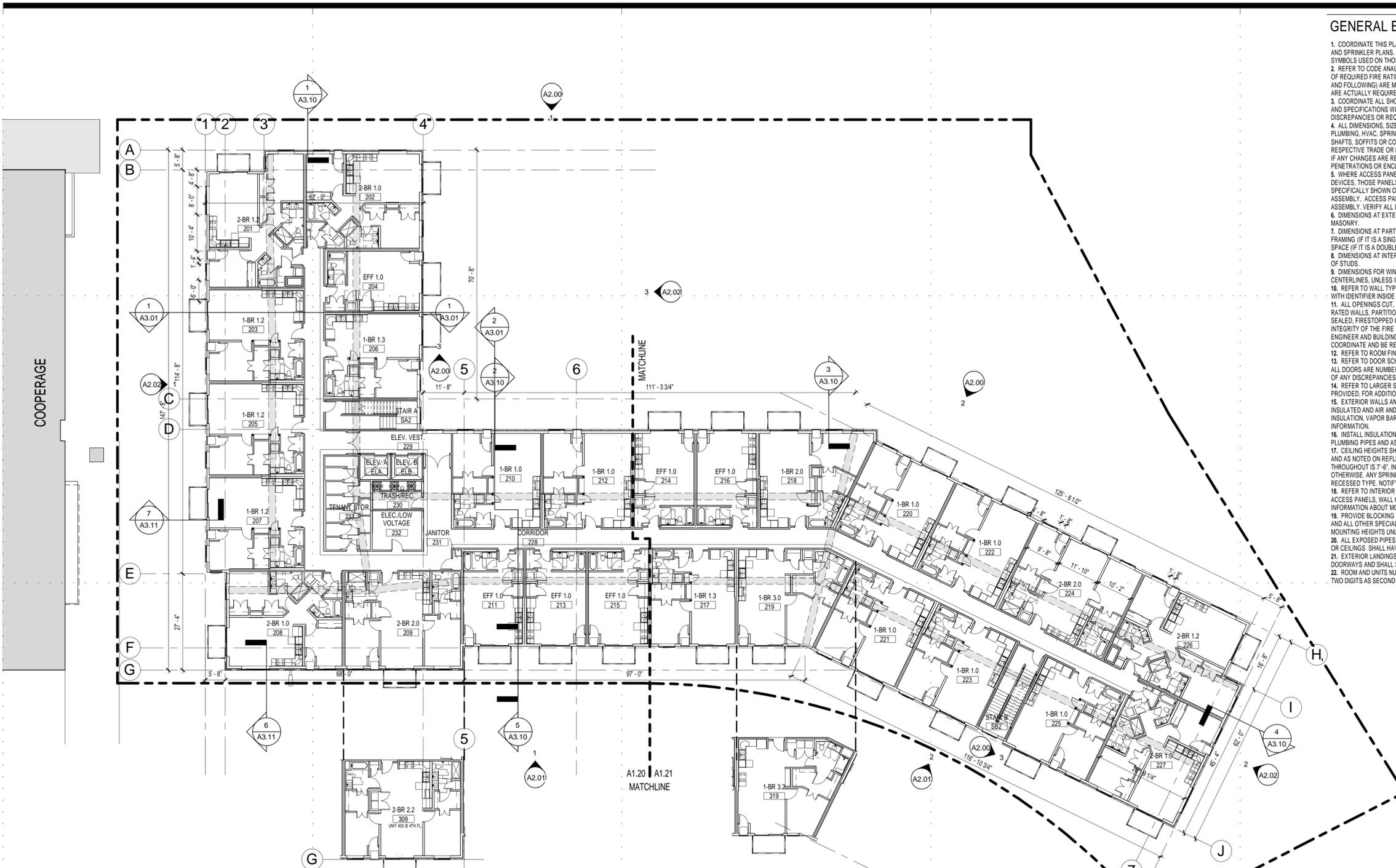
PROJECT NAME:
THE BESSEMER AT SEWARD

2200 Snelling Avenue
Minneapolis, MN 55404

DRAWING TITLE:
Overall Second - Fifth Floor Plan

FILE: XXX
DRAWN BY: Author
CHECKED BY: Checker
PROJ. NO: 130100
DRAWING NO:

A0.12



2 OVERALL THIRD - FOURTH FLOOR PLAN
1/16" = 1'-0"

3 OVERALL THIRD FLOOR PLAN
1/16" = 1'-0"

1 OVERALL SECOND - FIFTH FLOOR PLAN
1/16" = 1'-0"

SEE SHEETS A1.20 & A1.21 FOR ENLARGED SECOND FLOOR PLANS

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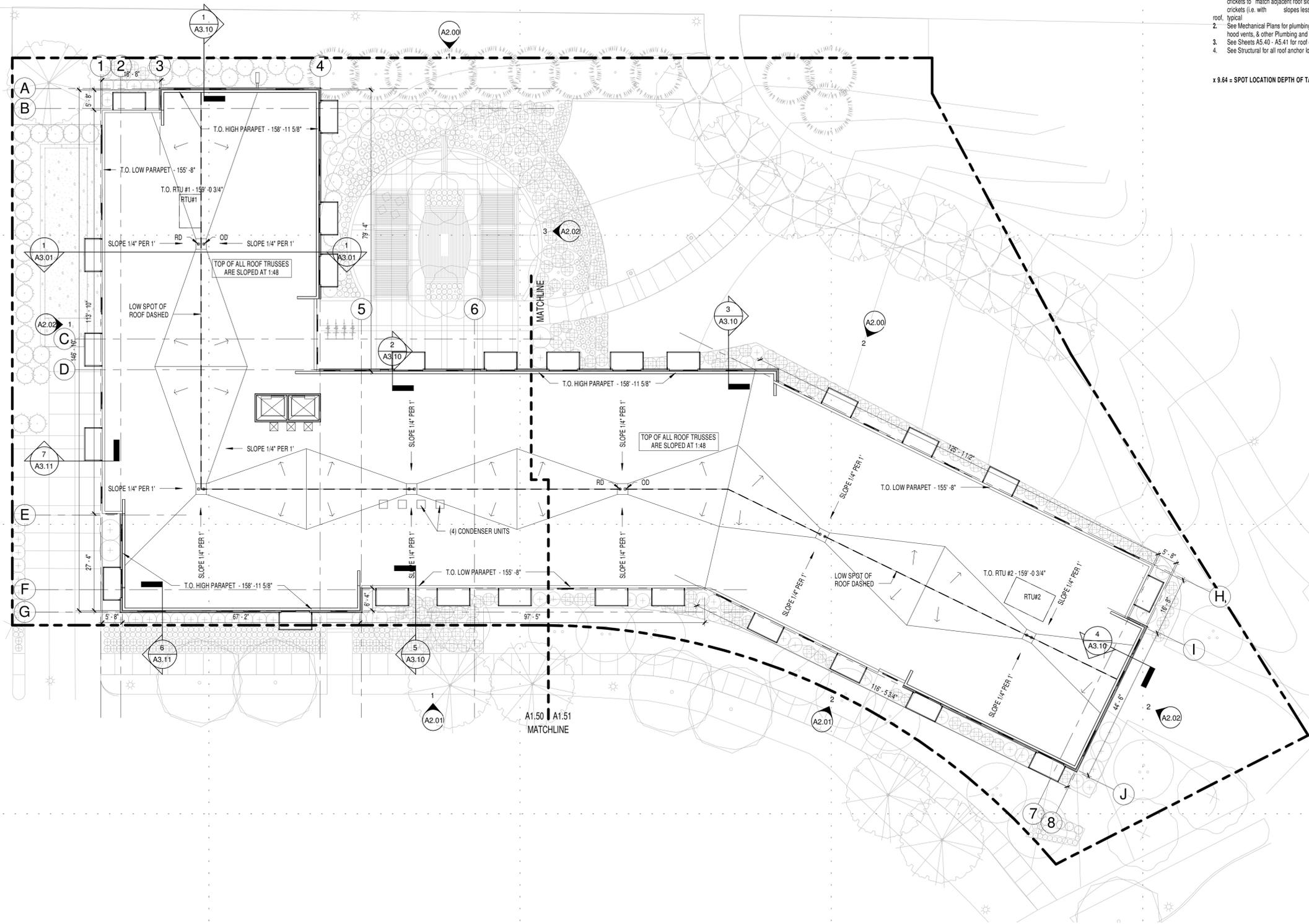
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GENERAL ROOF NOTES

1. Install crickets at all up-slope sides of framed vertical penetrations, and at roof intersections that result in horizontal valleys, to properly and adequately divert water around obstructions, out of horizontal valleys, and off of the roof. Slope of crickets to match adjacent roof slope, or as noted on the drawings. Low-slope crickets (i.e. with slopes less than 3.5:12) shall be covered with a membrane typical.
2. See Mechanical Plans for plumbing vents, exhaust fan hoods, dryer vents, range hood vents, & other Plumbing and Mechanical penetrations.
3. See Sheets A5.40 - A5.41 for roof details.
4. See Structural for all roof anchor locations and information.

x 9.64 = SPOT LOCATION DEPTH OF TAPERED RIGID INSULATION



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Minneapolis, MN 55401
REDESIGN
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Minneapolis, MN 55406

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NO	DATE	ISSUED FOR
	4/15/16	LAND USE SUBMITTAL
	3/22/16	DD SET
	3/21/16	PDR REVIEW
NO	DATE	REVISION

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5/2/16

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PROJECT NAME:
THE BESSEMER AT SEWARD

2200 Snelling Avenue
Minneapolis, MN 55404

DRAWING TITLE:
Overall Roof Plan

FILE: XXX
DRAWN BY: Author
CHECKED BY: Checker
PROJ. NO: 130100
DRAWING NO:

A0.13

SEE SHEETS A1.50 & A1.51 FOR ENLARGED ROOF PLANS

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5/2/2016 3:24:33 PM

1 OVERALL ROOF PLAN
1/16" = 1'-0"





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EXTERIOR ELEVATION KEY

AN	ADDRESS NUMBERS (1" TALL)
B	UTILITY SIZED BRICK
BCM	CONCRETE MASONRY UNIT (BURNISHED)
BS	BUILDING MOUNTED SIGN (48SF)
C	METAL CANOPY
CF	PREFINISHED METAL CAP FLASHING
CJ	CONTROL JOINT 3 / A5.12
CMU1	CMU SILL PROFILE 1, SEE
CO	ALUMINUM CORNICE
FB	FRENCH BALCONY (ONLY AT 1ST FLOOR UNITS)
FDC	FIRE DEPARTMENT CONNECTION
GM	GALVANIZED METAL
HM	INSULATED HOLLOW METAL DOOR
LF	WALL MOUNTED LIGHT FIXTURE
MG	LOCKABLE METAL GRATE
ML	MECHANICAL LOUVER
MPV	MAGIC PAK VENT
OH	OVERHEAD GARAGE DOOR
PD	GLIDING PATIO DOOR
R	BALCONY RAILING
RC	STEEL ROD & CLEVIS
S	STUCCO
SF	ALUMINUM STOREFRONT SYSTEM
V	VENT
UV	UNIT HEATER VENT
W	WINDOW UNIT

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NO	DATE	ISSUED FOR
5/16/16		LAND USE APPLICATION
5/9/16		GMP SET

NO	DATE	REVISION
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PROJECT NAME:
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2200 Snelling Avenue
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DRAWING TITLE:
Building Elevations

FILE: XXX
DRAWN BY: Author
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PROJ. NO: 130100
DRAWING NO:

A2.00



1 **SOUTHWEST ELEVATION**
3/32" = 1'-0"



2 **WEST ELEVATION**
3/32" = 1'-0"

3 **ELEVATION @ 1' RECESS**
3/32" = 1'-0"



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CJ	CONTROL JOINT
CMU1	CMU SILL PROFILE 1, SEE
CO	ALUMINUM CORNICE
FB	FRENCH BALCONY (ONLY AT 1ST FLOOR UNITS)
FDC	FIRE DEPARTMENT CONNECTION
GM	GALVANIZED METAL
HM	INSULATED HOLLOW METAL DOOR
LF	WALL MOUNTED LIGHT FIXTURE
MG	LOCKABLE METAL GRATE
ML	MECHANICAL LOUVER
MPV	MAGIC PAK VENT
OH	OVERHEAD GARAGE DOOR
PD	GLIDING PATIO DOOR
R	BALCONY RAILING
RC	STEEL ROD & CLEVIS
S	STUCCO
SF	ALUMINUM STOREFRONT SYSTEM
V	VENT
UV	UNIT HEATER VENT
W	WINDOW UNIT



1 NORTHEAST ELEVATION
3/32" = 1'-0"



2 EAST ELEVATION
3/32" = 1'-0"



SNELLING AVENUE ELEVATION
SCALE 1" = 50'



PERSPECTIVE VIEW







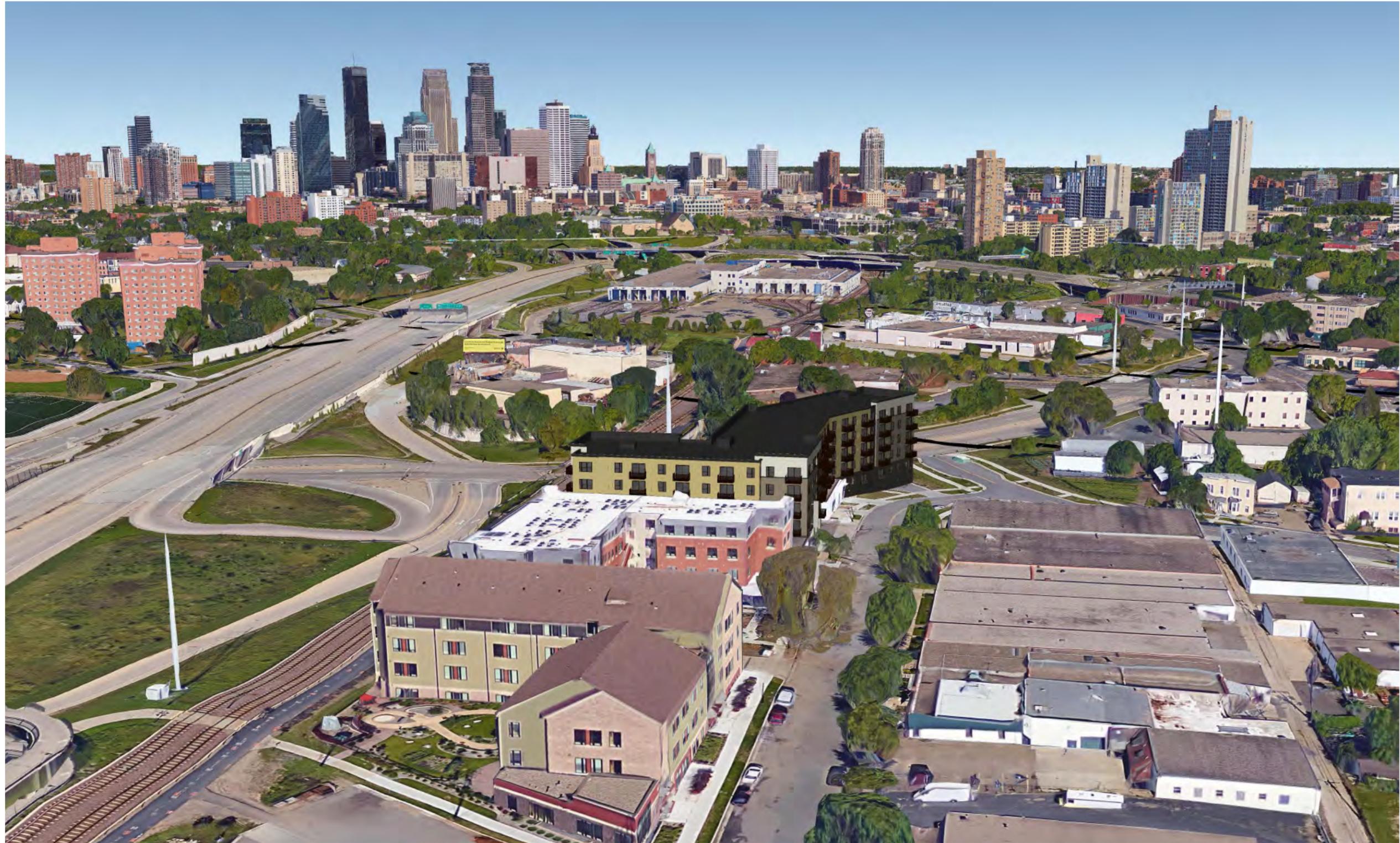














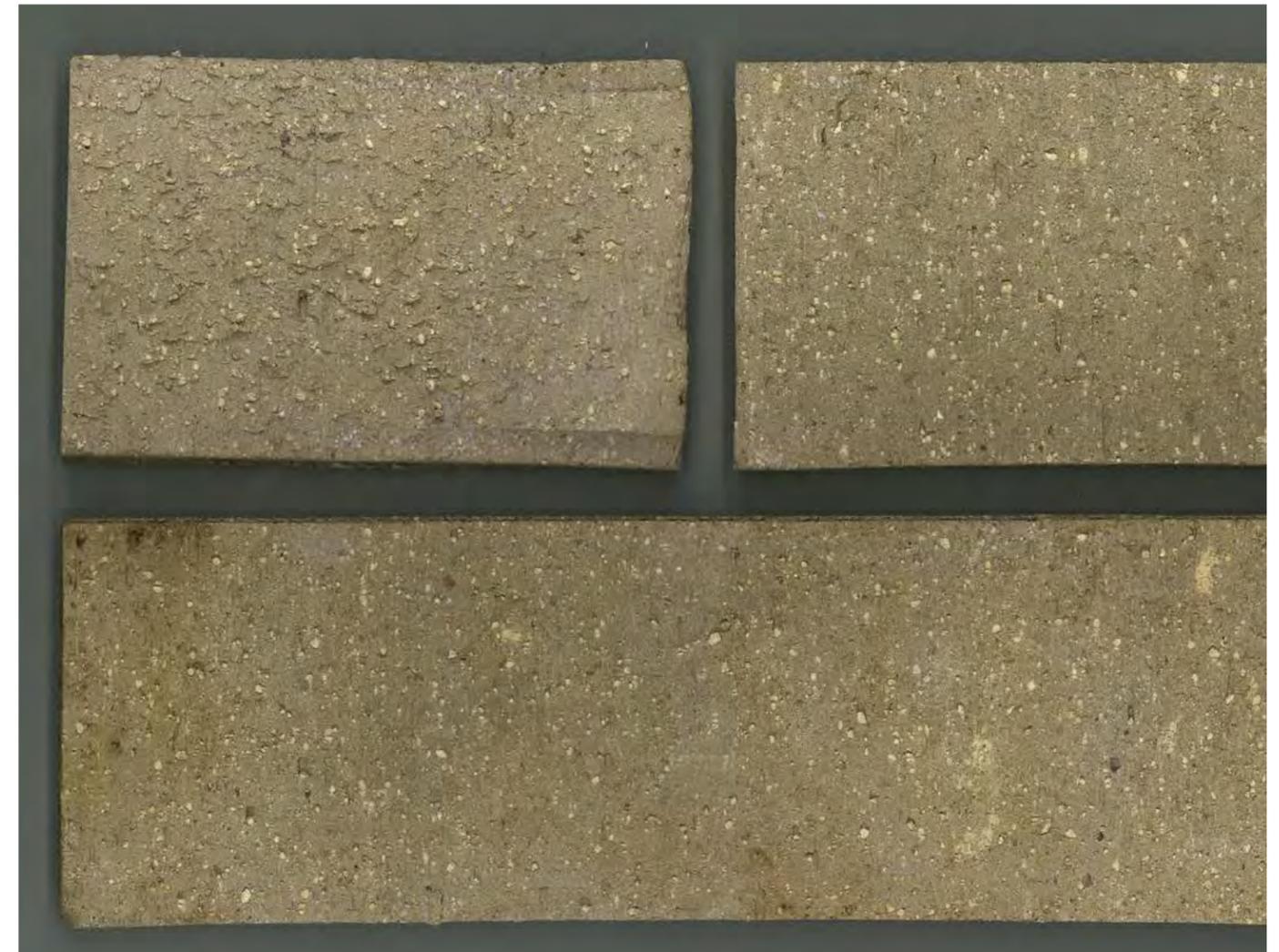




BURNISHED BLOCK SAMPLE



BRICK SAMPLE



Seward Commons Phase 3: Existing Conditions Photos



Above: View of the southern portion of the site, 2214 and 2218 Snelling, with the Blue Line LRT visible behind the building. **Below:** View from the same location, looking toward the northern portion of the site, 2200 and 2210 Snelling.



April 21, 2016

Mr. Brian Miller
Seward Redesign, Inc.
2619 E Franklin Ave
Minneapolis, MN 55406

**RE: Travel Demand Management Plan
Seward Commons Phase 3
TDM File #2016-04**

Dear Mr. Miller:

Enclosed please find a signed copy of the approved Travel Demand Management Plan for the Seward Commons Phase 3 project.

Should you have any questions, please feel free to contact Allan Klugman at (612) 673-2743.

Sincerely,

CITY OF MINNEAPOLIS


Jacob Brown
Program Assistant

Enclosure

Cc: Allan Klugman, Traffic & Parking Services
Dan MacLaughlin, Move Minneapolis

Travel Demand Management Plan
for Seward Commons Phase III
in Minneapolis, MN

Prepared for:
Seward Redesign, Inc.

2619 E Franklin Avenue
Minneapolis, MN 55406



Responsive partner.
Exceptional outcomes.

Prepared by:

WENCK Associates, Inc.
1800 Pioneer Creek Center
Maple Plain, MN 55359
Phone: 7963-479-4200
Fax: 763-479-4242

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1.0 Purpose and Background

Existing Site

The subject site is located at 2200 Snelling Avenue and is bounded by Snelling Avenue, Cedar Avenue, and E. 22nd Street. The existing site contains two industrial buildings and associated parking areas. The project location is shown in **Figure 1**.

Parking is located throughout the site, with all access points located on Snelling Avenue. On-street parking is allowed on Snelling Avenue and on E. 22nd Street.

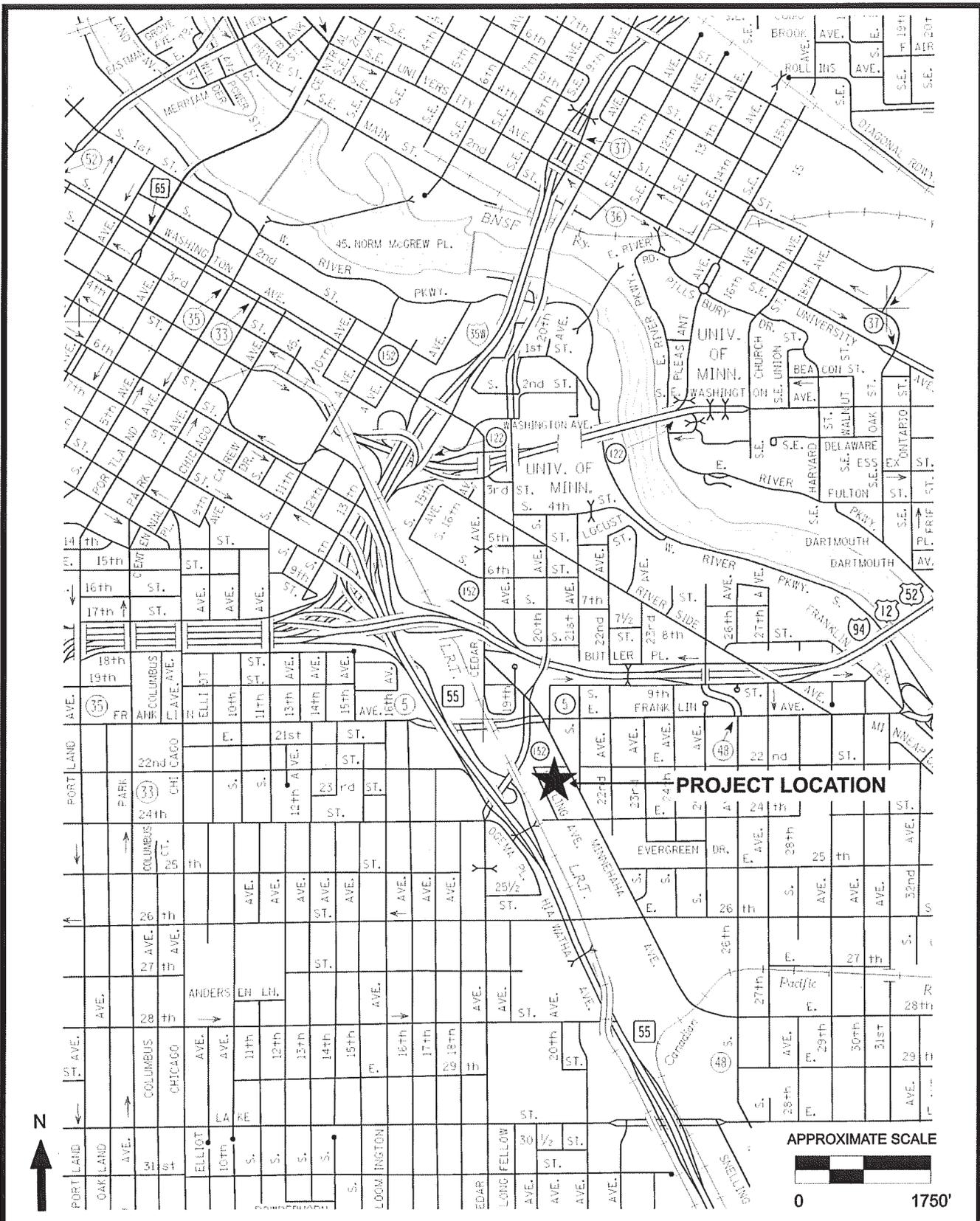
Proposed Development

The proposed project will consist of removing the existing industrial buildings and constructing a new multi-story apartment building with 121 dwelling units.

The proposed project will include 95 parking spaces located inside the building. Access for the proposed project will be provided directly from E. 22nd Street on the north end of the building and from Snelling Avenue on the south end. Trash collection and deliveries will occur at the Snelling Avenue access location.

The proposed site plan is shown in **Figure 2**. Existing conditions near the proposed project are shown in **Figure 3**.

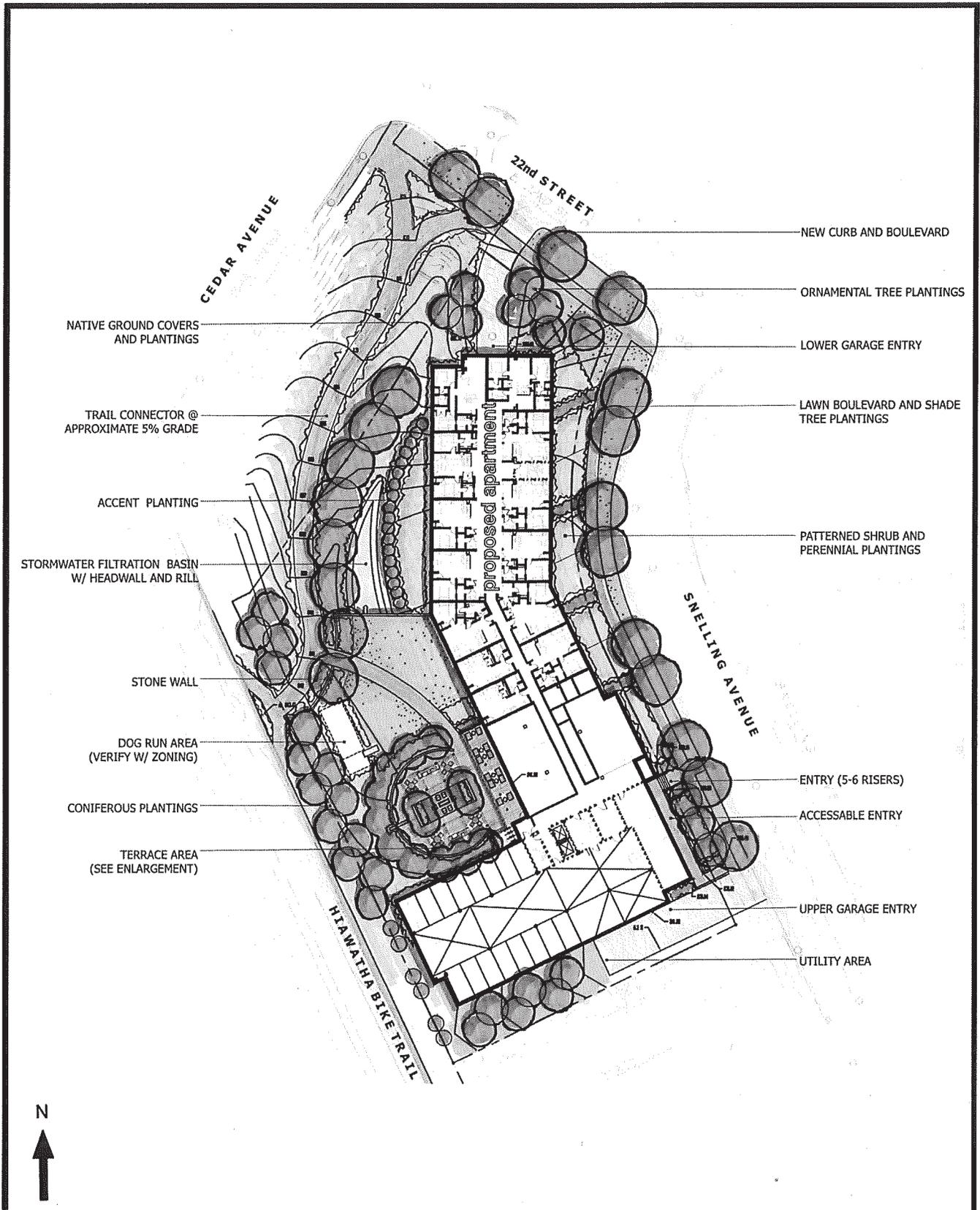
The proposed project will provide a minimum of 111 new bicycle parking spaces on-site. All of the bike parking spaces will be located inside the proposed building. The proposed project is expected to be complete and fully occupied by the end of 2017.



Responsive partner.
Exceptional outcomes.

**TRAVEL DEMAND
MANAGEMENT PLAN
FOR SEWARD COMMONS
PHASE III**

**FIGURE 1
PROJECT LOCATION**



NATIVE GROUND COVERS AND PLANTINGS

TRAIL CONNECTOR @ APPROXIMATE 5% GRADE

ACCENT PLANTING

STORMWATER FILTRATION BASIN W/ HEADWALL AND RILL

STONE WALL

DOG RUN AREA (VERIFY W/ ZONING)

CONIFEROUS PLANTINGS

TERRACE AREA (SEE ENLARGEMENT)

NEW CURB AND BOULEVARD

ORNAMENTAL TREE PLANTINGS

LOWER GARAGE ENTRY

LAWN BOULEVARD AND SHADE TREE PLANTINGS

PATTERNED SHRUB AND PERENNIAL PLANTINGS

ENTRY (5-6 RISERS)

ACCESSABLE ENTRY

UPPER GARAGE ENTRY

UTILITY AREA



TRAVEL DEMAND
MANAGEMENT PLAN
FOR SEWARD COMMONS
PHASE III

FIGURE 2
SITE PLAN



TRAVEL DEMAND
MANAGEMENT PLAN
FOR SEWARD COMMONS
PHASE III

FIGURE 3
EXISTING CONDITIONS



City Of Minneapolis' Transportation Policies

The following policies on transportation are included in Chapter 2 (Transportation) of the *Minneapolis Plan for Sustainable Growth*:

- 2.1 Encourage growth and reinvestment by sustaining the development of a multi-modal transportation system.
- 2.2 Support successful streets and communities by balancing the needs of all modes of transportation with land use policy.
- 2.3 Encourage walking throughout the city by ensuring that routes are safe, comfortable, pleasant, and accessible.
- 2.4 Make transit a more attractive option for both new and existing riders.
- 2.5 Ensure that bicycling throughout the city is safe, comfortable and pleasant.
- 2.6 Manage the role and impact of automobiles in a multi-modal transportation system.
- 2.7 Ensure that freight movement and facilities throughout the city meet the needs of the local and regional economy while remaining sensitive to impacts on surrounding land uses.
- 2.8 Balance the demand for parking with objectives for improving the environment for transit, walking and bicycling, while supporting the city's business community.
- 2.9 Promote reliable funding and pricing strategies to manage transportation demand and improve alternative modes.
- 2.10 Support the development of a multi-modal Downtown transportation system that encourages an increasingly dense and vibrant regional center.
- 2.11 Minneapolis recognizes the economic value of Minneapolis-St. Paul International Airport and encourages its healthy competition to reach global markets in an environmentally responsible manner.

Goals of the Travel Demand Management Plan

Seward Redesign, Inc. understands and acknowledges the overall transportation goals of the City. The goal of this Travel Demand Management Plan (TDMP) is to support the City of Minneapolis in achieving its transportation goals. Two purposes that the City has established for all Travel Demand Management Plans are as follows:

- 1) Determine the transportation implications of the proposed development and develop solutions to resolve any impacts, and
- 2) Examine how the proposed development meets the City's goals to reduce automobile traffic while identifying and implementing ways for the proposed development to increase alternative transportation modes.

The Travel Demand Management Plan for this development site identifies steps to be taken by the project owner to ensure that the proposed development is well integrated with existing transportation services, and that future users of the development are able to take full advantage of transit, pedestrian, and bicycle facilities. The overall mode split goal for this project is 50% Single Occupant Vehicle, 35% Transit, and 15% Pedestrian/Bicycle.

2.0 Traffic

Framework for Traffic Forecasts

To adequately address the impacts of the subject development, weekday a.m. and p.m. peak hour traffic forecasts were completed for the following intersections:

- Cedar Avenue/E. 22nd Street (E. 22nd Street stop sign controlled)
- E. 22nd Street/Snelling Avenue (Snelling Avenue stop sign controlled)
- Minnehaha Avenue/E. 22nd Street (E. 22nd Street stop sign controlled)

Traffic forecasts are typically considered for the year following expected completion of the proposed development. Accordingly, traffic forecasts were completed for the following three scenarios:

- Existing – Traffic volumes for the Cedar Avenue/E. 22nd Street and Minnehaha Avenue/E. 22nd Street were obtained from analysis and forecasts completed by Hennepin County for the Cedar/Franklin/Minnehaha reconstruction project. Traffic volumes for the E. 22nd Street/Snelling Avenue intersection were recorded from 7-9 a.m. and 4-6 p.m. in February, 2016 for this project.
- 2018 No-Build – Existing volumes increased by 1.0 percent per year to account for background traffic growth. This growth rate was based on the historic traffic volume growth on the surrounding street system.
- 2018 Build – Trips generated by the proposed development are added to the 2018 No-Build volumes.

Development Traffic Volumes

To accurately account for trips that could be generated by the proposed project, trip generation was completed using data presented in the Institute of Transportation Engineers' (ITE) *Trip Generation*, Ninth Edition. Discussions with City staff resulted in the application of reductions to the gross trip generation to account for the impact of adjacent transit options. Based on these discussions, the gross trip generation was reduced by 25% to account for transit impacts.

Table 2 presents the projected weekday a.m. and p.m. peak hour gross trip generation.

**Table 2
Weekday Gross Trip Generation for Seward Commons Phase III**

Land Use	ITE Code	Size	Weekday AM Peak Hour			Weekday PM Peak Hour			Weekday Daily
			In	Out	Total	In	Out	Total	Total
Proposed Project									
Apartments	220	121 du	12	50	62	49	26	75	805
Existing Uses Removed									
Light Industrial		32,000 sf	26	3	29	4	27	31	223

Notes: du = dwelling units, sf = square feet

Based on existing traffic patterns in the area, the surrounding roadway network, and locations of major trip attractions relative to the subject site, the following directional distribution percentages were established for development trips:

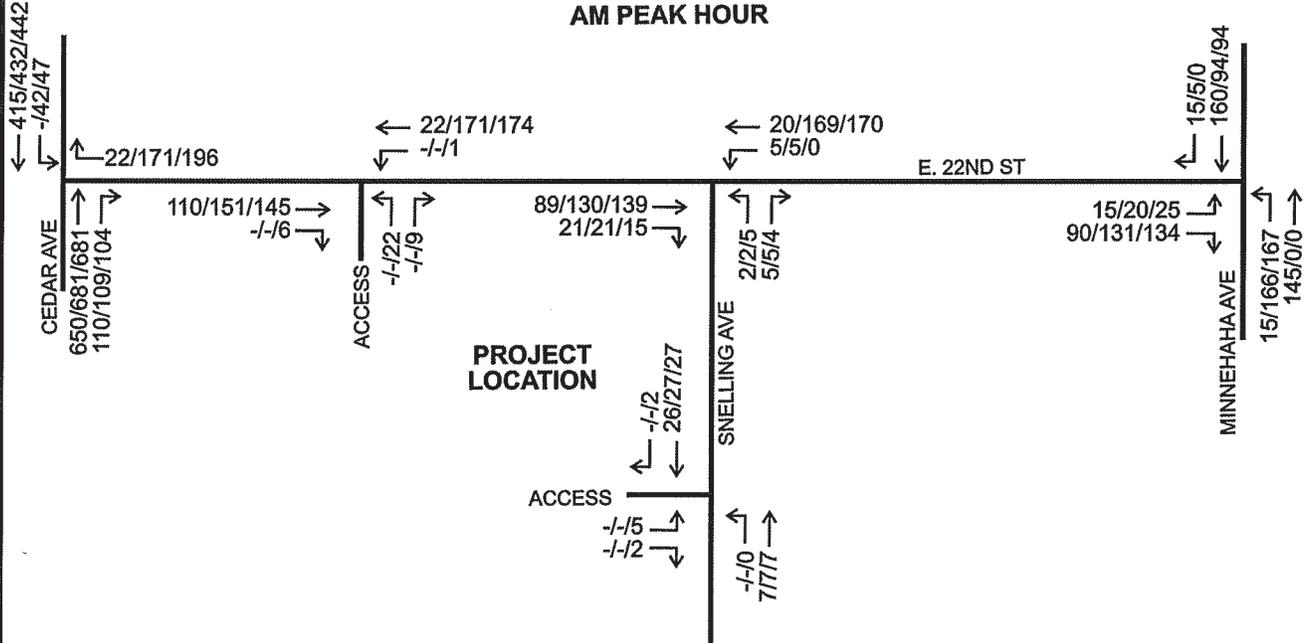
- 20 percent to/from the east on E. Franklin Avenue
- 10 percent to/from the south on Minnehaha Avenue
- 20 percent to/from the west on E. Franklin Avenue
- 20 percent to/from the north on Cedar Avenue
- 30 percent to/from the south on Cedar Avenue

Total Projected Volumes

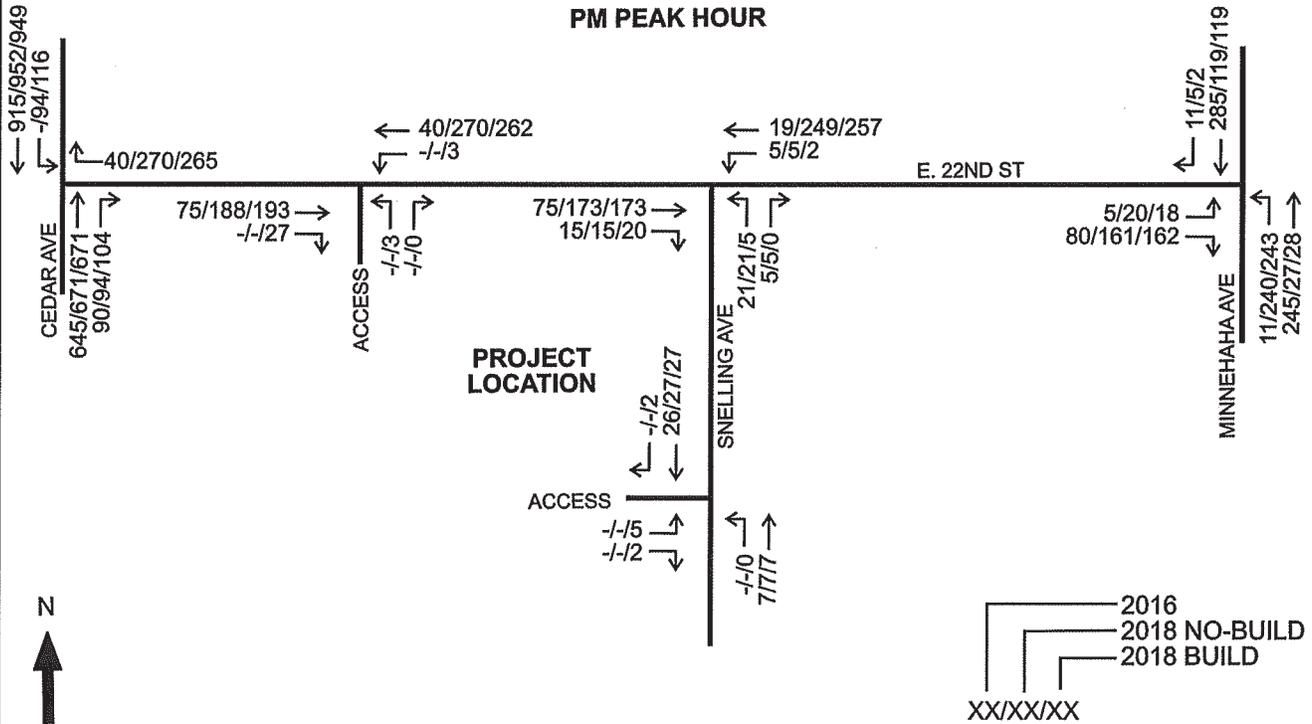
Total traffic volume projections have been established for all the components previously described. The weekday a.m. and p.m. peak hour traffic volumes for existing, 2018 No-Build, and 2018 Build scenarios are shown in **Figure 4**.

The existing volumes represent existing street conditions, with no changes to the Cedar/Franklin/Minnehaha area. The 2018 No-Build and 2018 Build volumes account for volume changes associated with the Cedar/Franklin/Minnehaha reconstruction project as forecasted by Hennepin County.

AM PEAK HOUR



PM PEAK HOUR



TRAVEL DEMAND
MANAGEMENT PLAN
FOR SEWARD COMMONS
PHASE III

FIGURE 4

AM AND PM PEAK
HOUR VOLUMES

Intersection Impacts

In order to better understand traffic related impacts at the subject intersections, capacity analyses were performed for the existing, 2018 No-Build, and 2018 Build scenarios. Capacity analysis results are presented in terms of level of service (LOS), which ranges from A to F. LOS A represents the best intersection operation, with very little delay for each vehicle using the intersection. LOS F represents the worst intersection operation with excessive delay. The capacity analyses were completed using existing geometrics and intersection control. The results are described below.

Cedar Avenue/E. 22nd Street (E. 22nd Street stop sign controlled)

During the a.m. peak hour, all movements operate at LOS B or better and the intersection operates at LOS A under all scenarios. During the p.m. peak hour, all movements operate at LOS C or better and the intersection operates at LOS A under all scenarios. No improvements are necessary to accommodate the proposed project.

E. 22nd Street/Snelling Avenue (Snelling Avenue stop sign controlled)

During the a.m. peak hour, all movements operate at LOS A and the intersection operates at LOS A under all scenarios. During the p.m. peak hour, all movements operate at LOS B or better and the intersection operates at LOS A under all scenarios. No improvements are necessary to accommodate the proposed project.

Minnehaha Avenue/E. 22nd Street (E. 22nd Street stop sign controlled)

During the a.m. peak hour, all movements operate at LOS B or better and the intersection operates at LOS A under all scenarios. During the p.m. peak hour, all movements operate at LOS B or better and the intersection operates at LOS A under all scenarios. No improvements are necessary to accommodate the proposed project.

E. 22nd Street/Access

During the a.m. peak hour, all movements operate at LOS B or better and the intersection operates at LOS A under the 2018 Build scenario. During the p.m. peak hour, all movements operate at LOS B or better and the intersection operates at LOS A under all scenarios.

Snelling Avenue/Access

During the a.m. peak hour, all movements operate at LOS A and the intersection operates at LOS A under the 2018 Build scenario. During the p.m. peak hour, all movements operate at LOS A and the intersection operates at LOS A under all scenarios.

Overall Traffic Impacts

On an overall basis, the proposed use has minimal impacts on intersection operations. All intersections have adequate capacity to accommodate the number of trips generated and therefore impacts on existing operations are minimal.

Accommodation of Deliveries and Trash Collection

Trash collection will occur at the Snelling Avenue access location. Trash collection will occur as often as necessary to fully serve the entire development. Deliveries will occur at designated surface areas provided within the site.

All deliveries will be coordinated to minimize congestion at the loading areas. All loading areas within the site are designed to accommodate the expected vehicle types and sizes.

3.0 Parking

Parking Supply

As described earlier, the proposed project includes 95 parking spaces within the building. In addition, on-street parking is allowed on Snelling Avenue and on E. 22nd Street.

Zoning Code Requirements

To determine the adequacy of the proposed parking supply, parking requirements were calculated based on the Zoning Code parking ratios. This site is located within ¼ mile of the Franklin Avenue Blue Line transit stop, resulting in reductions to the calculated parking requirements. The current parking requirements are a minimum of 61 spaces for the residential use. The minimum and maximum calculations are as follows:

- For the apartment use, the minimum is 1 space per dwelling unit which equates to 121 spaces. There is no maximum for the apartment use. The parking requirement for the apartment is 50% of the total due to the proximity to the light rail station, for a minimum total of 61 spaces.

Institute of Transportation Engineers (ITE) Data Calculations

In addition to the Zoning Code requirement, parking data from the Institute of Transportation Engineers (ITE) was also used to estimate the expected parking demand. Based on the ITE data, the peak weekday parking demand for the overall site is 115 spaces.

Similar to the reduction made to the expected trip generation for the site, the parking generation will likely be lower than typical ITE data due to the impact of adjacent transit options. Applying the 25% reduction to account for transit impacts results in a peak parking demand of 86 spaces.

Overall Parking Impact

As shown above, the peak parking demand for the site 86 spaces, which is 9 spaces less than the parking supply of 95 spaces. In addition, the number of spaces provided falls within the minimum and maximum zoning requirements. Therefore, adequate parking is provided for the site.

Parking Operation

All of the parking spaces are reserved for residents only. Parking for residents is purchased separately and is not included in the rent. The property manager will arrange short term rental for overnight guests of unleased parking spaces on a first-come, first-serve basis as they are available. No stalls will be permanently reserved for overnight guests.

Impacts to On-Street Parking

Currently there is an on-street parking bay with three parking spaces on the south curb line of 22nd Street between Cedar Avenue and Snelling Avenue. The location of the proposed driveway on 22nd Street will result in elimination of the three parking spaces.

In addition, the Hennepin County Franklin/Cedar/Minnehaha project will include the installation of a new left turn lane from southbound Cedar Avenue to eastbound 22nd Street. Along with this left turn lane, the curb radii of the Cedar Avenue/22nd Street intersection will be modified. The curb layout work for Seward Commons Phase III will need to coordinate with this County project.

6.0 Bicycle

The Hiawatha Bicycle Trail is located immediately adjacent to the project site, providing access to the greater Minneapolis bicycle trail system. Bicycle traffic is also allowed on the surrounding street system.

To effectively fulfill the TDM goal of promoting alternative modes of transportation, the project owner will provide 111 new bicycle parking spaces on-site. All of the spaces will be inside the proposed building. If the bicycle parking demand exceeds the available supply, the project owner will add more spaces.

7.0 Travel Demand Management Plan

The following TDM plan identifies the program, responsibilities, and detailed implementation of measures for the transportation issues presented in this report. Seward Redesign, Inc., Successor, or Property Manager specifically commits to implementing the following actions:

1. Seward Redesign, Inc., Successor, or Property Manager will provide 95 on-site parking spaces.
2. Seward Redesign, Inc., Successor, or Property Manager agrees that the on-site parking spaces are for residents only.
3. Seward Redesign, Inc., Successor, or Property Manager will provide 111 new bicycle parking spaces on-site. All of the spaces will be inside the proposed building. If the bicycle parking demand exceeds the available supply, the project owner will add more spaces.
4. Seward Redesign, Inc., Successor, or Property Manager, will create and distribute information that includes the following:
 - Maps that show the area light rail and bus routes, light rail and bus schedules, and bicycle and pedestrian facilities.
 - Information on starting and joining commuter programs.
 - Other information or actions that encourage use of alternative modes of transportation.
5. Seward Redesign, Inc., Successor, or Property Manager will provide information to tenants on available free smartphone applications (that tenants can download) that provide real-time transit information.
6. Seward Redesign, Inc., Successor, or Property Manager will provide bus route and light rail maps to tenants upon request and make them available at the management office.
7. It is understood that the City's desire is to minimize truck loading/unloading activity during peak periods for traffic on adjacent streets. To help the City fulfill this desire, Seward Redesign, Inc., Successor, or Property Manager, will encourage truck drivers and tenants to schedule truck service functions during off-peak periods.
8. Seward Redesign, Inc., Successor, or Property Manager will offer a pre-loaded Metro Transit Go-To Card to every new tenant that moves into the building.

8.0 Signatures

Dated: 4/12/2016

SEWARD REDESIGN, INC.
By Brian Miller

Dated: 4/18/2016

MINNEAPOLIS COMMUNITY PLANNING &
ECONOMIC DEVELOPMENT DEPARTMENT
By Stephen Coon
CPED Development Services Director

Dated: 4/14/2016

MINNEAPOLIS PUBLIC WORKS DEPARTMENT
By Allan Kugman
for Traffic Operations Engineer

9.0 Appendix

- Bus route maps

T METRO

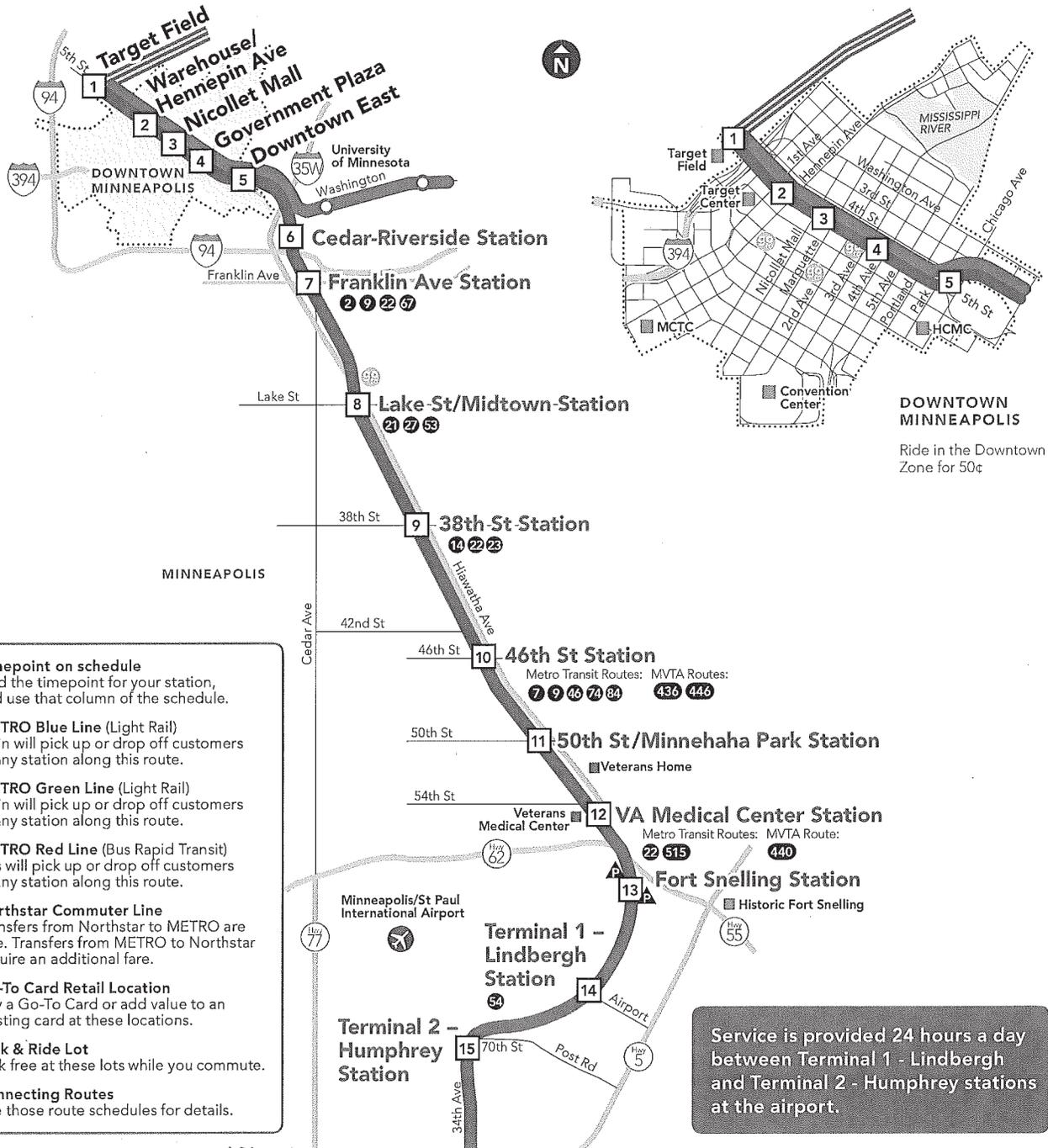
Blue Line

LIGHT RAIL



Map 1 of 2 pages

T Metro Transit



Map continued on page 2



Map
continued
on page 1



Park & Ride Locations

Park free at these lots while you commute. No overnight parking. At Fort Snelling North Lot and 28th Ave Station, limited overnight parking is available in designated areas. 24-hour limit.

Fort Snelling

North Lot: park in lot on east side of Minnehaha Ave between Hwy 62 and Fort Snelling Station.

South Lot: park in lot located between MndOT building and Fort Snelling Club south of Federal Dr.

Bloomington

28th Ave Station Ramp - corner of 82nd St and 28th Ave.

Go-to Card Retail Locations

Buy a Go-To Card or add value to an existing card at these locations.

Downtown Minneapolis

- Commuter Connection – 220 6th St S (US Bank Plaza)
- Metro Transit Store, 719 Marquette Ave
- Unbank, 727 Hennepin Ave

South Minneapolis

- Cub Foods, 2850 26th Ave S

Bloomington

- Mall of America Transit Station, 8240 24th Ave S

Holiday service operates on the following holidays: New Year's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving and Christmas.

Metro Transit may operate reduced service on days before or after Independence Day, Thanksgiving, Christmas or New Year's Day. Look for details at metrotransit.org or in *Connect* on buses and trains prior to these holidays.

Schedule subject to change.

This document is available in alternate formats to individuals with disabilities by calling 612-349-7365 (TTY 612-341-0140).

A portion of the funding for this route was provided by the Counties Transit Improvement Board.



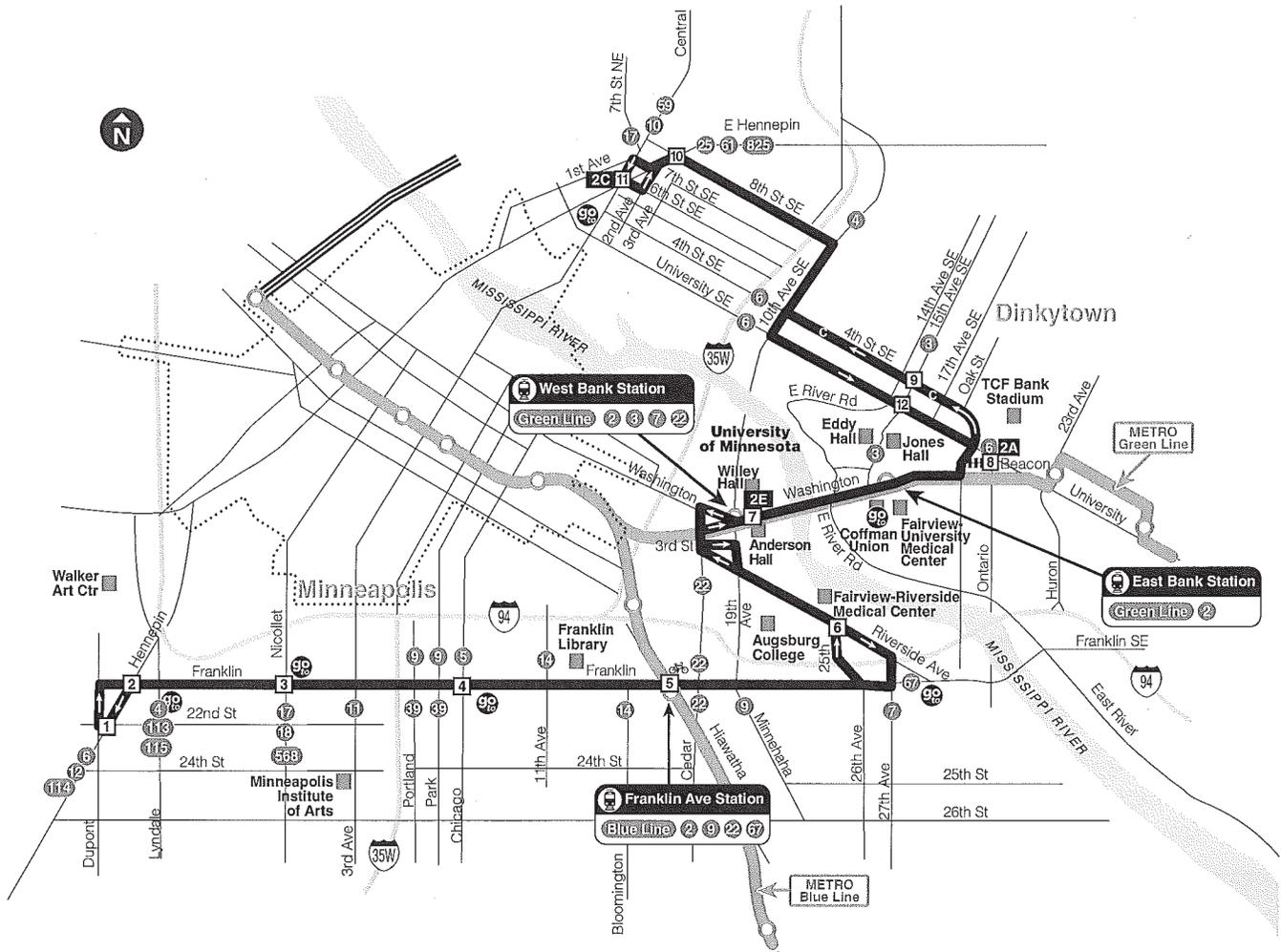
Buses and trains have free storage racks so you can bring your bicycle along.

Look for instructions on the rack. Lockers are also available for rent at most METRO stations. Details at metrotransit.org/bike.



Say hi to less waiting.

This route is part of the Hi-Frequency network. Segments of routes 5, 6, 10, 18, 19, 21, 64, 84 and 515, and all of route 54 and METRO Blue and Green lines operate at least every 15 minutes weekdays from 6 am–7 pm and Saturdays from 9 am–6 pm. See maps for details.



- 3** **Timepoint on schedule**
Find the timepoint nearest your stop, and use that column of the schedule. Your stop may be between timepoints.
- Regular Route**
Bus will pick up or drop off customers at any bus stop along this route.
- METRO Line(s)**
METRO trains and buses will pick up or drop off customers at any station along this line.
- Northstar Line**
Transfers from Northstar to light rail are free. Transfers from light rail to Northstar require an additional fare.
- 49S** **Route Ending Point**
Trips with the indicated number/letter end at this point. Number/letter is found in schedules and on bus destination signs.
- 324 11** **Connecting Routes to transfer to/from**
See those route schedules for details.
- Bike Locker**
These sites have weatherproof bike storage for rent.
- 90** **Go-To Card Retail Location**
Buy a Go-To Card or add value to an existing card at these locations.
- Rail Station**
Customers get on and off trains here. Buses connect with trains at stations.

90 Retail Locations
Buy a Go-To Card or add value to an existing card at these locations.

Minneapolis – South
Money Xchange, 12 Franklin Ave E
Seward Co-op, 2823 Franklin Ave E
The Wedge, 2105 Lyndale S
Unbank, 1009 Franklin Ave E

Minneapolis – SE
Coffman Union, 300 Washington Ave SE
Lunds, 25 University Ave SE

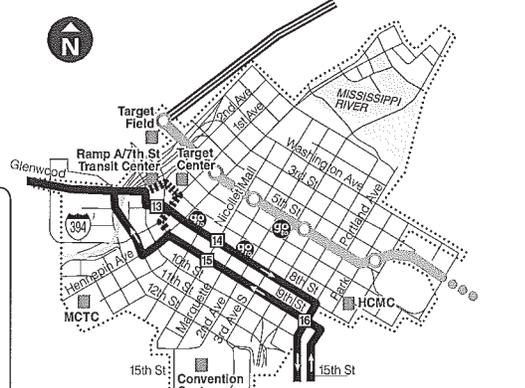
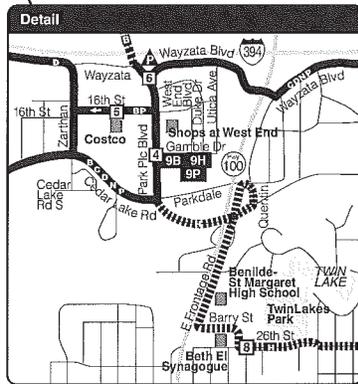
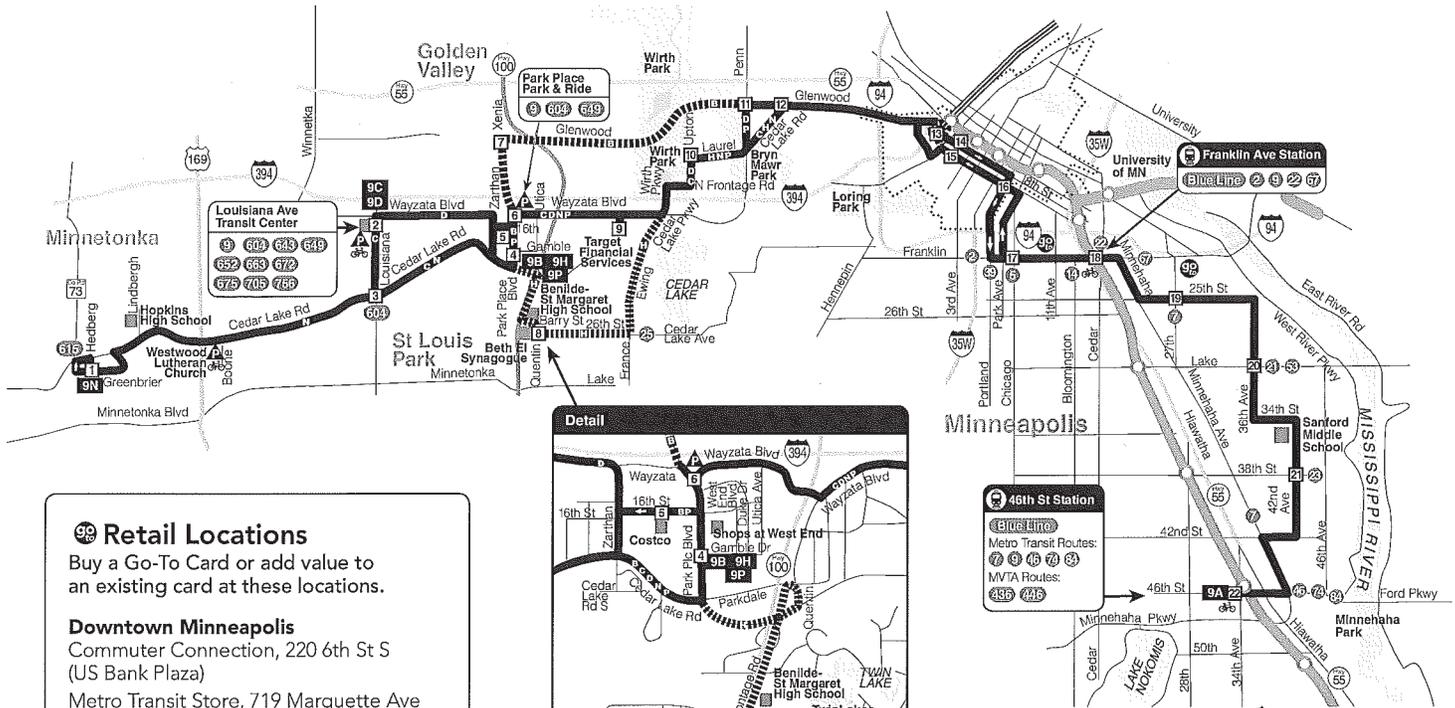
Bikes and trains have free storage racks so you can bring your bicycle along.

Look for instructions on the rack or visit metrotransit.org/bike

Holiday service operates on the following holidays: New Year's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving and Christmas.

Metro Transit may operate reduced service on days before or after Independence Day, Thanksgiving, Christmas or New Year's Day. Look for details at metrotransit.org or in *Connect* on buses and trains prior to these holidays.

Schedule subject to change. Traffic and weather conditions may delay buses. Please have exact fare ready. Bus fareboxes and drivers do not make change. This document is available in alternate formats to individuals with disabilities by calling 612-349-7365 (TTY 612-341-0140).



99 Retail Locations
Buy a Go-To Card or add value to an existing card at these locations.

Downtown Minneapolis
Commuter Connection, 220 6th St S (US Bank Plaza)
Metro Transit Store, 719 Marquette Ave
Unbank, 727 Hennepin Ave

Minneapolis - South
Cub Foods, 2850 26th Ave S
Seward Co-op, 2823 Franklin Ave E

- 3** **Timepoint**
Find the timepoint nearest your stop, and use that column of the schedule. Your stop may be between timepoints.
- Regular Route**
Bus will pick up or drop off customers at any bus stop along this route.
- METRO Line(s)**
METRO trains and buses will pick up or drop off customers at any station along this line.
- Northstar Line**
Transfers from Northstar to light rail are free. Transfers from light rail to Northstar require an additional fare.
- 49S** **Route Ending Point**
Trips with the indicated number/letter end at this point. Number/letter is found in schedules and on bus destination signs.
- B** **Route Letter**
Indicates which trips travel on this section of the route. Letter is found in schedules and on bus destination signs.
- 324 11** **Connecting Routes to transfer to/from**
See those route schedules for details.
- Bike Locker**
These sites have weatherproof bike storage for rent.
- Park & Ride Lot**
Park free at these lots while you commute.
- 99** **Go-To Card Retail Location**
Buy a Go-To Card or add value to an existing card at these locations.
- Trans** **Transfer Point**
Several routes serve this stop.
- Rail Station**
Customers get on and off trains here. Buses connect with trains at stations.

Park & Ride Location
Park free at this lot while you commute. No overnight parking.

St Louis Park
Westwood Lutheran Church - 9001 Cedar Lake Rd, lot west of church
Louisiana Ave Transit Center - Parking located next to south frontage road of I-394

Park Place Blvd Park & Ride - Park Place Blvd and Wayzata Blvd (south frontage road of I-394), NE corner of intersection

Buses and trains have free storage racks so you can bring your bicycle along.

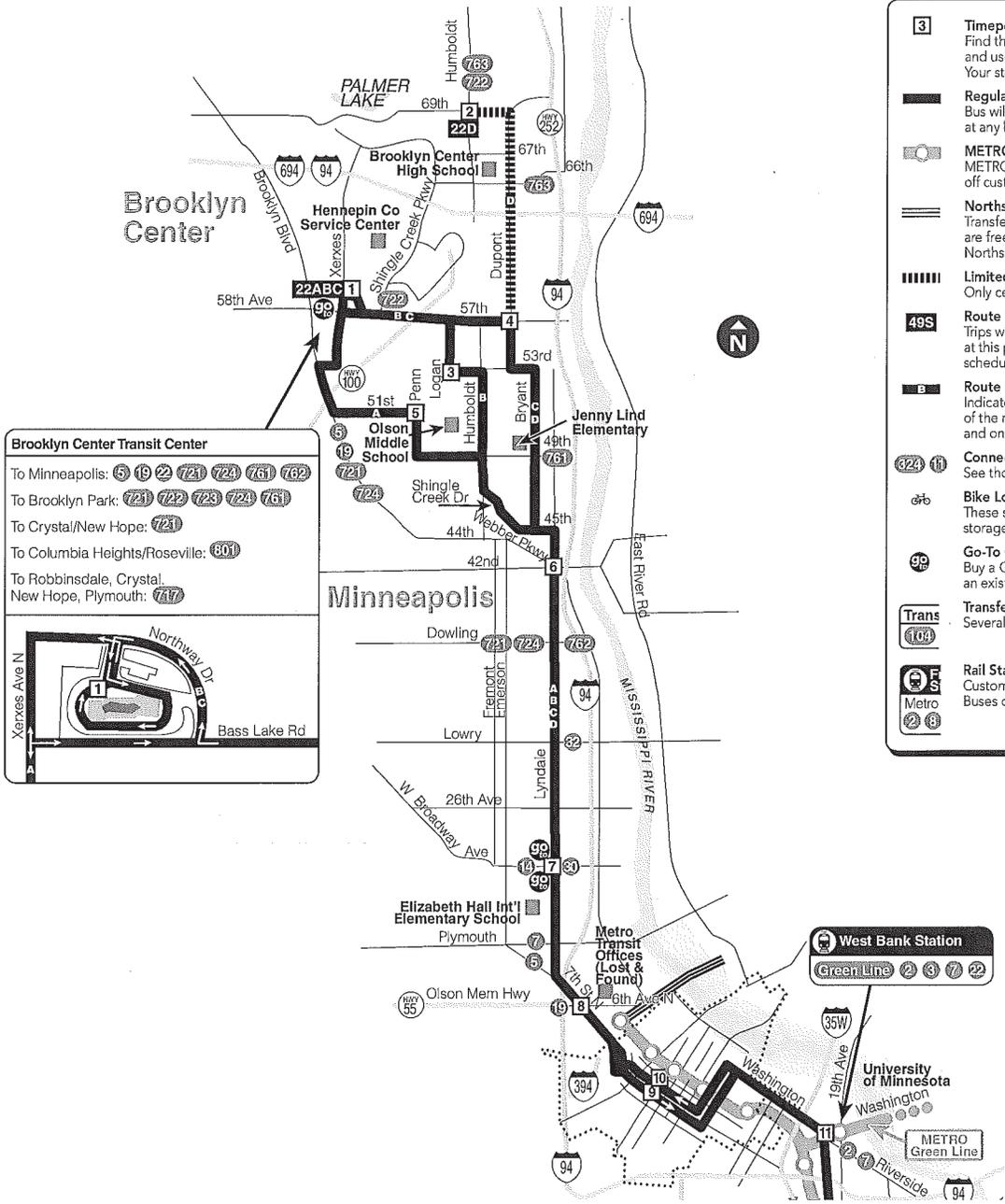
Look for instructions on the rack or visit metrotransit.org/bike

Schedule subject to change. Traffic and weather conditions may delay buses. Please have exact fare ready. Bus fareboxes and drivers do not make change. This document is available in alternate formats to individuals with disabilities by calling 612-349-7365 (TTY 612-341-0140).

Special Notes:
Between 11:45 PM-5:15 AM during Nicollet Mall construction, buses will be coordinated to allow transfers between routes on 3rd Ave S and routes on 6th, 7th, 8th and 9th streets. See map or footnotes for details.

Holiday service operates on the following holidays: New Year's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving and Christmas.

Metro Transit may operate reduced service on days before or after Independence Day, Thanksgiving, Christmas or New Year's Day. Look for details at metrotransit.org or in Connect on buses and trains prior to these holidays.



Brooklyn Center Transit Center

To Minneapolis: 5 19 22 723 724 761 762
 To Brooklyn Park: 721 722 723 724 761
 To Crystal/New Hope: 721
 To Columbia Heights/Roseville: 801
 To Robbinsdale, Crystal, New Hope, Plymouth: 747

- 3** **Timepoint on schedule**
Find the timepoint nearest your stop, and use that column of the schedule. Your stop may be between timepoints.
- Regular Route**
Bus will pick up or drop off customers at any bus stop along this route.
- METRO Line(s)**
METRO trains and buses will pick up or drop off customers at any station along this line.
- Northstar Line**
Transfers from Northstar to light rail are free. Transfers from light rail to Northstar require an additional fare.
- Limited Service**
Only certain trips take this route.
- 49S** **Route Ending Point**
Trips with the indicated number/letter end at this point. Number/letter is found in schedules and on bus destination signs.
- B** **Route Letter**
Indicates which trips travel on this section of the route. Letter is found in schedules and on bus destination signs.
- 324** **Connecting Routes to transfer to/from**
See those route schedules for details.
- BL** **Bike Locker**
These sites have weatherproof bike storage for rent.
- 99** **Go-To Card Retail Location**
Buy a Go-To Card or add value to an existing card at these locations.
- Trans 109** **Transfer Point**
Several routes serve this stop.
- F Metro** **Rail Station**
Customers get on and off trains here. Buses connect with trains at stations.

Map continued on page 2

Holiday service operates on the following holidays: New Year's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving and Christmas.

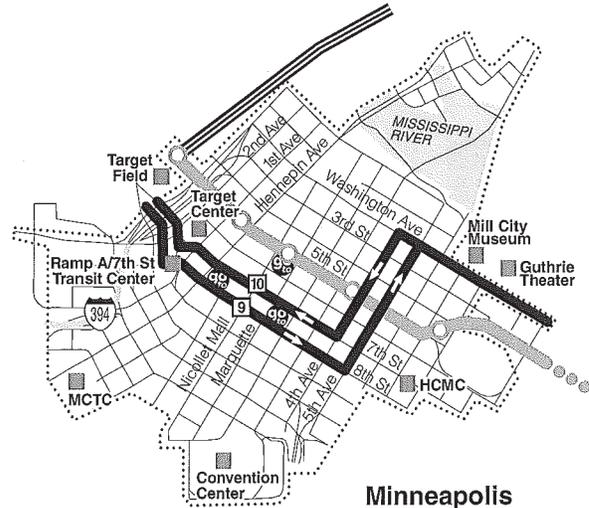
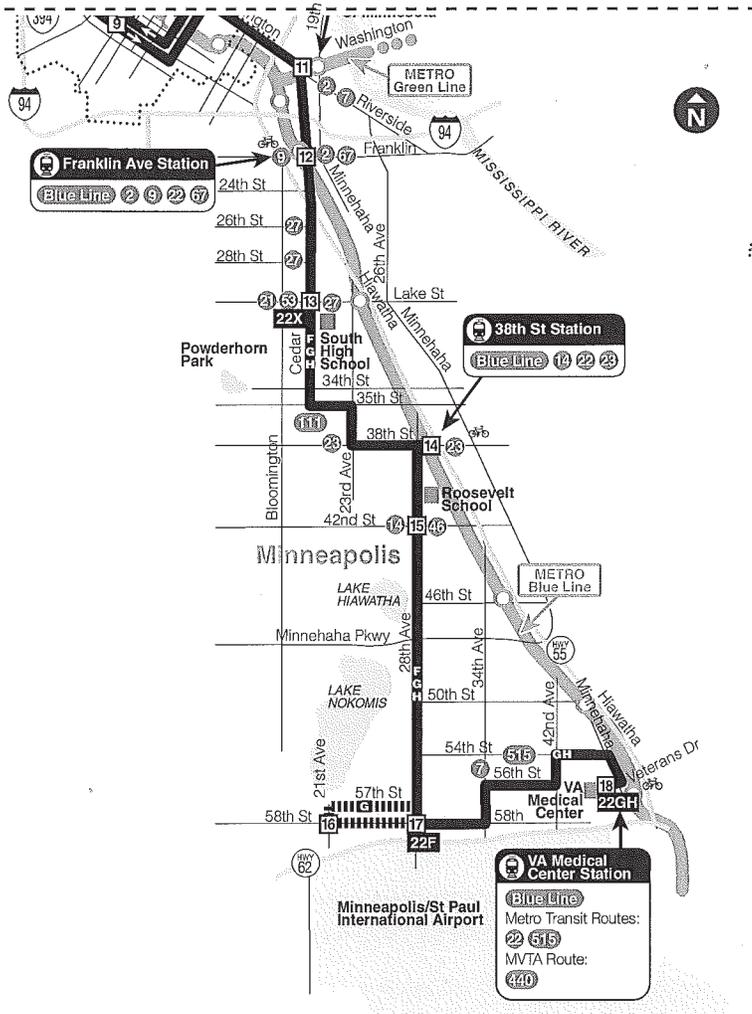
Metro Transit may operate reduced service on days before or after Independence Day, Thanksgiving, Christmas or New Year's Day. Look for details at metrotransit.org or in *Connect* on buses and trains prior to these holidays.

Buses and trains have free storage racks so you can bring your bicycle along.

Look for instructions on the rack or visit metrotransit.org/bike

Effective 12/12/15
metrotransit.org
 612-373-3333

Map continued on page 1



Minneapolis Downtown Zone
Ride in the Downtown Zone for 50¢

Special Note:

Between 11:45 PM–5:15 AM during Nicollet Mall construction, buses will be coordinated to allow transfers between routes on Hennepin Ave and routes on 6th, 7th, 8th and 9th streets. See map or footnotes for details.

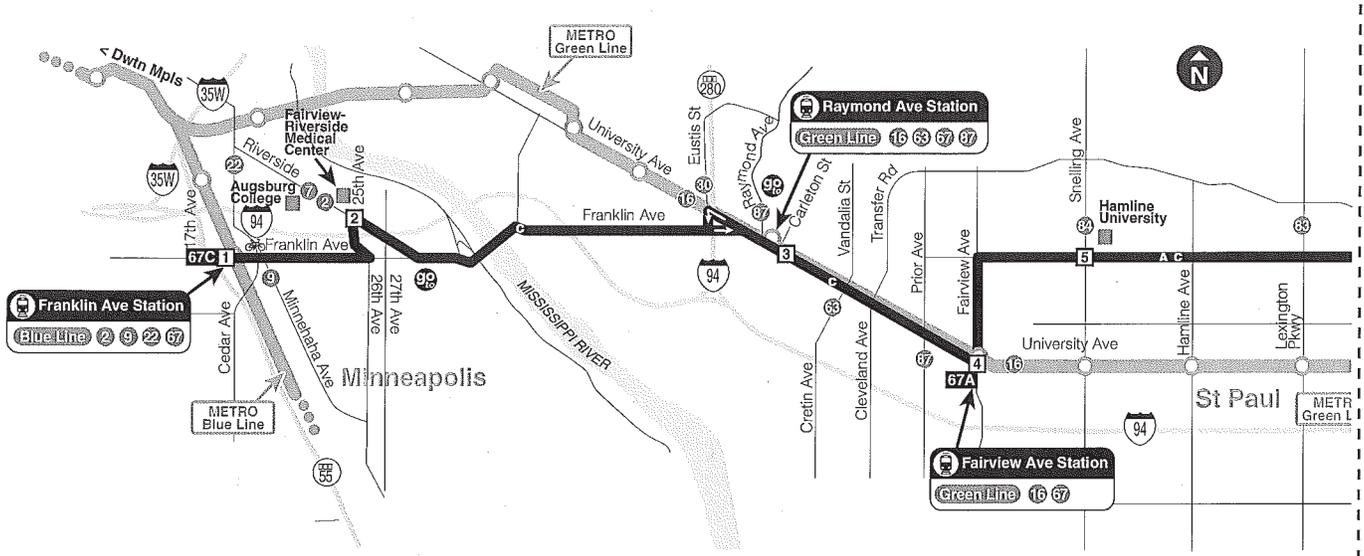
99 Retail Locations
Buy a Go-To Card or add value to an existing card at these locations.

Brooklyn Center
Cub Foods, 3245 Co Rd 10

Minneapolis – North
Cub Foods, 701 W Broadway Ave
Unbank, 700 W Broadway Ave

Downtown Minneapolis
Metro Transit Service Center,
719 Marquette Ave
Move Minneapolis, 505 Nicollet Mall
Unbank, 727 Hennepin Ave

Schedule subject to change. Traffic and weather conditions may delay buses. Please have exact fare ready. Bus fareboxes and drivers do not make change. This document is available in alternate formats to individuals with disabilities by calling 612-349-7365 (TTY 612-341-0140).



Map continued on page 2

 **Buses and trains have free storage racks so you can bring your bicycle along.**
 Look for instructions on the rack or visit metrotransit.org/bike

3 Timepoint on schedule
 Find the timepoint nearest your stop, and use that column of the schedule. Your stop may be between timepoints.

 **Regular Route**
 Bus will pick up or drop off customers at any bus stop along this route.

 **METRO Line(s)**
 METRO trains will pick up or drop off.

49S **Route Ending Point**
 Trips with the indicated number/letter end at this point. Number/letter is found in schedules and on bus destination signs.

B **Route Letter**
 Indicates which trips travel on this section of the route. Letter is found in schedules and on bus destination signs.

 **Connecting Routes to transfer to/from**
 See those route schedules for details.

 **Bike Locker**
 These sites have weatherproof bike storage for rent.

 **Go-To Card Retail Location**
 Buy a Go-To Card or add value to an existing card at these locations.

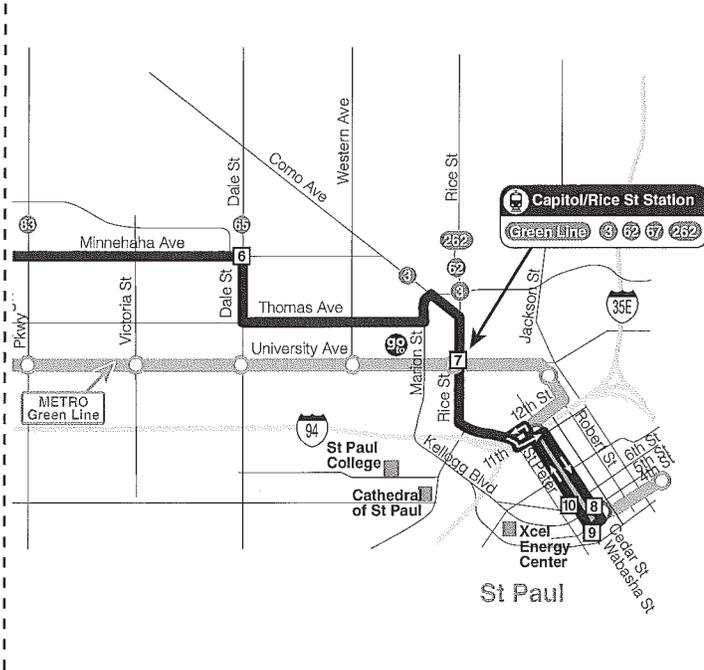
 **Rail Station**
 Customers get on and off trains here. Buses connect with trains at stations.

 **Retail Locations**
 Buy a Go-To Card or add value to an existing card at these locations.

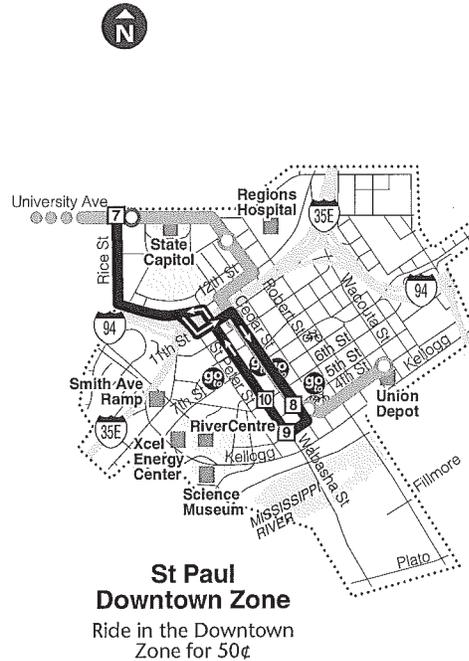
Minneapolis - South
 Seward Co-op, 2823 Franklin Ave E

St Paul
 Community Financial Center, 259 University Ave W, Suite C
 Hampden Park Co-op, 928 Raymond Ave

Downtown St Paul
 Charlie's Chocolates & Cravings, 30 E 7th St, Suite 159
 Charlie's Chocolates & Cravings, Town Square (skyway)
 Metro Transit Service Center, 101 5th St E, US Bank Center (skyway)
 Unbank, 467 St Peter St



Map continued on page 1



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SEWARD NEIGHBORHOOD GROUP

2323 E Franklin Avenue • Minneapolis, MN 55406

PHONE 612 338 6205
FAX 612 399 0739

May 13, 2016

E MAIL info@sng.org
WEB www.sng.org

Janelle Widmeier
250 South 4th Street, Room 300
Minneapolis, MN 55415

Dear Ms. Widmeier:

On May 10, 2016, the Seward Neighborhood Group Development Committee heard a presentation on the Bessemer at Seward, also known as Phase III of the Seward Commons redevelopment.

The committee understands that the project, located at 2214 Snelling Avenue, will consist of 121 market rate apartments. The building will have underground parking for residents, a fitness center and bicycle parking facilities. The project will also include a permanent pedestrian and bicycle connection from East 22nd Street to the LRT bike/pedestrian path.

The Seward Neighborhood Group recommends approval of the following Conditional Use Permits and variances by City of Minneapolis needed to build Seward Commons, Phase III:

1. Conditional Use Permit to allow a multifamily residential building in the Industrial Living Overlay District
2. Conditional Use Permit for a 5 story building
3. Variance for building setback from Snelling Avenue, Cedar Avenue and 22nd Street to allow a setback greater than 8 feet
4. Variance for an increased building setback from west property line to treat the LRT trail as a second frontage
5. Variance to reduce the required side yard between Phase III and the Cooperage to the south to allow a new electric transformer
6. Variances for sign height on the west façade of the building, and
7. Approval of the project's proposal to dedicate 10% of the property to the Minneapolis Park Board to facilitate the new public, ADA accessible, paved bicycle/pedestrian connection through the development to the Hiawatha Bicycle and Pedestrian Path and the LRT station, with a width of 12 feet.

Thank you for considering our support of this important project in our community.

Sincerely,



Marne Zafar
Co-President

Cc: Council Member Abdi Warsame